



# COMMITTEE OF THE WHOLE AGENDA

Tuesday, May 20, 2025, 11:00 a.m.

Council Chambers

City Hall, 11995 Haney Place

All meetings are hybrid, allowing virtual or in person participation.  
Online participation via zoom <https://mapleridge-ca.zoom.us/j/89860004010>  
The meeting is live streamed and recorded by the City of Maple Ridge.

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Pages

**1. CALL TO ORDER**

**Territory Acknowledgement**

The City of Maple Ridge carries out its business on the traditional and unceded territories of the Katzie (q'ícəy) First Nation and the Kwantlen (q'w̓a:nł'ən) First Nation.

**2. APPROVAL OF AGENDA**

Committee of the Whole Agenda - May 20, 2025

RECOMMENDATION:

THAT the May 20, 2025, Committee of the Whole Agenda be approved as circulated.

**3. ADOPTION OF MINUTES**

4

Committee of the Whole Minutes - May 6, 2025

RECOMMENDATION:

THAT the minutes of the May 6, 2025, Committee of the Whole be adopted as circulated.

**4. PRESENTATIONS AT THE REQUEST OF COUNCIL**

**5. DELEGATIONS**

**6. PUBLIC COMMENT ON AGENDA ITEMS**

**7. STAFF REPORTS**

**7.1 Investment Attraction and Business Retention and Expansion Strategies**

8

Strategies and an implementation plan outlining initiative for the City to implement to refine its vision for its economic future and ongoing operations, refine its brand and marketing efforts, improve outreach and networking to attract new investment, and better support local businesses

RECOMMENDATION:

THAT the May 20, 2025, report titled "Investment Attraction and Business



Retention and Expansion Strategies" be forwarded to the next Regular Council Meeting.

**7.2      2024-228-RZ, 12011 223 Street, CD-1-17** 259

To facilitate the future construction of the proposed six-storey mixed-use building for Phase 2 of the ERA development.

RECOMMENDATION:

THAT the May 20, 2025, report titled "2024-228-RZ, 12011 223 Street Zone Amending Bylaw No. 8034-2025" be forwarded to the next Regular Council Meeting.

**7.3      2020-309-DP, 12323 222 Street, Multi-Family Development Permit** 463

To allow for the future development of an eight-storey rental apartment building.

RECOMMENDATION:

THAT the May 20, 2025, report titled "2020-309-DP, 12323 222 Street, Multi-Family Development Permit" be forwarded to the next Regular Council Meeting.

**7.4      Transit-Oriented Areas: Land Use Planning Process Update** 549

Initial land use concepts for the three Transit-Oriented Areas in Maple Ridge.

RECOMMENDATION:

THAT the May 20, 2025, report titled "Transit-Oriented Areas: Land Use Planning Process Update" be forwarded to the next Regular Council Meeting.

**7.5      Albion Area Plan Update** 575

To provide a summary of the Official Community Plan amendments necessary to update the Albion Area Plan to reflect changes brought into effect with the new provincial housing legislation, as well as to redesignate portions of the North East Albion Area to support townhome style development.

RECOMMENDATION:

THAT the May 20, 2025, report titled "Albion Area Plan Update, Official Community Plan Amending Bylaw No. 8032-2025" be forwarded to the next Regular Council Meeting.

**7.6      Amendment to Maple Ridge Fees and Charges Bylaw No. 7575-2019** 631

To update fees and charges and consolidate fees from multiple bylaws into one consolidated fees and charges bylaw.

RECOMMENDATION:

THAT the May 20, 2025, report titled "Amendment to Maple Ridge Fees and Charges Bylaw No. 7575-2019" be forwarded to the next Regular Council Meeting.

**8.      OTHER MATTERS DEEMED EXPEDIENT**

**9. NOTICE OF CLOSED MEETING**

Resolution to Exclude the Public

RECOMMENDATION:

The meeting will be closed to the public pursuant to Sections 90(1) of the Community Charter as the subject matter being considered is related to the following:

Section 90(1)(e) - the acquisition, disposition or expropriation of land or improvements, if the council considers that disclosure could reasonably be expected to harm the interests of the municipality;

Section 90(1)(l) - discussions with municipal officers and employees respecting municipal objectives, measures and progress reports for the purposes of preparing an annual report under section 98 [annual municipal report];

Any other matter that may be brought before the Council that meets the requirements for a meeting closed to the public pursuant to Sections 90(1) and 90(2) of the Community Charter or Freedom of Information and Protection of Privacy Act.

**10. ADJOURNMENT**



## COMMITTEE OF THE WHOLE MEETING MINUTES

May 6, 2025, 11:00 a.m.  
Council Chambers  
City Hall, 11995 Haney Place

**Council Present:** Mayor D. Ruimy  
Councillor K. Carreras  
Councillor O. Dozie  
Councillor J. Dueck

**Councillor S. Schiller**  
**Councillor J. Tan**  
**Councillor A. Yousef**

**Staff Present:**

S. Hartman, Chief Administrative Officer	W. Oleschak, Director of City Operations, Acting Chief Operating Officer
C. Mushata, Director of Legislative Services and Corporate Officer	V. Richmond, Director of Facilities, Parks & Properties
C. Bevacqua, Clerk 3	J. Stiver, Director of Planning & Building
M. Adams, Director of Bylaw, Licensing & Community Safety	T. Thompson, Director of Finance
A. Grochowich, Manager of Community Planning	D. Purcell-Chung, Manager of Development and Environmental Services
S. Faltas, Director of Engineering	D. Rajasooriar, Planner 2
Z. Lifshiz, Director, Strategic Development, Communications and Public Engagement	E. Mark, Planning Technician
H. Nadvi, Deputy Director of Planning & Building	E. Aderneck, Industrial Lands Program Manager
C. Nolan, Deputy Director of Finance	R. Soni, Manager of Accounting

### 1. **CALL TO ORDER – 11:02 pm**

Councillor S. Schiller, Chair, called the meeting to order and provided the territory acknowledgement.

### 2. **APPROVAL OF AGENDA**

Committee of the Whole Agenda - May 6, 2025

**R/2025-CW-74**

**Moved by:** Councillor Dueck

**Seconded by:** Councillor Carreras

THAT the May 6, 2025, Committee of the Whole Agenda be approved as circulated.

**CARRIED**

**3. ADOPTION OF MINUTES**

Committee of the Whole Minutes - April 15, 2025

**R/2025-CW-75**

**Moved by:** Councillor Dozie

**Seconded by:** Councillor Tan

THAT the minutes of the April 15, 2025, Committee of the Whole be adopted as circulated.

**CARRIED**

**4. PRESENTATIONS AT THE REQUEST OF COUNCIL**

**5. DELEGATIONS**

**6. PUBLIC COMMENT ON AGENDA ITEMS**

**7. STAFF REPORTS**

**7.1 2019-055-VP, 11795 and 11839 267 Street, Development Variance Permit**

Variances to the lot width and lot depth of two of the proposed strata lots of a future 15 single-detached residential lot subdivision.

The Planning Technician gave a detailed presentation.

**R/2025-CW-76**

**Moved by:** Mayor Ruimy

**Seconded by:** Councillor Dueck

THAT the May 6, 2025, report titled "2019-055-VP, 11795 and 11839 267 Street, Development Variance Permit" be forwarded to the next Regular Council Meeting.

**CARRIED**

**7.2 2021-107-RZ, 20886 River Road, RS-1 to R-1 and RT-1**

To allow a future 9 lot subdivision.

The Planner 2 gave a detailed presentation and answered questions of Council.

**R/2025-CW-77**

**Moved by:** Councillor Yousef

**Seconded by:** Mayor Ruimy

THAT the May 6, 2025, report titled "2021-107-RZ, 20886 River Road, Zone Amending Bylaw No. 7718-2021" be forwarded to the next Regular Council Meeting.

**CARRIED**

**7.3 Bring to Market Industrial Land Study for the Albion Industrial Area, Fraser River, Kwantlen First Nation Lands**

Bring to Market Industrial Land Study for the Albion Industrial Area, Fraser River, Kwantlen First Nation Lands, intended to support new industrial uses and development in the area.

The Industrial Lands Program Manager gave a detailed presentation and answered questions of Council.

**R/2025-CW-78**

**Moved by:** Mayor Ruimy

**Seconded by:** Councillor Yousef

THAT the May 6, 2025, report titled "Bring to Market Industrial Land Study for the Albion Industrial Area, Fraser River, Kwantlen First Nation Lands" be forwarded to the next Regular Council Meeting.

**CARRIED**

**7.4 Strategic Update on the Maple Ridge Strategic Plan for Q1, 2025**

Insights from ongoing delivery of key results and initiatives from Council's Strategic Plan, for the first Quarter of 2025.

The Director of Strategic Development, Communications and Public Engagement gave a detailed presentation and answered questions of Council.

**R/2025-CW-79**

**Moved by:** Councillor Dueck

**Seconded by:** Mayor Ruimy

THAT the May 6, 2025, report titled "Strategic Update on the Maple Ridge Strategic Plan for Q1, 2025" be forwarded to the next Regular Council Meeting.

**CARRIED**

**7.5 2024 Consolidated Financial Statements**

Presentation of the Consolidated Financial Statements for the fiscal year ended December 31, 2024.

The Deputy Director of Finance introduced the new Manager of Accounting who gave a detailed presentation and answered questions of Council. The Director of Finance answered questions of Council.

**R/2025-CW-80**

**Moved by:** Councillor Yousef

**Seconded by:** Mayor Ruimy

THAT the May 6, 2025, report titled "2024 Consolidated Financial Statements" be forwarded to the next Regular Council Meeting.

**CARRIED**

8. **OTHER MATTERS DEEMED EXPEDIENT**
9. **NOTICE OF CLOSED MEETING**
10. **ADJOURNMENT – 12:18 pm**

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Councillor S. Schiller, Chair

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C. Mushata, Corporate Officer

DRAFT

## Investment Attraction and Business Retention and Expansion Strategies

### Recommendation:

**THAT Council receive the report titled “Investment Attraction and Business Retention and Expansion Strategies” dated May 20, 2025 for Information.**

### Report Purpose and Summary Statement:

The Economic Development Department is leading the development of the Investment Attraction Strategy, Costed Implementation Plan, and Business Retention and Expansion Strategy. The consultant retained for the project has conducted an extensive analysis of existing City strategies, local and regional employment trends, and economic and business opportunities, as well as a competitive assessment of jurisdictions within British Columbia and across North America.

### Previous Council Action:

1. February 12, 2024 – R/2024-S(C)-014 – That Council approve the list of one-time incremental requests in Attachment C – 2024 One-Time Incremental Requests of the Council report titled “Business Planning 2024-2028” of February 13, 2024 and that the financial implications be included in the 2024-2028 Financial Plan.
2. April 9, 2024 – That Maple Ridge 2024 – 2028 Financial Plan Bylaw No. 7989-2024 be given first, second and third reading.
3. January 16, 2025 – That Council direct staff to include their provided insights as a part of the Investment Attraction and Business Retention and Expansion Strategies and Implementation Plan.

### Strategic Alignment:

Economic Development Strategy (2021); Diversified, Thriving Economy; Governance & Corporate Excellence

**To:** Mayor and Council

**File number:** [6750.02]

## **Investment Attraction and Business Retention and Expansion Strategies**

### **BACKGROUND:**

The City of Maple Ridge is one of Metro Vancouver's fastest-growing municipalities, with a population currently exceeding 106,000. Projections estimate the population will grow to 150,000 within the next 15 years. To support this expansion and enhance the quality of life for its residents, the City is committed to diversifying the local economy by attracting investments in key local sectors that will provide employment opportunities in growing and innovative sectors for Maple Ridge residents and help to balance the City's tax base.

The City engaged EBP consultants to create an Investment Attraction Strategy and Costed Implementation Plan. This work involved research, data analysis, outreach to key groups and businesses within the local and regional economic development landscape, and opportunity matching to highlight key industry sectors where Maple Ridge is especially well-positioned to pursue investment attraction.

The Investment Attraction Strategy (Attachment A) was coordinated with a separate but concurrent Business Retention and Expansion (BRE) Strategy (Attachment B). Interviews and focus groups were conducted with private sector representatives, local government officials, community organizations, and other stakeholders in the regional economic development realm, collecting vital insights into the City's business climate and opportunities for enhancing business support services.

To support a diversified, thriving economy in the City of Maple Ridge, the Investment Attraction Strategy has been developed, informed by industry analysis and interactions with relevant groups, with the following goals:

1. Alignment with City's goals, existing strategies and plans.
2. Attract a balanced mix of industries to grow the non-residential tax base.
3. Create additional employment opportunities.
4. Multi-year strategy to encourage investment from industries that would find Maple Ridge attractive and further the City's goals regarding economic sustainability and quality of life.

### **ANALYSIS:**

#### **Discussion:**

Based on the findings from the analyses and engagements undertaken for the City, the consultant developed a comprehensive Investment Attraction Strategy that presents a set of recommendations organized across five action areas, accompanied by a foundational costed implementation plan. The focus was on identifying key industries and companies to target, ensuring that Maple Ridge becomes a top choice for diverse investments aligned with community goals. The target industries identified for development include film, manufacturing, transportation and warehousing, professional scientific and technical services, and life sciences. These industries were selected for their potential to drive



economic growth and align with the municipality's long-term strategic objectives. These recommendations are:

1. *Vision and Operations in City Government:* To improve Maple Ridge's competitiveness in attracting investments, Economic Development should foster a shared economic vision, improve and focus inter-departmental communications, support cohesion among inter-departmental processes, and advocate for a shared set of metrics to track progress toward goals. This includes:
  - a. Lead the City in developing a unified vision for economic development and investment attraction.
  - b. Prioritize focused and targeted interdepartmental communications.
  - c. Continue to streamline processes to improve permitting and regulatory timelines and user experiences.
  - d. Adopt data collection and dissemination best practices, and
  - e. Advocate for policies at all government levels that support economic development in Maple Ridge.
2. *Branding and Marketing:* Marketing and branding shape investor impressions, often before direct contact. For this reason, a strong, far-reaching presence is essential. Clear, compelling materials and online resources should make engagement easy while showcasing the City's strengths and investment opportunities. A consistent, visually strong, and data-driven approach helps build early trust and positions Maple Ridge as a competitive, business-friendly destination. This includes:
  - a. Developing updated marketing materials for investment attraction in key sectors.
  - b. Improving wayfinding on the City website, and
  - c. Developing dedicated web content for investment attraction.
3. *Networking and Communications:* Strategic industry outreach and collaboration are key to attracting investment and strengthening Maple Ridge's economy. By prioritizing networking, engaging with local businesses, real estate developers, and regional partners, and fostering relationships with government partners, First Nations and other groups, the City can align economic goals and expand investment opportunities. Proactive engagement with property owners and BC Assessment will further support a business-friendly environment. This includes:
  - a. Prioritizing industry networking, and
  - b. Continuing to engage with local businesses and partners in the regional economic development ecosystem.

4. *Community Outreach and Public Relations:* Given the impact of community perception on business activity and economic development in Maple Ridge, the City should prioritize developing strategies that build trust and positively influence residents' and businesses' perceptions of quality-of-life factors within the community. This includes:
  - a. Proactively influencing positive community perceptions, and
  - b. Continuing to support community and placemaking events that generate economic activity in the downtown.
5. *Infrastructure and Business Support:* Ensuring investment-ready land, improving relationships with service providers, and expanding transportation options will enhance the City's economic appeal. By strengthening these foundational assets, Maple Ridge can position itself as a competitive and well-equipped destination for business growth. This includes:
  - a. Maintaining an inventory of physical assets.
  - b. Proactively creating investment-ready sites, and
  - c. Prioritizing the establishment of a hotel in Maple Ridge.

### **Business Retention and Expansion:**

Maple Ridge is growing, and its business community is a key to driving economic growth. To support existing and new businesses, regardless of size or sector, the City must adopt proactive approaches to business retention and expansion. Given the City's strong presence of retail-focused and local businesses, targeted support from the City can help these businesses achieve rapid and impactful successes.

The BRE Strategy offers a framework for identifying and supporting businesses at various stages of growth; providing guidance on relationship management with key stakeholders such as industry associations, workforce development organizations, and utility providers; actionable strategies for outreach and engagement; and outlining processes for tracking and measuring success, ensuring accountability and continuous improvement.

### **Public Engagement:**

In coordination with the Economic Development Department, EBP conducted in-person focus groups in January 2025 with local businesses, community organizations and partners, and members of the local and regional economic development ecosystem to gather key insights on the experience of doing business in Maple Ridge and with the City. The focus groups included discussions about processes and structures that impact the City's ability to successfully attract investment and to support business retention and expansion. Specific topics included the City's regulatory processes, business environment, marketing and promotion efforts, and the current state of land development and infrastructure in Maple Ridge.

**Next Steps:**

EBP developed a comprehensive BRE strategy, Investment Attraction Strategy and a foundational implementation plan outlining initiatives for the City to implement within the next five years. This plan will help the City refine its vision for its economic future and ongoing operations, refine its brand and marketing efforts, improve outreach and networking to attract new investment, and better support local businesses. A key next step for the City will be to identify which of the recommended industries to prioritize, as this decision will inform and could significantly shape future strategies (e.g., the update to the Official Community Plan).

Alongside the key performance indicators detailed in the strategy, the City will monitor metrics over time. These metrics serve as indicators of economic growth and can reflect the effectiveness of investment attraction initiatives. They will be gathered and observed on an annual basis whenever feasible.

**CONCLUSION:**

Maple Ridge is an integral part of the growing Metro Vancouver region, which is transitioning into a talent and technology-driven economy. Particularly, as industrial lands become increasingly scarce across the region, the City of Maple Ridge is well-positioned to play a pivotal role in attracting investment and fostering growth in key industries such as manufacturing, filming, professional scientific and technical services, life sciences and transportation and warehousing. By targeting investment attraction efforts on these key industries and focusing on strategies for business retention and expansion for existing businesses, Maple Ridge can capitalize on emerging opportunities, drive economic development, and strengthen its position within the broader regional economy.

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Prepared by: Aalika Kohli, Senior Advisor

**Attachments:**

- (A) Investment Attraction Strategy
- (B) Business Retention and Expansion Strategy
- (C) Costed Implementation Plan

## Report Approval Details

Document Title:	Investment Attraction and Business Retention and Expansion Strategies.docx
Attachments:	<ul style="list-style-type: none"><li>- Investment Attraction Strategy.pdf</li><li>- Business Retention and Expansion Strategy.pdf</li><li>- Costed Implementation Plan Summary - Investment Attraction Strategy.xlsx</li></ul>
Final Approval Date:	May 13, 2025

This report and all of its attachments were approved and signed as outlined below:

Eric Aderneck, Industrial Lands Program Manager

James Stiver, Director of Planning and Building

Zvi Lifshiz, Director Strategic Development, Communications & Public Engagement

Carolyn Mushata, Director of Legislative Services and Corporate Officer

Scott Hartman, Chief Administrative Officer

# Investment Attraction Strategy

CITY OF MAPLE RIDGE

MAY 2025



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## Executive Summary

The City of Maple Ridge, situated in British Columbia's Lower Mainland, is one of Metro Vancouver's fastest-growing communities, with a current population exceeding 105,000 and projections estimating growth to 150,000 within the next 15 years. To support this expansion and enhance the quality of life for its residents, the City is committed to diversifying its economy by attracting investments in sectors that will provide employment opportunities in growing and innovative sectors for Maple Ridge residents and help to balance the City's tax base.

The Investment Attraction Strategy contained in this document covers the work performed by EBP, a multi-national firm that specializes in economic development and investment attraction. This work includes data analyses, engagement with key groups and businesses in the local and regional economic development ecosystem, and opportunity matching to identify key industry sectors that the City of Maple Ridge is particularly well suited to focus investment attraction efforts towards. The insights from the EBP team's research and analyses informs the Strategic Action Plan and Costed Implementation Plan. This Investment Attraction Strategy was developed in parallel with a separate Economic Development initiative, the Business Retention and Expansion (BRE) Strategy. EBP has developed both strategies and refers to the BRE Strategy throughout this report.

The development of the Investment Attraction Strategy involved an in-depth analysis of existing economic networks, competitive advantages of the City relative to peer municipalities, and foreign direct investment trends to identify high-opportunity target sectors. The research team engaged with private sector representatives, local government, community groups, and other members of the regional economic development ecosystem in interviews and focus groups, gathering key insights into the City's business environment and the dynamics of and opportunities for improving investment attraction efforts in the City. Additionally, the research team conducted a thorough assessment of the City and region's physical assets through the eyes of a corporate site selector and evaluated their impact on the City's investment attraction efforts.

The analyses and research enabled the EBP team to evaluate the City's overall strengths and weaknesses, and the opportunities and threats that the City faces in attracting future investment. Similarly, the EBP team evaluated political, economic, social, and technological factors that may impact the City's ability to attract investment.

Building from this, the EBP team identified the following key industry sectors with strong growth opportunities for Maple Ridge as it works to develop a diversified, thriving economy – a summary of specific opportunities is provided in Table 1.



- **Film** (motion picture and video production)
- **Manufacturing** (food manufacturing, medical equipment, and aerospace)
- **Transportation & Warehousing** particularly as it may serve the other sector opportunities mentioned here
- **Professional, Scientific, and Technical Services** (especially related to engineering, architecture, environmental, Information Technology (IT), and communications)
- **Life Sciences** (diverse cluster comprising various subsectors, e.g., manufacturing, sports medicine, R&D)

Table 1. Summary of Industry Sector Opportunities

Sector	Opportunity
Film	<ul style="list-style-type: none"> <li>• Build out a more complete film hub by supporting other sector activities, e.g., pre-production, planning, writing, casting, post-production, special effects, a sound stage, and acting school.</li> <li>• Support ancillary sectors that support film activity, e.g., construction, carpentry and other trades, accommodation &amp; food services</li> <li>• Improve Maple Ridge's permitting ease and efficiency.</li> </ul>
Manufacturing	<ul style="list-style-type: none"> <li>• Ensure sufficient power and water services to support food manufacturing needs (e.g., water treatment and cold storage)</li> <li>• Foster school-to-work pipelines for local manufacturing via post-secondary programming in Maple Ridge.</li> <li>• Capitalize on Maple Ridge's proximity to an aerospace cluster and the region's strong R&amp;D activity to foster manufacturing.</li> </ul>
Transportation & Warehousing	<ul style="list-style-type: none"> <li>• Explore opportunities for refrigerated transportation and warehousing to support food manufacturing and life science activity. Electric trucking may be advantages to support sustainability values.</li> <li>• Explore opportunities for air-based transportation, given local assets.</li> </ul>
Professional, Scientific, & Technical Services	<ul style="list-style-type: none"> <li>• Foster school-to-work pipelines via post-secondary programming in Maple Ridge.</li> <li>• Market underutilised or vacant commercial properties to attract subsectors (e.g., design, marketing, legal services, public relations) synergistic with the film industry, manufacturing, and life sciences.</li> </ul>
Life Sciences	<ul style="list-style-type: none"> <li>• Explore the feasibility and market demand for wet lab spaces or other specialized facilities for life sciences.</li> </ul>

Building on the insights gained from the work described above, the EBP team has developed a comprehensive Strategic Action Plan and Costed Implementation Plan outlining a series of initiatives for the City to implement within the next five years. Included in the plan are detailed descriptions of each action item, as well as associated timelines, priority levels, responsible parties, and cost estimates for implementing each action. This plan will help the City refine its vision for its economic future and ongoing operations, hone its brand and marketing efforts, improve outreach and networking to attract new investment, and better support local businesses. Major activities of the Strategic Action Plan are summarized below:

**Table 2. Summary of Recommendation Areas and Action Items**

Recommendation Area	Action Item
Economic Development Vision and Operations	<ul style="list-style-type: none"> <li>• Lead on a Unified Vision for Economic Development in the City</li> <li>• Deploy targeted interdepartmental communications</li> <li>• Continue to streamline permitting and regulatory processes</li> <li>• Adopt data collection and dissemination best practices</li> <li>• Advocate for policy that supports Economic Development in the City</li> </ul>
Branding and Marketing	<ul style="list-style-type: none"> <li>• Develop updated marketing materials for investment attraction in key sectors</li> <li>• Improve wayfinding for Economic Development services on City website</li> <li>• Develop dedicated web content for investment attraction</li> </ul>
Industry and Local Business Networking and Communications	<ul style="list-style-type: none"> <li>• Prioritize industry networking</li> <li>• Continue to engage with local businesses &amp; partners in the regional Economic Development ecosystem</li> </ul>
Community Outreach and Public Relations	<ul style="list-style-type: none"> <li>• Proactively influence positive community perceptions</li> <li>• Continue to support community and placemaking events that generate economic activity in Town Centre</li> </ul>
Infrastructure and Business Supports	<ul style="list-style-type: none"> <li>• Maintain Inventory of Local and Regional Physical Assets that support investment attraction and business retention</li> <li>• Proactively create investment-ready sites</li> <li>• Prioritize the establishment of a reliable hotel in the City</li> </ul>

# 1. Introduction

As the City of Maple Ridge works to further the priorities laid out in the Maple Ridge Council Strategic Plan 2023-2026, the Investment Attraction Strategy should be considered as a key tool for helping the City to develop a diversified and thriving economy through focused investment attraction efforts in key industry sectors that will provide high-value employment opportunities for Maple Ridge residents and grow the City's non-residential tax base.

**Figure 1. Maple Ridge Council Strategic Priorities**



Source: Maple Ridge Council Strategic Plan. Figure developed by EBP.

In an effort to support a diversified, thriving economy in the City of Maple Ridge, the Investment Attraction Strategy has been developed with the following goals, as shown in the figure below.

**Figure 2. Goals of the Investment Attraction Strategy**

In addition to seeking alignment with the Council Strategic Plan 2023-2026, the EBP team reviewed other City of Maple Ridge strategies and plans, as well as plans and studies at the regional, provincial, and federal levels in order to ensure alignment with the goals and priorities of these plans. These include the following documents.

### Maple Ridge

- City of Maple Ridge Economic Development Strategy 2021
- City of Maple Ridge Tourism Strategy 2022
- Maple Ridge Retail Market Analysis 2023
- Hotel Market Study 2022
- Ridge Meadows Post-Secondary Needs Assessment 2023
- City on the Move, 2023 Annual Report
- Maple Ridge Moves, Transit Infrastructure Portfolio Overview
- Financial Overview Report, Financial Plan 2024-2028
- Maple Ridge Film Activity Report, 2024
- Maple Ridge Sector Profiles
- Maple Ridge Industrial Impact Analysis, 2024
- Maple Ridge Strategic Transportation Plan, 2023
- City of Maple Ridge Zoning Bylaw No. 7600-2019, 2024
- City of Maple Ridge Sanitary Master Plan, 2016
- City of Maple Ridge Official Community Plan, 2014 (last amended 2020)
- Sanitary Master Plan, 2016
- Area Concept Plans and Neighbourhood Plans

- Urban Forest Management Plan, 2024
- Housing Needs Report, 2024
- Proposed Housing Action Plan, 2024
- Quarterly Hiring Demand Summaries, 2023

### **Regional and Provincial**

- Metro Vancouver, Metro 2050: Regional Growth Strategy, 2022
- Transport 2050, Regional Transportation Strategy
- BC Trade Diversification Strategy, 2023
- Invest Vancouver Website and Key Industry Sectors
- Invest Vancouver FDI Opportunities Monthly Reports, 2024
- Fraser Valley Alliance FDI Sector and Target Market Analysis and Investment Readiness Strategy
- Provincial Housing Supply Act
- BC Agricultural Land Commission Website and Maps

### **Federal**

- Canada's Indo-Pacific Strategy 2023
- Canada's Defence Vision: Our North, Strong and Free
- Industry Profile of Screen-Based Media in Canada, 2023

## 2. Investment Attraction Best Practices

This section outlines best practices and principles in investment attraction. It serves to both clarify the principles embedded in EBP's methods and recommendations, and as a dynamic checklist the City of Maple Ridge ("the City") can continually use for investment attraction efforts.

Figure 3. Investment Attraction Best Practices



### Know Your Community

**Investment attraction is not an armchair exercise; maintain ongoing conversations with individual local businesses.** To keep a pulse on the region's competitive aspects and anticipate future threats and opportunities, foster ongoing conversations with industry leaders:

- Directly engage with local companies in potential target sectors to gain firsthand understanding of industry trends and local (dis)advantages. Understand how businesses operate and what keeps them (or not) in the region. Don't simply rely on a handful of highly engaged businesses. Relying on a small number of the vocal businesses can lead to a biased (often rose-coloured) understanding of a community. Take proactive steps to speak with businesses have recently departed or declined to invest in the region. Hosting one-on-one BRE meetings may also yield more honest and diverse takeaways than large business roundtables where only a handful of businesses speak.



Develop a thorough understanding of current industry dynamics, including prevalent challenges, emerging trends, and potential growth opportunities. Keep an eye on what regions attract foreign direct investments in sectors of interest.

**Understand the community's competitive (dis)advantages.** A successful investment attraction strategy begins with understanding your community's strengths and gaps. This involves thoroughly mapping production and distribution networks and industry interconnections. By tracing how goods and services move through the local or regional economy, the City can identify local competitive advantages that will make a compelling value proposition. Common factors include infrastructure (transportation, water, electric, gas, broadband), cost of living, non-residential tax rates), proximity to feedstock and secondary businesses, and market access. Regularly mapping the region's strengths, weaknesses, opportunities, and threats offers an opportunity to show what the jurisdiction has done to improve conditions and how it could offer further support to close gaps.

While the City or region should not lead with its disadvantages in its marketing materials, it is important to openly discuss them during internal strategy development. It is crucial to be honest conversations around disadvantages so that the City or region is clear on what factors are within their control (and to form appropriate strategies to mitigate or improve these factors), and to have a clear understanding of factors that are outside of the City or region's control.

**Identify sector gaps.** A strategic sector gap analysis helps communities identify unmet consumer and producer needs and potential business opportunities. This process goes beyond simple observation, requiring professional insights into supply and demand thresholds and market dynamics. By systematically analyzing current goods and services and conducting local surveys of businesses across the supply chain, communities can pinpoint precisely which new businesses would not only survive but thrive in their local ecosystem, as well as businesses that are at risk and may not survive in coming years. Moreover, the City's economic development team is uniquely positioned to connect businesses with each other and identify possible innovations that will fill gaps.

## Targeted Efforts

One of an economic development organization's (EDO's) primary challenges is to discern what activities will yield the greatest impact on the local economy. At the same time, it is important that an EDO not overextend its resources due to mission creep, i.e., the slow expansion of its scope of activities. For investment attraction, this means that focusing on a value proposition that plays to the community's strengths and is tailored to just a handful of target industries.

**Remain highly focused on a few key sectors and avoid a “shotgun” approach.** To maximize the impact of investment attraction efforts, the City should focus on a select few industries with the highest potential for successful attraction efforts. For example, the City should be selective in how many resources are dedicated to responding to non-target inquiries.

Target sectors should align with the City’s existing assets, workforce strengths, infrastructure, and political priorities. In selecting target industries, a city should consider:

- Alignment with Local Attributes: Focus on sectors that match the City’s current economic foundation, such as workforce skills, available facilities, and regional supply chain capabilities.
- Demonstrated Growth Potential: Prioritize industries with a track record of consistent growth or those that show resilience in challenging economic conditions.
- Supply Chain Adaptability: Target industries that are less vulnerable to disruptions in international trade and can adjust to shifts in foreign trade policies by diversifying suppliers.
- Economic Fit: Choose sectors where the City can offer a unique competitive advantage, such as proximity to resources, favorable logistics, or access to specialized talent.
- Political viability: Consider Council’s priorities and ensure political support for target sectors and proposed strategies. Identify political risks that could complicate or derail a particular strategy. Actively seek a political champion to promote the proposed target sector strategy.

## Holistic Approach

Investment attraction is more than just promoting available sites. It’s about demonstrating a region’s economic and community vitality.

**Align attraction efforts with the broader economic development strategy.** Ensure that investment attraction efforts are not pursued in isolation but are integrated with the City’s overarching economic development strategy. Every initiative should be a deliberate, strategic move that directly connects to the broader economic vision of the community. Similarly, it is important that all departments work toward this shared strategy – from those maintaining infrastructure to those building GIS maps and those processing development or building permits. The City’s economic development staff play a role akin to a coach in that they help channel and coordinate all stakeholders’ efforts. Alignment across the entire economic development ecosystem promotes coherence and prevents scattered efforts.



**Align attraction efforts with broader community goals.** Community goals can be distinct from economic goals, although both are rolled up into one strategy. Balancing community and economic priorities is especially important in areas that are somewhat economically mature or where there may be latent or existing opposition to change or “overdevelopment.” By distinguishing these different goals, the City can make a more compelling vision that gains more engagement and support from its diverse stakeholders.

**Do not neglect talent attraction.** In past decades, economic developers were often focused on “smokestack chasing” – attracting big employers, often in manufacturing. But due to economic and workforce shifts, investment attraction has become more about creating a holistic ecosystem of economic vitality. To that end, it is important that the City foster the amenities and sense of place that attract the talent best suited for the City’s target sectors. Assess the current talent pool, identify in-demand skills, and create strategies to attract and retain the talent that makes your community attractive to potential investors. Develop a comprehensive approach that focuses on building—and marketing—a high quality of life with diverse amenities.

Talent attraction has taken on even more importance amid a shift toward more remote/hybrid work and at-home businesses. Remote/hybrid workers, who may occasionally commute to more distant employment hubs like Vancouver, bring economic value to Maple Ridge. The City can foster key amenities like high-speed internet and downtown vibrancy to attract and retain talent.

**Develop and track diverse metrics.** Job creation and retention and capital expenditures are still key metrics, but they are not the end-all-be-all of economic development. Consider metrics that focus on job quality, resilience, and the experience of doing business in the City. For example, how vulnerable are certain industries and jobs to volatile markets, technological changes, and supply chain changes? Metrics can be qualitative or quantitative. For example, the City can not only track how long development and building permit issuance takes but also whether the processes are clear, replicable, and give prospective investors the confidence they need to move forward.

## High Visibility

To compete globally, the City must enhance its online presence and build strong investment networks. A clear value proposition, showcased through professional digital platforms, and strategic relationships with industry leaders can significantly boost visibility. Moreover, marketing resources must be visually engaging, have accessible data, and be easy to navigate to quickly identify key issues for target industries.

**Promote a highly visible value proposition tailored to target audiences.** Marketing materials designed to attract investors should not have the same tone as government-to-resident

communications. Instead, investor-oriented materials should be more similar to business-to-business (B2B) marketing.

One key tool in tailored marketing is online presence. Online platforms can provide visibility and exposure to investors. For example, the City of Glasgow recently launched a [tech ecosystem platform](#) targeted toward governments, venture capitalists, banks, corporate investors, and startups. Using storytelling through case studies, the platform creates a compelling narrative. Moreover, the existence of an up-to-date, aesthetic platform signals that the City is poised to work with investors to connect them with the right individuals.

[Waterloo EDC](#) (Ontario) is another example of an information-packed site. Note also that Waterloo EDC offers diverse and customizable services and resources (e.g., custom data packages, maps) for specific industry clusters. Their automated system not only reduces labour but also captures contact information for prospective investors, enabling the City to better track and engage businesses.

[Invest in Murcia](#) (Spain) offers another good model for an engaging and navigable website. Murcia is noted for its high win rate for FDI projects.

It is important that such online tools are designed to be accessible for not only desktop users but also mobile phone users.

**Leverage all brands at the City's disposal.** The City of Maple Ridge can benefit from using various brands tailored to different investors who may have varying degrees of familiarity with the region, province, or country. For example, from an international perspective, the City can leverage the "Canada" brand, but when competing within North American or regional markets, it can lean more on its British Columbia or Metro Vancouver brand. The City's economic development team should feel prepared to address questions from the federal level to the local level.

**Grow and leverage an investment network.** Build a comprehensive network of industry leaders and potential investors across target economic sectors. Cultivating key relationships can create a multiplier effect, where initial connections amplify the City's investment profile through extended professional networks. By fostering strategic collaborations, the City can develop a sophisticated and effective approach to attracting investment opportunities.

**Routinely benchmark against competitor regions with an eye on global competitiveness.**

Systematically benchmark your city by closely monitoring performance against peer municipalities, regional competitors, and global industry leaders. Track critical metrics including investment rates, talent attraction, infrastructure development, and regulatory environments.

Extend analysis beyond local boundaries to gain insights from international economic ecosystems.

Remember that investment attraction is a *global* question. By maintaining a comprehensive, dynamic perspective on comparative economic indicators, cities can identify competitive advantages, address potential gaps, and strategically refine their economic development approaches. However, if local competitors are not globally competitive outside the immediate region, it is critical to benchmark against the global competitors. It is important that a City not rest on its laurels because it is the most competitive among its immediate neighbours if it is still not nationally or globally competitive.

## Preparedness

Effective investment attraction starts with preparation. The City must ensure it has the tools, resources, processes, sites, and approvals in place to meet investor needs through a seamless investment attraction protocol. Governments must strive to respond quickly and effectively to meet private investors' expectations.

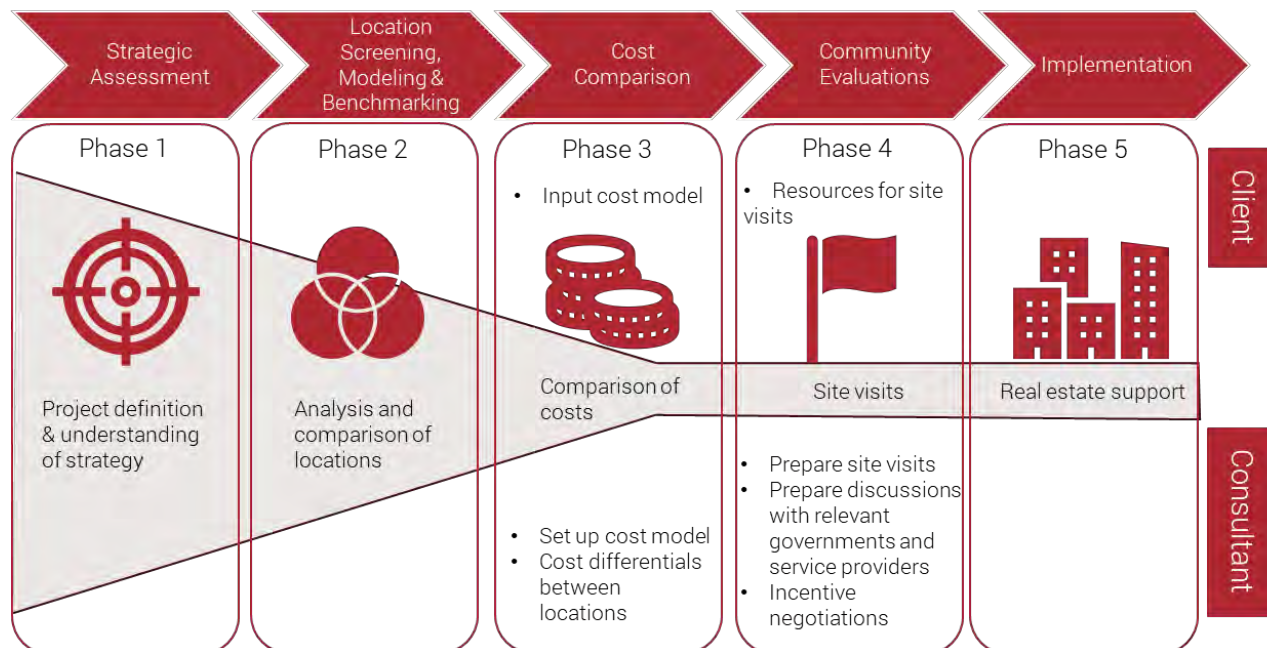
**Identify and prepare investment-ready sites.** Create a comprehensive, accessible, and up-to-date inventory of zoned and serviced commercial and industrial lands. Ensure that potential investors can easily visualize and access suitable locations for their businesses. Having clearly defined, ready-to-develop sites is crucial in making your community an attractive destination for new investments. Consider how the City can leverage City-owned property.

**Invest in the right tools.** Technology is critical for improving the City's visibility and reach to global audiences. Conduct research on what tools (e.g., Zoom Prospector, fDi Markets, FT Locations) work best for the City's target industries. It's also important to note that data is only as useful as the actionable insights it provides. As for customer relationship management (CRM) tools to respond to inquiries from prospective investors, consider what target sector representatives will be most interested in. There should be a clear inquiry response process and industry-specific FAQs that anticipate investors' needs. Also prioritize inquiries from industries for which the City is competitive; don't overextend resources to address low-prospect or low-priority opportunities.

**Audit the City's lead generation and business recruitment process.** Conduct a thorough review of each step in the economic development team's recruitment strategy, from handling initial inquiries to supporting site selection. Evaluate how effectively the City communicates with potential investors, provides necessary information, and addresses challenges. This process should identify gaps in communication, inefficiencies in response times, and any barriers that could discourage investment along each of the steps identified in Figure 2. Speak with existing

businesses and investing businesses to incorporate their feedback. Ongoing assessment helps ensure the City remains a fast-acting and investment-ready location.

**Figure 4. The typical corporate site selection process**



**Implement, monitor, and continuously improve.** Launch your investment attraction strategy with a commitment to ongoing evaluation and adaptation. Conduct regular progress reviews, assess your achievements against initial objectives, and be prepared to adjust your approach in response to changing economic conditions. Recognize that successful investment attraction is a long-term, dynamic process requiring patience and flexibility.

### 3. Engagement with Relevant Groups

In coordination with the Economic Development Department, the EBP team conducted in-person focus groups in January 2025 with local businesses, community organizations and partners, and members of the local and regional economic development community to gather key insights on the experience of doing business in and with the City of Maple Ridge. The focus groups included discussions about processes and structures that impact the City's ability to successfully attract investment, and to support business retention and expansion. Specific topics included the City's regulatory processes, business environment, marketing and promotion efforts, and the current state of land development and infrastructure in Maple Ridge.

Key findings and opportunities identified during the focus groups are summarized in the table below. For more detail on the in-person focus groups, please see the BRE Strategy and Implementation Plan.

**Table 3. Key Findings and Opportunities for Maple Ridge as identified in Focus Group Sessions**

Challenge faced by the City	Resulting Implications for Businesses	Opportunities for the City Leadership
Infrastructure Gaps	Businesses must invest heavily in basic amenities like water and sewer services, leading to increased costs (e.g., up to \$300 million for water upgrades in some areas).	Streamline communication, implement modular infrastructure solutions, and ensure leadership support for growth-focused policies
Limited Industrial Land	Businesses struggle to find suitable locations for operations and expansion, restricting economic growth.	Identify and rezone land for industrial use and explore mixed-use developments to optimize available space
Transportation Connectivity Issues	Congestion, particularly on Lougheed Highway and Golden Ears Bridge, creates logistical challenges and increases transportation costs.	Advocate for regional transportation improvements and explore mobility solutions to ease congestion
Slow Permitting and Approval Processes	Lengthy timelines (up to 2.5 years instead of 6 months) discourage investment, delay project completion, and put government grants at risk.	Streamline approval processes by enhancing interdepartmental coordination

Challenge faced by the City	Resulting Implications for Businesses	Opportunities for the City Leadership
		Better support and improve awareness of concierge services within EcDev team to keep businesses informed through the process
Insufficient Temporary Accommodations	Lack of hotels and short-term housing impacts industries like film and construction, making it harder to attract workers.	Encourage hotel and rental housing development through incentives and partnerships with private developers
Lagging Commercial Development	Residents commute outside the City for amenities and services, reducing local consumer spending and business viability.	Support local business growth by attracting retail and service-sector investments to enhance commercial offerings, such as celebrating 'Shop Local Week'
Historical Lack of Interdepartmental Collaboration	Inefficient processes and bureaucratic delays worsen business development challenges, causing frustration among business owners.	Strengthen EcDev's role in interdepartmental working groups to improve communication and accelerate decision-making



## 4. Benchmarking Analyses

The EBP team conducted several data analyses to benchmark the City of Maple Ridge's competitive position against various local and global competitors. These data analyses also explore trends in foreign direct investment, employment, and other economic indicators that reveal sectors where the City of Maple Ridge and the Metro Vancouver Region excel. These analyses ultimately help the EBP team to identify fast growing industry sectors in which the City is well suited to attract investment.

### FDI Benchmarking Analysis

fDi Markets is a comprehensive database that tracks greenfield foreign direct investments (FDI) globally. It provides real-time data to identify trends, opportunities, and drivers in cross-border investment activity. The database includes detailed information on over 130,000 active investors and allows analysis by source market, destination market, sector, subsector, and business activity.

EBP reviewed FDI projects using fDi Markets from January 2020-December 2024 at the national, provincial, and municipal levels by sector, subsector, and business activity. This analysis allows us to evaluate the competitive marketplace and potentially identify new trends the City may wish to consider in its positioning.

Generally, sectors with high job creation tend to be labour-intensive and align well with the City's areas of interest, including software & IT services, biotechnology, consumer products, and renewable energy. Sectors with high CapEx spending tend to include extractive industries like nonrenewable energy and mining, manufacturing, renewable energy, real estate, and transportation and logistics. Several of these sectors tend to have high job creation and CapEx spending and align well with the City's areas of interest.

This analysis includes tables listing top sectors in which jobs were created in the last five years, and associated Capital Expenditures (CapEx). Given the City's rapidly growing employment base, population density, and location in the Vancouver metropolitan region, we believe prioritizing high job creation sectors—some of which also have high CapEx spending—is the most useful lens through which to examine FDI trends.

fDi Markets lists just one project associated with the City of Maple Ridge—the 2023 E-One Moli Energy (Molicel) plant—in the last five years. Vancouver has much better data availability, and we used Vancouver FDI as a proxy for understanding industries in which Maple Ridge may compete.

Throughout this analysis, jobs are presented as count of single jobs and CapEx as millions or billions of US dollars. Companies do not always release public information on investment amount or job creation; fDi Markets uses a proprietary econometric algorithm to estimate jobs and investment value where the actual figures are unknown. In lists of source markets and sectors, data is discussed in descending order of magnitude.

## High-investment FDI Sectors by Geography

### Destination: Canada

**Table 4. Sectors with Greatest Job Creation, 2020-2024**

Sector	Jobs	CapEx (US \$M)
Software & IT services	57,554	20,336
Consumer products	21,598	5,136
Electronic components	12,909	20,176
Business services	12,156	1,938
Automotive OEM	8,158	13,538

**Table 5. Subsectors with Greatest Job Creation, 2020-2024**

Subsector	Jobs	CapEx (US \$M)
Custom computer programming services	23,434	3,329
Software publishers, except video games	18,764	9,436
Non-store retailers	15,125	2,760
Batteries	10,412	18,038
Video games, applications and digital content	7,085	3,867

**Table 6. Business Activities with Greatest Job Creation, 2020-2024**

Business Activity	Jobs	CapEx (US \$M)
Manufacturing	43,021	72,253



Business Activity	Jobs	CapEx (US \$M)
Research & Development	33,543	20,831
Business Services	31,659	3,718
Logistics, Distribution & Transportation	30,909	8,818
Sales, Marketing & Support	19,347	5,855

### Jobs Trends: Canada

Software & IT services was the sector with the most jobs created by companies from any origin, with top source markets including the US (27,913), India (8,472), the UK (4,320), France (2,906), and Germany (1,039).

France- (3,246) and UK-based companies (1,894) created significant jobs in the business services sector. Companies from Germany (3,975), the Netherlands (3,150), and Sweden (3,050) created significant jobs in the electronic components sector.

The US created the most jobs in the Software & IT services and Consumer products (18,750) sectors.

### CapEx Trends: Canada

The greatest CapEx investment categorized by source country came from companies in Germany in the electronic components sector at almost \$7 billion in five years. Other countries investing in the Canadian electronic components sector include Sweden (\$5 billion), the Netherlands (\$4.268 billion), Switzerland (\$910 million), and the United States (\$731 million).

The US made significant CapEx investments totaling \$11 billion in software & IT services, \$10.9 billion in communications, and \$5.4 billion in automotive OEM. Japanese companies led CapEx investment in automotive OEM at \$6.6 billion.

### Destination: British Columbia

Table 7. Sectors with Greatest Job Creation, 2020-2024

Sector	Jobs	CapEx (US \$M)
Software & IT services	6,634	2,387
Consumer products	2,678	360

Communications	1,893	2,450
Transportation & Warehousing	1,309	592
Business services	1,243	278

**Table 8. Subsectors with Greatest Job Creation, 2020-2024**

Subsector	Jobs	CapEx (US \$M)
Custom computer programming services	2,898	243
Non-store retailers	2,525	509
Software publishers, except video games	2,443	1,297
Motion picture & sound recording industries	1,601	1,791
Warehousing & storage	837	364

**Table 9. Business Activities with Greatest Job Creation, 2020-2024**

Business Activity	Jobs	CapEx
Logistics, Distribution & Transportation	4,881	1,400
Research & Development	4,672	3,692
Business Services	4,397	570
Sales, Marketing & Support	1,880	517
Manufacturing	1,349	2,437

**Jobs Trends: British Columbia**

Table 7 presents FDI in British Columbia by sectors that added the most jobs in the last five years. Those with high job counts and CapEx added include Software & IT services and Communications, with over US \$2 billion in CapEx each. Software & IT services appears as a sector with exceptionally high jobs added as well as CapEx investment.

By sector, the US created 3,581 jobs in the software & IT sector, a smaller but still notable number in communications, transportation & warehousing, and business services (under 1,000 each). By subsector, the greatest number of jobs by source market were created by US companies in Non-store retailers (2,525) and Software publishers, except videos games (1,704), followed by custom computer programming services by Indian companies (1,247).

### CapEx Trends: British Columbia

The largest CapEx invested in British Columbia in the last five years was in Software and IT services by the US (\$1.663 billion)—far outpacing the next largest source market in this sector, Switzerland (\$345 million). The US also invested significant CapEx in communications (\$1.313 billion), consumer products (\$630 million), and transportation and warehousing (\$168 million).

Other investments of note include one project worth \$175 million from South Korea in the communications sector, along with investments from India (\$317 million), the UK (\$300 million), and New Zealand (\$158 million).

It is worth noting that the US \$1 billion investment by E-One Moli Energy (Molicel) in Maple Ridge is listed in fDi Markets under the British Columbia destination market, belonging to the Electronic components sector, Batteries subsector, and Manufacturing business activity. No location motive was cited in publicly available communications for this investment. Originally announced in December 2023, the project has since been put on hold, with the investor stating “the economics of battery power electrification have [since] changed”<sup>1</sup>. The project is expected to create 350 permanent jobs in Maple Ridge. From January 2020-December 2024, this is the only FDI project available for the City of Maple Ridge in fDi Markets.

By subsector, batteries received the largest amount of CapEx investment originating primarily from Taiwan (\$1 billion), Norway (\$516 million), and Germany (\$500 million). Switzerland invested \$345 million in software publishers except video games. The US had almost \$682 million in investment in the motion picture and sound recording subsectors, including two projects by parent company Disney.

By economic activity, the most investment was made by the US in Research & Development

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<sup>1</sup> <https://www.msn.com/en-ca/money/other/investor-hits-pause-on-billion-dollar-maple-ridge-battery-plant-expansion/ar-AA1vhWON?ocid=BingNewsVerp>

(\$2.398 billion), far exceeding the next leading investment amount from the UK in the same area (\$429 million). Manufacturing was next largest activity receiving investment from Taiwan (\$1 billion), Norway (\$516 million), Germany (\$500 million), and the US (\$319 million).

### Destination: City of Vancouver

**Table 10. Sectors with Greatest Job Creation, 2020-2024**

Sector	Jobs	CapEx (US \$M)
Software & IT services	5,276	2,139
Communications	1,417	1,925
Consumer products	1,153	315
Business services	1,054	231
Transportation & Warehousing	870	341

**Table 11. Subsectors with Greatest Job Creation, 2020-2024**

Subsector	Jobs	CapEx (US \$M)
Software publishers, except video games	2,443	1,297
Custom computer programming services	1,951	198
Motion picture & sound recording industries	1,283	1,457
Nonstore retailers	1,000	194
Warehousing & storage	758	330

**Table 12. Business Activities with Greatest Job Creation, 2020-2024**

Business Activity	Jobs	CapEx (US \$M)
Research & Development	3,695	2,953
Business services	3,141	453
Logistics, Distribution & Transportation	2,123	605

Sales, Marketing & Support	1,584	426
Technical Support Centre	184	19.8

### Jobs Trends: City of Vancouver

Most jobs were created by US companies in software & IT services (2,985) and Consumer products (1,153), followed by communications (551). The UK (631), India (610), and France (184) also created jobs in Software & IT, though the US created over three times more than the next leader. The UK (282) and India (264) created a moderate number of jobs in communications. The UK created 370 jobs in Business services. The UAE created 641 jobs in Transportation & warehousing.

### CapEx Trends: City of Vancouver

The US invested the highest amount of CapEx in software and IT services (\$1.456 billion). Switzerland was the next leader, with less than a quarter as much investment (\$345 million). The US also invested \$963 million in communications and \$315 million in consumer products. UAE companies invested \$260 million in Transportation & warehousing.

By subsector, US-based companies made the greatest investment in software publishers, except video games (\$796 million) followed by motion picture & sound recording industries (\$524 million). The US also invested in Other Software & IT services (\$427 million) and Data processing, hosting, & related services (\$439 million).

## Top Source markets for FDI

### Destination: British Columbia

Table 13. Source markets with greatest job creation, 2020-2024

Sector	Jobs	CapEx (US \$M)
United States	9,374	4,695
India	1,793	450
United Kingdom	1,423	536
UAE	698	286
Germany	445	585

**Jobs Trends: British Columbia**

Top origin countries for projects creating jobs in British Columbia include India, US, UK, Japan, Switzerland, and Australia. Within the US, top source markets were Washington, California and New York.

**CapEx Trends: British Columbia**

Source markets with the highest total of CapEx investments in the last five years in British Columbia include the US, Switzerland, the UK, India, and New Zealand. Within the US, top source markets were California, Washington, New York, and Pennsylvania.

**Destination: City of Vancouver****Table 14. Source markets with greatest job creation, 2020-2024**

Source Country	Jobs	CapEx (US \$M)
United States	5,365	2,987
United Kingdom	1,283	507
India	1,073	418
UAE	669	274
Australia	329	38.8

**Jobs Trends: City of Vancouver**

Top origin countries for projects creating jobs in Vancouver include the US, UK, India, UAE, and Australia. Within the US, top source markets were Washington, California, and New York.

**CapEx Trends: City of Vancouver**

Source markets with the highest total of CapEx investment in the last five years in Vancouver include the US, UK, India, Switzerland, and UAE. Within the US, top source markets were California, Washington, Massachusetts, and Texas.

## FDI Trends for Competitor Municipalities

EBP reviewed fDi data available for the cities included in Table 15. Maple Ridge identified 15 competitor jurisdictions of interest located throughout Canada, the United States, Australia, and Sweden. EBP included them in this analysis based on data availability. Where possible, we identified a nearby proxy location to include in the analysis.

There was no municipal-level data for Stonnington, Australia or Sundsvall, Sweden. We included Melbourne as a stand-in for Stonnington though - like Vancouver - it has received FDI in every highlighted sector, likely due to its size and industrial diversity. This makes Melbourne a less useful comparator jurisdiction than smaller municipalities in Table 15.

Other locations lacking data in competitive sectors listed in Table 15 in fDi Markets include: Strathcona County Alberta; St. Albert, Alberta; Langley Township, BC; Mission, BC; and Eugene, Oregon.

Table 15 identifies sectors in which investors have added at least 20 jobs in the last five years in the corresponding municipalities. More significant job counts added in the sectors include: 700 jobs in Consumer products for Langley, British Columbia; 251 jobs in Software & IT services and 280 jobs in Biotechnology for Boulder, Colorado; 200 jobs in Consumer products for Pickering, ON; and 193 jobs in Transportation & Warehousing for Whangarei, New Zealand.

**Table 15. FDI jobs by competitor municipality, 2020-2024**

Competitor	Software & IT services	Consumer products	Transportation & Warehousing	Communications	Biotechnology	Industrial equipment	Food & Beverages	Electronic components
Melbourne (Australia)	X	X	X	X	X	X	X	X
Vancouver (BC)	X	X	X	X	X	X	X	X
Langley (BC)		X					X	
Boulder (CO, US)	X				X	X		
Pickering (ON)		X						
Whangarei (New Zealand)			X					
Port Coquitlam (BC)	X							
Chilliwack (BC)							X	
North Vancouver (BC)	X							X
Bellingham (WA, US)								X
Airdrie (AB)				X				



## Motive Determinants for Investment by Sector

Table 16. Companies citing location motives by sector, 2020-2024

Sector	Companies
Communications	Elcome, Weta Digital, Alkemy X, View Industrial Light and Magic, Weta Digital, Vaudeville Sound Group
Software & IT Services	InfoStride, Monstar Lab, HCL Technologies, WillowTree, Wayve Technologies, Indrivo, Boomi, Kaseya, Accutiv
Business Services	RCP, Egon Zehnder, Innova Research, Vaco, Indexsy, Bara Consulting, Leyton, Lingsight (Bruce Shaw Group)
Industrial Equipment	Metis Cyberspace Technology, Homag Canada, Wolseley Canada, JASA Packaging Solutions, Metso Corporation
Transportation & Warehousing	ShipBob, FedEx Ground
Electronic components	Kongsberg Maritime, Beckhoff Canada, Hexagon Purus
Automotive OEM	Tesla Motors

Table 17. Company motives for locating in British Columbia by sector\*, 2020-2024

	Motion picture & sound recording industries**	Software & IT services	Communications	Electronic Components	Manufacturing-related***	Transportation & Warehousing
Skilled workforce availability	X	X	X	X	X	X
Proximity to markets or customers	X	X	X	X	X	X
Domestic market growth	X	X	X		X	
Quality of life	X	X	X			
Taxes & incentives	X		X			



Industry Cluster	X	X	X			
Technology & Innovation		X	X	X		
Regulatory Environment						
Universities & Research hubs		X				
Transport infrastructure			X			X

\*Definitions and examples of company motive categories are outlined in the fDi Markets data dictionary<sup>2</sup>

\*\*fDi Markets considers Motion picture & sound recording industries a subsector; communications is the corresponding sector. We isolated this category as a subsector of interest to the City.

\*\*\*fDi Markets had one project with motive listed for manufacturing as a business activity category. However, this column represents motives cited by companies in additional, related sectors including Automotive OEM, Food & beverages, and Industrial equipment.

## Sectors of Interest and Data Availability

Table 16 lists a sampling of companies citing motives for choosing British Columbia as an investment location in public communications compiled by fDi markets, categorized by sector. Table 17 outlines the motives these companies cited by sector.

EBP reviewed motives in sectors of interest that we co-identified with the City. Companies in sectors of interest appear to prefer British Columbia for its skilled workforce availability, proximity to markets and customers, domestic market growth, quality of life, and relevant industry clusters.

Notably, three sectors—software & IT services, electronic components, and communications—cited technology and innovation as an important motive. Companies in communications and motion picture & sound recording categories noted favorable taxes and incentives were key to their location decisions.

EBP reviewed differences in motives between companies locating in Vancouver specifically and found no major divergence from data available for all of British Columbia. Vancouver has slightly lower data availability compared to British Columbia given it is a smaller geography. Only a small subset of investment projects available for both jurisdictions cited a location motive in publicly available communications.

<sup>2</sup> <https://app.fdimarkets.com/application/assets/fDi%20Markets%20Client%20Data%20Dictionary.pdf>

fDi Markets categorizes investment by sector based on a combination of industry classifications (NAICS, SIC, and ISIC) and the specific economic activity of FDI projects tracked. To make sector, sub-sector, and business activity categories internationally applicable, fDi Markets does not align with any single industry classification system.

The following industries of interest the City identified were not well represented in fDi Markets with motives listed: tourism (agritourism), agritech, hotels, manufacturing, renewable energy, and mass timber. FDI may still have occurred in these categories, but no motive for locating in British Columbia was publicly listed, or the type of economic activity is captured under a related sector category. Some tourism activity, for example, may be represented in related support sectors including Real Estate, Non-automotive transport OEM (boating), Communications (i.e., Disney), and Food & Beverage.

Business activity related to the tourism industry may not be adequately reflected in FDI data, as it typically occurs among small, local entrepreneurs and independent contractors, including: small retail, restaurants, Airbnbs, transportation and ride-sharing providers.

Agritech investments may be categorized under Software & IT services, Industrial equipment, Transportation & Warehousing, or Biotechnology, depending on the project type. Jasa Packaging Solutions, for example, announced a \$6-million CapEx investment in 2022 and cited British Columbia's "rich greenhouse industry" as a key motive. The company specializes in weighing and packaging machines for fresh produce, and fDi markets categorizes the project as belonging to the Industrial equipment sector.

Finally, fDi Markets records historic FDI and is not appropriate for analysis of forthcoming business opportunities and investment trends. For these reasons, further analysis should be conducted to examine the City's value proposition in sectors and subsectors of interest, including Location Quotient and Shift Share analysis.

## Competitive Benchmarking Analysis

The Competitive Benchmarking Analysis component of this research includes **location quotient (LQ)** analysis, **shift-share** analysis, and **reverse site selection (RSS)** analysis to answer key questions related to investment attraction strategy:

- Which are the fastest growing sectors in the region?
- What are Maple Ridge's most and least competitive sectors?
- What is Maple Ridge's competitive position relative to local, national, and global peer competitors?
- What limiting factors does Maple Ridge face in investment attraction?

Each method provides a distinct perspective on the local economy:

- **LQ analysis** measures sectoral concentration relative to a broader benchmark, identifying industries where Maple Ridge is more (or less) specialized compared to regional, national, or global peers.
- **Shift-share analysis** breaks down job growth into national trends, industry trends, and local competitive effects, revealing where the City is gaining or losing ground due to its own unique advantages or disadvantages.
- **RSS analysis** evaluates Maple Ridge's attractiveness for investment relative to peer cities.

Together, these analyses identify Maple Ridge's current economic position, highlight sectors with strong or emerging competitive advantages, and provide insights into where strategic policy or program interventions—such as workforce development, infrastructure investment, or business incentives—can enhance sectoral concentration (LQ) and drive more robust local competitive growth (shift-share) in the future.

The sector definitions in this analysis follow the [North American Industry Classification System](#) (NAICS), a standardized framework used across Canada, the US, and Mexico to ensure consistency and comparability across regions. Due to limitations in Canadian census data at the city level for the years of analysis (2021-2024), this report focuses on major industry sectors (2-digit NAICS), meaning that more detailed subsector trends (3-digit and beyond) cannot be independently captured. Most importantly, these analyses can be performed again in the future to measure progress in the City's economic development initiatives.

The City of Maple Ridge demonstrates an overall competitive position, ranking 3rd in EBP's RSS model. Maple Ridge's competitive edge diminishes, however, when compared to its local peers in British Columbia (BC)

Despite its smaller population and workforce compared to peers, the City has maintained strong growth momentum. The analyses indicate emerging opportunities in manufacturing, where significant job growth has been driven by local competitive advantages. At the same time, the city's manufacturing employment concentration remains below average, signaling a less developed hub than other competitor regions. The analysis also found local uncompetitive factors for construction employment, which could reflect slowing activity. Construction employment concentration remains high, but it is uncertain what drives this local lack of competitiveness.

The City also faces workforce and logistical challenges, including lower educational attainment than competitors, particularly in bachelor's degrees, and infrastructure limitations with longer drive times to major airports and rail facilities. However, these are partially offset by competitive advantages including low unemployment, lower wages than many competitors, competitive commercial tax rates, and access to a substantial market within a one-hour drive.

## Peer Comparators

Business location decisions are comparative, with companies evaluating potential sites not just on their individual merits but on how their attributes compare with other jurisdictions. Site selectors assess factors like infrastructure, workforce, and market access through this comparative lens, weighing the relative advantages and disadvantages of each location against alternatives.

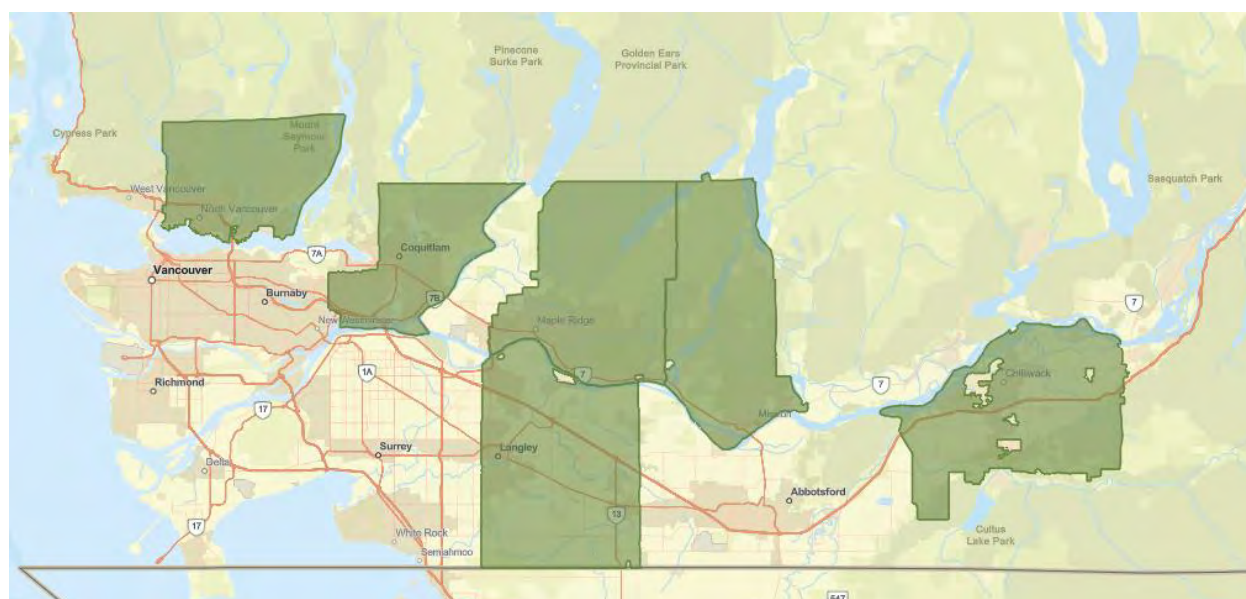
To identify Maple Ridge's competitive (dis)advantages from a local, national, and global perspective, we simulated a corporate site selection ("reverse site selection") with 15 competitors across British Columbia as well as jurisdictions across Canada, the US, and three other countries (Sweden, Australia, and New Zealand), as shown in the table below. This selection is based on our firm's experience and consultation with the City of Maple Ridge economic development staff. These peers are comparable to Maple Ridge in their population size, recent growth patterns, proximity to major population and employment hubs, economic composition (e.g., manufacturing, professional & technical services), and proximity to natural environment.

**Table 18. Comparator Regions**

Comparator	Region	Notes
Airdrie (City)	Alberta	Proximate to Calgary, Suburban market, has similar sectors as Maple Ridge
Bellingham (City)	Washington, USA	Suburban market with proximity to nature; redeveloping areas (e.g., waterfront) to attract business
Boulder (City)	Colorado, USA	Suburban market with proximity to Denver and nature
Chilliwack (City)	British Columbia	Local competitor in Fraser Valley / Lower Mainland of British Columbia
Coquitlam (City)	British Columbia	Local competitor in Metro Vancouver
Eugene (City)	Oregon, USA	Suburban market with proximity to nature; notable manufacturing sector
Langley (Township)	British Columbia	Local competitor in Metro Vancouver

Comparator	Region	Notes
Mission (City)	British Columbia	Local competitor in Fraser Valley / Lower Mainland of British Columbia
North Vancouver (City)	British Columbia	Comparison often made between Maple Ridge and North Vancouver of two decades ago
Pickering (City)	Ontario	Proximate to Toronto, similar population and economy
St. Albert (City)	Alberta	Proximate to Edmonton, similar population
Stonnington (City)	Australia	Proximate to Melbourne, similar population, similar sectors as Maple Ridge
Strathcona (County)	Alberta	Proximity to Edmonton and natural environment (e.g., a national park that must be accessed via the municipality)
Sundsvall (City)	Sweden	Experiencing significant growth, potential for developing industrial lands
Whangārei (City)	New Zealand	Smaller jurisdiction with potential for industrial lands

**Figure 5. Regional Peers: Chilliwack, Coquitlam, Langley, Mission, North Vancouver, and Maple Ridge.**



## Location Quotient Analysis

This analysis examines Maple Ridge's industries' employment concentrations vis-à-vis its comparators. Location Quotient (LQ) measures the concentration of employment in a particular industry for a specific geographic area relative to a benchmark (in this case, Canada), where a value of 1.0 represents the national average. Values greater than 1.0 indicate regional specialization, which suggests that the industry plays a relatively significant role in the local



economy and that the region may have strengths or locational advantages that attract and support the industry. Conversely, an LQ less than 1.0 suggests that the local industry is underrepresented compared to the national average.

LQ analysis key findings<sup>3</sup>:

- **Wholesale trade (LQ: 1.5)** and **construction (LQ: 1.6)** stand out with the highest LQs within the City's economy and among the selected comparators, although Maple Ridge's relative advantage is less pronounced among local peers.
- **Information & cultural industries (LQ: 1.2)** have above-average concentration in Maple Ridge, likely reflecting the City's film industry. While Maple Ridge may have strong advantages in the film subsector, it does not stand out decisively among its comparators at the major sector level. In other words, the City's strengths in the film industry may be obscured by the inclusion of other subsectors within this broader classification.
- **Retail trade (LQ: 1.1)** in Maple Ridge is slightly above the national average, but it does not stand out among its peers, but the comparators vary little in their concentrations in this sector, whose LQs range from 0.8 to 1.2.
- **Manufacturing (LQ: 0.8)** employment concentration is below the national average and does not stand out among its peers. Although Maple Ridge is slightly more a hub than some local peers like Coquitlam and North Vancouver, it is still a relatively less concentrated hub than Bellingham, Mission, and Chilliwack. Note that 2024 data do not reflect the new E-One Moli manufacturing plant and its anticipated 450 permanent jobs.<sup>4</sup>
- **Professional, technical, and scientific services (LQ: 0.7)** appear notably less concentrated in Maple Ridge than in its peers. Maple Ridge falls short of its comparators, including Boulder, North Vancouver, Coquitlam, and Pickering. Local educational institutions could increase this sector's workforce, so long as post-degree work opportunities are also local.
- **Agriculture, forestry, fishing, and hunting (LQ: 0.5)** have very low employment concentration, which stands in stark contrast with local peers including Chilliwack, Langley, and Mission. While the sector may have high production value, it may not represent a large employment opportunity.

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<sup>3</sup> While LQ analysis sheds light on employment hubs, it does not necessarily speak to economic output. Employment is only one, albeit important, metric for economic vitality.

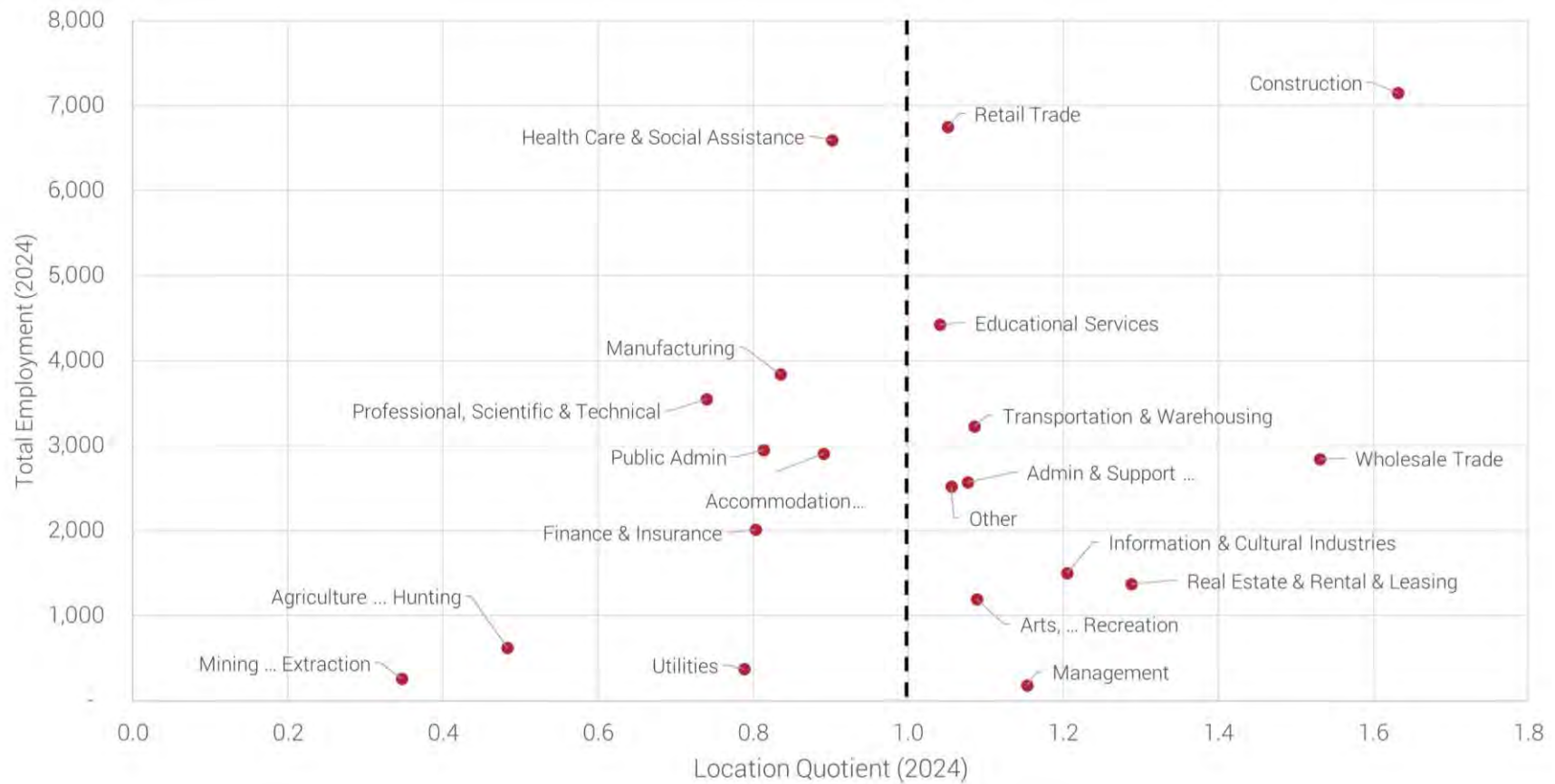
<sup>4</sup> <https://globalnews.ca/news/10091780/billion-dollar-battery-plant-maple-ridge/>

Table 19. Maple Ridge Sectors by LQ and Employment (2024)

Sector	LQ	Employment
Construction	1.6	7,152
Wholesale Trade	1.5	2,843
Real Estate & Rental & Leasing	1.3	1,376
Information & Cultural Industries	1.2	1,502
Management of Companies & Enterprises	1.2	185
Retail Trade	1.1	6,748
Transportation & Warehousing	1.1	3,229
Administrative & Support, Waste Management & Remediation	1.1	2,573
Arts, Entertainment & Recreation	1.1	1,192
Other Services (Except Public Administration)	1.1	2,521
Educational Services	1	4,428
Health Care & Social Assistance	0.9	6,593
Accommodation & Food Services	0.9	2,908
Utilities	0.8	375
Manufacturing	0.8	3,842
Finance & Insurance	0.8	2,012
Public Administration	0.8	2,949
Professional, Scientific & Technical Services	0.7	3,547
Agriculture, Forestry, Fishing & Hunting	0.5	622
Mining, Quarrying, Oil & Gas Extraction	0.4	259
<b>Total Industry Employment</b>	<b>-</b>	<b>56,856</b>

Source: EBP analysis of Esri BAO data.

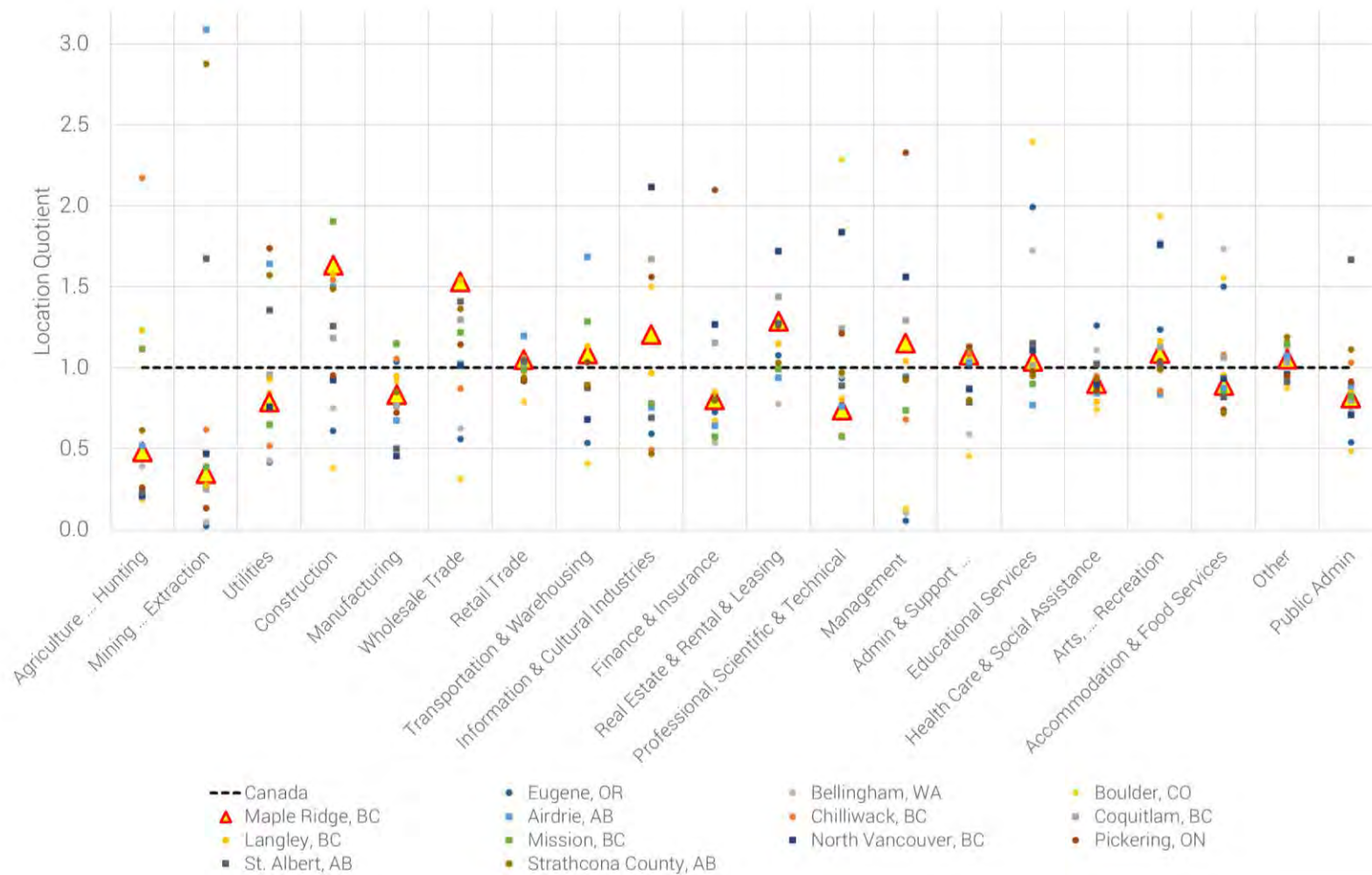
Figure 6. Maple Ridge Location Quotients and Employment by Sector



Source: EBP analysis of Esri BAO data



Figure 7. Maple Ridge LQ Comparison with Peers.



Source: EBP analysis of Esri BAO data. Note: US comparator LQs were calculated using Canadian national employment as a baseline.

## Detailed Sector Descriptions

- Agriculture, Forestry, Fishing, and Hunting.** Maple Ridge's agricultural sector plays a minor role in terms of employment (LQ: 0.5), although its high production value may be a key local economic input. Most of Maple Ridge's competitors have very small concentrations in this sector, but some local competitors have decisive advantages, e.g., Chilliwack (LQ: 2.2), Langley (LQ: 1.2), and Mission (LQ: 1.1). At the same time, Maple Ridge appears more competitive than other local peers, e.g., Coquitlam (LQ: 0.2) and North Vancouver (LQ: 0.2). Overall, Maple Ridge ranks 6th among the comparator regions.
- Mining, Quarrying, and Oil & Gas Extraction.** With a low LQ of 0.4 and small employment of 259, this sector has negligible activity in Maple Ridge. Only Maple Ridge's competitors in Alberta—Airdrie (LQ: 3.1), Strathcona County (LQ: 2.9), and St. Albert (LQ: 1.7)—have high concentrations of employment attributed to Alberta's notable oil & gas activity. Maple Ridge's LQ ranks 5th among the comparator regions.
- Utilities:** Maple Ridge has a relatively small utilities sector (LQ: 0.8), as do most local peers. The only comparators with above-average employment concentrations, e.g., Pickering (LQ: 1.7), Airdrie (LQ: 1.6), Strathcona County (LQ: 1.6), and St. Albert (LQ: 1.4), are all located in Alberta and Ontario. Maple Ridge's LQ ranks 8th among the comparator regions.
- Construction.** Maple Ridge has a high employment concentration in construction (LQ: 1.6), but its comparative advantage appears small among local peers. Maple Ridge stands out with the second highest LQ (1.6) among the comparator regions. With 7,200 workers, construction is Maple Ridge's largest employment sector. Although Maple Ridge ranks highly, when compared to only local peers, e.g., Mission (LQ: 1.9), Langley (LQ: 1.6), and Chilliwack (LQ: 1.5), Maple Ridge's competitive edge diminishes. Moreover, 8 of the regions have above-national-average employment concentrations, suggesting that all these regions are experiencing robust economic activity.
- Real Estate.** Related to construction activity, Maple Ridge's real estate sector (LQ: 1.3) ranks 3rd among the comparator regions, only below North Vancouver (LQ: 1.7) and Coquitlam (LQ: 1.4).
- Manufacturing.** Maple Ridge has a smaller concentration of manufacturing employment than most peers, although none has a notably pronounced manufacturing employment hub. Only a few regions, including Bellingham (LQ: 1.2), Mission (LQ: 1.1), Chilliwack (LQ: 1.1), and Eugene (LQ: 1.0), have above-average employment concentrations. By contrast, Maple Ridge (LQ: 0.8) has below-average concentration, ranking 8th, just ahead of some local peers, e.g., Coquitlam (LQ: 0.8) and North Vancouver (LQ: 0.5).
- Wholesale Trade.** Maple Ridge has a strong advantage in wholesale trade (LQ: 1.53), but like with construction, the City's comparative advantage appears less decisive among

most local peers, including Langley (LQ: 1.54), Coquitlam (LQ: 1.3), and Mission (LQ: 1.2). Although Chilliwack is comparable with Maple Ridge in construction, it has a smaller employment concentration in wholesale trade (LQ: 0.9).

- **Retail Trade.** Maple Ridge fits among its peers in terms of retail trade (LQ: 1.1). Unlike for other sectors, peers varied very little, with LQs ranging from 0.8 (Boulder) to 1.2 (Airdrie).
- **Transportation & Warehousing.** Although not a transportation or warehousing hub, Maple Ridge (LQ: 1.09) ranks 4th among its peers and is comparable with Mission (LQ: 1.3) and Langley (1.13). Maple Ridge appears marginally more competitive than Coquitlam (LQ: 0.9), Chilliwack (LQ: 0.9), Bellingham (LQ: 0.7), and North Vancouver (LQ: 0.7). The only outlier among Maple Ridge's peers is Airdrie (LQ: 1.7), which serves as a warehousing hub servicing Metro Calgary as well as the Prairies at large, including parts of southern BC.
- **Information & Culture.** Maple Ridge has above-average employment concentration in the information & cultural sector (LQ: 1.2), but the City is the least competitive among regions with above-average employment. While North Vancouver (LQ: 2.1), Coquitlam (LQ: 1.7) have notable employment hubs, other Metro Vancouver regions have much smaller hubs—Chilliwack (LQ: 0.6), Mission (LQ: 0.8), and Langley (LQ: 1.0). Pickering (LQ: 1.6), and Boulder (LQ: 1.5) also have notable hubs in this sector.
- **Professional, Scientific, and Technical Services.** Maple Ridge has below-average employment concentration in professional, scientific, and technical services. Maple Ridge (LQ: 0.7) notably falls short of Boulder (LQ: 2.3), North Vancouver (LQ: 1.8), Coquitlam (LQ: 1.2), Pickering (LQ: 1.2). Maple Ridge ranks 11<sup>th</sup> of 13, only marginally ahead of Chilliwack (LQ: 0.6) and Mission (LQ: 0.6).
- **Finance & Insurance:** With an LQ of 0.8 and 2,000 workers, finance & insurance employment is less concentrated in Maple Ridge than in its competitors, especially Pickering (LQ: 2.1), North Vancouver (LQ: 1.3), and Coquitlam (LQ: 1.2).
- **Management:** With an LQ of 1.2 and employment of 185, management shows a moderate concentration in Maple Ridge. By contrast, Pickering (LQ: 2.3), North Vancouver (LQ: 1.6), and Coquitlam (LQ: 1.3) have greater management hubs.
- **Educational Services:** With an LQ of 1.0 and employment of 4,400, educational services have a moderate concentration in Maple Ridge. Most comparators have average concentrations, except for the three US comparators—Boulder (LQ: 2.4), Eugene (LQ: 2.0), and Bellingham (LQ: 1.7).
- **Health Care and Social Assistance:** Employing 6,600 workers and an LQ of 0.9, this is Maple Ridge's third-largest employment sector and a major contributor to the local economy. Maple Ridge fits squarely among most comparators, except for Eugene (LQ: 1.3) and Bellingham (LQ: 1.1), which are the only comparators with above-average levels of specialization.

- **Arts, Entertainment, and Recreation:** With an LQ of 1.1, this sector reflects an average concentration in Maple Ridge. Most comparators have average concentrations, except for Boulder (LQ: 1.9), Bellingham (LQ: 1.8), and North Vancouver (LQ: 1.8).
- **Accommodation and Food Services:** This sector employs 2,900 workers and has an LQ of 0.9, demonstrating below-national-average specialization and employment density. By contrast, Bellingham (LQ: 1.7), Boulder (LQ: 1.6), and Eugene (LQ: 1.5) stand out as having a greater hub. Maple Ridge fits among its local peers Chilliwack (LQ: 1.1), Coquitlam (LQ: 1.1), Langley (LQ: 1.0), and Mission (LQ: 0.8).

## Shift Share Analysis

This shift share analysis examines to what extent changes in Maple Ridge's employment from 2021 to 2024 can be attributed to overall national economic trends, industry-level trends, or unique local competitive factors. This analysis enables Maple Ridge to identify its competitive advantages and strategize for business attraction and investment.

In this analysis, "local competitive factors" broadly capture Maple Ridge employment trends' deviation from expectations based on overall national and industry-level trends. In other words, "local competitive factors" reflects local employment shifts unexplained by broader national shifts. This deviation can sometimes be attributed to a particular business losing or gaining employee, but at other times it can point to a more generalized trend having to do with the City's competitive position for a given sector.

Maple Ridge's economic landscape shows notable strengths and challenges across various sectors. Manufacturing emerges as the standout performer, with substantial job growth driven by local competitive advantages. Smaller sectors like mining, quarrying, and oil & gas extraction, and agriculture also show promising local competitive gains. Accommodation & food services and health & social assistance demonstrate strong comparative advantages, which is noteworthy because these sectors are large employers in absolute terms.

At the same time, this analysis found some possible challenges for Maple Ridge. Most notably, the construction sector exhibited local uncompetitive factors, potentially signaling a broader challenge. The information & culture sector also faced minor local competitive disadvantages, although the sector saw modest overall growth due to national and industry trends.

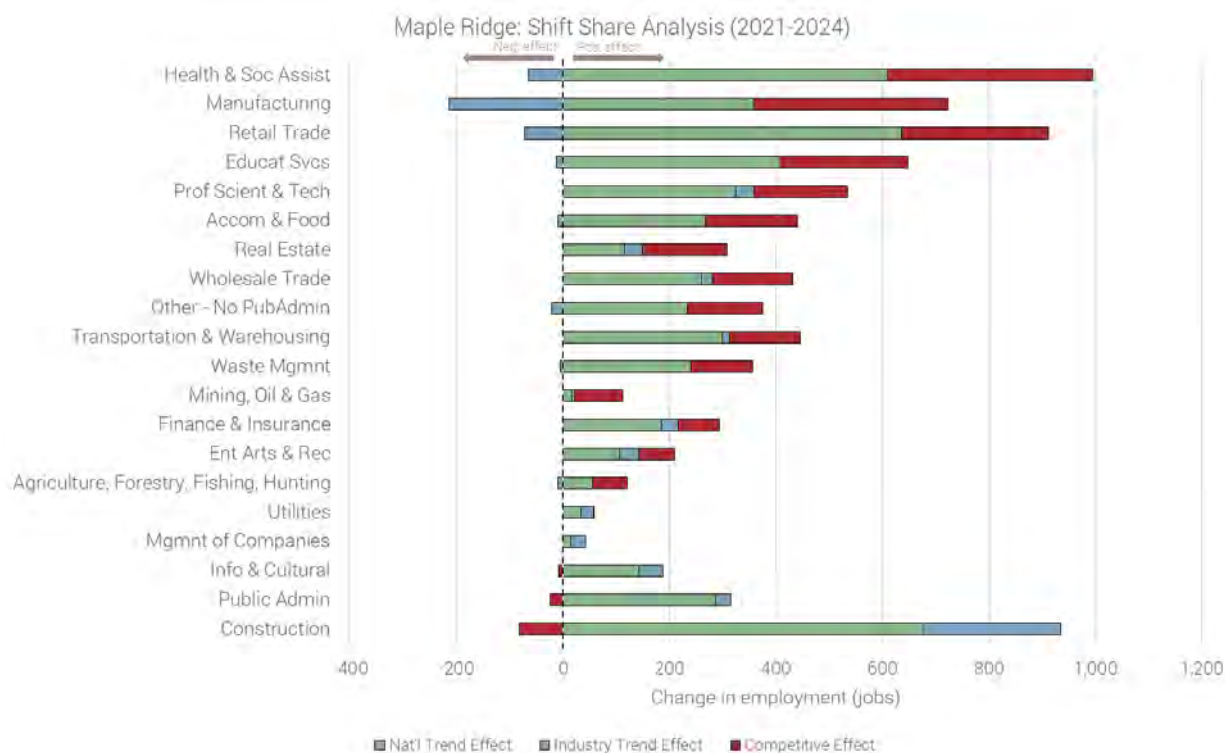
Shift share analysis findings:

- **Manufacturing** stands out as having a significant local competitive advantage despite national industry trends. Maple Ridge's large gain in manufacturing employment is largely

attributed to local factors. This suggests that Maple Ridge may have had a strong comparative advantage in this sector in attracting these new manufacturing jobs.

- **Mining, quarrying, and oil & gas extraction and agriculture, forestry, fishing, and hunting** are not large employment sectors in absolute terms, but their employment increases in the last years are largely attributed to local competitive factors.
- **Accommodation and food services** also has a strong local competitive advantage.
- **Health & social assistance** has a significant comparative advantage in Maple Ridge, which is especially noteworthy because the sector is a major employer in absolute terms.
- **Information & culture** experienced modest employment growth, but local competitive factors had a small negative effect. Note, however, that may reflect more than Maple Ridge's film industry. At the same time, some film-related jobs may not be classified as jobs in Maple Ridge's jurisdiction.
- **Construction** in Maple Ridge exhibits local uncompetitive factors, which could reflect slowing site development activity for a particular sector, the completion of major projects, or broader shifts in market conditions. Monitoring whether this trend is due to discrete causes or a symptom of longer-term issues will be critical for future economic planning. The City should return to this data point as new data becomes available.

Figure 8. Maple Ridge Shift Share Analysis (2021-2024)



Source: EBP analysis of Esri Canadian employment data (2021-2024).

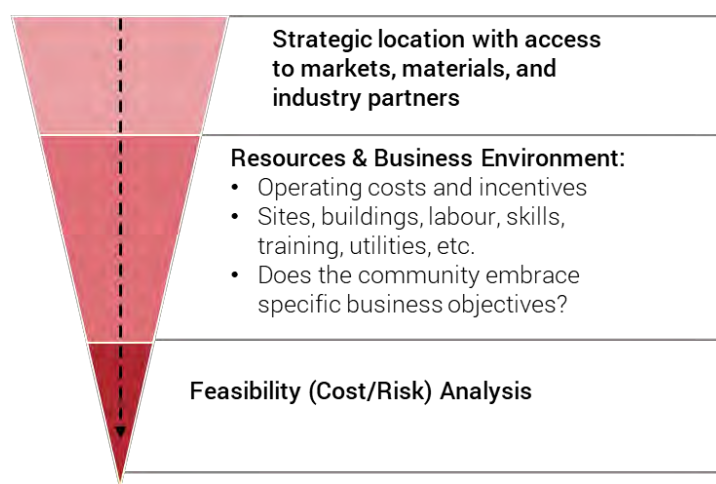


## Reverse Site Selection Analysis

The RSS analysis is a tool to understand how the City of Maple Ridge fares against its peers in a competitive context. This analysis builds on the LQ and shift share analyses, which provided some insight on the region's strengths and weaknesses. The reverse site selection model includes a weighting and ranking analysis using actual corporate location decision factors to help us determine strengths and weaknesses. Factors compared in the model include:

- Population size and population change
- Labour force size and unemployment rates
- Tax climate assessment
- Occupation and industry employment strengths
- Housing market statistics
- Access to transportation and to markets
- Educational attainment
- Access to educated workforce
- Quality of life

**Figure 9. The Corporate Site Selection Process**



## Methodology

Companies making expansion or relocation decisions typically employ a multi-phase process of selecting the location that best fits their needs. This process, the “site selection analysis,” uses a data-driven approach to weight and rank locations based on a variety of categories, including but not limited to labour markets, access to markets, infrastructure, and the tax and regulatory environment.

Investors and site selectors—the professionals who guide these decisions—typically assign weighted importance to different factors based on the industry and project type. Some of the most critical attributes include:

- **Labour Market:** Availability of skilled talent, wage levels, and workforce stability.
- **Market Access & Logistics:** Proximity to key suppliers, customers, and major transportation hubs (highways, ports, rail, and airports).
- **Business Costs & Tax Environment:** Corporate tax rates, incentives, operating costs, and real estate affordability.

- **Infrastructure & Utilities:** Availability of industrial/commercial space, energy reliability, broadband access, and water/waste management capacity.
- **Regulatory Environment & Ease of Doing Business:** Permitting speed, zoning flexibility, and overall business-friendliness.
- **Quality of Life & Talent Attraction:** Housing affordability, education, healthcare, and lifestyle factors that influence workforce attraction and retention.

Once the quantitative analysis produces a short list of locations, companies move into the field confirmation phase, conducting site visits and engaging with local stakeholders to assess qualitative factors that data alone cannot reveal. These include community support, business culture, workforce readiness, and real-world infrastructure capacity.

The reverse site selection analysis uses these same criteria to assess the competitive standing of a community from a corporate project perspective. By reversing this analysis, a community can identify its own strengths and weaknesses compared to its competitors, thereby helping it strategize how to capture opportunities by addressing threats and positioning for future growth.

This methodology was applied to evaluate the City of Maple Ridge. The City was evaluated against municipalities in Alberta, British Columbia, Ontario, and the United States. These municipalities were identified by EBP in consultation with Maple Ridge economic development staff.

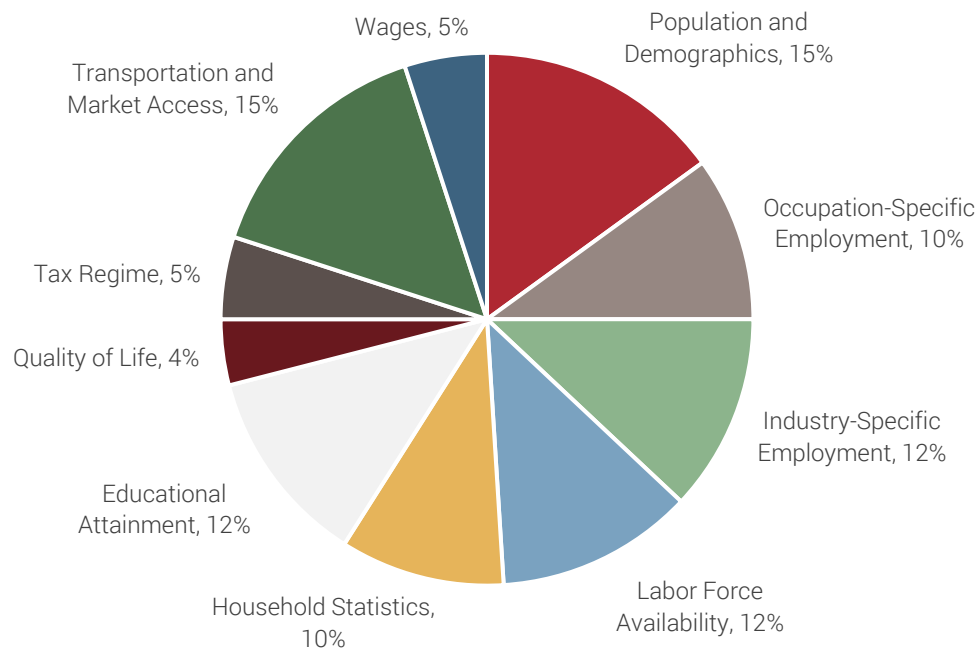
Data sources for this analysis include ESRI Business Analyst Online, municipal websites, and Google Maps. Demographic estimates vary slightly from Stats Can figures but provide reasonable benchmark figures for all locations from a single data source. For this reason, population and employment estimates may differ from those referenced elsewhere in this report.

This analysis was conducted using a standard weighting system that does not approach the benchmarking from a particular industry sector or activity's point of view but instead generalizes the importance of the various assessment categories. Using this weighting system, as shown in Figure 10, the communities were then ranked on competitiveness, with 1st being best and 13th being worst. The following figure identifies the weighting scheme for the various measures for which the City of Maple Ridge was scored relative to comparator locations.

Please note that the RSS is designed to mimic *early-stage* site selection and should not be treated as deterministic of ultimate site selection choices. The purpose is to determine whether Maple Ridge shows up on site selectors' short lists and what issues pose the greatest challenge to Maple Ridge's competitive position. That said, at this early stage, small differences among generally comparable regions can appear much larger when statistically compared with one

another. Final site selection decisions will be grounded in site visits, not simply birds-eye statistical comparisons. For that reason, it is most important that Maple Ridge be a short-list contender for a site visit.

**Figure 10. Weighting Scheme for Maple Ridge Reverse Site Selection Analysis**



Please note that this is a generalized weighting scheme to simulate a broad range of possible location drivers. The weighting has been tested to confirm the location criteria for specific industry and use types.



Table 20. EBP Reverse Site Selection Model results

Location	Overall Rank	Population and Demographic Rank	Occupation-Specific Employment Rank	Industry-Specific Employment Rank	Labour Force Availability Rank	Household Statistics Rank	Educational Attainment Rank	Quality of Life Rank	Tax Regime Rank	Transportation and Market Access Rank	Wages
Langley	1	1	5	1	1	2	9	12	1	2	2
Airdrie	2	2	2	11	7	4	2	1	3	1	2
Maple Ridge	3	5	7	2	3	2	7	10	3	6	1
Chilliwack	4	7	1	3	4	1	10	7	2	2	1
Coquitlam	5	3	5	5	3	5	8	8	1	5	2
Bellingham	6	2	6	9	3	4	4	9	2	3	4
Eugene	7	6	8	8	1	4	5	5	1	9	3
Mission	8	8	4	4	5	2	9	11	2	2	1
North Vancouver	9	6	10	6	3	6	7	6	3	8	4
Boulder	10	6	11	10	2	4	1	4	5	11	5
St. Albert	11	10	9	11	6	2	3	3	4	4	3
Pickering	12	4	10	9	6	3	6	2	5	7	3
Strathcona County	13	9	3	7	4	1	2	2	4	10	5

Table 21. Summary of Reverse Site Selection Findings for Maple Ridge

Advantage	Disadvantage
<b>Population and Demographics<sup>5</sup></b> <b>RANK: 5<sup>th</sup></b>	
<ul style="list-style-type: none"> <li>The City's annual growth rate from 2019 to 2024 (2.4%) was higher than all comparators except Langley and Airdrie.</li> <li>The City is projected to grow 1.8% annually over the next four years. Only three competitors (Bellingham, Langley, and Chilliwack) are projected to grow faster.</li> <li>Median age of the population (39) is younger than the national average (40). Similarly, 28% of the City's population is age 25-44, which is on par with most competitors, except Airdrie (33%).</li> </ul>	<ul style="list-style-type: none"> <li>The City has a smaller population base (105,000 in 2024,) than other municipalities and therefore a relatively smaller resident labour force. By contrast, cities like Coquitlam (238,000), Langley (189,000), North Vancouver (165,000), and Eugene (180,000) have much larger populations with similar proportions of working-age people.</li> <li>Some competitors have large populations with much lower median ages—Boulder (31), Bellingham (34), Eugene (37), and Airdrie (36).</li> </ul>
<b>Household Statistics<sup>6</sup></b> <b>RANK: 2<sup>nd</sup></b>	
<ul style="list-style-type: none"> <li>The City has a higher percentage of owned homes (78.5%), rather than rented dwellings in comparison to most competitors.</li> <li>New housing units are projected to grow at a faster rate (2.1%) than most competitors through 2028, which is on par with Langley, Strathcona County, and Chilliwack but notably slower than Airdrie (3.7%).</li> <li>Median household income is higher than many of its local competitors (Chilliwack, Coquitlam, Mission, Langley) but falls short of Strathcona County, Pickering, Boulder, St. Albert, and North Vancouver.</li> <li>Project growth in per capita income (3.5%) and median household income (3.5%) is comparable with local competitors but still outpaces most other regions.</li> </ul>	<ul style="list-style-type: none"> <li>Per capita income (\$46,000) is on par with local competitors—except North Vancouver (\$66,000)— but is significantly lower than most competitors.</li> </ul>

<sup>5</sup> EBP analysis of Esri Canadian and US demographic and employment data (2021, 2024).

<sup>6</sup> EBP analysis of Esri Canadian and US household and income data (2021, 2024).

Advantage	Disadvantage
<b>Labour Force Availability<sup>7</sup></b> <b>RANK: 3<sup>rd</sup></b>	
<ul style="list-style-type: none"> <li>The unemployment rate in Maple Ridge (6.3%) is notably lower than many of its competitors, including Pickering (9.6%), Airdrie (8.1%), and Coquitlam (7.7%).</li> </ul>	<ul style="list-style-type: none"> <li>Maple Ridge has a middle-of-the-pack sized labour force (54,000), but some competitors like Coquitlam (121,000) and Langley (95,000) have nearly twice the labour force.</li> </ul>
<b>Educational Attainment<sup>8</sup></b> <b>RANK: 7<sup>th</sup></b>	
<ul style="list-style-type: none"> <li>The City has a higher share of the population with an apprenticeship or trades certificate (11.0%) in comparison with several nearby competitors.</li> </ul>	<ul style="list-style-type: none"> <li>Lower concentration of workers with a bachelor-level diploma or degree (22.3%) compared with some other competitors like Boulder (37.4%), Airdrie (25.5%), and Strathcona County (24.8%).</li> <li>Maple Ridge similarly has a lower share of the population with a non-university certificate or diploma or university education below the bachelor level (26.3%), falling short of Boulder (39.1%), Bellingham (35.0%), Eugene (32.1%), and Airdrie (29.4%).</li> <li>Maple Ridge has the third highest share of residents without any certificate, diploma or degree (6.9%), which is still below Mission (10.9%) and Chilliwack (9.8%).</li> </ul>
<b>Industry-Specific Employment<sup>9</sup></b> <b>RANK: 2<sup>nd</sup></b>	
<ul style="list-style-type: none"> <li>The City has a significantly large portion of the workforce in construction (12.3%), slightly ahead of all competitors except Mission (14.4%).</li> <li>The City is middle-of-the-pack in terms of manufacturing employment (6.6%) but still behind some local competitors like Mission (9.1%), Bellingham (8.9%), and Chilliwack (8.4%).</li> </ul>	<ul style="list-style-type: none"> <li>The City has a higher concentration of employment in retail and wholesale trade than most competitors. These industries tend to be less desirable in terms of attracting investment than other industries.</li> <li>The City has a notably lower share of employment in professional, scientific, and technical services.</li> </ul>
<b>Occupation-Specific Employment<sup>10</sup></b> <b>RANK: 7<sup>th</sup></b>	

<sup>7</sup> EBP analysis of Esri Canadian and US workforce data (2024).

<sup>8</sup> EBP analysis of Esri Canadian and US education data (2024).

<sup>9</sup> EBP analysis of Esri Canadian and US employment (industry) data (2024).

<sup>10</sup> EBP analysis of Esri Canadian and US employment (occupation) data (2024).

<b>Advantage</b> <ul style="list-style-type: none"> <li>The City has higher shares of employment tied to occupations in business and finance, and trades and transportation/ equipment operators.</li> </ul>	<b>Disadvantage</b> <ul style="list-style-type: none"> <li>Less occupational workforce than various regions in occupations associated with management, primary industries, and manufacturing.</li> </ul>
<b>Transportation and Market Access</b> <b>RANK: 6<sup>th</sup></b>	
<ul style="list-style-type: none"> <li>The City ranked 5<sup>th</sup> among competitor municipalities for the size of its available market (3.2m people within a 1-hour drive) and 4<sup>th</sup> in terms of households.<sup>11</sup></li> <li>Maple Ridge's projected household growth (1.9%) is on par with most competitors except St. Albert (2.6%) and Strathcona County (2.5%).<sup>12</sup></li> <li>Proximity to a regional airport (Abbotsford).</li> <li>Proximity to Canada's largest seaport with access to Asian markets.</li> </ul>	<ul style="list-style-type: none"> <li>While the City has access to CPKC rail infrastructure, Maple Ridge has a longer drive time to a Class 1 Rail yard or facility than many other competitors that have centrally located facilities. That said, this model uses an "average" geographic approach from the labelled centre of a comparator to the facility.<sup>13</sup></li> <li>A longer drive time to a major airport (~45 minutes).<sup>14</sup></li> </ul>

<sup>11</sup> Esri Canadian and US household data (2024).

<sup>12</sup> Ibid.

<sup>13</sup> Google Maps data (mid-day traffic estimates)

<sup>14</sup> Google Maps data (mid-day traffic estimates)

Advantage	Disadvantage
Quality of Life RANK: 10 <sup>th</sup>	
<ul style="list-style-type: none"> <li>Maple Ridge's average annual temperature is on par with comparators in British Columbia but notably warmer than those in Alberta.<sup>15</sup></li> <li>Access to popular provincial park and other natural features, including the Fraser River, Pitt River, and Stave River.</li> <li>Various events, festivals, and community celebrations.</li> </ul>	<ul style="list-style-type: none"> <li>Maple Ridge experiences much more annual precipitation, more than all comparators.<sup>16</sup></li> <li>Although the median list price for a detached single-family home (\$1.24m) is still more affordable than North Vancouver (\$1.85m), Coquitlam (\$1.67m) and Langley (\$1.41m), it is still more expensive than Mission (\$1.18m) and Chilliwack (\$0.89m), and Airdrie (\$0.64m).<sup>17</sup></li> <li>As of 2020, Maple Ridge had a higher property and violent crime rates but still notably lower than Langley and Chilliwack.<sup>18</sup> Note that US and Canadian jurisdictions record and report crime rates in slightly different ways, which makes comparison difficult.</li> <li>Maple Ridge ranks low in life expectancy (81.1 years) where comparators rank between 78.5 to 84.5.<sup>19</sup> This range could relate to demographic makeup, occupation, healthcare access, lifestyles, diets, etc.</li> <li>Homelessness counts have risen throughout BC (+32% from 2020 to 2023), according to Homelessness Services Association of BC. The percent increase in Ridge Meadows (Maple Ridge and Pitt Meadows) was less pronounced (+17%) but still has still seen large increases over the last decades. About 44 of 135 Ridge Meadow's homeless are unsheltered, i.e., sleeping outside or in makeshift tents, vacant buildings, or vehicles. The Provincial Integrated Data Project (IDP) offers a higher estimate of homeless individuals (392).<sup>20</sup></li> </ul>

<sup>15</sup> Climate Data: <https://en.climate-data.org/>

<sup>16</sup> Ibid.

<sup>17</sup> Houseful.ca data (accessed February 2025)

<sup>18</sup> Statistics Canada (2020) via [www.areavibes.com](http://www.areavibes.com)

<sup>19</sup> BC Centre for Disease Control, "[BC Community Health Data](#)" (2011-2015)

<sup>20</sup> Homelessness Services Association of BC, "[2023 Homeless Count in Greater Vancouver](#)" (Oct 2023). Homeless counts not used in ranking calculation. See also Maple Ridge's Housing Needs Report (2024). Homelessness is not used in the statistical model.

Advantage	Disadvantage
<b>Wages<sup>21</sup></b> <b>RANK: 1<sup>st</sup></b>	
<ul style="list-style-type: none"> <li>Wages are significantly lower than in North Vancouver, Pickering, Strathcona County, St. Albert, Eugene, Bellingham, and Boulder.</li> </ul>	<ul style="list-style-type: none"> <li>Wages are on par with local competitors, albeit marginally higher than Chilliwack and Mission.</li> </ul>
<b>Tax Regime<sup>22</sup></b> <b>RANK: 3<sup>rd</sup></b>	
<ul style="list-style-type: none"> <li>The City's commercial property tax rates are much lower than in Boulder, Airdrie, Pickering, St. Albert.</li> </ul>	<ul style="list-style-type: none"> <li>The City's tax rates are slightly higher than in local comparators including Chilliwack, Coquitlam, Langley, Mission, North Vancouver.</li> </ul>

The reverse site selection analysis indicates that Maple Ridge maintains a competitive position among its peer municipalities but is more closely comparable with its local competitors in Metro Vancouver. While Maple Ridge has a relatively smaller population base compared to competitors like Coquitlam and Langley, the City has a strong projected annual growth rate outpacing most competitors. This growth trend is expected to continue, albeit at a more moderate rate.

While Maple Ridge's workforce size is modest compared to larger competitors, the City boasts a lower unemployment rate, outperforming cities like Pickering and Airdrie.

Maple Ridge demonstrates strength in home ownership, higher than most competitors. The housing market is expected to grow at a healthy rate through 2028, matching the pace of cities like Langley and Strathcona County. While median household income is competitive within its local market, it lags behind some competitors like Strathcona County and North Vancouver. However, projected growth in both per capita and median household income is encouraging and outpaces most regions.

In terms of workforce capabilities, Maple Ridge shows notable strengths in specific sectors. The City has a particularly strong construction sector, second only to Mission among competitors. The City also maintains a competitive position in manufacturing employment, though not leading in this category. However, Maple Ridge faces challenges in professional, scientific, and technical services employment, with lower concentrations than desired for attracting investment in these sectors.

<sup>21</sup> Esri Canadian and US income data (2024).

<sup>22</sup> 2024 non-residential municipal tax rates. Does not account for other taxes at the municipal, provincial, or federal level or other permitting/regulatory costs. Also note that tax rates often has a relationship with infrastructure quality, in that lower taxes may be enticing to site selectors but could also raise infrastructure quality questions.

The educational attainment profile presents both opportunities and challenges. While the City shows strength in trades and apprenticeships, it lags in bachelor's degree attainment compared to some competitors like Boulder and Airdrie. This suggests potential opportunities for workforce development initiatives.

From a location perspective, Maple Ridge benefits from access to a substantial market within an hour drive. However, the City faces some infrastructural challenges, including slightly longer average drive times to major airports and Class 1 rail facilities compared to competitors. These factors may impact its attractiveness for certain industries requiring immediate access to these transportation networks.

The City's tax regime is competitive, with commercial property tax rates lower than many competitors like Boulder, Airdrie, and Pickering, though slightly higher than some local peers. This, combined with relatively lower wages compared to North Vancouver and other major competitors, could provide cost advantages for businesses considering location decisions.

Quality of life indicators present a mixed picture, with more affordable housing than some nearby competitors like North Vancouver and Coquitlam, though still notably higher than cities like Airdrie. While the City faces some challenges with crime rates and life expectancy similar to other neighbouring municipalities, it benefits from milder average temperatures compared to its Alberta competitors, though with higher precipitation levels.

## 5. Physical Asset Inventory

A community's potential for economic development depends upon its supportive infrastructure, including human capital, physical assets, and public policy assets. Physical assets are crucial to furthering economic development objectives because it provides the foundation for businesses, industries, and communities to function efficiently.

Key economic functions that physical assets serve include:

- Enhance productivity by providing reliable transportation, energy, and communication networks
- Make industrial lands more desirable by preparing them for swift development and connecting to vital utilities
- Enable domestic and international commerce via ports, air cargo operations, roads, railways, and digital connectivity
- Create jobs during construction and long-term through the industries they support
- Support quality of life for workforce health and productivity
- Improve economic resilience and environmental sustainability

The purpose of this analysis is to examine Maple Ridge's physical assets and infrastructure from a site selector's perspective to understand how they serve businesses. This analysis should inform City strategies for upgrading existing infrastructure or investing in new physical assets.

This inventory synthesizes information and analyses from the following sources:

- City of Maple Ridge Site Selector Tool, accessed March 2025
- Maple Ridge Industrial Impact Analysis, 2024
- Maple Ridge Strategic Transportation Plan, 2023
- City of Maple Ridge Zoning Bylaw No. 7600-2019, 2024
- City of Maple Ridge Sanitary Master Plan, 2016
- City of Maple Ridge Official Community Plan, 2014 (last amended 2020)
- Field notes from EBP site visit to Maple Ridge, January 2025
- Plans and publications from provincial and private utilities, transportation, and intermodal organizations, various years



## Regional Assets and Market Connections

Maple Ridge benefits from a well-connected transportation network that facilitates the flow of goods and services from multiple directions, enhancing opportunities for economic development and access to key domestic and international markets.

Some of these key market connections include:

- Western: Metro Vancouver and Pacific markets via Port of Vancouver, Highway 7 (Lougheed Highway) and Highway 1 (Trans-Canada Highway), Vancouver international airport, and Fraser River
- Eastern: Alberta and Prairie provinces, Fraser Valley via Highway 1, CP and CN Rail
- Southern: US and Mexican markets via US border crossings at Peace Arch, Pacific Highway, and Aldergrove; Provincial markets south of Maple Ridge via Fraser River
- International via Pacific Gateway: 50+ countries with which Canada has free trade agreements, including key Asia-Pacific markets like China, Japan, and South Korea

Several businesses located in Maple Ridge also rely on the Pitt Meadows Regional Airport (YPK) as a local air cargo connection to rural markets throughout Canada.

In addition to key business advantages that Maple Ridge's location provides, the City, greater Vancouver metro region, and Southern British Columbia offer a wealth of recreational opportunities and is surrounded by several provincial parks that attract tourists to the area. These natural assets provide a variety of activities, from hiking and camping to boating and fishing. They include:

- Golden Ears Provincial Park
- Popular hiking and skiing destinations within a 2-hour drive of Maple Ridge, including Squamish and Whistler
- Fishing and boating on Alouette Lake, Mike Lake, and Whonnock Lake
- Local parks and trails including Maple Ridge Park, Kanaka Creek Regional Park, Alouette River Dykes, UBC Malcolm Knapp Research Forest, Silver Valley Trails, Mike Lake Trails, and Jackson Farm Loop

## Industrial Sites

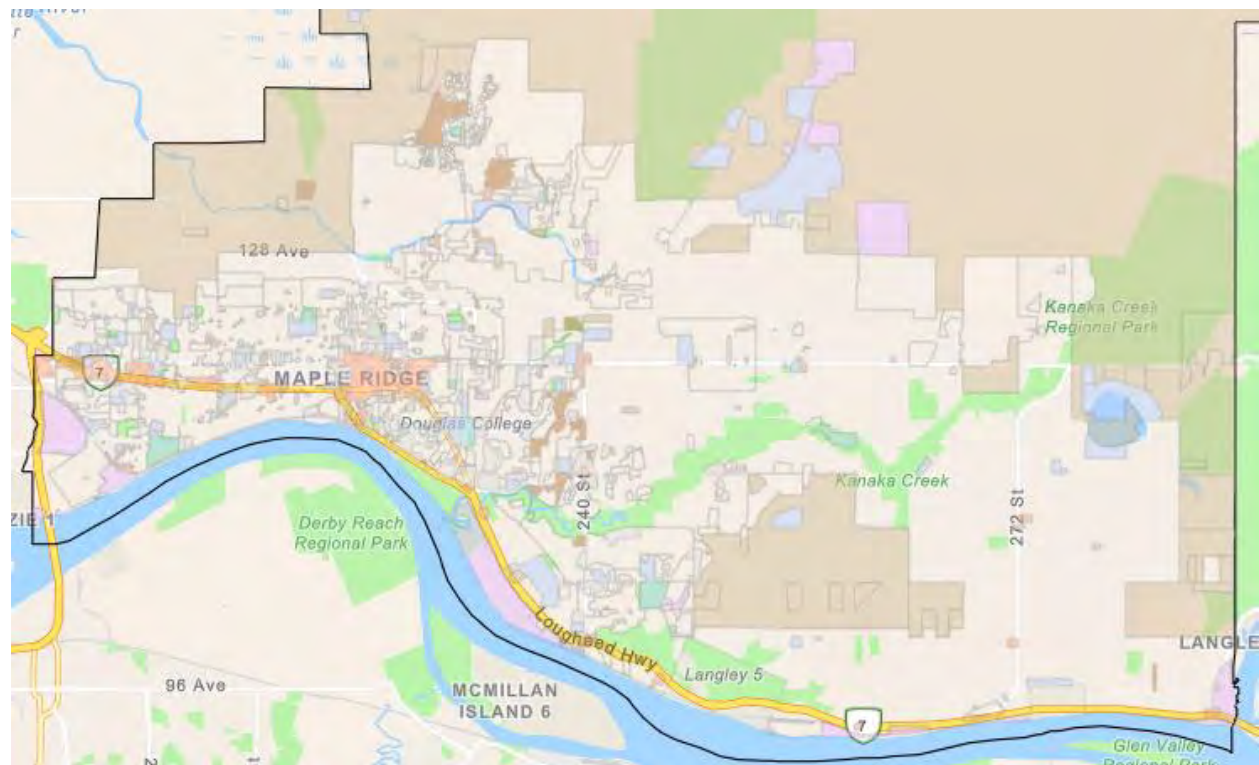
Maple Ridge contains over 730 hectares of industrial land, which is less than nearby Surrey, Richmond, Delta, Burnaby, and Langley Township. This land accounts for six percent of the Metro Vancouver Region's total industrial lands, including those yet to be developed.<sup>23</sup>

Most of the industrial land in Maple Ridge's industrial areas and parks is zoned as either M-2 General Industrial or M-3 Business Park Industrial. According to the City's Zoning Bylaw, the principal uses listed in Table 22 are permitted in these zones. In addition to these principal uses, the Zoning Bylaw also allows for some accessory uses of office, retail, refueling facilities, and caretaker residential in both M-2 and M-3 zoned areas.

The map below depicts the City's key industrial areas, shown in pink, as seen in the City's online Site Selector Tool. The following figure specifies permitted land uses in industrially zoned areas, as specified in the City of Maple Ridge's Zoning Bylaw.

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<sup>23</sup> Please note that the City of Maple Ridge currently has multiple initiatives underway to explore the expansion of industrial lands in multiple of its Industrial Areas. This Physical Asset Inventory reflects information that is publicly available as of the writing of this report, so as to mimic the lens of a site selector or potential investor looking at opportunities in Maple Ridge.

**Figure 11. Maple Ridge Industrial Areas**

Source: Maple Ridge Site Selector Tool, accessed March 28, 2025. Note that industrial areas are depicted in light pink. This map is consistent with the Zoning Bylaw Map Schedule 'A' found in the City of Maple Ridge's Zoning Bylaw No. 7600-2019, September 24, 2024.

**Table 22. Principal Land Uses Permitted in M-2 and M-3 Industrial Zoning**

M-2 General Industrial	M-3 Business Park Industrial
<ul style="list-style-type: none"> <li>• Cement plant</li> <li>• Indoor commercial recreation*</li> <li>• Industrial Repair Services;</li> <li>• Industrial Trade Schools;</li> <li>• Media Production Studio;</li> <li>• Off-Street Parking*</li> <li>• Recycling Plant;</li> <li>• Refueling and washing of Commercial, Industrial and fleet Vehicles;</li> <li>• Restaurant</li> <li>• Retail sale and rental of Industrial Vehicles, Trailers, and heavy equipment;</li> <li>• Towing yard;</li> <li>• Unenclosed Storage;</li> <li>• Vehicle and Equipment Repair Services;</li> <li>• Waste transfer station;</li> <li>• Wrecking and salvaging of goods; and</li> <li>• Industrial, excluding the following business types: <ul style="list-style-type: none"> <li>○ asphalt, rubber, and tar production and product manufacturing;</li> <li>○ chemical plants;</li> <li>○ hydrocarbon refining and bulk storage*</li> <li>○ Extraction Industrial;</li> <li>○ sand and gravel plants;</li> <li>○ septic tank services;</li> <li>○ stockyards and abattoirs;</li> <li>○ waste reduction plant; and</li> <li>○ wrecking, salvaging and storing of Wrecked Vehicles or salvaged Vehicles</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>• Auction mart;</li> <li>• Commercial laundry;</li> <li>• Commercial upholstery, including equipment and Vehicle covers;</li> <li>• Customer service call centre and marketing call centre;</li> <li>• Distillery and brewery;</li> <li>• Equipment appraisal;</li> <li>• Indoor Commercial Recreation;</li> <li>• Laboratory;</li> <li>• Light Industrial;</li> <li>• Liquor Primary Establishment*</li> <li>• Media Production Studio;</li> <li>• Office related to construction, Industrial, high technology, utility companies, and Government;</li> <li>• Off-Street Parking of unoccupied passenger, Commercial Vehicles and Recreational Vehicles and Equipment;</li> <li>• Preparation of food and food products for off-site consumption, including catering;</li> <li>• Printing, publishing, and reproduction services;</li> <li>• Recreational Facilities and instructional facilities limited to: Industrial Trade Schools; dance Schools; arts and culture Schools; fitness centres; and gymnastic Schools;</li> <li>• Repair and servicing of light non-Industrial machinery and non-Industrial equipment;</li> <li>• Restaurant, excluding Drive-Through;</li> <li>• Retail sale, repair and rental of Industrial Vehicles, heavy equipment, and trailers</li> <li>• Vehicle and Equipment Repair Services;</li> <li>• Warehouse Storage; and</li> <li>• Wholesale Distribution</li> <li>• Industrial, excluding the following business types: <ul style="list-style-type: none"> <li>○ asphalt, rubber, and tar production and product manufacturing;</li> <li>○ chemical plants;</li> </ul> </li> </ul>

M-2 General Industrial	M-3 Business Park Industrial
	<ul style="list-style-type: none"> <li>o concrete and cement plants, and manufacturing concrete products;</li> <li>o hydrocarbon refining and bulk storage;</li> <li>o sawmills, shake mills and pulp mills;</li> <li>o septic tank services;</li> <li>o stockyards and abattoirs;</li> <li>o Unenclosed Storage;</li> <li>o waste reduction plant and waste transfer station; and</li> <li>o wrecking, salvaging and storing of wrecked or salvaged goods, materials and things, or Wrecked Vehicles</li> </ul>

Source: City of Maple Ridge, Zoning Bylaw No. 7600-2019, July 30, 2024. Please note that an update including some additional uses on M-3 lands was made to the Zoning Bylaw in September 2024.

\* Applicable to only some specific lots, or some exceptions apply.

Further explored in the following sections for each industrial area, common constraints to industrial development in Maple Ridge include the following:

- Adequate servicing of water and sanitary services
- Transportation access points and proximity to transportation corridors, as well as the perception of access
- Some environmental constraints including steep topography and flood plains
- Land use types and zoning potentially restrict activities
- Approximately two-thirds of the City's vacant industrial land is currently designated as Rural Resource in the Official Community Plan

## North 256 Industrial Area

The **North 256 Industrial Area** is currently home to approximately 71 hectares of industrially zoned lands (M-2 General Industrial and M-4 Extraction Industrial) with the possibility to expand pending North 256 Area Plan work.<sup>24</sup> Current developed industrial lands in this area are split into two groups: **Kanaka Business Park** and **Websters Corner Business Park**.

As shown in Figure 12, the Maple Ridge Site Selector tool as of March 2025 depicts an estimated 30 vacant parcels in the Kanaka Business Park and seven vacant parcels in the Websters Corner Business Park that are zoned as M-2 General Industrial. In addition to many parcels in Kanaka that are currently leased or for sale, many are outdoor storage sites and are not actively used for industrial business activity.

### Current Business Activity in North 256

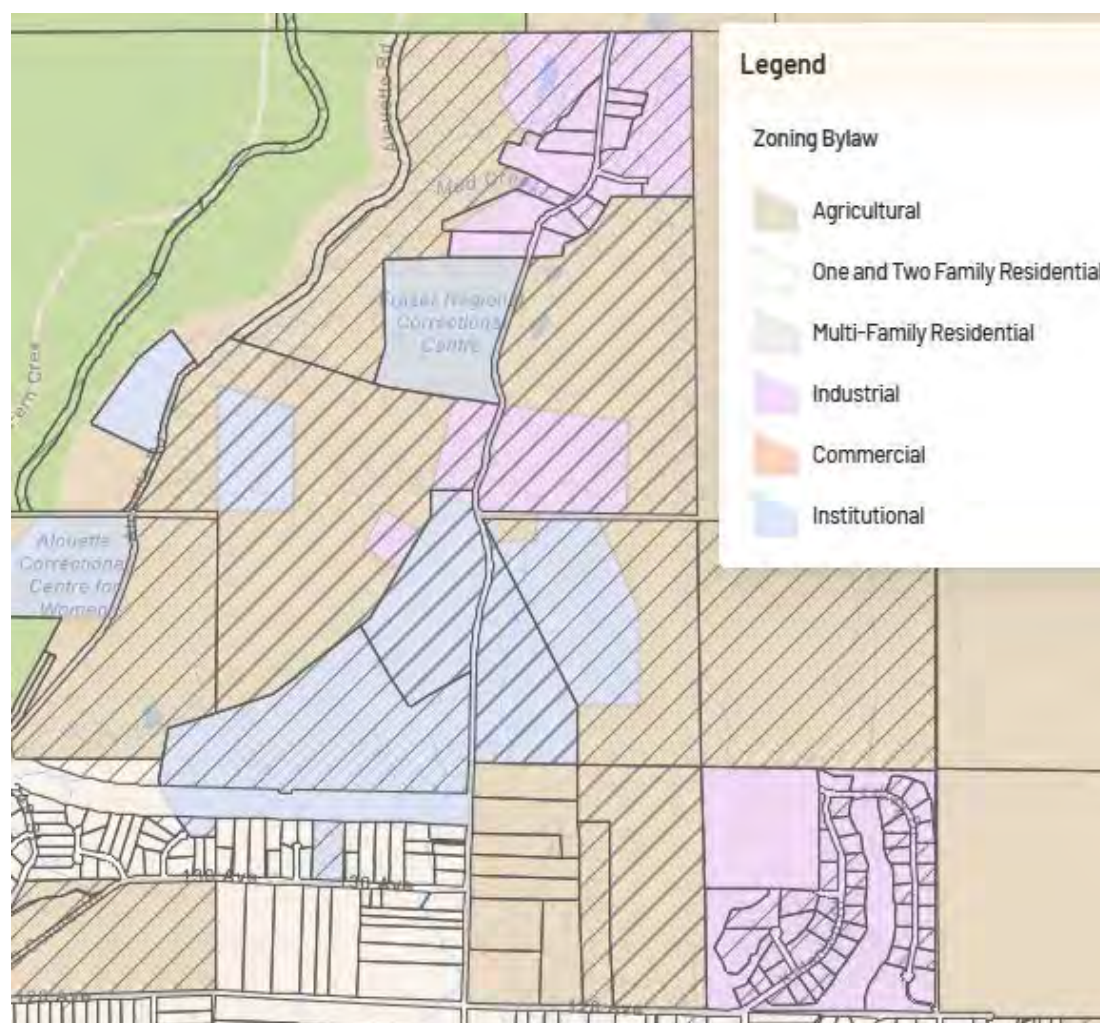
- Light industrial manufacturing and automotive and heavy equipment repair
- Outdoor storage of forest products
- Vehicle salvage
- Aggregate extraction and sales
- Heavy equipment sales and repairs

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<sup>24</sup> The City of Maple Ridge approved the North 256 Street Industrial Lands Area Plan Scope of Work in February 2025. This work will seek to facilitate the full industrial development potential of North 256 lands through infrastructure investments, development projects, construction, business occupancy, and creating local jobs.



Figure 12. Zoning and Parcels in North 256 Industrial Area



Source: Maple Ridge Site Selector Tool, accessed March 6, 2025. Note that vacant parcels are depicted with diagonal lines.

Table 23. North 256 Industrial Area – Summary of Advantages and Disadvantages

Advantages	Disadvantages
<ul style="list-style-type: none"> <li>The availability of possible additional industrial lands makes North 256 Industrial area a high potential area for investment attraction.</li> <li>Several transportation improvement projects are underway to improve access to this industrial area. This includes the Abernethy Way expansion project.</li> </ul>	<ul style="list-style-type: none"> <li>While vacant, underutilized lands present opportunities for development, a significant portion of these lands are currently zoned as agricultural and do not permit industrial uses at present.</li> <li>Topographic features like slopes and creeks are main <u>geographic constraints</u></li> <li><u>Needed upgrades to water and sanitary networks</u> for greater capacity to support more industrial uses and build forms/ density. <ul style="list-style-type: none"> <li>While water and sanitary connections are available nearby, there may not be enough</li> </ul> </li> </ul>



Advantages	Disadvantages
	<p>capacity to provide fire flows for sustained water flow, and there are servicing limitations to supporting this.</p> <ul style="list-style-type: none"> <li>○ Not currently enough water service to support growth in this area</li> </ul> <ul style="list-style-type: none"> <li>• <u>Transportation and Road Access</u> <ul style="list-style-type: none"> <li>○ Constrained by configuration of 256<sup>th</sup> street and Dewdney Trunk Road – lack of regional connections. This is basically the only road in and out, however, some improvement projects are seeking to improve this.</li> <li>○ Road access to North 256 Industrial Area does not support heavy truck travel well due to the nature of the roadways as being rural with varying grades.</li> <li>○ 256 and Dewdney Trunk Road intersection does not accommodate semi-trailer trucks well. Similarly, steep grading makes it difficult for semi-trailer trucks to approach this intersection from the north.</li> </ul> </li> </ul>

## Albion Industrial Area

The **Albion Industrial Area** comprises of over 80 hectares of industrial land (zoned as M-2 General Industrial and M-3 Business Park Industrial). Located along Highway 7 and west of 240<sup>th</sup> Street, this area has direct access to CP railway and to the Fraser River.

Figure 13 depicts current zoning and vacant parcels in the Albion Industrial Area as of March 2025, as depicted in the Maple Ridge Site Selector Tool. According to the Site Selector Tool, there were approximately 16 vacant parcels of varying sizes available within

### Current Business Activity in Albion Industrial

- Lumber product manufacturers,
- cement and aggregates businesses,
- the City's waste and recycling centre,
- automotive wrecking and recycling businesses,
- various other fabrication and industrial businesses.

the industrial area as of March 2025, all of which are zoned M-2 General Industrial.

Figure 13. Zoning and Parcels in Albion Industrial Area



Source: Maple Ridge Site Selector Tool, accessed March 6, 2025. Note that vacant parcels are depicted with diagonal lines.

**Table 24. Albion Industrial Area – Summary of Advantages and Disadvantages**

Advantages	Disadvantages
<ul style="list-style-type: none"> <li>Well-connected to Highway 7, CPKC railway, and accessible via the 748 and 746 bus routes.</li> <li>Access to water and sanitation is better than some other industrial areas in the City,</li> <li>Electrical and gas infrastructure are currently in place to serve Albion Industrial Area</li> <li>River access for barging, marine operations, and short sea shipping</li> </ul>	<ul style="list-style-type: none"> <li><u>Limited availability of storm water infrastructure and sanitary sewer connections</u> limits industrial applications to low-density, open-air storage and distribution uses. Sanitary upgrades are currently underway, and are expected to be complete by 2026.</li> <li><u>Transportation needs</u> in areas surrounding the industrial area: <ul style="list-style-type: none"> <li>The intersection that connects Highway 7 to the western end of River Road, one of few access roads in and out of the industrial area, is a three-way intersection with no traffic signal.</li> <li>Congestion due to railway crossings coupled with limited space for trucks and other vehicles, which contributes to perceptions of inconvenient goods movement through the area.</li> <li>Access to the area is perceived to be poor, but in reality it is well-served by highway and rail</li> </ul> </li> <li><u>Significant flood risk.</u> There is a lack of flood protection infrastructure, which would likely be improved by dike extension</li> <li>Possible <u>contamination</u> at some industrial sites due to heavy industry</li> <li>Poor soil conditions and likely geotechnical challenges</li> </ul>

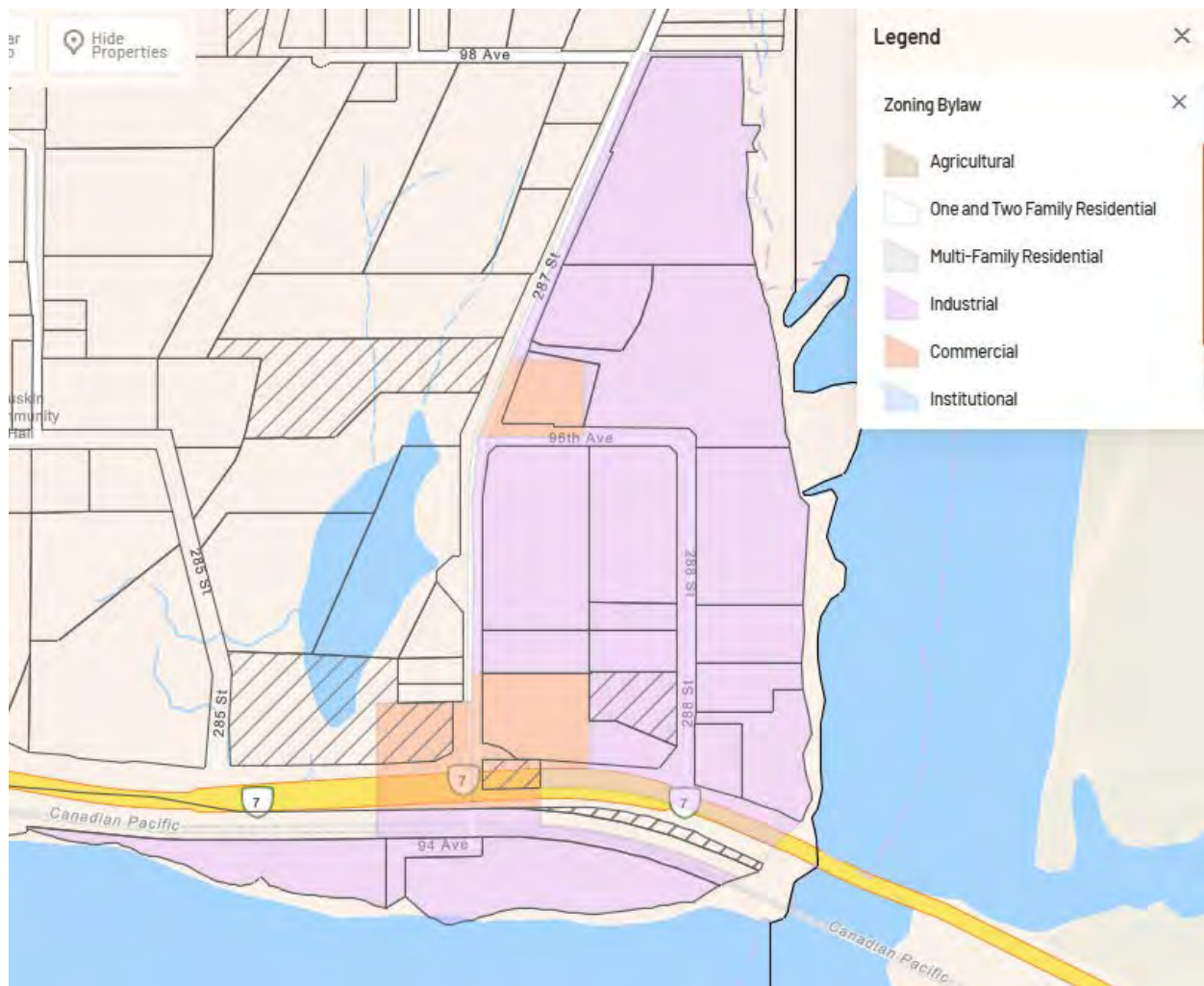


According to the Maple Ridge Site Selector Tool, the Fraser River Industrial Corridor and Ruskin have limited available, vacant, industrial zoned parcels as of February 2025.

- Aggregates extraction and sales
- Lumber mills
- Lumber yards
- Wood product manufacturing
- Excavation and contracting businesses
- Steel sales and fabrication
- Welding
- Concrete sales

62

Figure 15. Zoning and Parcels in Ruskin



Source: Maple Ridge Site Selector Tool, accessed February 20, 2025. Note that vacant parcels are depicted with diagonal lines.

**Table 25. Fraser River Industrial Corridor – Summary of Advantages and Disadvantages**

Advantages	Disadvantages
<ul style="list-style-type: none"> <li>• Highway 7 improvements between 266<sup>th</sup> Street and 287<sup>th</sup> Street to ease congestion and improve capacity and safety</li> <li>• Fraser River and potential for short sea shipping, especially with implementation of Port of Vancouver Inland Hub Facility concept. While this Fraser River Corridor is outside of Port of Vancouver jurisdictional area, the inland hub concept will have impacts in the Fraser River Area.</li> <li>• Highway 7 and CP Rail connection to eastern and southern markets, and areas to the east with potential for future redevelopment, and potential forestry sector activity</li> </ul>	<ul style="list-style-type: none"> <li>• <u>Absence of water and sanitary servicing</u> – the area is located outside of the Metro Vancouver Urban Containment Boundary and the Greater Vancouver Sanitary Sewage District. The CPKC rail line and Highway 7 constrain development of water or sanitary lines.</li> <li>• <u>Transportation constraints</u>: <ul style="list-style-type: none"> <li>◦ Some intersections between Highway 7 and access roads to the industrial corridor lack traffic signals</li> </ul> </li> <li>• Some <u>contaminated parcels</u> that limit development or require remediation</li> </ul>

## Maple Meadows Business Park

Located in the Hammond neighbourhood, the Maple Meadows Business Park primarily consists of M-3 Business Park Industrial zoned parcels. As shown in Figure 16, there are currently limited available, vacant parcels in this business park. There are approximately 40 acres of vacant land south of the Maple Meadows Business Park that are currently under development permit and review for rezoning for the Kingston Business Park development project.<sup>25</sup>

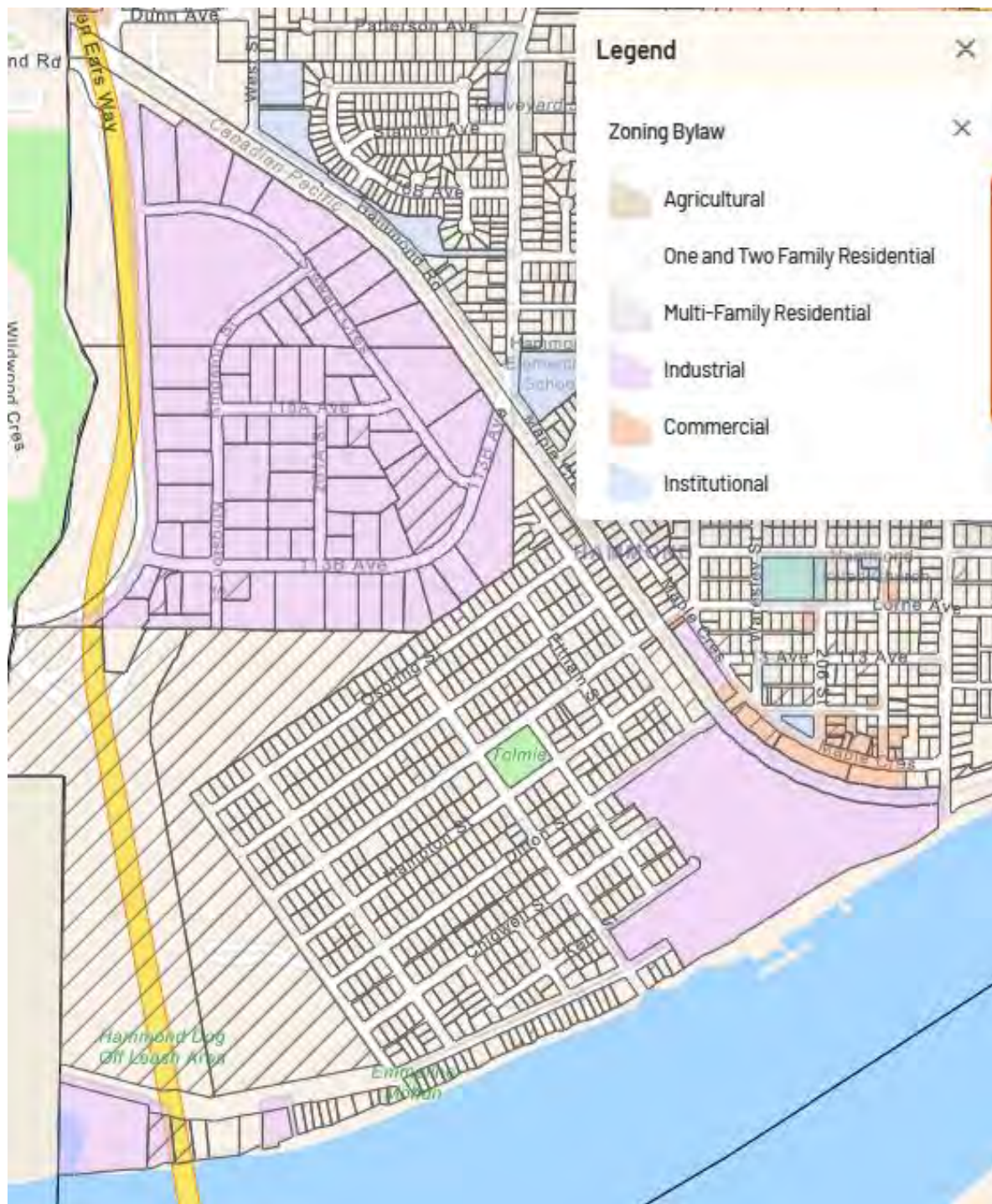
### Current Business Activity in Maple Meadows Business Park

- Self-Storage Facilities serving businesses and residents
- Food Manufacturing
- Special Effects
- Ophthalmic Goods Manufacturing
- Watersports Equipment Manufacturing

<sup>25</sup> <https://cedarcoast.com/projects/kingston-business-park/>



Figure 16. Zoning and Parcels in Maple Meadows Business Park and Hammond



Source: Maple Ridge Site Selector Tool, accessed March 6, 2025. Note that vacant parcels are depicted with diagonal lines.



Table 26. Maple Meadows Business Park – Summary of Advantages and Disadvantages

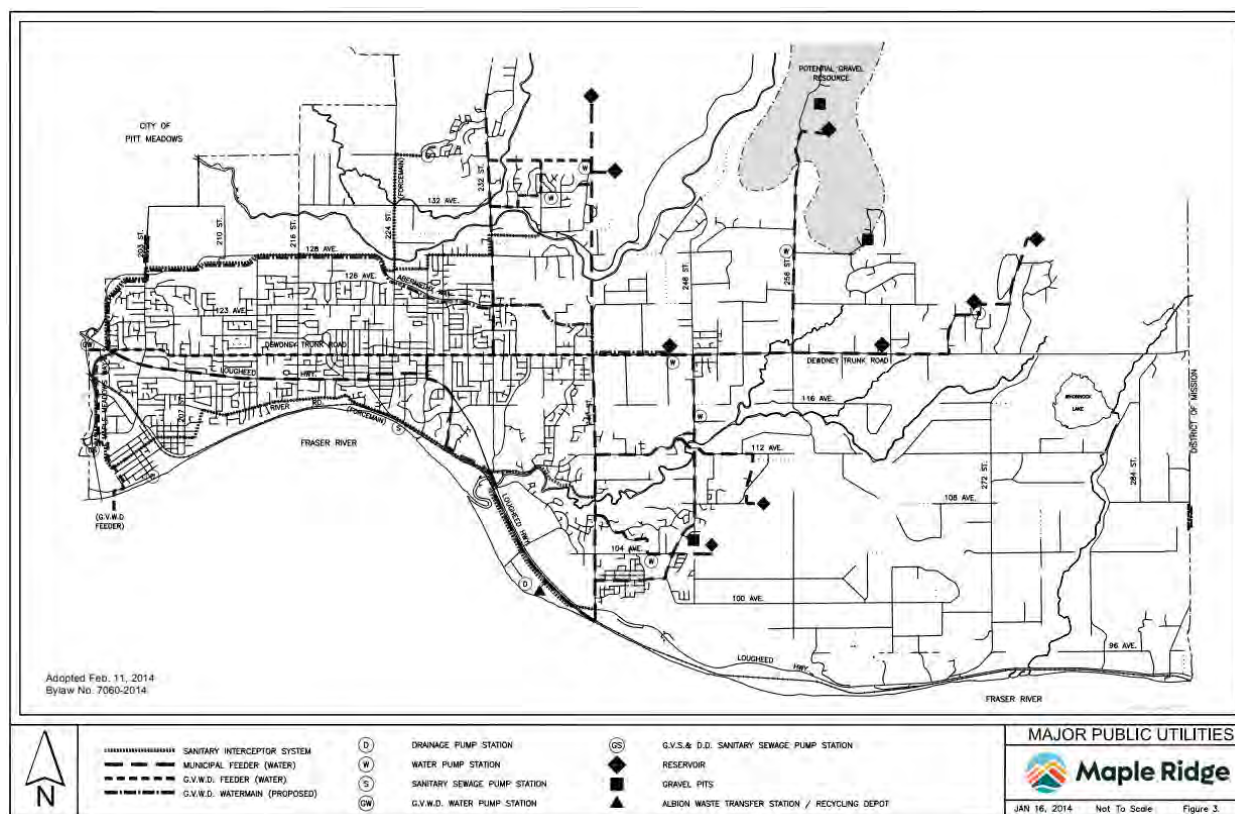
Advantages	Disadvantages
<ul style="list-style-type: none"> <li>• Close access to transportation infrastructure including Highway 7 and Golden Ears Bridge, in addition to CPKC Rail, port, and transload infrastructure.</li> <li>• Well serviced by utilities, including water, sanitary, and electricity.</li> </ul>	<ul style="list-style-type: none"> <li>• Limited growth or expansion opportunities available.</li> <li>• Parking constraints</li> </ul>

## Utilities Infrastructure

### Water, Stormwater, and Sanitary

Maple Ridge is a part of the **Greater Vancouver Water District** and the Greater Vancouver Sewerage and Drainage District, operated by Metro Vancouver. Metro Vancouver supplies water to Maple Ridge from Coquitlam Lake. The City also provides well water under permit by Fraser Health Authority. The figure below shows major public utilities in the City, including Greater Vancouver Water District feeders and watermain, in addition to municipal feeders. Water utility infrastructure reaches the North 256 Industrial Area, Albion Industrial Area, and Maple Meadows Business Park. The Fraser River Industrial Corridor is not well serviced by the Greater Vancouver Water District and relies primarily on well water and pumps.

Figure 17. Greater Vancouver Water District Service Lines, as of 2014



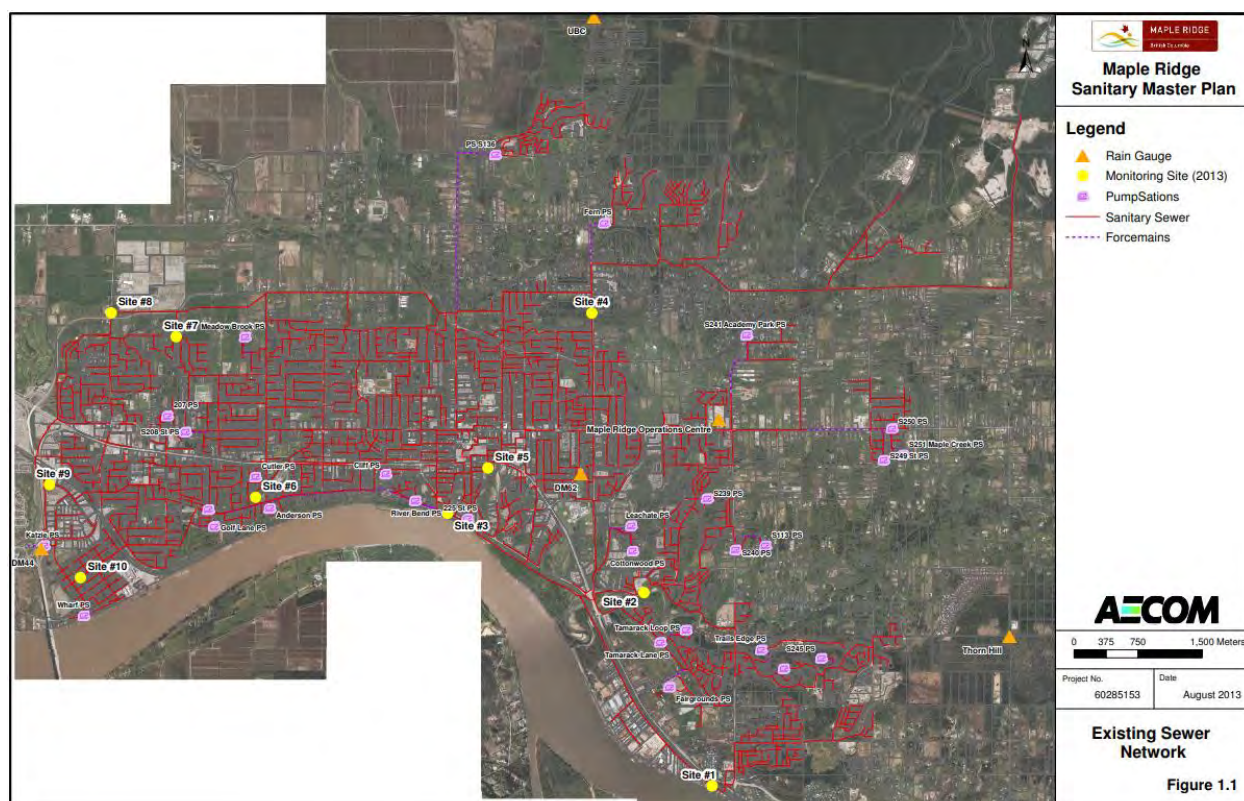
Source: Maple Ridge Official Community Plan.

Maple Ridge maintains a stormwater drainage system consisting of a drainage ditch, culvert, and storm sewer system. The City has developed two comprehensive integrated stormwater management plans (ISMP) to assess and manage the impact of development in the South Alouette, Kanaka Creek, Blaney, North Alouette and Fraser River watersheds. As identified in the two ISMPs, there are a number of areas in Maple Ridge in which current stormwater management mains and culverts are over capacity, particularly in or near the Town Centre. The Albion Industrial Area currently has limited stormwater infrastructure. The North 256 Industrial Area needs improvements to stormwater infrastructure, whereas the Fraser River Industrial Corridor is lacking in stormwater infrastructure.

A significant portion of Maple Ridge falls within **Metro Vancouver's Fraser Sewerage Area (FSA)**, meaning that many properties within the City may connect to the municipal sewer system to Metro Vancouver's regional system and treatment facilities. Sewage from Maple Ridge properties within the FSA eventually discharges into Metro Vancouver's Katzie Pump Station, which also receives wastewater from areas of Pitt Meadows. The figure below, reported in the City's 2016 Sanitary Master Plan, shows the City's sewer network. As shown, sanitary infrastructure is somewhat lacking in the Albion Industrial Area, the North 256 Industrial Area,

and fully lacking from the Fraser River Industrial Corridor. The 2016 Sanitary Master Plan provides some recommendations related to maintaining and upgrading current pump stations and mains, but does not explore possibilities of Metro Vancouver service expansion.

Figure 18. Existing Sewer Network, Maple Ridge Sanitary Master Plan, 2016



Source: Maple Ridge, Sanitary Master Plan, 2016

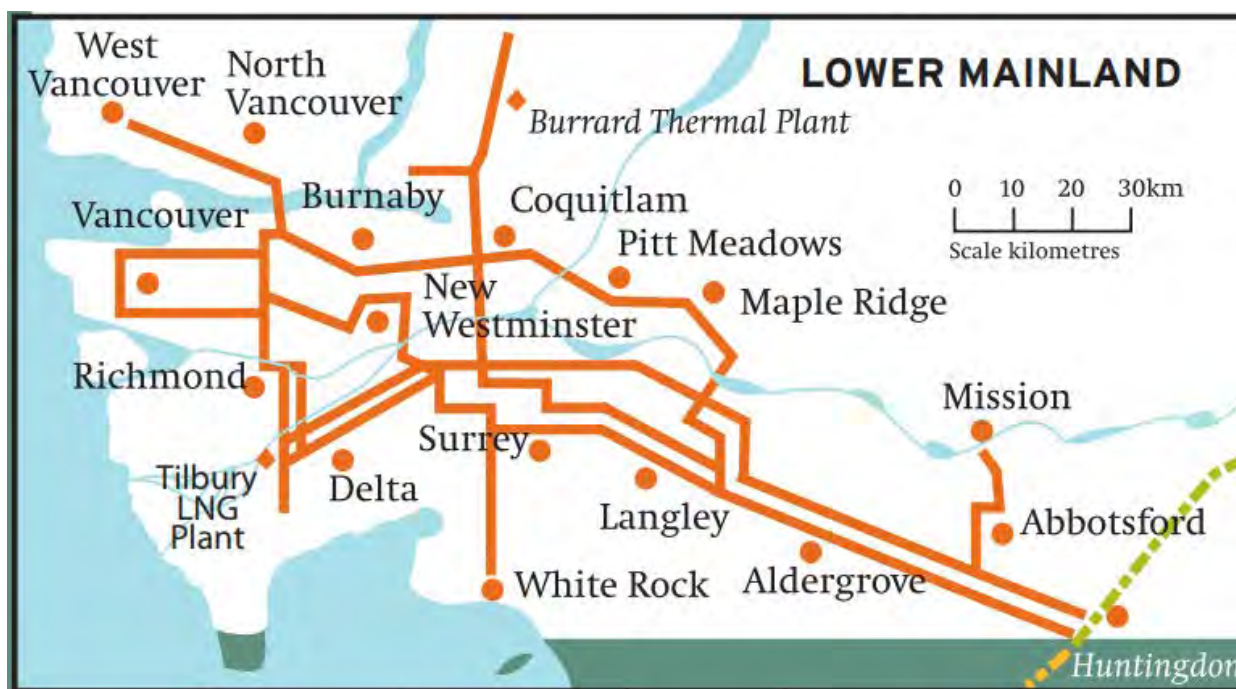
## Natural Gas

Natural gas to businesses and homes in Maple Ridge is serviced by **FortisBC**, a regulated private sector service provider.<sup>26</sup> FortisBC's pipeline system in the Lower Mainland is depicted in the figure below. There are some industrial lands in Maple Ridge that are being brought to market that lack natural gas servicing – there is a need to have gas lines serve these areas to support growth and development, but some public hesitancy to do so. The City has been working to develop regular engagements with FortisBC to explore this and other questions related to natural gas.

<sup>26</sup> <https://www.fortisbc.com/>



Figure 19. FORTIS Pipeline System in Lower Mainland



Source: FORTIS BC, [https://www.cdn.fortisbc.com/libraries/docs/default-source/services-documents/wins\\_fortisbc\\_pipeline\\_system\\_map\\_2593.pdf?sfvrsn=139cf23\\_2](https://www.cdn.fortisbc.com/libraries/docs/default-source/services-documents/wins_fortisbc_pipeline_system_map_2593.pdf?sfvrsn=139cf23_2)

## Electricity

Maple Ridge is served by **BC Hydro**, a provincial Crown corporation. Electrical infrastructure in the City includes a 500kV Transmission line that runs through North Maple Ridge, as well as 69kV lines that run through the town centre and through the Ruskin area. Just across the City's eastern boundary with Mission, BC Hydro operates the Ruskin (RUS) Hydro Dam and Generating Station, which produces 105 MW.<sup>27</sup> In 2024, BC Hydro announced a 10 Year Capital Plan consisting of \$36 billion in planned community and regional infrastructure investments in the province.<sup>28</sup> Infrastructure investments will specifically include improvements to generation assets, large transmission infrastructure, and community substations and local wires in order to make the system more reliable, efficient, and to reduce emissions. The plan has earmarked \$7.2 billion for infrastructure improvements in the Lower Mainland. Maple Ridge businesses have expressed concerns about service being expensive and infrastructure upgrades slow, in addition to concerns about experiencing long wait times for electricity servicing. The implementation of

<sup>27</sup> <https://www.bchydro.com/energy-in-bc/operations/our-facilities/lower-mainland.html>

<sup>28</sup> BC Hydro, Power Pathway, <https://app.bchydro.com/content/dam/BCHydro/customer-portal/documents/corporate/capital-plan/capital-plan-2024.pdf>

the Capital Plan may help to ease some of these concerns, such as reducing costs and improving system reliability, over the next 10 years.

The **Electrified Film Kiosk** at the Railway Lot near the Haney Bypass is a sustainability initiative completed at the end of 2024 designed to power film productions in Maple Ridge that would otherwise depend on diesel powered generators. This 800-amp state-of-the-art kiosk is the third of its kind in Canada, with similar kiosks located in Toronto and Vancouver.<sup>29</sup> Film production circuses (central production hubs where trucks, gear, talent trailers, and other equipment are parked) can directly connect to the kiosk at the railway lot and can also use the kiosk as a high efficiency charging station for mobile battery trailers to power off-site filming.<sup>30</sup> With film productions saving an estimated 400-800 litres of diesel burning per day and an estimated \$160,000 annually in fuel costs, the opening of this kiosk aligns with film industry sustainability initiatives to reduce fossil fuel consumption.

Figure 20. Electrified Film Kiosk



## Telecommunications

Businesses and residents in Maple Ridge depend on reliable internet and telecommunications coverage. TELUS and Rogers are the two primary telecommunications services providers that service Maple Ridge. Since the federal government generally controls telecommunications location decisions, the City and Province cannot override decisions for locating infrastructure.<sup>31</sup> Locations of telecommunications infrastructure may contribute to spotty coverage in some areas of the City. While TELUS and Rogers report full coverage in most of Maple Ridge, as shown in the figures below, residents and businesses sometimes complain about gaps in coverage. Some areas of the North 256 Industrial Area fall outside of the national broadband data coverage.<sup>32</sup> In 2024, TELUS announced \$6 million in investments in Maple Ridge for enhancing

<sup>29</sup> Corbett, N., December 8, 2024, "Maple Ridge joins big cities with new film industry infrastructure", Maple ridge-Pitt Meadows News, <https://www.mapleridgenews.com/local-news/maple-ridge-joins-big-cities-with-new-film-industry-infrastructure-7692237>

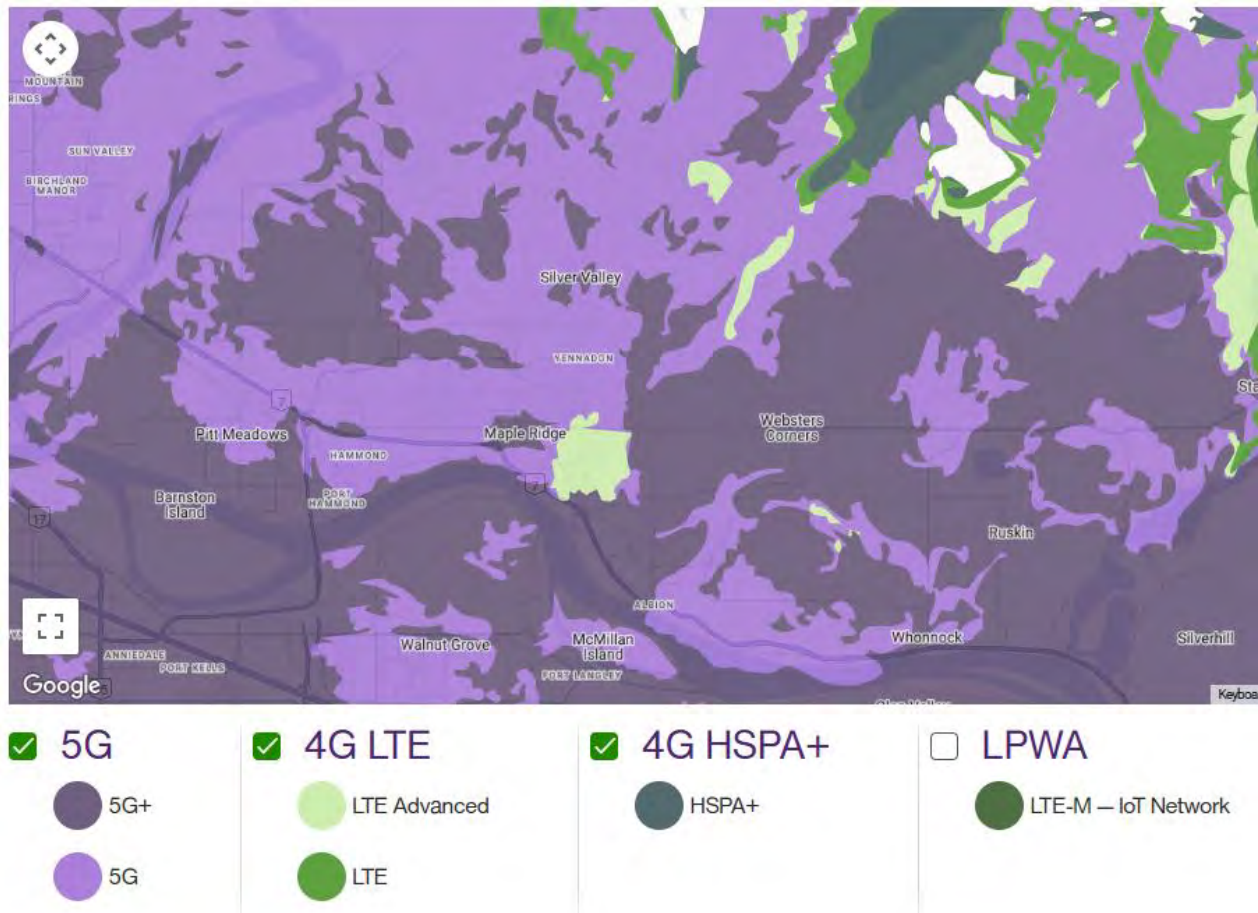
<sup>30</sup> <https://www.mapleridge.ca/news/city-champions-sustainable-film-industry-new-electric-film-kiosk>

<sup>31</sup> Telecommunications companies are required to consult with local property owners in some cases when developing tall structures.

<sup>32</sup> Maple Ridge Site Selector Tool

wireless connectivity and connecting residents and businesses to its PureFibre network. This investment is a part of a broader \$17 billion investment over the next five years for improving and expanding service throughout the province.<sup>33</sup>

Figure 21. TELUS Service Map, Maple Ridge

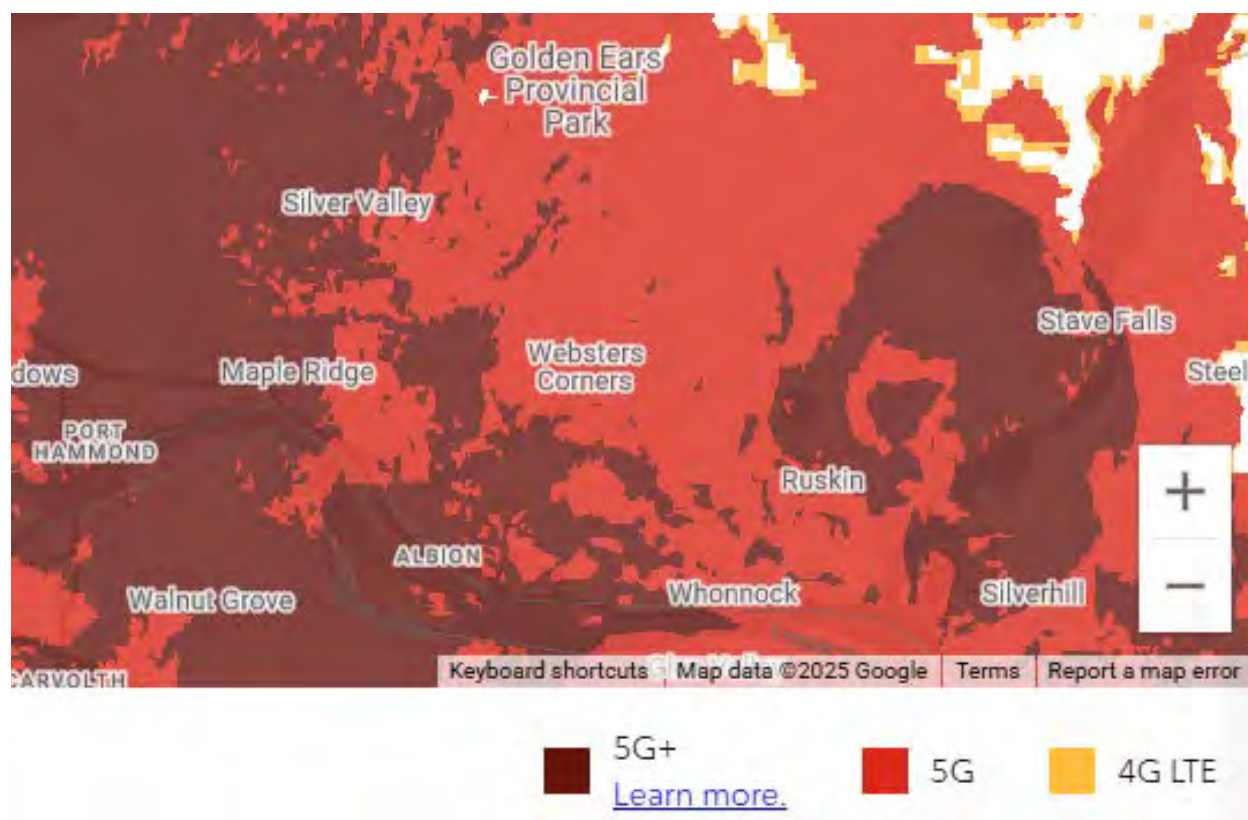


Source: TELUS, Coverage Map, accessed on February 20, 2025, <https://www.telus.com/en/mobility/network/coverage-map>

<sup>33</sup> TELUS, June 12, 2024, "TELUS investing \$6 million in Maple Ridge", <https://www.telus.com/en/about/news-and-events/media-releases/telus-investing-6-million-in-maple-ridge>



Figure 22. Rogers 5G Network Service Map



Source: Rogers, Network Coverage, accessed on February 20, 2025, <https://www.rogers.com/mobility/network-coverage-map>

Table 27. Utilities Infrastructure – Summary of Advantages and Disadvantages

Advantages	Disadvantages
<ul style="list-style-type: none"> <li>The Electrified Film Kiosk is a strong competitive advantage for Maple Ridge's film industry.</li> <li>Capital investments by utility and service providers such as BC Hydro and TELUS may improve electricity and telecommunications services for businesses and residents over the coming decade.</li> </ul>	<ul style="list-style-type: none"> <li>Metro Vancouver-operated and maintained sanitary and water, utilities make it difficult for the City to extend key utility services to underserved areas.</li> <li>Lack of services like water and sanitary may impair investment attraction efforts and may incentivize existing companies in underserved areas to develop and operate private systems, which can fall out of regulatory oversight.</li> <li>Long servicing times and uncertain time frames from BC Hydro contribute to the electric utility being cost prohibitive and uncertain for some businesses</li> </ul>



## Transportation Infrastructure

### Roadways

#### Highway 7 (Lougheed Highway)

**Highway 7**, also referred to as Lougheed Highway for much of its length, is a major corridor that travels from Vancouver along the north side of the Fraser River into the Fraser River Valley, connecting the City of Maple Ridge to surrounding communities in the Metro Vancouver and Fraser Valley regions. Highway 7 enters Maple Ridge on its western border with Pitt Meadows and offers a direct connection to the Maple Meadows Industrial Park and to the Golden Ears Bridge heading south. The road travels east and briefly splits between Highway 7, which travels along the river, and Lougheed Highway, which travels through downtown Maple Ridge before curving south to re-converge with Highway 7 just north of the Albion Industrial Park. Highway 7 then continues to travel along the river, also serving the Ruskin Industrial Park, until it reaches Maple Ridge's western border with Mission.

Over 24,000 vehicles travel along Highway 7 each day; the highway supports and connects residents, tourists, business activities, and the movement of goods related to port and rail.<sup>34</sup> Lougheed Highway is also a codified disaster route for the region.<sup>35</sup> As a provincial highway, this corridor is under the jurisdiction of the British Columbia Ministry of Transportation and Transit (MOTT) for the entirety of its length that passes through Maple Ridge. Some portions of the highway, not located in Maple Ridge, fall under the jurisdiction of TransLink.

As a part of the Lougheed Transit Corridor Development Plan,<sup>36</sup> construction began in December 2022 on the Highway 7 Widening project, which consists of expanding a four-kilometre segment of roadway between 266<sup>th</sup> Street and 287<sup>th</sup> Street from two to four lanes for improved safety, reliability, capacity, efficiency, and to better accommodate large trucks. The project, totaling just over \$130 million in costs funded by the provincial and federal governments, is expected to be completed in the fall of 2025.

#### Golden Ears Way / Abernethy Corridor

**Golden Ears Way / Abernethy Corridor** is a key transportation corridor in Maple Ridge connecting Highway 7 and the Golden Ears Bridge to the North 256<sup>th</sup> Industrial Area, Golden Ears Provincial

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<sup>34</sup> British Columbia, Highway 7 Widening – 266<sup>th</sup> St. to 287<sup>th</sup> St., <https://www2.gov.bc.ca/gov/content/transportation-projects/other-transportation-projects/highway-7-widening>

<sup>35</sup> Lower Mainland-Disaster Response Route Network, <https://www.mapleridge.ca/media/file/disaster-response-route-map1pdf>

<sup>36</sup> Lougheed Transit Corridor, <https://www.mapleridge.ca/your-government/city-projects/lougheed-transit-corridor>

Park, and to the Silver Valley neighbourhoods. Consisting of Golden Ears Way just north of the Golden Ears Bridge and Abernethy Way traveling east, this route is identified as a key corridor in the City's Transportation Improvement Strategy. There are currently three expansion and improvement projects along Abernethy Corridor to improve congestion, safety, and connections<sup>37</sup>:

- Phase 1 of Abernethy Way Corridor Improvements widened Abernethy Way between 210 and 224 Street. (completed in 2017).<sup>38</sup>
- Phase 2 will widen Abernethy Way between 224<sup>th</sup> Street and 232<sup>nd</sup> street to a four lane roadway. Work on Phase 2 is currently underway.
- Phase 3 will extend Abernethy Way from 232<sup>nd</sup> Street to 240<sup>th</sup> Street, providing improved connections to the Albion Industrial Area and Albion community, located just south along 240<sup>th</sup> Street. This extension will also help to alleviate congestion on Lougheed Highway and along Dewdney Trunk Road.
- Phase 4 of the project will further extend Abernethy Way from 240<sup>th</sup> Street to 256<sup>th</sup> Street, further improving transportation options to the North 256 Industrial Area in addition to improving active transportation infrastructure with sidewalks and a multi-use path.
- The Abernethy Way Expansion project is complemented by a project to develop a new crossing over the Alouette River at the 240<sup>th</sup> Street alignment north of 124<sup>th</sup> Avenue. This new bridge is key to developing an alternative emergency route to the Silver Valley neighbourhood, as well as an alternative route to Golden Ears Provincial Park.
- The Golden Ears Way Four-Laning Project involves widening the portion of roadway just north of the Golden Ears Bridge and Lougheed Highway from two to four lanes to improve congestion. It should be noted that Golden Ears Way is under the jurisdiction of Translink, not the City of Maple Ridge. The timeline on this project is still to be determined.

These improvement projects in total cost an estimated \$188.5 million and, with the exception of the Golden Ears Way Four-Laning Project, are all expected to be complete by the end of 2027.

## Bridges

The **Golden Ears Bridge** is a six lane, 968metre-long bridge connecting Maple Ridge to Langley and other communities south of the Fraser River. Completed in 2009, the bridge was designed to replace the Albion Ferry for vehicles, pedestrians, and cyclists. The Golden Ears Bridge is one of five bridges owned and maintained by TransLink and has been toll-free since 2017.<sup>39</sup> The development of this bridge has greatly improved the ease of north-south travel, making many key

<sup>37</sup> Maple Ridge, Transportation Improvement Strategy Overview

<sup>38</sup> <https://www.mapleridge.ca/AbernethyPhase2B>

<sup>39</sup> <https://www.translink.ca/rider-guide/driving/golden-ears-bridge>

destinations throughout the region, including the US border, a 30–35-minute drive from downtown Maple Ridge.

The **Pitt River Bridge**, located approximately 20 minutes west of Maple Ridge along Highway 7, provides east-west connections, connecting Maple Ridge/Pitt Meadows to Port Coquitlam and into Vancouver. With four lanes heading eastbound and three lanes heading westbound, the toll-free bridge facilitates the flow of goods transportation around the region.

**Table 28. Roadways – Summary of Advantages and Disadvantages**

Advantages	Disadvantages
<ul style="list-style-type: none"> <li>Maple Ridge has direct access to key regional infrastructure including Highway 7 and the Golden Ears Bridge, which improve east-west and north-south connections for industrial, commercial, and residential travel alike.</li> <li>The City and Province are investing in roadway infrastructure improvements along key corridors including the Abernethy Corridor and Highway 7 to improve congestion, capacity, and improve transportation connections to industrial lands.</li> </ul>	<ul style="list-style-type: none"> <li>Roadway infrastructure in Maple Ridge currently does not optimally serve some of the City's industrial areas and business parks, namely North 256 Industrial Area and the Kanaka Business Park – there is currently only one way in and out of these areas.</li> <li>While roadway expansions will provide some improvements to capacity and congestion, the most effective strategies for easing congestion involve multimodal improvement that give road users the ability to shift modes. This could include, for example, the ability for commuters to shift toward transit or active transportation, and for industrial and commercial transportation to shift to rail or barge.</li> </ul>

## Air

The **Vancouver International Airport (YVR)** is located approximately 45 km west of downtown Maple Ridge and offers linkages through Canada and internationally. Drive time from downtown Maple Ridge to Vancouver International Airport is approximately 50 to 60 minutes, depending on traffic. YVR services 55 airlines offering more than 125 destinations throughout the world. In 2023, nearly 25 million passengers boarded or disembarked planes at YVR.<sup>40</sup> The airport is also

<sup>40</sup> Korstrom, "Businesses moved record amount of air cargo through YVR in 2024", Business Intelligence for BC, <https://www.biv.com/news/transportation/businesses-moved-record-amount-of-air-cargo-through-yvr-in-2024-10212907#:~:text=It%20said%20in%20a%20news,disembarked%20planes%20at%20the%20airport.>

the second busiest cargo airport in Canada, moving over 315,000 tonnes of cargo valued at \$19 billion in 2023.<sup>41</sup> The airport’s top exports include seafood, cherries, and mining equipment to trade partners such as the US, China, Australia, Germany, and Japan. Top imports in 2023 include electronics, ecommerce, and perishable goods from the US, China, Mexico, Vietnam, and Germany.

The **Pitt Meadows Regional Airport (YPK)** is located 9 km or a 15-minute drive west of downtown Maple Ridge. While the airport is physically located in Pitt Meadows, the airport is jointly owned by the City of Maple Ridge and the City of Pitt Meadows. YPK is a general aviation satellite airport that has seven helipads and three runways ranging from 2485 to 5000 feet in length which can accommodate small commercial and private jets. There are over 300 acres of land currently available for on-airport development at YPK, including terminal and office space, commercial support services, aviation hangars, and waterfront opportunities.<sup>42</sup> As the airport has waterfront lots along the Fraser River that are within the jurisdictional boundaries of the Port of Vancouver, there are potential opportunities for port and industrial related development. The airport is adjacent to the Golden Ears Business park, 3 km and a 5-minute drive away from the Maple Meadows Business Park in Maple Ridge, and is a comparable drive time to Highway 7 and the Golden Ears Bridge. YPK is designated as an AOE/15 (Airport of Entry) for general aviation for flights on private, company-owned, or charter aircraft carrying no more than 15 people (including crew).

Table 29. Air – Summary of Advantages and Disadvantages

Advantages	Disadvantages
<ul style="list-style-type: none"><li>• Close proximity to YVR, a major commercial and cargo airport, supports businesses in a variety of industries, residents, and tourists in Maple Ridge through transportation connections and trade.</li><li>• The Pitt Meadows Airport may directly support some business travel and operations in the nearby Maple Meadows Business Park.</li></ul>	<ul style="list-style-type: none"><li>• The Pitt Meadows Airport is currently only for general aviation purposes and does not support commercial travel or freight operations, meaning that there are limited applications for businesses, residents, and tourists in Maple Ridge.</li></ul>

<sup>41</sup> YVR Airport, Cargo, <https://www.yvr.ca/en/business/cargo>

<sup>42</sup> YPK Airport, Development, <https://www.flyypk.ca/development>

## Rail and Intermodal

The City of Maple Ridge is well served by **Canadian Pacific Kansas City (CPKC) Rail**, a Class 1 Railroad that provides direct access to markets in Canada, the US, and Mexico. The CPKC Rail line runs along the Fraser River, just south of Highway 7, and passes directly through several of the city's business and industrial parks, including Maple Meadows Industrial Business Park, Albion Industrial Park, and Ruskin Industrial Park. To the west, the CPKC Rail line directly connects to an intermodal yard located in Port Coquitlam, just 12 km northwest of downtown Maple Ridge. With over 100 acres of expansion potential, the Port Coquitlam intermodal facility offers direct connections to the Port of Vancouver. Southeast of Maple Ridge, the CPKC Rail line passes over the Mission Bridge, through Abbotsford, and connects to BNSF Rail in the United States, servicing markets on the US west coast and connecting to other rail carriers serving the central and eastern US and Mexico.

The **Port of Vancouver**, located roughly 40 km west of downtown Maple Ridge and directly connected by CPKC Rail, is the largest port in Canada moving about \$300 billion in goods annually to and from Canada.<sup>43</sup> Operated by the Vancouver Fraser Port Authority, the Port has 29 major marine terminals, connects to three Class 1 railroads, a regional short line railroad, and the capacity to handle cargo such as automobiles, breakbulk and project cargo, bulk, and containers, in addition to passengers on cruise ships. The Vancouver Fraser Port Authority is responsible for dredging parts of the Fraser River to maintain marine related trade corridors, and to ensure ease of access and navigation. The Port Authority manages 7.5 km of shoreline in Maple Ridge.

Over the past 15 years, the Vancouver Fraser Port Authority and regional partners have been exploring options for short sea shipping of containerized goods, a cost-effective and sustainable alternative to move non-bulk cargo on barges along waterways in and around the port.<sup>44</sup> A 2019 study by the Port led to the creation of the **Inland Hub Facility** concept, which would provide a common user facility along the Fraser River with some complementary container services on site (e.g., import and export transloading, rail loading and unloading, etc.). Depending on available land and funding, there are two potential options for the Inland Hub Facility:

1. Comprehensive option: provide a wide array of complementary container services and would require the establishment of a new container services cluster and barge service at or near an existing railhead in the Lower Mainland with direct access to the Fraser River, as well as existing road and rail infrastructure. This comprehensive option would require greater land and funding resources, including a parcel of land larger than 60 acres.

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<sup>43</sup> Port of Vancouver, <https://www.portvancouver.com/cargo-and-terminals>

<sup>44</sup> Port of Vancouver, Short Sea Shipping Project, <https://www.portvancouver.com/project/short-sea-shipping>

2. Compact option: provide a limited range of complementary container services and would be established at an existing container services cluster. The compact option requires fewer resources to implement, including a 30–40-acre parcel.

Regardless of which option is implemented, the development of the Inland Hub Facility would further improve freight movement throughout the region and specifically between the Port of Vancouver and communities along the Fraser River. The Inland Hub Facility concept presents an opportunity for more freight flow to and from Maple Ridge, specifically to the Fraser River Industrial Corridor, Maple Meadows Business Park, and Albion Industrial Area located on or near to the Fraser River.

**Table 30. Rail and Intermodal – Summary of Advantages and Disadvantages**

Advantages	Disadvantages
<ul style="list-style-type: none"> <li>• CPKC Rail directly serves the Maple Meadows Business Park, Albion Industrial Area, and Ruskin Industrial Area, with direct connections to an intermodal yard in Port Coquitlam and further connections to the Port of Vancouver.</li> <li>• Maple Ridge is near the Port of Vancouver, Canada's largest port. Several roadway, rail, and barge routes facilitate the movement of goods and services from Maple Ridge to the Port.</li> <li>• Once implemented, the Inland Hub Facility concept will further improve the movement of goods along the Fraser River, to and from Maple Ridge.</li> </ul>	<ul style="list-style-type: none"> <li>• The North 256 Street Industrial Area and Kanaka Business Park lack rail service.</li> </ul>



## Transit

The **West Coast Express (WCE)** commuter rail service, the only commuter train in western Canada, connects Maple Ridge to Downtown Vancouver. Operated by TransLink and in service since 1995, the WCE has more than 2.6 million boardings each year.<sup>45</sup> Each weekday, the WCE runs up to five trains made up of between three to ten bi-level cars: five trains travel westbound for the morning commute, and five trains travel eastbound in the afternoon and evening. The WCE has two stops in Maple Ridge at Port Haney and Maple Meadows. The WCE operates on CPKC track infrastructure. While there has been interest from the community to expand the WCE further into the Fraser Valley,<sup>46</sup> there are currently no plans in place for service expansion.

**Bus Rapid Transit (BRT)** in the Metro Vancouver region, operated by TransLink, is a key priority of the agency in their 2024 10-Year Investment Plan.<sup>47</sup> Maple Ridge is currently home to one Rapidbus route (a pre-BRT service) and is expecting expanded service in the coming years. The existing R3 Rapidbus route to Coquitlam CentreCentre connects riders from Maple Ridge Haney Place to the SkyTrain's Millennium Line in approximately 35 minutes. This route includes two stops in Maple Ridge along Lougheed Highway.

TransLink is planning to develop three new BRT corridors, including a BRT route connecting Maple Ridge Haney Place to Langley Willowbrook. The Langley-Haney Place BRT route will run 22 km, featuring 13 stations and buses running every 10 minutes during peak hours. In Maple Ridge, the route will include four stations along Lougheed Highway. Expected to reduce travel time by over 40 percent between Haney Place and

Figure 23. Proposed Langley-Haney Place BRT



<sup>45</sup> TransLink, West Coast Express, <https://www.translink.ca/about-us/about-translink/operating-companies/west-coast-express>

<sup>46</sup> Transport Action British Columbia, Should we expand the West Coast express?, <https://bc.transportaction.ca/publications/news/expanding-the-west-coast-express/>

<sup>47</sup> TransLink, 10-Year Investment Plan, <https://www.translink.ca/plans-and-projects/strategies-plans-and-guidelines/transit-and-transportation-planning/ten-year-investment-plan>



Willowbrook, this BRT route will directly connect Maple Ridge residents to the future Surrey-Langley SkyTrain.<sup>48</sup>

The **SkyTrain** is Metro Vancouver's automated rapid transit system, which has 79.6 km of track and transported over 140 million riders in 2023. SkyTrain runs along three main lines: the Canada Line, the Millennium Line, and the Expo Line. While Maple Ridge is not directly served by a SkyTrain line, riders can currently connect from Maple Ridge Haney Place to the Millennium Line in Coquitlam Centre via the R3 Rapidbus route. TransLink is planning a 16-km expansion project of the Expo Line from Surrey to Langley, adding eight stations and ending at Langley City Centre. Riders from Maple Ridge will also be able to connect to this future SkyTrain via the future Langley-Haney Place BRT route. The Surrey Langley SkyTrain project is expected to come into operation in 2029.<sup>49</sup>

TransLink operates multiple limited- and full-service **bus** routes in the City of Maple Ridge. Approximately 10 bus routes, in addition to the R3 Rapidbus route, travel through and within Maple Ridge, connecting at the Haney Place transit hub. These bus routes connect Maple Ridge residents and workers to jobs, school, shopping, health care, and other important destinations. The Maple Meadows Business Park is well-served by roughly three different bus routes. Albion Industrial Park has access to at least three different bus routes. The Fraser River Industrial Corridor is not as well served by bus, with access to just two limited-service bus routes. The North 256 Industrial Area is not currently accessible by bus.

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<sup>48</sup> TransLink, Bus Rapid Transit Program, <https://www.translink.ca/plans-and-projects/projects/rapid-transit-projects/bus-rapid-transit>

<sup>49</sup> Surrey Langley SkyTrain, <https://surreylangleyskytrain.gov.bc.ca/>

Effective January 2025

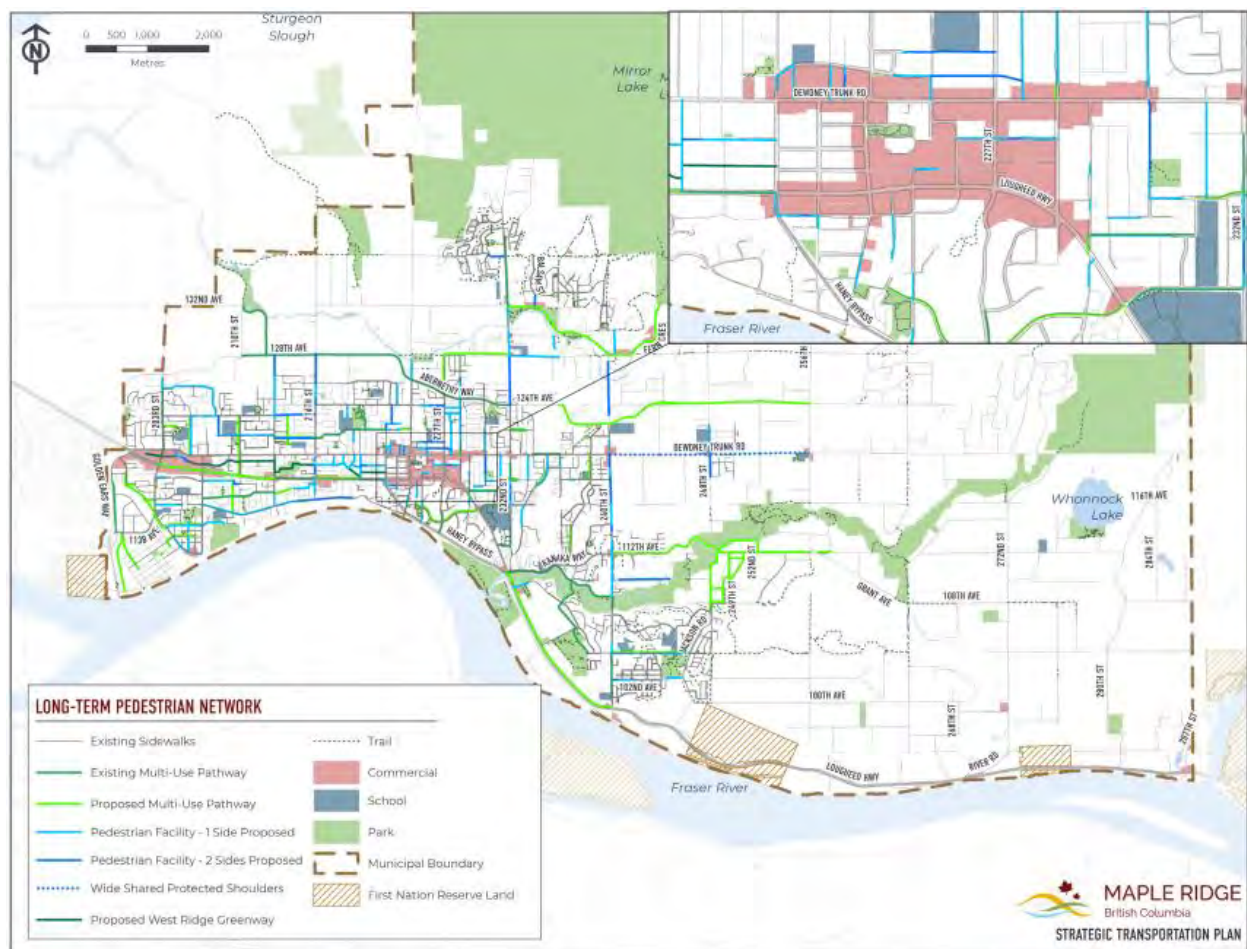
Map of the Maple Ridge Transit System showing bus routes and stations. The map includes major roads like 128th Ave, 130th Ave, and 132nd Ave, and features like Karaka Creek Regional Park and Brookside Lake. Bus routes are color-coded: green for Meadowtown, orange for Haney Place, purple for Port Haney, and blue for other routes. A north arrow and the text "Effective January 2025" are in the top right corner.

## Active Transportation

- Complete community connections so that residents can walk or bike to schools, commercial areas, transit, community centres, and other amenities.
- Prioritize personal safety and comfort by ensuring walking infrastructure is comfortable and accessible, including improved crossings and lighting.
- Invest and partner for a walkable Regional City Centre and Lougheed Transit Corridor Area to ensure most trips in these areas can be safely and comfortably made by walking.
- Partner to complete regional cycling network that connects to neighbouring communities.

Figures 25, 26, and 27 respectively show the Long-Term Pedestrian Network identified by the City, the current Cycling Network as reported by TransLink, and the Long-Term Cycling Network as identified by the City. As depicted in the figures, there are a variety of envisioned multi-use paths that will improve active transportation infrastructure to key transit connections along Lougheed Highway, near industrial and business areas such as Maple Meadows Business Park and Albion Industrial Area, and some improved infrastructure near the North 256 Industrial Area. Some of these improvement projects include the 132 Avenue and Fern Crescent Multi-Use Path and Intersection Improvements at 132 Avenue and Fern Crescent, as well as the Hammond Road Multi-Use Path from West Street to 203 Street.<sup>50</sup>

**Figure 25. Long-Term Pedestrian Network, Maple Ridge**



Source: Maple Ridge Strategic Transportation Plan 2023

<sup>50</sup> Maple Ridge, City Projects, <https://www.mapleridge.ca/your-government/city-projects>



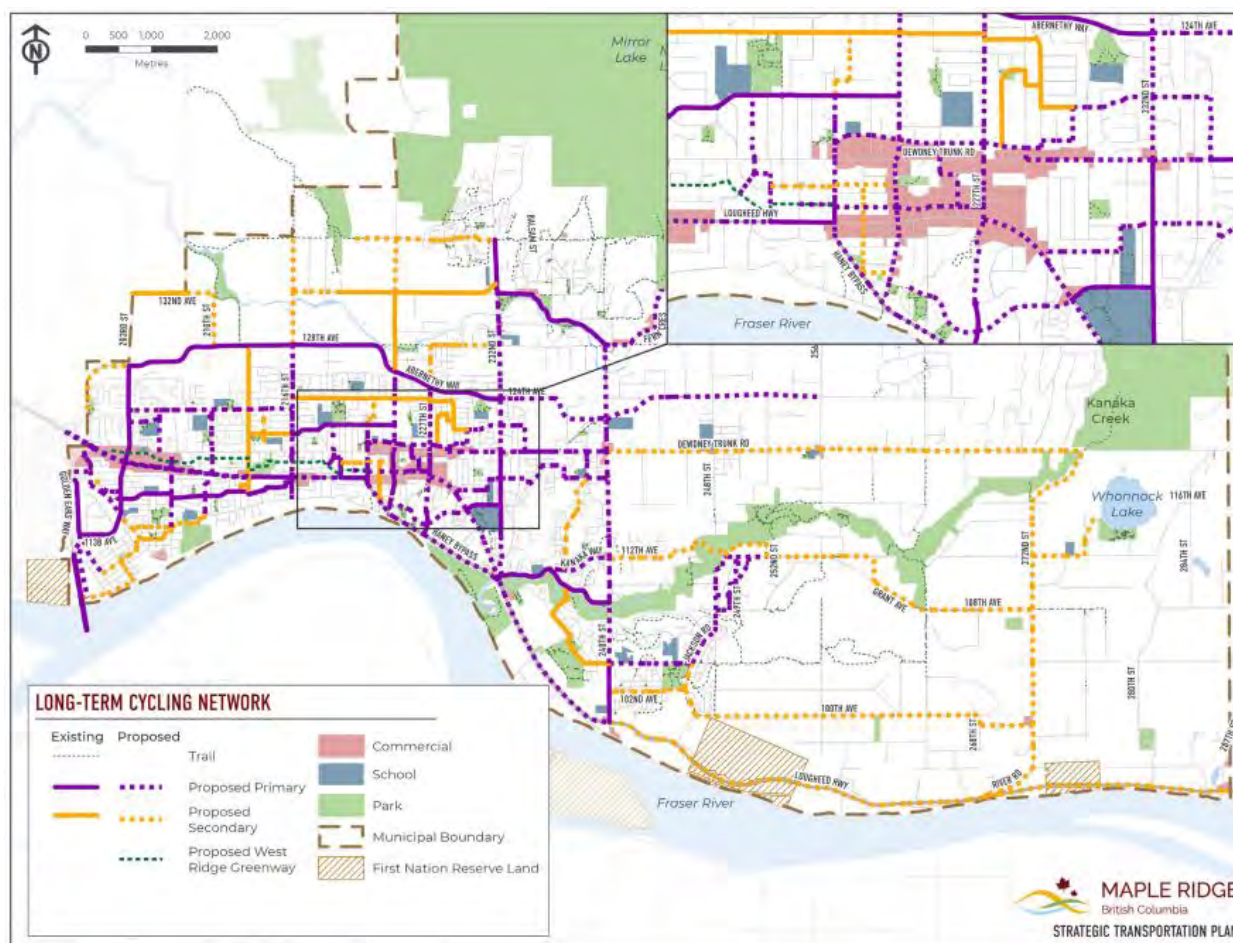
Figure 26. Current Cycling Route Map, Maple Ridge



Source: TransLink, Cycling in Metro Vancouver, Maple Ridge, Pitt Lake, and Pitt Meadows Local Area Map, <https://www.translink.ca/rider-guide/bike-and-ride-on-transit/cycling-in-metro-vancouver#cyclimg-maps>.

Note: All green lines indicate a signed or marked cycle route. Solid Dark Green lines indicate separated bike lanes with barriers or unseparated lanes at low automobile speeds that are considered to be comfortable for most cyclists. Dotted Solid Green lines indicate routes that are comfortable for some cyclists, consisting of either separated bike lanes with no traffic barriers, or unseparated lanes with automobiles traveling at medium speeds. Dotted Outlined Green lines indicate routes that are comfortable for few cyclists, consisting of bike lanes not separated from traffic on roads with vehicles traveling at high speeds. All purple lines indicate an unsigned cycle route. Solid Purple Lines indicate off-street routes. Dotted Solid Purple lines indicate local street routes where automobiles travel at medium speeds. Dotted Outlined Purple lines indicate arterial and collectors, in which vehicles travel at high speeds.

Figure 27. Long-Term Cycling Network, Maple Ridge



Source: Maple Ridge Strategic Transportation Plan 2023

Table 31. Public &amp; Active Transportation – Summary of Advantages and Disadvantages

Advantages	Disadvantages
<ul style="list-style-type: none"> <li>Maple Ridge has existing bus and commuter rail services that connect residents to the Metro Vancouver region.</li> <li>TransLink investments to develop a new BRT line to Langley to connect with the future Surrey-Langley SkyTrain improves regional connectivity between Maple Ridge and across the Metro Vancouver region.</li> <li>Use of public transit in Maple Ridge exceeds pre-pandemic levels.</li> </ul>	<ul style="list-style-type: none"> <li>Access via public transit to employment centres in industrial areas is inconsistent, with some industrial areas heavily or entirely lacking in public transit service.</li> <li>Some industrial areas are also not easily accessible via active transportation like cycling or walking.</li> <li>TransLink funding challenges present difficulties for maintaining and expanding operations to meet higher public transit demand.</li> </ul>

## Natural Features

### Golden Ears Provincial Park

**Golden Ears Provincial Park** is the third busiest park in British Columbia, covering 62,529 hectares of land and attracting approximately 610,000 visitors annually. This park has over 443 vehicle and tent campsites, making it one of the largest campgrounds in the province and able to accommodate activities such as hiking, horseback riding, climbing, cycling, fishing, canoeing, swimming, waterskiing, and windsurfing. While physically located in Maple Ridge, the park is provincially maintained with park operations privately contracted out. The only way to access the park is via Fern Crescent and the Golden Ears Parkway. Most visitors to the park, if not residents of the City of Maple Ridge or nearby communities, will stay in hotels and accommodations in Langley, Mission, or other neighbouring communities, given that there is no reliable hotel option within Maple Ridge.

### Fraser River

The **Fraser River** is the longest river within British Columbia, flowing 1400 km from the western slopes of the Canadian Rockies to the Pacific coast just south of Vancouver. The Fraser River creates the southern border of Maple Ridge. The river has long played a key role for industry throughout the Lower Mainland region, and is today used for activities including commercial shipping, transportation, fisheries, log storage, and foreshore agriculture activities.<sup>51</sup> According to the Fraser River Industrial Association, approximately 23 percent of use of the river is for industrial purposes, 17 percent for agricultural purposes, and 10 percent for transportation.<sup>52</sup> Long used for transportation and freight activity associated with the Port of Vancouver, the Port has expressed plans for further utilization of the river for short sea shipping initiatives, involving the development of a logistics hub further inland along the Fraser River.

Dredging is an important component of river maintenance to both ensure continued use of the river for trade purposes, in addition to providing key flood protection to keep water levels below dike levels during periods of increased water flow such as the annual freshet. Prior to 1998, dredging was conducted by the federal government but has since been made the responsibility of the Vancouver Fraser Port Authority, which self-funds and carries out dredging programs that are largely focused on primary trade channels. Due to the shift in responsibility and funding challenges, there is concern among Lower Fraser River communities that local secondary

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<sup>51</sup> Richmond Chamber of Commerce, The Economic Importance of the Lower Fraser River, <https://fria.ca/wp-content/uploads/2016/04/The-Economic-Importance-of-the-Lower-Fraser-River.pdf>

<sup>52</sup> Fraser River Industrial Association, <https://fria.ca/the-mighty-fraser/>



channels are not being adequately dredged, contributing to increased flood risk and reducing access to the river for industrial, commercial, and recreational uses.<sup>53</sup>

**Table 32. Natural Features – Summary of Advantages and Disadvantages**

Advantages	Disadvantages
<ul style="list-style-type: none"> <li>As one of the busiest parks in the province, Golden Ears Provincial Park brings hundreds of thousands of visitors through Maple Ridge each year.</li> <li>Future short sea shipping plans by the Vancouver Fraser Port Authority present increased trade opportunities for industry in Maple Ridge located near the river, such as businesses in Albion Industrial Area and in the Fraser River Industrial Corridor.</li> </ul>	<ul style="list-style-type: none"> <li>Dredging challenges in the Lower Fraser River increase risks of flooding and reduce commercial, industrial, and recreational access to the riverfront.</li> <li>Lack of a reliable hotel in Maple Ridge means that visitors to Golden Ears Provincial Park must stay outside of the City, causing additional leakages for business activity like retail and restaurants.</li> </ul>

<sup>53</sup> Hoekstra, G., April 11, 2023, "Lower Fraser River communities call for long-term dredging plan and federal funding", Vancouver Sun, <https://vancouversun.com/news/local-news/lower-fraser-river-communities-call-for-long-term-dredging-plan-and-federal-funding>



## 6. PEST and SWOT Analyses

The following matrices explore a diverse array of factors that may support or impair investment attraction efforts by the City of Maple Ridge. Factors are explored in two matrices: The Strengths, Weaknesses, Opportunities, and Threats Analysis, and the Political, Economic, Social, and Technological Analysis.

### Strengths, Weaknesses, Opportunities, Threats Analysis

Figure 28. SWOT Analysis

<b>Strengths</b> <ul style="list-style-type: none"> <li>• Renewed pro-business focus</li> <li>• Film and media business</li> <li>• Strategic economic vision</li> <li>• Competitive value proposition</li> <li>• Sustainability and climate action</li> </ul>	<b>Weaknesses</b> <ul style="list-style-type: none"> <li>• Regulatory and permitting challenges</li> <li>• Industrial land and infrastructure constraints</li> <li>• Data and market intelligence gaps</li> <li>• Limited industrial investment vision</li> <li>• Transportation and connectivity challenges</li> <li>• Hotel and business amenities deficit</li> </ul>
<b>Opportunities</b> <ul style="list-style-type: none"> <li>• Education and workforce growth</li> <li>• Strategic location and market access</li> <li>• Nearby transportation and infrastructure investments</li> <li>• Opportunities in high-value sectors</li> <li>• Existing financial incentives</li> <li>• Economic and housing trends</li> </ul>	<b>Threats</b> <ul style="list-style-type: none"> <li>• Political and economic uncertainty</li> <li>• Land and infrastructure constraints</li> <li>• Reputation and business climate</li> <li>• Complex utility access</li> <li>• Economic vision misalignment</li> </ul>

In a dynamic and ever-changing economic and political landscape, the City's ability to navigate challenges or seize opportunities hinges on its internal strengths and weaknesses. These internal factors are within the City's control and can be strategically addressed through targeted investments, strengthening areas of vulnerability while leveraging existing assets.

Conversely, external opportunities and threats stem from broader local, regional, and global trends that lie beyond the City's control. However, these external forces still play a crucial role in shaping the type and volume of business the City attracts. By proactively identifying and assessing these external factors, the City can adapt its economic development strategy in real-time to maximize potential benefits and mitigate risks, ensuring a resilient and thriving local economy.

## Strengths

**Renewed Pro-Business Focus.** The City has demonstrated its renewed and enhanced strategic focus at the City for supporting business growth and expansion, including with financial resources. The City has developed a team of skilled economic development staff and invested in formalizing industrial attraction and business retention and retention strategies.

**Film & Media Business.** The City has strong existing relationships in the film and media industry. The electrical kiosk at the Railway Lot is a strong asset for film production.

**Strategic Economic Vision.** The City has vision to upgrade investment in the transportation and warehousing sector, attracting more sophisticated, high-employment industry. Other opportunities that align with the City's vision and values include (1) growing an athletic training and sports treatment business cluster to support outdoor physical activity in the area; and (2) building mobility infrastructure to support community needs with emphasis on multimodal and bicycle and pedestrian infrastructure.

**Competitive Value Proposition.** The City offers affordable land and lease rates, strong housing growth strategy, and competitive tax and labour costs relative to the greater Vancouver area. This provides a strong foundation for the City's value proposition and demonstrates obvious advantages to investing in Maple Ridge. The City directly influences many of these advantages by planning and creating favourable conditions for housing growth and setting competitive tax rates. Labour costs, however, are not something over which the City exerts direct influence.

**Sustainability & Climate Action.** Maple Ridge's future Climate Action Plan will set sustainability parameters for residential, commercial, and industrial development.

## Weaknesses

**Regulatory & Permitting Challenges.** Long, unpredictable permitting processes hinder development in the City. Ideally, the City is proactive at identifying gaps in its regulatory processes, responsive to requests for information and clarification, and efficient at processing development permits.

**Industrial Land & Infrastructure Constraints.** Limited available land, ALR restrictions, and inadequate utilities and transportation infrastructure for new development.

**Data & Market Intelligence Gaps.** During our analysis, EBP discovered a lack of aggregated data available depicting recent Foreign Direct Investment (FDI) in Maple Ridge, especially high-CapEx, high-employment investments over the last five years. This limits site selectors' understanding of factors that differentiate Maple Ridge from regional peers and competitors. Similarly, the

absence of NAICS codes in City data hinders understanding of existing business activity and opportunities for expansion.

**Limited Industrial Investment Vision.** There is limited local enthusiasm for attracting industrial, high-employment investment. Strategic industrial investment attraction efforts can create industry clusters, spurring entrepreneurship and business growth for suppliers. Additionally, large industrial investment provides employment opportunities, as well as potential investment in training and workforce development for residents.

**Transportation & Connectivity Issues.** There are limited transit options available to industrial areas, making it less accessible for employees and less attractive to potential investors. Additionally, two major transportation assets in the City—the Pitt Meadows Regional Airport and the Fraser River—are underutilized for facilitating business activity.

**Hotel & Business Amenities Deficit.** No reliable hotel within the City means that business visitors must stay outside of the City, resulting in lost income for supporting businesses, as well as lost sales tax revenue for the City.

## Opportunities

**Education & Workforce Growth.** Strong population and labour force growth in Maple Ridge creates opportunity to expand local post-secondary institutions, reducing the number of residents that must leave the City to receive higher education and secure skilled employment.

**Strategic Location & Market Access.** Maple Ridge's position at the Gateway between Vancouver, especially the Port of Vancouver, and Fraser River Valley is a vital opportunity to expand participation in the freight, trade, and logistics economy. Proximity to major air, port, and rail connections make Maple Ridge a good candidate for becoming a manufacturing, warehousing, and distribution hub for Canada's west coast. Pharmaceutical distribution, for example, requires high-skill labour to manage just-in-time shipping, cold chain storage, and regulatory compliance. However, the City has slightly less desirable connectivity to these transportation assets than local peers.

**Nearby Transportation & Infrastructure Investments.** Regional investments in transportation infrastructure such as widening and extension projects on Highway 7 and Abernethy Corridor will likely ease congestion and improve access to Maple Ridge's industrial areas. Additionally, the Port of Vancouver's Inland Hub Facility concept will open greater opportunities for short sea shipping to the Fraser River Valley.

**Opportunities in High-Value Sectors.** Maple Ridge is well-positioned to attract higher value investment in the transportation and warehousing sector. Potential activities may include electric

trucking, sophisticated e-commerce warehousing and logistics operations, retail headquarters, food and beverage manufacturing, and cold-chain storage for pharmaceutical goods. High skill employment opportunities in this industry include logistics managers, supply chain analysts, fleet managers, automation technicians, and data analysis. Moderate skill roles include forklift operators, dispatch coordinators, and warehouse supervisors.

**Existing Financial Incentives.** Existing financial incentives Maple Ridge could leverage to attract investment in desired industries include: BC Interactive Digital Media Tax Credit (IDMTC), Training Tax Credit for Employers, Canada-BC Job Grant, BCIC Innovator Skills Initiative Program (BCIC-ISI), Scientific Research and Experimental Development (SR&ED) program, Heavy-Duty Low and Zero Emission Vehicles Trucking Project (HDZEV) and pilot project For the Long Haul. Additionally, Innovate BC provides opportunity for scaling innovative small businesses that align with the City's vision and values.

**Economic & Housing Trends.** Strong forecasted per capita income and median household income growth in Maple Ridge, outpaces nearby region and is on par with local peers.

## Threats

**Political & Economic Uncertainty.** Political instability in the US (further explored below under the PEST analysis) has created global dynamic change and has deep implications for multilateral trade. Optimistically, this is a temporal change, and trade will resume historic patterns when political stability returns.

**Land & Infrastructure Constraints.** Limited industrial land, misaligned infrastructure, and inadequate utility servicing increase costs and risks for developers. To some extent, these items are outside the City's direct control. However, the City can exert some influence through strategic planning, investment, policy, and interjurisdictional collaboration.

**Reputation & Business Climate.** The City struggles with attracting businesses due to a poor regional reputation and quality-of-life concerns. Slow regulatory processes coupled with opaqueness and lack of predictability contribute to this image. While the City can take contrary action to combat this image, it cannot directly control its regional reputation.

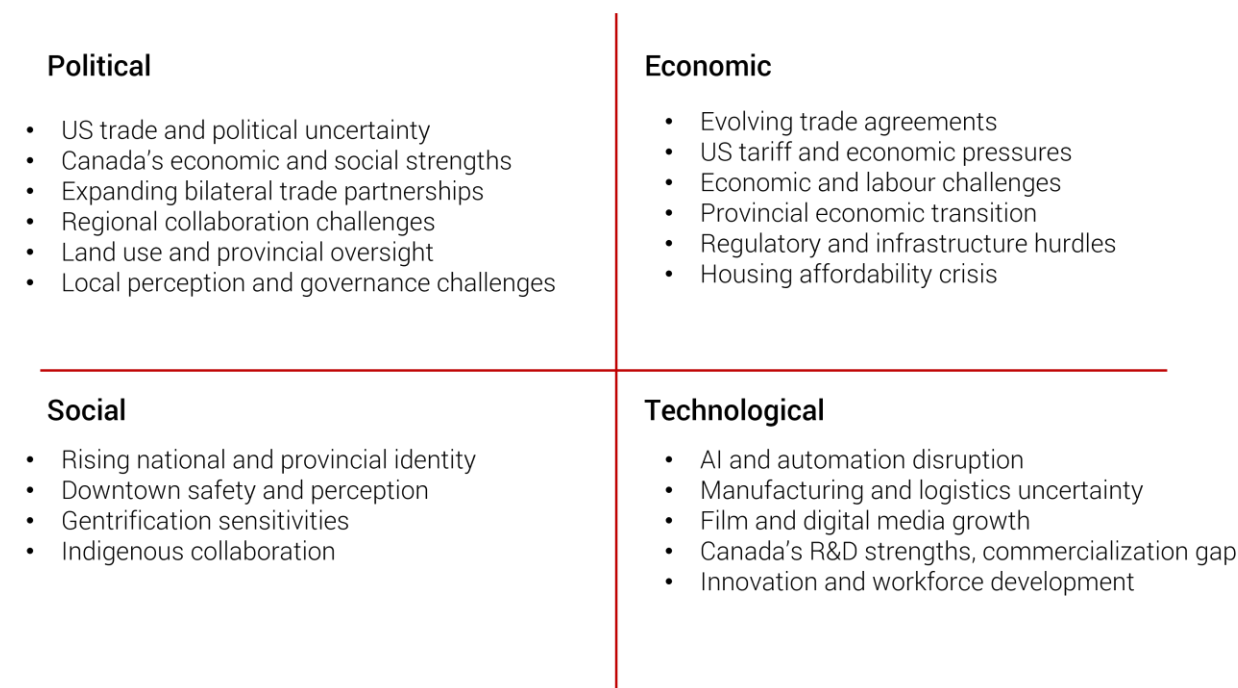
**Complex Utility Access.** Metro Vancouver's water management systems complicate connections for businesses and developers. This challenge may contribute to long lead times for preparing sites, in addition to lengthy City regulatory processes. A lack of sufficient water and sewer servicing to all City industrial areas contributes to high costs and risks for developers.

**Economic Vision Misalignment.** Differing priorities between the City and property owners create tension in development planning. While the City can take some action to improve relationships

with developers and property owners, it cannot control these actors' wants, needs, and motivations.

## Political, Economic, Social, and Technological Analysis

Figure 29. PEST Analysis



Like the SWOT analysis, the PEST matrix categorizes issues into four categories by the nature in which they influence the type and volume of business the City attracts. The PEST matrix is useful for assessing and managing risk and understanding the regional, national, and global context of business decisions which impact the effectiveness of City efforts.

### Political

**US Trade & Political Uncertainty.** There is a significant threat to established trade and business patterns due to political instability in the United States. Ongoing political pressure from the US exacerbates impacts on Canada.

**Canada's Economic & Social Strengths.** Though strained relations with the US and impending upending of the CUSMA are a threat to Canada's Western Hemisphere economic relationships, relative political stability in Canada, strong post-pandemic economic recovery, and robust social supports addressing underlying needs that affect business operations (housing, childcare, labour availability, education, healthcare) make it a desirable place to do business relative to the US. A

recent rise in Canadian nationalism has led to increased support of Canadian industry and business.

**Expanding Bilateral Trade Partnerships.** There is strong encouragement at the federal level to increase Canadian collaboration and trade with Asian and European partners. Additionally, there is interest in strengthening Canadian ties with Mexico amid strained US-Mexico relations.

**Regional Collaboration Challenges.** There is an increased regional drive toward global competitiveness, attracting international investment, and being a key trading partner to the Indo-Pacific region accessed off Canada's west coast. However, infighting and lack of coordinated regional efforts hinder economic growth and investment attraction.

**Land Use & Provincial Oversight.** Agricultural Land Reserve limits development, while key assets remain under provincial control (Lougheed Highway, Golden Ears Park, etc.).

**Local Perception & Governance Challenges.** Previous politicians in Maple Ridge have exacerbated negative discussions of public safety and perception. Breadth of opportunities and challenges facing the City may make it difficult to find focus.

## Economic

**Evolving Trade Agreements.** Several key bilateral trade agreements have recently been established or are currently in negotiation: Canada-Ecuador, Canada-UK (post-Brexit), Canada-Indonesia, Canada-ASEAN. Industries most impacted by these agreements include mining, agriculture, manufacturing, fisheries, technology, and services. Recently, stronger ties with Europe have led to an uptick in exports and imports (e.g., France-Quebec, Alberta-Germany). Canada has 16 active Free Trade Agreements.

**US Tariff & Economic Pressures.** Political instability, tariffs, and supply chain adjustments create uncertainty but also open opportunities for industry shifts. While tariffs may result in devaluing of the Canadian dollar, they also open opportunities for industry. Amid US tariffs, supply chains will take time to adjust (e.g. aluminum and steel). Canadian export support programs are aiding industries to diversify and shift toward other markets.

**Economic & Labour Challenges.** Labour productivity across Canada is trending down (productivity nearly 5% lower than before the pandemic). Inflation balanced with Bank of Canada's efforts to combat inflation (nervous about recession, but hesitant to further devalue the Canadian dollar). Border traffic between the US and Canada is down, limiting labour mobility.

**Provincial Economic Transition.** The British Columbia economy is experiencing a shift from natural resource-based to professional and technical services. The provincial government is



currently fast-tracking some resource and infrastructure projects to combat the threat of US tariffs.

**Regulatory & Infrastructure Hurdles.** Local municipal peers recognize the importance of clear, efficient regulatory processes and offer concierge programs to cut costs for businesses. Long wait times for servicing from BC Hydro complicates the regulatory landscape.

**Housing Affordability Crisis.** Limited developable land and high housing costs strain economic growth and workforce stability across the region.

## Social

**Rising National & Provincial Identity.** Growing Canadian nationalism has bolstered support for Canadian industry and business. Relative to the US, Canadians can take pride in the country's robust social supports and pro-immigration policies that make it a desirable place to do business. Similar dynamics are occurring at the provincial level as British Columbia strengthens its identity as a desirable destination to live, work, and visit.

**Downtown Safety & Perception Issues.** Concerns over visible homelessness, neglected public spaces, and poorly maintained storefronts impact community sentiment.

**Gentrification Sensitivities.** Multi-use residential developments may create tensions around neighbourhood change. There is a need to balance dual priorities of maintaining the small business culture of Maple Ridge, as well as attracting larger retailers and industry. The City must carefully bridge opportunities to preserve historic identity and promote modern development.

**Indigenous Collaboration.** The City is making concerted efforts to strengthen reconciliation and partnerships with Katzie and Kwantlen First Nations.

## Technological

**AI & Automation Disruption.** Global technological shifts are reshaping traditional industries.

**Manufacturing & Logistics Uncertainty.** Political instability complicates industry adaptation to automation and AI-driven systems. Unpredictability in the global political and economic environment makes it harder to develop and adapt AI algorithms and automated systems.

**Film & Digital Media Growth.** Maple Ridge has strong existing relationships in the film and digital media industry which is driven by technological growth and innovation.

**Canada's R&D Strengths & Commercialization Gap.** Canada is a known leader in supporting research and development efforts, and British Columbia has robust innovation incentives in

place. However, R&D support does not necessarily lead to successful commercialization of innovation. Additionally, there has been strong employment creation and capital investment in the software sector in the last five years.

**Innovation & Workforce Development.** British Columbia's incentives foster employment and investment, but continuous skill development is essential to remain competitive. A skilled workforce is critical to continue attracting investment to the region; strong higher education partners can assist the City in demonstrating its available skilled workforce and developing new market-responsive training programs.

## 7. Opportunity Match Profiles

This section explores specific opportunities, key requirements, and industry outlooks in the following sectors of interest. The project team identified these target sectors through the preceding Foreign Direct Investment (FDI) Benchmarking and Competitive Benchmarking Analyses, findings from engagement with relevant groups, and coordination with the Economic Development Department, the Physical Asset Inventory, and the SWOT and PEST Analyses.

- **Film** (motion picture and video production)
- **Manufacturing** (food manufacturing, medical equipment, and aerospace)
- **Transportation & Warehousing**
- **Professional, Scientific, and Technical Services** (especially related to engineering, architecture, environmental, Information Technology (IT), and communications)
- **Life Sciences** (diverse cluster comprising various subsectors, e.g., manufacturing, sports medicine, R&D)

**Table 33. Summary of Opportunities**

Sector	Opportunity
Film	<ul style="list-style-type: none"> <li>• Build out a more complete film hub by supporting other sector activities, e.g., pre-production, planning, writing, casting, post-production, special effects, a sound stage, and acting school.</li> <li>• Support ancillary sectors that support film activity, e.g., construction, carpentry and other trades, accommodation &amp; food services</li> <li>• Improve Maple Ridge's permitting ease and efficiency.</li> </ul>
Manufacturing	<ul style="list-style-type: none"> <li>• Ensure sufficient power and water services to support food manufacturing needs (e.g., water treatment and cold storage)</li> <li>• Foster school-to-work pipelines for local manufacturing via post-secondary programming in Maple Ridge.</li> <li>• Capitalize on Maple Ridge's proximity to an aerospace cluster and the region's strong R&amp;D activity to foster manufacturing.</li> </ul>
Transportation & Warehousing	<ul style="list-style-type: none"> <li>• Explore opportunities for refrigerated transportation and warehousing to support food manufacturing and life science activity. Electric trucking may be advantages to support sustainability values.</li> <li>• Explore opportunities for air-based transportation, given local assets.</li> </ul>

Professional, Scientific, & Technical Services	<ul style="list-style-type: none"> <li>• Foster school-to-work pipelines via post-secondary programming in Maple Ridge.</li> <li>• Market underutilized or vacant commercial properties to attract subsectors (e.g., design, marketing, legal services, public relations) synergistic with the film industry, manufacturing, and life sciences.</li> </ul>
Life Sciences	<ul style="list-style-type: none"> <li>• Explore the feasibility and market demand for wet lab spaces or other specialized facilities for life sciences.</li> </ul>

Detailed opportunities are explored in the following matrices called “Opportunity Match Profiles”, which provide specifics on the following areas of information:

- **Specific Role & Opportunities.** This area highlights potential competitive advantages and assets that could be further leveraged. It also touches on market opportunities given political, economic, social, or technological trends.
- **Specific Description.** This area defines the (sub)sector under the North American Industry Classification System (NAICS). It serves to focus investment attraction to an agreed-upon scope of industrial activity. Moreover, using this standard classification is useful in identifying and tracking industry trends and progress over time.
- **Current Industry Outlook.** This area draws on industry reports to discuss how industry outlook and trends may impact local industry and its opportunities. This provides a forward-looking context to promote a sustainable, long-term investment attraction strategy that accounts for future risks and opportunities.
- **Example Companies.** This area offers a sample of the types of firms classified under the defined (sub)sector. While not exhaustive, the list aims to shed light on existing firms and their diverse activities and historical investment decisions. In this way, the example companies may be target companies themselves or lead to broader networks of similar firms more locally suitable.
- **Key Requirements or Value Chain.** This area enumerates critical requirements for attracting and retaining businesses within the (sub)sector. This helps identify potential local advantages and disadvantages, which in turn inform and prioritize next steps to bolster investment readiness.

## Film

Film	
Specific Roles and Opportunities	<ul style="list-style-type: none"> <li>• Maple Ridge can build out its hub to support other activities in the film sector, e.g., pre-production, planning, writing, casting, post-production, special effects, a sound stage, and acting school. This would help Maple Ridge capitalize on its proximity to talent and existing activity hubs in Vancouver and Langley (Martini Studios). Maple Ridge is well positioned to capitalize on the re-energization of local/domestic filming.</li> <li>• Similarly, Maple Ridge can support ancillary activities related to construction, carpentry and other trades, equipment rentals, and accommodation &amp; food services. Hoteling is particularly critical (see “Proposed Maple Ridge Hotel” Market Study).</li> <li>• Maple Ridge can explore ways to further leverage its competitive infrastructural advantage as one of three cities in Canada with an electrified film kiosk, located at the Port Haney Rail Lot.</li> <li>• Maintain and sharpen Maple Ridge’s edge in film permitting efficiency, which is critical to attracting activity.</li> </ul>
Specific Description <sup>54</sup>	<ul style="list-style-type: none"> <li>• <b>Motion Picture and Video Production (NAICS 512110)</b> comprises activity related to producing films, videos, television programs, or commercials. It most closely captures Maple Ridge’s on-site and studio-based filming, as well as related activities such as script development, casting, and production management. Post-production activities and distribution are captured under different NAICS categories. Other ancillary activities related to trades, rental companies, machinery fabrication, and set building are similarly captured in other categories.</li> </ul>

<sup>54</sup> [NAICS Association](#)

Film	
<p><b>Current Industry Outlook<sup>55</sup></b></p>	<ul style="list-style-type: none"> <li>• <b>Foreign location and service production hub.</b> The Canadian film industry has seen a lot of growth and investment in the last decades, with Vancouver being a major hub for film production, especially for foreign location and service (FLS) production.</li> <li>• <b>Recent downturn.</b> Overall production in the province shrank by 27.2% from 2023 to 2024, particularly in FLS production, which contracted 36.5%. This was largely due to the Hollywood strikes. Domestic content production helped offset these disruptions. Production is predicted to return to normal levels despite concerns about box office performance.</li> <li>• <b>Incentives.</b> Tax incentives may further stimulate film investment in the area. Canada and British Columbia offer film industry tax credits. At the same time, many provinces are introducing incentives, which diminish British Columbia's competitive edge. Maple Ridge has an advantage as a "regional" film site, which allots an extra 6% tax credit through British Columbia's program.</li> <li>• <b>Shifting finance.</b> The Canada Media Fund continues to support TV production, though it contributes an increasingly smaller share of financing, creating opportunities for increased private sector investment. Maple Ridge's affordability and local infrastructure could attract more domestic and international film productions looking for cost-effective options outside major urban centers. At the same time, a potential decline in the value of the Canadian dollar may make US-based investments even more attractive.</li> <li>• <b>Recent investment trends.</b> fDi Markets has no recorded foreign direct investment (FDI) in Maple Ridge in the Motion Picture &amp; Sound Recording subsector. That said, from 2020 to 2024, FDI into this subsector in British Columbia was the fourth largest job creator (1,601 jobs) and \$1.8B in capex (see EBP's FDI trends analysis). Except for two investments made in Port Moody (Feb 2023) and Kelowna (Nov 2020), Vancouver accounts for all investments made since 2003. Many companies cited the region's well-established market (particularly in visual effects and post-production), growing skilled workforce, local placemaking, and site availability as motivations for their investments.</li> </ul>

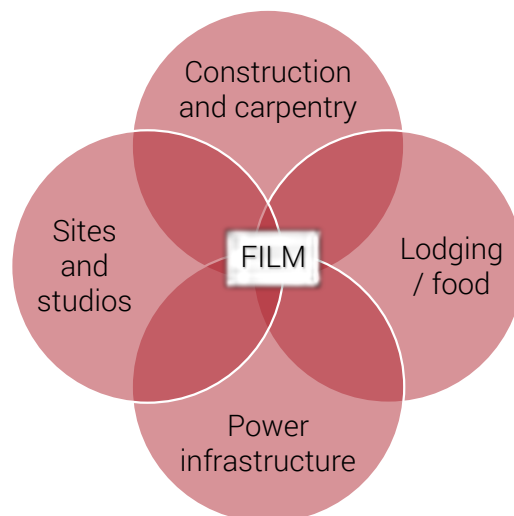
<sup>55</sup> [Economic Report on the Screen-Based Media Production Industry in Canada](#) (2024)



Film	
Example Companies	<p>Local businesses<sup>56</sup></p> <ul style="list-style-type: none"> <li>• Element 7 Productions (Seattle)</li> <li>• Fixer Production (North Vancouver)</li> <li>• Hand Crank Films (Bellingham)</li> <li>• PPI Camera Corp (Vancouver)</li> <li>• NoCo/NoMo (Vancouver)</li> <li>• First Avenue Films, Inc. (Orcas)</li> <li>• Skyland Plan-net (Vancouver)</li> <li>• Talking to Crows LLC (Bellingham)</li> <li>• Diamondhead Films (Vancouver)</li> <li>• Anthill Farms (Squamish)</li> <li>• Dark Mass Productions (Maple Ridge)</li> <li>• Wild Bus Films (Vancouver)</li> <li>• Epic Global Media (Vancouver)</li> <li>• Companyman Production Services (Vancouver)</li> <li>• BW Video Productions (Everett)</li> <li>• The Capital Media Company (Vancouver)</li> <li>• Social Media Films (Langley)</li> <li>• Roll. Focus. (Victoria)</li> </ul>
	<p>Ancillary services</p> <ul style="list-style-type: none"> <li>• Sunbelt Rentals</li> <li>• MBS Equipment Company</li> <li>• Cinelease</li> </ul>
	<p>Large businesses<sup>57</sup></p> <ul style="list-style-type: none"> <li>• Time Warner Cable Enterprises LLC</li> <li>• Lions Gate Entertainment Inc</li> <li>• Starz LLC</li> <li>• Cinemark Holdings Inc</li> <li>• MGM Holdings Inc</li> <li>• Warner Communications LLC</li> <li>• Universal Studios Company LLC</li> <li>• Fubotv Inc</li> <li>• ABC Family Worldwide Inc</li> <li>• Lucasfilm</li> <li>• Industrial Light and Magic (Walt Disney)</li> <li>• Basilic Fly Studio</li> <li>• Vaudeville Sound Group</li> <li>• Sony Pictures Imageworks</li> <li>• Steamroller Studios</li> <li>• Tower33 VFX</li> </ul>

Film	
Key Requirements or Value Chain	<ul style="list-style-type: none"> <li>• <b>Filming Locations &amp; Infrastructure</b> – Diverse landscapes, historic and modern settings, and the electrified film kiosk make Maple Ridge a desirable location. Local studios would enhance appeal.</li> <li>• <b>Local Workforce &amp; Talent</b> – Skilled film crews, actors, technicians, and supporting services (e.g., set designers, makeup artists) are essential for productions to operate efficiently. Construction and carpentry workforce is also critical for set construction.</li> <li>• <b>Support Services</b> (accommodation and food services) – Reliable transportation, accommodations, catering, and equipment rental services ensure smooth on-site production. Lodging is particularly critical, as the City lacks hotel options.</li> <li>• <b>Support Services</b> (equipment rentals) – Filming requires rental equipment for grips, lights, etc. from companies including Sunbelts, Cinelease, and MBS Equipment Company, but the Maple Ridge area is currently underserved.</li> <li>• <b>Regulatory &amp; Permit Process</b> – A streamlined and supportive film permit system allows productions to operate efficiently while maintaining community relations.</li> <li>• <b>Cultural Infrastructure</b> – Awareness and celebration of film in the community is key to maintaining the City's reputation and support for film activity.</li> </ul>

Figure 30. Film – Key Requirements and Supports



56 Production Hub  
 57 NAICS Association; FDI Markets

## Food Manufacturing

Food Manufacturing	
Specific Roles and Opportunities	<ul style="list-style-type: none"> <li>• <b>All manufacturing.</b> Shift share analysis suggests that Maple Ridge has very significant local competitive advantages. Due to data limitations, the analysis does not indicate subsectors trends, though this trend supports the finding that the City has a strong manufacturing employment base to support food manufacturing opportunities.</li> <li>• <b>All manufacturing.</b> Maple Ridge is far enough from Vancouver to have lower cost sites but is close enough to have good market access to major population centres in western North America, as well as access to a major Pacific port. This is an advantage for attracting manufacturing investments, generally, though market access is particularly relevant for food manufacturing to optimize shipping costs, food freshness, and sizable consumer population.</li> <li>• <b>Food manufacturing (agribusiness).</b> Local agricultural production offers food manufacturers attractive local supply chains, and proximity to population and retail centers in the Vancouver area are key advantages for food manufacturers. Maple Ridge is also positioned to connect agribusiness with local agri-tourism and farm-to-table restaurants.</li> </ul>
Specific Description <sup>58</sup>	<ul style="list-style-type: none"> <li>• <b>Food Manufacturing (NAICS 311)</b>— This sector comprises a range of activities, including meat processing, dairy production, grain and oilseed milling, bakery and confectionery manufacturing, seafood processing, and specialty food production. These businesses may package, preserve, and distribute food products for retail, food service, and export markets.</li> </ul>

<sup>58</sup> [Statistics Canada, NAICS Canada 2022 Version 1.0](#)

Food Manufacturing	
Current Industry Outlook	<ul style="list-style-type: none"> <li>• <b>Food manufacturing.</b> Food and beverage processing is the largest manufacturing industry in Canada in terms of production value and employment.<sup>59</sup> Grain &amp; oilseed milling and bakery &amp; tortilla production have recently led growth in Canada.<sup>60</sup></li> <li>• Farm Credit Canada (FCC) described 2023 as a “lean year” for manufacturers due to decreases in consumer spending amid high inflation.<sup>61</sup></li> <li>• FCC reported slight improvement in 2024 and predicts sales to continue improve as raw material costs decline and to offset high labour costs. At the same time, FCC expects that Canadian consumers will continue to face tighter budgets, resulting in less spending on alcohol and more on food and non-alcoholic beverages. FCC is most optimistic about fruit/vegetable canning and freezing and dairy products.</li> <li>• fDi markets does not show any agribusiness investments in Maple Ridge but does indicate geographically diverse investments in area, including in Chilliwack, Delta, Duncan, Kelowna, Langley, Prince George, Richmond, Surrey, and Vancouver.</li> <li>• 2025's outlook is also complicated amid tariff discussions with the US, which accounts for over three-quarters of Canadian food/beverage exports.<sup>62</sup> At the same time, as of 2021, 58% of Canadian agricultural imports came from the US. In other words, tariffs could have mixed impacts across agricultural subsectors.<sup>63</sup></li> </ul>

<sup>59</sup> [Agriculture and Agri-Food Canada](#) (AAFC)

<sup>60</sup> [Food in Canada](#); For more on outlook, see also [BC Food & Beverage](#).

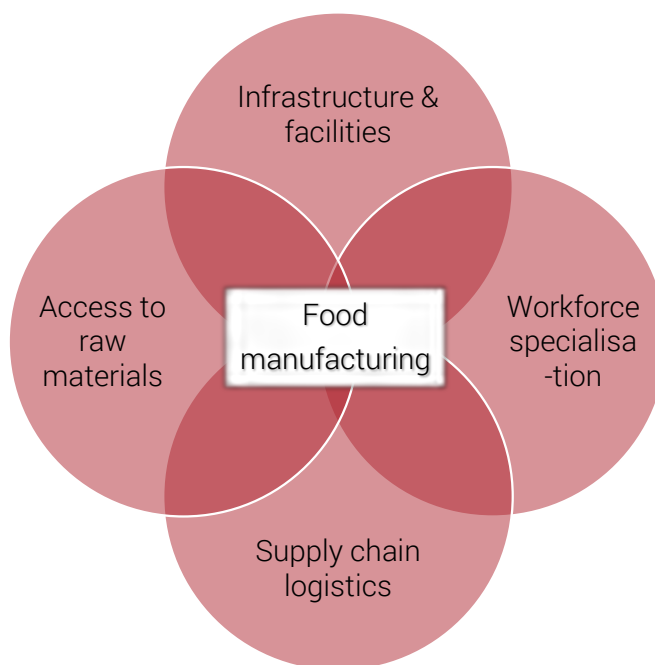
<sup>61</sup> Farm Credit Canada, [Food & Beverage Report](#)

<sup>62</sup> Farm Credit Canada, [Food and Beverage: 2025 FCC Economic Outlook](#). Note that the 2025 report with detailed subsector outlooks will be published in April 2025.

<sup>63</sup> USDA Foreign Agricultural Service, “[Canada 2021 Export Highlights](#)” (2021)

Food Manufacturing	
Example Companies	<p>Local businesses (Maple Ridge businesses)</p> <ul style="list-style-type: none"> <li>• Basco Foods Inc</li> <li>• Blacksmith Bakery</li> <li>• Hearty Vegan</li> <li>• Luna Coffee</li> <li>• Prime Health Ltd.</li> <li>• Pureaqua Water Company Ltd.</li> <li>• Purple Heather Bake Shop</li> <li>• Seajoy Foods Ltd</li> <li>• Pacific Sunrise Foods Ltd</li> <li>• Seana's Delectables</li> <li>• Shah Foods Inc</li> <li>• STS Sweets</li> <li>• Swan Cakes</li> <li>• The Happy Donut Inc</li> <li>• YVR Cookie Corp</li> </ul> <p>Large global firms</p> <ul style="list-style-type: none"> <li>• Ingredion</li> <li>• Bonduelle</li> <li>• Kraft Heinz</li> <li>• Saputo Inc.</li> <li>• McCain Foods Limited</li> <li>• Agropur Cooperative</li> <li>• Maple Leaf Foods Inc.</li> </ul>
Key Requirements or Value Chain	<ul style="list-style-type: none"> <li>• Access to industrial facilities with good power infrastructure – this may be a challenge in Maple Ridge (some stakeholders described this). Some subsectors also have refrigeration needs, which requires greater power on site and in transit.</li> <li>• Access to key transportation nodes to connect products to end users or other steps of the value chain. Transit may also be important for workforce, if workers are not local.</li> <li>• Manufacturing workforce with the right technical skills (local education and technical schools may help)</li> <li>• Some manufacturers (especially those that produce fats, oils, and grease as byproducts) have greater need for water treatment infrastructure or biodigestion and power production (i.e., renewable natural gas).</li> <li>• Access to raw materials (i.e., agricultural fields)</li> <li>• Access to efficiency technologies to ensure competitiveness</li> </ul>

Figure 31. Food Manufacturing – Key Requirements and Supports



## Other Manufacturing Opportunities

Other Manufacturing Opportunities – Medical Equipment and Aerospace	
Specific Roles and Opportunities	<ul style="list-style-type: none"> <li>• <b>All subsectors.</b> Shift share analysis suggests that Maple Ridge has significant competitive advantage in manufacturing, with local competitive factors contributing around 360 jobs from 2021 to 2024. Due to data limitations, the analysis does not indicate subsectors trends.</li> <li>• <b>All subsectors.</b> Maple Ridge is located far enough from Vancouver to have comparatively lower cost sites but is close enough to have good market access.</li> <li>• <b>Medical Equipment and Supplies Manufacturing.</b> Strong linkages to support the Vancouver region's healthcare sector.</li> <li>• <b>Aerospace Product and Parts Manufacturing.</b> Maple Ridge is connected to the existing local cluster of aerospace industry concentrated in the Vancouver area, with proximity to Boeing facilities in Seattle, WA. Maple Ridge's lower cost and available land, but sufficient proximity can be a pull factor for firms.</li> </ul>



Other Manufacturing Opportunities – Medical Equipment and Aerospace	
Specific Description <sup>64</sup>	<ul style="list-style-type: none"> <li>• <b>Medical Equipment and Supplies Manufacturing</b> (NAICS 33911)– This sector comprises establishments primarily engaged in manufacturing medical equipment and supplies (including eyeglass lens).</li> <li>• <b>Aerospace Product and Parts Manufacturing</b> (NAICS 3364)– This sector comprises establishments primarily engaged in manufacturing aircraft, missiles, space vehicles and their engines, propulsion units, auxiliary equipment, and parts thereof. The development and production of prototypes is classified in this industry, as is the factory overhaul and conversion of aircraft and propulsion systems. In Canada, Montreal boasts the largest aerospace cluster.</li> </ul>

<sup>64</sup> [Statistics Canada, NAICS Canada 2022 Version 1.0](#)

Other Manufacturing Opportunities – Medical Equipment and Aerospace	
Current Industry Outlook	<ul style="list-style-type: none"> <li>• <b>Medical Equipment and Supplies Manufacturing.</b> Medical manufacturing is expected to grow as Canada's population becomes older. Diagnostic apparatuses are the largest single product, followed by consumables, patient aids, orthopedic and prosthetic products, and dental products.<sup>65</sup></li> <li>• fDi Markets shows no foreign direct investments in this subsector in BC in the last ten years.</li> <li>• <b>Aerospace Product and Parts Manufacturing.</b> Aircraft manufacturing struggled during the pandemic, but the sector is expected to grow in the near term with an emphasis on emissions reduction technology. R&amp;D expenditures have returned to pre-pandemic levels.<sup>66</sup></li> <li>• Canada's Arctic Foreign Policy may also boost funding for North American Aerospace Defence Command (NORAD) system upgrades, which will include military aircraft and missile system production.<sup>67</sup></li> <li>• Space vehicle and missile manufacturing has seen declines due to low demand for satellite TV subscriptions in favor of internet-based products. Lunar Gateway (US-led initiative for a lunar outpost) will also stimulate the sector.<sup>68</sup> There is still demand for civil aircraft (e.g., business jets, helicopters) and engines (e.g., turboprop, helicopter, turbofan)<sup>69</sup></li> <li>• Aerospace is particularly vulnerable to supply chain challenges due to the variety of inputs needed. Trade disruptions remain top-of-mind issues for aerospace manufacturers. That said, as demand for travel fully recovers, aerospace is projected to grow in the near term.<sup>70</sup></li> <li>• fDi Markets shows no investments in Maple Ridge but does show investments made in Vancouver as well as Delta (MTU Aero Engines) and Abbotsford (Marshall Aerospace and Defence)</li> </ul>

<sup>65</sup> [IBIS World](#), Medical Instrument & Supply Manufacturing in Canada - Market Research Report (2014-2029)

<sup>66</sup> Innovation, Science and Economic Development Canada, "[State of Canada's Aerospace Industry](#)" (Summer 2024)

<sup>67</sup> [Canada's Arctic Foreign Policy](#)

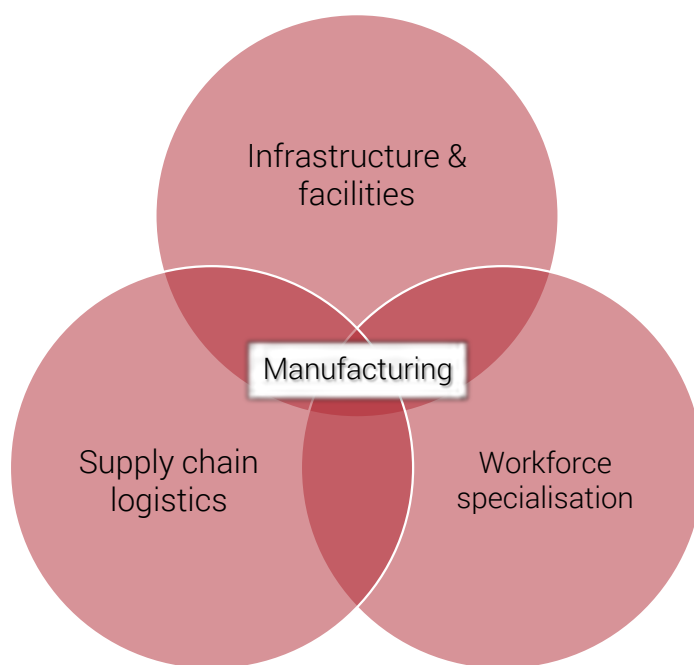
<sup>68</sup> [IBIS World](#), Aircraft, Engine & Parts Manufacturing in Canada - Market Research Report (2014-2029)

<sup>69</sup> Innovation, Science and Economic Development Canada, "[State of Canada's Aerospace Industry](#)" (Summer 2024)

<sup>70</sup> [IBIS World](#), Space Vehicle & Missile Manufacturing in Canada - Market Research Report (2014-2029)

<p><b>Example Companies</b></p>	<p><b>Medical Equipment and Supplies Manufacturing</b></p> <p>BC-based firms</p> <ul style="list-style-type: none"> <li>• StarFish Medical (Victoria, BC)</li> <li>• Clarius Mobile Health (Vancouver, BC)</li> <li>• Advanced Cyclotron Systems, Inc. (Richmond, BC)</li> <li>• Photon Control Inc. (Richmond, BC)</li> <li>• Datrend Systems Inc. (Richmond, BC)</li> <li>• AdvanTech Sterilizers Inc. (Abbotsford, BC)</li> <li>• BioArtron Diagnostics Inc. (Burnaby, BC)</li> <li>• Precision NanoSystems Inc. (Vancouver, BC)</li> <li>• Neovasc Inc. (Richmond, BC)</li> <li>• 2020 Accessory Source (Maple Ridge, BC)</li> </ul> <p>Other large firms</p> <ul style="list-style-type: none"> <li>• Baxter</li> <li>• Johnson &amp; Johnson</li> <li>• Stryker Corp</li> <li>• Permobil AB</li> <li>• EXFO Inc.</li> <li>• Abbott Laboratories</li> <li>• Danaher Corporation</li> </ul> <p><b>Aerospace Product and Parts Manufacturing</b></p> <p>BC-based firms<sup>71</sup></p> <ul style="list-style-type: none"> <li>• Avcorp Industries Inc. (Delta, BC)</li> <li>• Anodyne Electronics Manufacturing Corporation (Kelowna, BC)</li> <li>• Indrocorp Technologies Inc. (Vancouver, BC)</li> <li>• Alpine Aerotech Limited Partnership (West Kelowna, BC)</li> <li>• H-S Tool &amp; Parts Inc. (Richmond, BC)</li> <li>• Harwood Custom Composites Inc. (Sidney, BC)</li> <li>• Cascade Aerospace (Abbotsford, BC)</li> <li>• KF Aerospace (Kelowna, BC)</li> <li>• Boeing Vancouver (Vancouver, BC)</li> <li>• Chinook Helicopters (Abbotsford, BC)</li> <li>• Conair Group Inc. (Abbotsford, BC)</li> <li>• Coulson Aviation (Port Alberni, BC)</li> </ul> <p>Other large firms</p> <ul style="list-style-type: none"> <li>• Bombardier, Inc.</li> <li>• Raytheon Technologies Corporation (inc. subsidiary Pratt &amp; Whitney Canada)</li> <li>• The Boeing Company</li> <li>• Honeywell International, Inc.</li> <li>• Calian Technologies Ltd.</li> <li>• Magellan</li> </ul>
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Figure 32. Manufacturing – Key Requirements and Supports



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<sup>71</sup> Trade and Invest British Columbia, "[Aerospace](#)"; [Aerospace Industries Association of Canada](#)

## Transportation and Warehousing

Transportation & Warehousing	
<b>Specific Roles and Opportunities</b>	<ul style="list-style-type: none"> <li>• CPKC Rail connection provides direct access to markets in Canada and the US. Passes through several key business and industrial parks. Provides connection to intermodal yard and Port of Vancouver.</li> <li>• Potential Inland Hub Facility to be developed presents opportunity for increased freight flow (with attention to goods that require special handling, e.g., refrigeration) to and from Maple Ridge.</li> <li>• Cold chain warehousing and distribution plays critical role in agricultural and pharmaceutical supply chains. Cold chain expansion occurring elsewhere in Canada.<sup>72</sup></li> <li>• New technologies may enable Remotely Piloted Aircraft System (RPAS) transportation via drones, which would necessitate air traffic corridors. This mode of transportation is still relatively nascent, however.</li> <li>• Local freight trucking offers an opportunity to create synergies with cold storage. Electric trucking is also an opportunity for Maple Ridge to attract higher skill employment with business activity that aligns with its sustainability values. Electric trucking businesses require skilled drivers, fleet maintenance technicians, charging infrastructure specialists, and logistics analysts. These businesses are well-positioned in Maple Ridge to serve the greater Vancouver region, including south of the Fraser River via the Golden Ears Bridge.</li> </ul>

<sup>72</sup> <https://www.freightwaves.com/news/lineage-logistics-expanding-cold-storage-capacity-in-western-canada>

Transportation & Warehousing	
Specific Description	<ul style="list-style-type: none"> <li>• Scheduled air transportation (NAICS 481110) – This Canadian industry comprises establishments primarily engaged in transporting passengers and/or goods by aircraft, over regular routes and on regular schedules. Establishments in this industry have less flexibility with respect to choice of airports, hours of operation, load factors and similar operational characteristics than do establishments in 4812, Non-scheduled air transportation.</li> <li>• Mainline freight rail transportation (NAICS 482113) – This Canadian industry comprises establishments primarily engaged in operating railways for the transport of goods over a mainline rail network. A mainline rail network is a system that usually comprises one or more trunk lines, into which a network of branch lines feed. The branch lines may be part of the mainline establishment or may be separate establishments of short-haul freight railways.</li> <li>• Inland water transportation (except by ferries) (NAICS 483213) – This Canadian industry comprises establishments primarily engaged in the inland water transportation of freight and passengers, except by ferries.</li> <li>• Deep Sea, Coastal, and Great Lakes Water Transportation (NAICS 483115) – This industry comprises establishments primarily engaged in providing deep sea, coastal, Great Lakes, and St. Lawrence Seaway water transportation.</li> <li>• General freight trucking, local (NAICS 484110) – This industry comprises establishments primarily engaged in local general freight trucking. These establishments primarily provide trucking services within a metropolitan area and its hinterland. Generally, the trips are same-day return.</li> <li>• Specialized freight (except used goods) trucking, local (NAICS 48422) – This industry comprises establishments primarily engaged in providing local trucking services using specialized equipment. Local trucking establishments provide trucking services within a metropolitan area.</li> <li>• Refrigerated warehousing and storage (NAICS 493120) – This Canadian industry comprises establishments primarily engaged in operating refrigerated warehousing and storage facilities. These establishments provide public and contract warehouse and storage services, using equipment designed to keep goods frozen or refrigerated. The services provided include blast freezing, tempering and modified atmosphere storage, in addition to the warehousing services typically provided by establishments in this industry group.</li> </ul>



Transportation & Warehousing	
<b>Current Industry Outlook</b>	<ul style="list-style-type: none"> <li>• LQ score of 1.1, comparable with Mission and Langley.</li> <li>• General Warehousing &amp; Storage in Canada – These services have seen strong growth since 2019, especially amid the pandemic and rise of ecommerce. Technological advancements to track and scan goods to reduce supply chain errors have increased this sector's value and efficiency. Strong economic conditions since the pandemic have increased demand for manufactured goods and therefore capacity for warehousing and storage.<sup>73</sup></li> <li>• Scheduled Air Transportation in Canada – Inflation has recently constrained passenger discretionary income to spend on travel. As a result, some airlines repurposed passenger aircrafts to freight aircrafts to address supply chain issues and carry cargo to make up for lost revenue. High interest rates, however, have weighed on freight volumes.<sup>74</sup></li> <li>• Local Freight Trucking in Canada – Persistent driver shortages are intensified by experienced truck drivers retiring, as well as low wages and poor work-life balance for new hires. Scarcity of drivers is a limiting factor for businesses. Trends toward just-in-time inventory and outsourced distribution bolsters local trucking by increasing shipment frequency. Trucking companies often offer additional services including warehousing, distribution, repacking, storage, and freight forwarding. Trucking companies compete on price, reliability, and speed.<sup>75</sup></li> <li>• fDi Markets shows a high level of investment in the last decade, particularly in Vancouver's water transportation sector. The database shows no investments in Maple Ridge but does show investments (particularly in distribution logistics) in Surrey, Langley, and Kelowna</li> </ul>
<b>Example Companies</b>	<ul style="list-style-type: none"> <li>• Local freight trucking <ul style="list-style-type: none"> <li>◦ West Coast Logistics</li> <li>◦ BC Trucking Company</li> <li>◦ Vancouver Freight Services</li> </ul> </li> <li>• Air freight transportation <ul style="list-style-type: none"> <li>◦ FedEx Express</li> <li>◦ UPS Airlines</li> <li>◦ Morningstar Air Express</li> <li>◦ Skylink Express</li> </ul> </li> <li>• Warehousing and storage <ul style="list-style-type: none"> <li>◦ Lineage Logistics</li> <li>◦ Ever-Cold Storage Ltd</li> <li>◦ SubZero Cold Logistics</li> </ul> </li> </ul>

<sup>73</sup> <https://www.ibisworld.com/canada/industry/general-warehousing-storage/1224/>

<sup>74</sup> <https://www.ibisworld.com/canada/industry/scheduled-air-transportation/1125/>

<sup>75</sup> <https://www.ibisworld.com/canada/industry/local-freight-trucking/1149/>

Transportation & Warehousing	
Key Requirements or Value Chain	<ul style="list-style-type: none"> <li>• Access to rail, aviation, ports, distribution centers (sites), highway access, market access</li> <li>• Utilities infrastructure</li> <li>• Industrial land</li> </ul>

## Professional, Scientific, and Technical Services

Professional, Scientific, and Technical Services	
Specific Roles and Opportunities	<ul style="list-style-type: none"> <li>• <b>Tax base impacts.</b> Opportunity to attract high income, highly educated professionals to grow the local tax base and increase local spending</li> <li>• <b>Real estate.</b> Maple Ridge offers affordable office, industrial space/real estate in greater Vancouver market</li> <li>• <b>Employment pipeline opportunity.</b> Software &amp; IT services is both a high employment and high CapEx sector, making it a valuable business attraction opportunity. Local post-secondary institutions could help support this pipeline.</li> <li>• <b>Diverse investors.</b> FDI to BC in business services and software &amp; IT services came from diverse source markets including the US, India, UK, France, Germany, Netherlands, Switzerland, and Sweden. Diverse investment sources hedges against political uncertainty.</li> <li>• <b>Strong investments.</b> Businesses associated with graphic design, video game design, and other computer design and consulting have synergies with media production, as well as software &amp; IT services that have seen strong FDI growth across Canada from diverse markets.</li> <li>• Strong FDI growth in last 5 years in Vancouver in software &amp; IT services, research &amp; development, business services, and technical support centres. Similar FDI trends also occurred in greater BC and at the national level.</li> <li>• <b>Linkage with media production.</b> There may be some synergies with the media production sector and advertising and public relations services. Attracting business in this sector does not require major infrastructure investment.</li> <li>• <b>Existing development.</b> Maple Ridge has a strong LQ in construction (1.63), showing employment growth resulting from new development in the city and region. This presents an opportunity to establish an architecture and engineering related business cluster and offer more affordable real estate compared to urban Vancouver for offices and employees to locate.</li> </ul>

Professional, Scientific, and Technical Services	
Specific Description	<ul style="list-style-type: none"> <li>• <b>Architectural, Engineering &amp; Related Services</b> (NAICS 5413) – This industry group comprises establishments primarily engaged in providing architectural, engineering and related services, such as structure design, drafting, building inspection, landscape design, surveying and mapping, laboratory and on-site testing, and interior, industrial, graphic and other specialized design.</li> <li>• <b>Management, Scientific &amp; Technical Consulting Services</b> (NAICS 5416) – This industry comprises establishments primarily engaged in providing advice and assistance to other organizations on management issues, such as strategic and organizational planning; financial planning and budgeting; marketing objectives and policies; human resource policies, practices and planning; and production scheduling and control planning. Includes environmental consulting services.</li> <li>• <b>Graphic design services</b> (NAICS 541430) – This Canadian industry comprises establishments primarily engaged in planning, designing and managing the production of visual communication, so as to convey specific messages or concepts, clarify complex information or project visual identities.</li> <li>• <b>Computer systems design and related services</b> (NAICS 54151) – This industry comprises establishments primarily engaged in providing expertise in the field of information technologies through one or more activities, such as writing, modifying, testing and supporting software to meet the needs of a particular customer, including custom video game design and development and Internet webpage development; planning and designing computer systems that integrate hardware, software and communication technologies; on-site management and operation of clients' computer and data processing facilities; providing advice in the field of information technologies; and other professional and technical computer-related services, such as training and support after sales.</li> <li>• <b>Advertising, public relations, and related services</b> (NAICS 5418) – This industry group comprises establishments primarily engaged in creating mass-media advertising or public relation campaigns; placing advertising in media for advertisers or advertising agencies; selling media time or space to advertisers or advertising agencies for media owners; creating and implementing indoor or outdoor display advertising campaigns; creating and implementing direct mail advertising campaigns; delivering (except by mail) advertising materials or samples; creating and implementing specialty advertising campaigns; providing related services, such as sign painting and lettering, welcoming services and window trimming services.</li> </ul>

Professional, Scientific, and Technical Services	
Current Industry Outlook	<ul style="list-style-type: none"> <li>• <b>Engineering Services.</b> Technological adoption is crucial for engineering firms to stay competitive. These firms thrive in both renewable and traditional energy sectors. Smaller firms also thrive by catering to unique regional needs.<sup>76</sup></li> <li>• <b>Management consulting.</b> This sector has seen some revenue growth, but also volatility. The pandemic hampered demand for services because private businesses struggled, then buoyed demand for restructuring, cost cutting, and risk management advice. Most players are in big provinces such as Ontario, Quebec, and BC, and expertise generally reflect the makeup of the economies in which they operate. Nonresidential construction in 2022-2023 spurred demand for services.<sup>77</sup></li> <li>• <b>Environmental Consulting.</b> Environmental consultants are growing more important as environmental awareness spreads and regulation becomes more rigorous. Clients in energy and construction demand remediation, pollution control, and environmental impact services. Growth in construction since 2020 spurred demand for services and provided a boost to revenues.<sup>78</sup></li> <li>• <b>IT Consulting.</b> Growth of cloud-based services, mobile apps, and other software has driven demand for experts in this sector. Increasing private investment in computer and networking infrastructure will likely be key to driving industry performance and growth. Strong demand for IT services and low barriers to entry attract new entrants, though larger operators continue to expand and increase their share of the market.<sup>79</sup></li> </ul>

<sup>76</sup> <https://www.ibisworld.com/canada/industry/engineering-services/1403/>

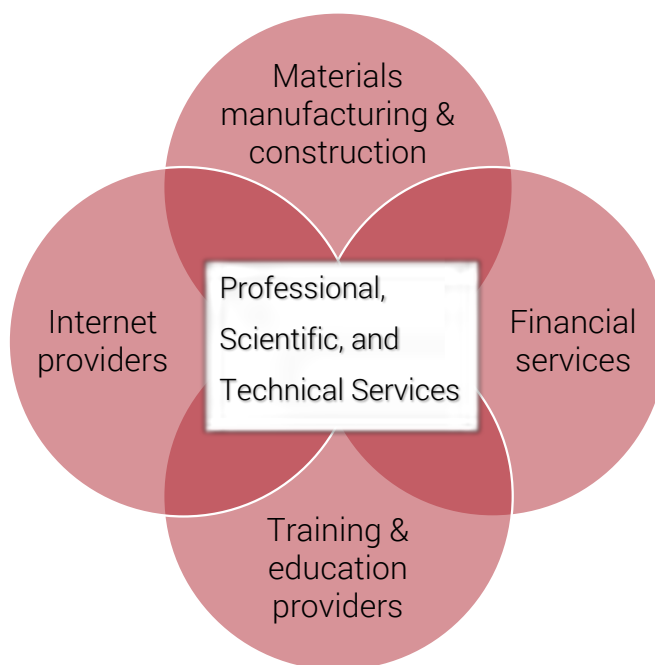
<sup>77</sup> <https://www.ibisworld.com/canada/industry/management-consulting/1421/>

<sup>78</sup> <https://www.ibisworld.com/canada/industry/environmental-consulting/1427/>

<sup>79</sup> <https://www.ibisworld.com/canada/industry/it-consulting/1415/>

Professional, Scientific, and Technical Services	
Example Companies <sup>80</sup>	<ul style="list-style-type: none"> <li>• Advertising &amp; Public Relations               <ul style="list-style-type: none"> <li>◦ Hill+Knowlton</li> </ul> </li> <li>• IT               <ul style="list-style-type: none"> <li>◦ CGI Group</li> <li>◦ Affiliated Computer Services, Inc</li> <li>◦ IBM</li> </ul> </li> <li>• Engineering and project management               <ul style="list-style-type: none"> <li>◦ AMEC</li> </ul> </li> <li>• Consulting               <ul style="list-style-type: none"> <li>◦ Clear HR Consulting</li> <li>◦ Iron Spear</li> <li>◦ ENVIRON Holdings, Inc.</li> <li>◦ Courtyard Group</li> </ul> </li> <li>• Graphic design               <ul style="list-style-type: none"> <li>◦ Digia</li> <li>◦ Steady Studio</li> </ul> </li> </ul>
Key Requirements or Value Chain	<ul style="list-style-type: none"> <li>• Office space</li> <li>• Proximity to clients and markets</li> <li>• Reliable, high-bandwidth internet</li> <li>• Educated workforce with clear school-to-work pipelines. Maple Ridge is currently exploring the feasibility of a post-secondary institution in the area.</li> </ul>

<sup>80</sup> <https://blogs.ubc.ca/gradcareer/2015/08/21/list-of-major-consulting-companies-in-vancouver/>

**Figure 33. Professional, Scientific, and Technical Services – Key Requirements and Supports**

## Life Sciences

The life sciences industry cluster encompasses a diverse range of (sub)sectors, including manufacturing and professional services, sports medicine, and biomedical R&D.

This industry is highly competitive, with well-established hubs in the region. There's been an uptick in life science investments in BC in the last few years (2021-2024), not only in Vancouver but inland at Burnaby and Surrey. The 12 investments related to R&D, manufacturing, sales, and if Maple Ridge were to pursue life sciences as a target sector, it would be playing catch-up in an environment where firms already have access to key resources, talent, and infrastructure.

A major challenge would be the need for specialized lab spaces, which have significantly higher power and niche infrastructure requirements compared to standard office or industrial facilities. Given these constraints, Maple Ridge would need a strategic approach, possibly leveraging niche opportunities within the broader life sciences ecosystem, offering incentives, or developing purpose-built lab spaces to attract firms looking for expansion outside of more saturated markets.



## 8. Strategic Action Plan and Costed Implementation Plan

Based on the findings from previous analyses in this document, this Strategic Action Plan and Costed Implementation Plan presents a set of recommendations organized across five action areas. Each recommendation includes a set of proposed actions. The information presented in full detail in this section is summarized in a table format under the following section, Costed Implementation Plan (CIP) Summary Table. Please note that the cost estimates provided in this section should be reviewed and updated by responsible parties associated with each action; while EBP has provided recommendations for required resources, the City of Maple Ridge is ultimately most knowledgeable about its capabilities and required resources to fulfill many of the core and suggested functions outlined throughout this plan.

### Vision and Operations in City Government

Economic development is a team sport, and Maple Ridge Economic Development is uniquely positioned to serve as coach by coordinating with private and public actors in the local and regional economic development ecosystem and spurring effective strategies. Effective teamwork first requires a shared vision around which activities and processes can be organized.

City government plays a critical role because its departments directly shape the financial, regulatory, and permitting environments, which in turn impact investors' project risks and bottom lines. To improve Maple Ridge's competitiveness in attracting investments, Economic Development should foster a shared economic vision, improve and focus inter-departmental communications, support cohesion among inter-departmental processes, and advocate for a shared set of metrics to track progress toward goals.

#### 1. Lead the City in developing a unified vision for Economic Development and Investment Attraction.

The current focus of City leadership is on retail and restaurant activity in the Town Centre. While supporting businesses and vibrancy in the downtown area is an important priority, the City must balance this with the understanding that industrial and commercial investment in the City's industrial areas and business parks can generate greater employment opportunities and tax revenues. It is critical that City Council, the Senior Leadership Team, and key City departments share an understanding of the economic development vision and goals for the City. Otherwise, economic development initiatives can be pulled in too many or less critical directions.

- a. **Action:** Regularly engage with City Council and the Senior Leadership Team on the importance of industrial and commercial investment for supporting high-value job growth and building a larger tax base. Clearly articulate processes around investment attraction, and teach City leadership about key considerations for businesses when they are making location decisions (e.g., is an available site adequately serviced by utilities and transportation infrastructure, does the site have close access to markets and to other businesses in the value chain). Build consensus among City leadership to better support industrial and commercial activity, including supporting utility and transportation infrastructure development to service promising areas for investment (businesses prefer to invest in areas that are serviced by utilities and transportation infrastructure). Regularly collaborate with City Council, the Senior Leadership Team, and other City departments to actively establish and agree upon a unified vision for the City's economic future.
- This action includes regularly highlighting recent developments in North 256 Industrial, Maple Meadows, Albion, and the Fraser River Industrial Corridor to demonstrate high employment and tax impacts. Specifically engage key partners in the City that engage with infrastructure, zoning, and other processes that impact development.
  - **Timeline:** Immediate and Ongoing. Once every quarter.
  - **Priority Level:** Very High
  - **Responsible Parties:** Economic Development Department, City Council, Senior Leadership Team, Planning and Building Department
  - **Cost Estimate:** Approximately 16 person hours of preparatory time for Economic Development. Approximately 1 hour of preparatory time for members of City Council and SLT. Approximately 1 hour of discussion for all meeting attendees.
  - **KPIs:** Council and the SLT are informed and engaged participants in investment attraction discussions.
- b. **Action:** Ensure that key functions are properly assigned and resourced within the Economic Development Department and other City departments whose work relates to investment attraction. While the Economic Development department will likely have a coordinating or participating role in a wide variety of tasks, it will be important to note where other departments will have a lead role.

Advocate for sufficient resource allocation for the Economic Development Department to fulfill its core functions that help to develop a diversified and thriving economy in Maple Ridge by supporting investment attraction, job growth, and larger tax revenues. Identify where additional resources are needed for the department to fulfill its core functions.

Core functions of the Economic Development Department that require staff, time, and funding, include but are not limited to:

- Supporting business attraction and investment promotion. Core functions include identifying and marketing the City's competitive advantages to prospective investors, communicating with and providing local and market data to site selectors and potential investors, collaborating with regional partners on investment attraction, and assisting potential investors with navigating local permits and regulations (supporting the Concierge Service).
- Business Retention and Expansion. (See the BRE Implementation Plan). Core functions include regular outreach to existing businesses and assisting them to address their needs, as well as connecting businesses to resources and programs.
- Supporting small businesses and entrepreneurship. Core functions include providing business advisory services and facilitating networking opportunities for small businesses, and partnering with local incubator and accelerator programs.
- Supporting workforce development and attracting talent. Core functions include collaborating with post-secondary institutions and workforce development agencies to support skills development in the local workforce that meet the needs of key sectors.
- Supporting infrastructure development. Core functions include close collaboration with the Planning Department to ensure that land use and zoning policies support investment attraction in key growth sectors. Similarly, coordinating on infrastructure development such as utilities and transportation infrastructure, in areas with high investment potential: where the City services infrastructure is where development takes place, where the City provides facilitative zoning or development processes is where you can better see the types of investment you want as a community.
- Supporting Tourism, Arts, and Culture in Maple Ridge. Helping to coordinate initiatives that support visitors to Maple Ridge that stimulate economic activity in the City. Supporting creative industries like Film.
- Promoting regional collaboration and partnerships. Core functions include working together with regional partners in economic development to align on initiatives that support investment to the region, as well as to partner with local business groups like Chambers.
- Facilitating strategic economic planning to support policy development. Core functions include conducting market research and economic analyses to inform the development of municipal policies. Similarly, to engage decision-makers at

other levels of government to advocate for policy that supports economic development.

- Monitoring and Reporting Key Economic Indicators. Core functions include monitoring key economic indicators that measure the economic health of the municipality and region, and communicating findings to City leadership and staff. (See Monitoring and Evaluation)
- **Timeline:** Immediate and Ongoing (Within 12 months and Ongoing)
- **Priority Level:** Very High
- **Responsible Parties:** Economic Development Department, City Council, Senior Leadership Team, Planning and Building Department
- **Cost Estimate:** These are some of the core functions of the Economic Development Department staff as they relate to investment attraction.
- **KPIs:** Economic Development Department has the necessary staff and tools to fulfill core functions. Ability and capacity to collect and track metrics in the Monitoring and Evaluation section (see Monitoring and Evaluation Metrics).

## 2. Prioritize focused and targeted interdepartmental communications.

There is a wide array of parallel initiatives happening within the City related to land use and zoning, infrastructure, business licensing, and planning, many of which have implications for the Economic Development Department and investment attraction efforts to support a diverse and thriving economy. (Linkages between Community Improvement Plan, Master Plans, and Investment Attraction & Economic Development Initiatives)

- a. **Action:** Establish clear channels for interdepartmental communication, which may include regular interdepartmental meetings on clear topics including challenges and getting alignment on initiatives, as well as developing a more standardized interdepartmental reporting structure.
  - **Timeline:** Immediate and Ongoing. At a regular cadence to be determined by the Director of Economic Development.
  - **Priority Level:** Very High
  - **Responsible Parties:** Economic Development Department & Other City Departments
  - **Cost Estimate:** Economic Development to allocate at least 4 to 8 hours per month of staff time to engage with other City departments on topics of economic development and investment attraction.
  - **KPIs:** City staff are informed and engaged participants in investment attraction discussions.

- b. **Action:** Develop standards for interdepartmental information and data sharing. Map out departments and their data responsibilities (collection, organization, analysis, dissemination). Using a flowchart, map the flow of information among stakeholders (e.g., platforms used, timelines), and note high-impact inefficiencies, redundancies, bottlenecks and missed opportunities. Work with department stakeholders to mutually agree on the most pressing challenges and shepherd efforts for a mutually beneficial solution.
- **Timeline:** Immediate and Ongoing. Engage in an interdepartmental workshop 2 times per year.
  - **Priority Level:** High
  - **Responsible Parties:** Economic Development Department & Other City Departments
  - **Cost Estimate:** Approximately 4 to 8 hours of preparatory time for participants to:
    - 1) Inventory current data resources and processes
    - 2) Identify gaps in departmental needs
    - 3) Consider opportunities for addressing gaps in needs
- 2 hours per participant for meeting or workshop.
- **KPIs:** improved ability to share data between departments, facilitate or enhance collaboration between departments

### 3. Continue to streamline processes to improve permitting and regulatory timelines and user experiences.

The City of Maple Ridge currently has a regional reputation for being slow and inconsistent when it comes to processing permits and addressing regulatory matters for developers and investing businesses. The City is currently in the process of developing a concierge service for developers, in which a City representative works directly with applicants who are seeking approvals, helping the applicant to understand the process and key requirements. It is anticipated that the concierge program will operate through the Planning Department with close connection to the Economic Development Department. Additionally, the City is conducting a review of the development application process to identify and remove redundancies to streamline approvals.

- a. **Action:** Prioritize resources to adequately support the City's Concierge Program. This will most likely include some of the following strategies:

- Interdepartmental Coordination: Involved City departments (e.g., Planning, Economic Development, Business Services, Zoning, etc.) must align on the goals of the program and share a clear understanding of the scope of the Concierge Program (e.g., will the Concierge Program support all businesses, or only businesses in targeted sectors?). Hold regular interdepartmental meetings and communications on the Concierge Program.
- Technical Support: Support the internal use of the CRM system, Dynamics 365. The CRM system is a key tool that can help to track business and industry contacts, how frequently and when communications happen with these contacts, and other important information to help organize communications and marketing for investment attraction purposes. Similar to the CRM system, the City should also support the internal use of an online business portal for businesses to track progress on permits, licenses, and other regulatory steps.
- Funding: Ensure that there is sufficient funding to support the success of the program, which may include exploring public-private partnerships or applying for grant funding. Funding sources that may support the Concierge Program include but are not limited to:
  - Community Economic Development and Diversification in British Columbia<sup>81</sup>
  - Union of BC Municipalities Funding Programs<sup>82</sup>
  - Economic Development Funding from the Province of British Columbia<sup>83</sup>
- Marketing and Communications: Promote the program through marketing and outreach efforts, and with the help of local and regional partners such as Chambers and the Downtown Business Improvement Area.
- Implement feedback mechanisms with businesses utilizing the Concierge Program to continuously improve the service.
- **Timeline**: 1 year and Ongoing
- **Priority Level**: High
- **Responsible Parties**: City Council, Senior Leadership Team, Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department, Human Resources Department

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<sup>81</sup> <https://www.canada.ca/en/pacific-economic-development/services/funding/community-economic-development-diversification.html>

<sup>82</sup> <https://www.ubcm.ca/funding-programs>

<sup>83</sup> <https://www2.gov.bc.ca/gov/content/funding?>



- **Cost Estimate:** To be determined by responsible parties. The cost to implement this action is equal to the cost of one of more staff positions to serve as Concierge. Please note that costs will vary according to the assignment of and roles of the Concierge.
  - **KPIs:** Number of Business Inquiries, Average time to process business permits, number of businesses successfully assisted
- b. **Action:** Regularly engage with local businesses and industry to understand challenges and opportunities for improvement when it comes to improving the user experience of permitting and regulatory processes. (See BRE Implementation Plan)

#### 4. Adopt Data Collection and Dissemination Best Practices

The City should continue to build upon and improve current internal data collection, storage, and dissemination capabilities. This should include the adoption of economic data best practices.

##### The Importance of NAICS Codes

Throughout North America, it is the best practice of businesses, researchers, and municipal, provincial, state, and federal governments to use North American Industry Classification System (NAICS) codes, which enable easier data collection and more useful data analysis of industry and business data. NAICS codes are a standardized business categorization system that allow for consistent data collection and help governments and businesses to make better decisions. The current lack of NAICS at the City is a significant limiting factor which introduces undue burdens for data users, complicates the City's ability to understand Maple Ridge's employment and industry makeups, and hinders the City's ability to conduct analyses and disseminate key information to businesses that may be interested in investing in Maple Ridge. As noted, the use of NAICS is a best practice for municipalities and is a valuable tool for decision making – a lack of NAICS adoption puts Maple Ridge behind its peers.

- a. **Action:** Advocate for the adoption of NAICS codes throughout the City and communicate the value of using NAICS codes.<sup>84</sup> Ensure that City leadership and City departments that work with any kind of business-related information (e.g., Economic Development, Business Licensing, Planning, Zoning, etc.) understand the value of using NAICS. Understand where there may be resistance within the City to the use of NAICS codes and target communications to these parties. Benefits to using NAICS codes include:

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<sup>84</sup> <https://www.naics.com/what-is-a-naics-code/>

- Ability to make direct comparisons with peers across North America and,
- Supports sector-based economic analysis to identify strengths, gaps and investment attraction opportunities.
- **Timeline:** 6 months and Ongoing
- **Priority Level:** High
- **Responsible Parties:** Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department
- **Cost Estimate:** Approximately 4-8 individual staff hours of preparatory time for the Economic Development Department and other City Departments to understand:

- 1) The importance of NAICS Codes
- 2) How to work with NAICS Codes
- 3) Where within City processes NAICS Codes should be used

As is required, time to engage with City Council and SLT on the importance and use of NAICS Codes. This would require an estimated 8 hours of staff time from Economic Development. Refined cost and staff time estimates to be determined by responsible parties.

- **KPIs:**
  - Number of one-on-one meetings with key stakeholders – meetings with departments or leadership teams to address concerns and encourage adoption;
  - Percentage of relevant city departments using NAICS – Track the adoption rate across different City departments.

- b. **Action:** Institute the use of NAICS codes throughout the City. Building on the previous action, work together with relevant City departments to ensure standardized data collection and maintenance of NAICS codes. Ensure that Business Licensing collects NAICS codes of new businesses in the City. This will enable metric tracking and benchmarking of City trends vis-a-vis the Vancouver Metro region, province, and country.

- **Timeline:** 1 year and Ongoing
- **Priority Level:** High
- **Responsible Parties:** Economic Development Department, Planning and Building Department, and Bylaw, Licensing and Community Safety Department
- **Cost Estimate:** Dependent upon the findings of Action 4a. Costs may include the resources needed for collecting industry data from existing businesses, resources to regularly collect industry data from future businesses, and resources for coordination with provincial offices (e.g., Provincial Assessor's Office or the

Ministry of Revenue) or other partners who may also collect or use NAICS codes for Maple Ridge businesses.

○ **KPIs:**

- Use of NAICS in economic reports & analyses – Track how often NAICS-based data is used in sector analysis and investment strategies;
- Percentage of new business licenses issued with NAICS codes – Ensure all new business licenses include NAICS classifications.
- Percentage of existing business licenses re-classified using NAICS

c. **Action:** Continue to collect and organize data that is relevant for site selectors and other location professionals.<sup>85</sup> Some data may be collected through a BRE Survey. (See BRE Implementation Plan). Maintain organized sourcing information for all collected data and include citations when disseminating data. Data should be updated on at least an annual basis. See “Monitoring and Evaluation” in this document for a list of metrics.

- **Timeline:** 6 months and Ongoing (collect and update data on an annual or semiannual basis)
- **Priority Level:** High
- **Responsible Parties:** Economic Development Department
- **Cost Estimate:** Core function of Economic Development Department. Updating Monitoring and Evaluation metrics will take a minimum of 16 hours of staff time.
- **KPIs:** Improved ability to provide data to interested investors or site selectors. Enhanced response time to site selector requests for information.

d. **Action:** Continue to prioritize maintenance of City datasets and outward facing tools that support investment attraction and use interdepartmental data. Tools to maintain include the GIS Site Selector Tool, which contains publicly facing data on zoning, available land and sites, as well as demographics and socioeconomic data. A potential investor will look at this tool early on in their consideration of Maple Ridge, so it is important for the tool to have up-to-date information.

- **Timeline:** Immediate and Ongoing (update 2-4 times annually)
- **Priority Level:** High
- **Responsible Parties:** Economic Development Department, Information Technology Department, Planning and Building Department

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<sup>85</sup> <https://www.areadevelopment.com/corporate-consultants-survey-results/q1-2024/20th-annual-consultants-survey-clients-prioritize-access-to-skilled-labour-responsive-state-local-government.shtml>

- **Cost Estimate:** Cost of hosting for different platforms or data sources. Approximately 4 to 8 hours of work hours to update data within each individual outward facing tool. Refined cost and staff time estimates to be determined by responsible parties.
- **KPIs:** Number of website visits to publicly available online tools, length of time to provide data to inquiring investors

## 5. Advocate for Policy that Supports Economic Development in Maple Ridge

- a. **Action:** As a core function of the Economic Development Department, continue to determine how to enhance competitiveness within the region, and coordinate efforts at the regional, provincial, and federal levels to better support investment in targeted sectors. Ensure that policy advocacy efforts align with other City strategies, including the Maple Ridge Intergovernmental Strategy. Efforts may involve the following considerations and strategies:
  - **Goals:** Decide on the goals or desired outcomes for policy advocacy. For example, this may include adopting a targeted incentive program at the provincial level, or a different policy that may help to lower the cost of doing business and boost global competitiveness.
  - **Decide where, how, and who:** Decide the venues at which the City should engage in policy advocacy, as well as where, how, and who to engage in policy advocacy. This will involve regular engagements with partners in the regional economic development ecosystem:
    - Quarterly or semiannual meetings with the Katzie and Kwantlen First Nation communities to discuss investment attraction efforts, economic development goals and needs, and opportunities for collaboration (Please see the City of Maple Ridge Intergovernmental Strategy).
    - Annual or semiannual meetings with Union of BC Municipalities (UBCM), Local Government Association of British Columbia (LGMA), Ministry of Jobs, Economic Development, and Innovation (JEDI), PacifiCan, and utilities providers such as BC Hydro and FORTIS.
    - Quarterly or semiannual interest holders' meeting to discuss and develop white papers about the health of the economic development ecosystem and competitive climate.
  - **Timeline:** Immediate and Ongoing (quarterly, semiannual, or annual meetings with Economic Development partners, as needed)
  - **Priority Level:** High

- **Responsible Parties:** Economic Development Department, Manager of Intergovernmental Affairs
- **Cost Estimate:** Approximately 8 work hours of scheduling and preparatory time per engagement. 1 to 2 hours of work hours per engagement.
- **KPIs:** Number of annual meetings with regional economic development partners, Reported business satisfaction over time

## Branding and Marketing

Marketing and branding are key first impressions for potential investors. Clear, compelling materials and online resources make engagement easy, showcasing the City's strengths and investment opportunities. A consistent, visually strong, and data-driven approach builds trust and positions the City as a competitive, business-friendly destination.

### 6. Develop updated marketing materials for investment attraction in key sectors

- a. **Action:** Continue to define clear value propositions for the City and for its Key Sectors. In order for the City to successfully market itself to potential investors, it is imperative that it communicate clear and concise value propositions as it relates to both the City of Maple Ridge, and its key sectors for investment attraction. Based off of the analyses in this strategy, we recommend that value propositions include data and details on the following (please see report sections "Opportunity Match Profiles", "SWOT Analysis", "Physical Asset Inventory", and "Competitive Benchmarking Analysis"):
- Market Access, and rough costs of transportation to move goods
  - Multimodal Transportation Infrastructure that support business activity, trade, and market access
  - Population and Socioeconomic Factors
  - Workforce, including educational attainment and labour costs
  - Business Climate, including taxation, millage rates, and comparisons to other jurisdictions
  - **Timeline:** Immediate (6 months). Revisit on an annual basis.
  - **Priority Level:** High
  - **Responsible Parties:** Economic Development Department
  - **Cost Estimate:** An estimated 40-60 hours of staff time involving developing value proposition statements for key sectors and getting buy-in from Council and other City Departments as may be needed or required.
  - **KPIs:** Number of business inquiries in target sectors

- b. **Action:** Develop updated marketing materials for target sectors. Consider different audiences, and prioritize clear, concise, targeted communications for specific audiences. Baseline materials can be developed internally, while outside assistance may be needed for more detailed materials.
- Investment Attraction brochures and investor guides: brochures should include information on the City's key value propositions for why a business should invest in the City, including information on key sectors and common requirements such as market access, infrastructure, population, workforce, business climate, and available incentives and business supports.
  - One-pagers on each target sector communicating why a business should invest in Maple Ridge: to include the City's key value propositions for each target sector, key economic statistics, other key sectors, and business advantages to investing in Maple Ridge.
  - Focused Reports on Key Sectors: Focused reports should include information on workforce availability, growth potential of the sector, and highlight recent investments or success stories.
  - **Timeline:** 6-9 months
  - **Priority Level:** High
  - **Responsible Parties:** Economic Development Department
  - **Cost Estimate:** The cost for this action may vary. We estimate that it will require at least 40 hours of Economic Development Department staff time per sector to develop content, and at least 40 hours of graphic design work per sector to develop updated marketing materials for each sector.<sup>86</sup> Refined staff time and cost estimates to be refined by responsible parties.
  - **KPIs:** Number of businesses that marketing materials are distributed to, number of business inquiries

## 7. Improve wayfinding on the City of Maple Ridge Website

The City should enhance the website's user experience to make business services and investment attraction resources easier to find. Clear navigation, intuitive design, and improved search functionality will help local businesses build awareness of and access services and

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<sup>86</sup> Guidance on costs for different types of marketing initiatives and strategies can be found here: <https://emulent.com/blog/marketing-cost-cheat-sheet-how-much-marketing-costs-if-done-correctly/>



support. Streamlining content and reducing clicks to key information will ensure a smoother experience, making it easier for investors and business owners to engage with the City.

- a. **Action:** Conduct user testing and gather feedback and recommendations from businesses about ease of use. Similarly, the City should solicit feedback on whether or not there are types of information that users would like to see available on the website, that currently are not available.
  - o **Timeline:** 1 year
  - o **Priority Level:** Medium
  - o **Responsible Parties:** Economic Development Department, Information Technology Department, Bylaw, Licensing and Community Safety (Business Licensing)
  - o **Cost Estimate:** Approximately 40 hours for workshop design, scheduling, and results.
  - o **KPIs:** Businesses are engaged and interested in participating in user testing. Businesses speak freely about website user experience and provide constructive recommendations.
- b. **Action:** Implement recommendations from user testing. While changes to the user experience of the website should reflect the needs of local businesses, residents, and potential investors, the following are suggestions to consider from our experience:
  - o Consolidate some of the options in the “Build & Do Business” menu, such as those related to Business Resources and Licensing, Development, and Filming. The drop-down menu for each main section (e.g., “Resident Services”, “Parks & Recreation”, “Community Safety”, “Build & Do Business”, “Your Government”, “Explore Maple Ridge”), should have fewer options to cut down on clutter and redundancies.
  - o Consider incorporating a comprehensive nesting dropdown list. Menu options currently only show up once the user has clicked through to a new page, there is no clear or complete map of pages up front, which makes the website more difficult for unfamiliar users to navigate.
  - o **Timeline:** 2 years
  - o **Priority Level:** Medium
  - o **Responsible Parties:** Economic Development Department, Information Technology Department, Bylaw, Licensing and Community Safety (Business Licensing)
  - o **Cost Estimates:** Dependent on the findings of Action 7a. This could include work time from Economic Development and Other Involved Departments to clarify or

update information relevant to businesses, and could involve IT work time to update the City website. Refined staff time and cost estimates are to be refined by responsible parties.

- **KPIs:** Visitors to City Website (specifically “Build & Do Business”), Business Licence Applications, Business Inquiries

## 8. Develop dedicated web content for investment attraction

The City should consider developing either a dedicated micro website for investment attraction, or a dedicated section on the City’s website that is easy for potential investors and site selectors to navigate. A micro website for investment attraction can be a powerful marketing tool, acting as a “one stop shop” for information pertinent to site selectors and potential investors in the City’s key sectors. Branding should remain consistent across all platforms.

On the City’s website, Economic Development is currently instead nested under the directory, with its key functions nested under different “Build & Do Business” sections. While there is an “Invest in Maple Ridge” section under “Build & Do Business”, it has minimal information on the City’s value proposition and key sectors and, aside from the Site Selector Tool, does not provide helpful links or callouts of key information that a site selector or interested investor may be interested in, such as figures on employment composition, market access, transportation infrastructure, or news releases highlighting recent investments.

### Components of an Effective Investment Attraction Website

1. Information on Target Sectors, including existing businesses in these sectors, and the City’s value proposition for these sectors
2. Data pertinent to site selectors, including Workforce and Demographics
3. Information pertinent to an investing business, including licensing information and available land
4. Lifestyle information, including Cost of Living, Educational Institutions, Healthcare, Housing, Recreation, and Arts and Culture

- a. **Action:** Determine what content should be highlighted on the investment attraction micro site. This should include determining which existing investment attraction content should be migrated from the City website. Content should be tailored to its key audience segments (including site selectors for target sectors and business looking to expand to Maple Ridge). Much of the content developed for updated Branding and Marketing recommendations can be included on the micro site. Important information for existing businesses in Maple Ridge should remain on the City website.
  - **Timeline:** 1-2 years
  - **Priority Level:** Medium

- **Responsible Parties:** Economic Development Department, Information Technology Department
  - **Cost Estimate:** Approximately 40 to 80 work hours for reviewing web content, project development and management, and perhaps developing an RFP.
  - **KPIs:** A clear plan for content to highlight on the investment attraction micro site, Number of interested vendors for an RFP to develop micro site
- b. **Action:** Contract a vendor to develop and launch a new investment attraction micro site. Leverage services from Invest Vancouver, PacificCan, Trade and Invest BC, and federal resources to assist. Examples of effective investment attraction websites include:
- Richmond, BC: [Business in Richmond](#)
  - Chilliwack, BC: [Business in Chilliwack](#)
  - Surrey, BC: [Invest Surrey](#)
  - Fort Saskatchewan, AB: [Invest Fort Sask](#)
  - Edmonton, AB: [Edmonton Global](#)

Once the investment attraction website is launched, communicate with regional economic development partners like Invest Vancouver to ensure that their sites link to the Maple Ridge Investment Attraction site.

- **Timeline:** 2-3 years. The contracted working period for the development of a microsite to take approximately 3-6 months.
- **Priority Level:** Medium
- **Responsible Parties:** Economic Development Department, Information Technology Department, Invest Vancouver and other regional partners
- **Cost Estimate:** Approximately \$70,000 CAD. Potential to receive matching funds from grants, such as those issued through the CanExport Community Investments Program from Global Affairs Canada.<sup>87</sup>
- **KPIs:** Number of visitors to investment attraction micro site, business inquiries

## Networking and Communications

Strategic industry outreach and collaboration are key to attracting investment and strengthening Maple Ridge's economy. By prioritizing networking, engaging with local businesses, real estate developers, and regional partners, and fostering relationships with First Nations and advocacy

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<sup>87</sup> EBP developed this estimate based on a search of relevant RFPs issued by comparable municipalities through online bid portals such as CanadaBuys. A reference RFP for Microsite Development issued by the City of Coquitlam, BC, disclosed that half of the project's funding would be from a CanExport grant, <https://www.coquitlam.ca/DocumentCenter/View/13836/24-108-RFP---Investment-Attraction-Microsite?bidId=437>

groups, the City can align on economic goals and expand investment opportunities. Proactive engagement with property owners and BC Assessment will further support a business-friendly environment.

## 9. Prioritize Industry Networking

Leverage industry networking opportunities to improve Maple Ridge's presence and bolster the City's preparedness for attracting these sectors. Attending industry conferences is an investment for the City in positioning itself for economic growth, as the knowledge, relationships, and business opportunities gained from attendance can directly lead to new investments and job creation.

### Best Practices for making the most of conference attendance:

Be clear on the goals for attendance. Typically, the goal of attending an industry conference is to better understand the industry (current and future trends, market shifts and emerging sectors, key requirements for businesses in the industry, etc.), as well as to market the City and engage with potential partners and prospective investors. Similarly, conference attendance signals to potential investors that the City is proactive and engaged on investment attraction and economic development.

Engage with Neighbouring Municipalities and Regional Partners on attendance. Know if other economic development organizations in the region will be in attendance and coordinate with them.

Regularly follow up with connections after the conference: (1) organize follow up notes and action items to engage with different connections (e.g., potential investors, industry associations, developers, participants in the regional economic development ecosystem, peer jurisdictions, etc.) made at each conference (2) keep track of partners or potential investors engaged at each conference in a CRM, such as Dynamics 365. Include the responsible party from the City that will maintain contact with each connection.

- a. **Action:** Prioritize attendance at key conferences within the City's targeted industries, and regularly follow up with business contacts made at conferences. (See Select Conferences in Canada by Target Sector Table below). Follow the best practices for conference attendance provided in the above box. From connections made at conferences, prioritize follow-up communications and ensure that there is a regular touchpoint with the connection (e.g., on an annual or quarterly basis, as needed). Bring business connections through the steps of targeting investment, starting with a prospect, to lead, qualified lead, and over time, an investor. Utilize a CRM system such as Dynamics 365 to track communications and marketing efforts with connections and to build up an internal list of potential targets and leads.
  - o **Timeline:** 2 years

- **Priority Level:** High
- **Responsible Parties:** Economic Development Department, Intergovernmental Affairs, Invest Vancouver and other regional partners
- **Cost Estimate:** The cost of this action will vary depending on location, duration, registration cost, and needed preparatory and post-attendance time by conference. Refined staff time and cost estimates are to be refined by the Economic Development Department and the Direct of Economic Development. The cost of conference attendance may include:
  - Hotels, meals, travel, and staff time associated with conference attendance.
  - Costs associated with planning, registration, preparing for attendance, etc. (see table)
  - Costs associated with follow up of conference attendance, such as organizing notes, information, entering contacts into the CRM system, and follow up communications.
- **KPIs:** Number of conferences attended per year (by sector), number of new connections made per year, number of connections that the City actively communicates with each year (aim to communicate with each connection at least once a year), track the number of potential investors at each level of lead generation: prospect, lead, qualified lead, investor.

Table 34. Select Conferences by Target Sector

Target Sector	Conference Name	Location	Description	Time of Year	Registration Cost
Film	<a href="#">FactualWEST</a>	Vancouver	"provides professional and project development opportunity for creators, producers and crew"	November 1-2 days	Unclear. \$25 Membership fee. Includes free admission to most events and workshops
Film	<a href="#">ShowCanada</a>	Victoria	Movie Theatre Association of Canada's conference for	May 3 days	\$1,428 -\$1,652

Target Sector	Conference Name	Location	Description	Time of Year	Registration Cost
			all sectors the film industry in Canada and US		Registration and trade show registration
Food Manufacturing	<a href="#">Grocery &amp; Specialty Food (GSF) West</a>	Vancouver	"Features top retailers, manufacturers, and suppliers ranging from food services and technology to equipment, finance, and beyond."	April 2 days	<b>\$519</b>  for 2-day trade show pass and 1-day conference pass
Food Manufacturing	<a href="#">Thrive Conference</a>	Vancouver	BC Food & Beverage's conference; includes manufacturers, processors, suppliers, consultants	May 1 day	<b>\$279</b>  Access to all speaker events, lunch, reception, and networking
Aerospace Manufacturing	<a href="#">Aerospace, Defence and Security Expo (ADSE)</a>	Abbotsford	Has "numerous B2B and networking opportunities"	August 2 days	<b>\$595</b> "Government Registration"
Manufacturing (general)	<a href="#">CME BC Manufacturing Conference</a>	Burnaby	Canadian Manufacturers & Exporters Conference	March 1 day	<b>Unclear.</b> (sold out, inactive registration link)
Medical Equipment Manufacturing	<a href="#">MedTech Conference</a>	Various (e.g., San Diego, Toronto)	Focus on tech and innovation	October 3 days	<b>\$1,800</b> for non-member govt/non-profit
Transportation & Warehousing	<a href="#">Transportation Conference</a>	Vancouver	BC-specific transportation sector	January 2 days (1 virtual)	<b>Unclear</b> (registration link inactive)



Note: This is not a comprehensive list of conferences – the City should also keep track of and attend local, low cost conferences or shows in targeted sectors, such as the ICSC in Whistler and Farm Show in Abbotsford. In addition to this table, the City should eventually focus efforts on attending international conferences. While this is likely 4-5 years out, international conferences should be on the City's radar.

## 10. Continue to Engage with Local Businesses & Partners in the Regional Economic Development Ecosystem

When it comes to attracting investment, it is key for the City to remember that it fits within a broader ecosystem of industry partners.

a. **Action:** Identify and engage with key actors and ambassadors within targeted sectors.

Work with these parties to help them to:

- Identify gaps in value chain or opportunities to enhance competitiveness
- Hone message to value proposition
- Identify further, addressable needs and shortcomings in infrastructure, policy, or other areas
- **Timeline:** Immediate and Ongoing
- **Priority Level:** Very High
- **Responsible Parties:** Economic Development Department
- **Cost Estimate:** This is a core function of the Economic Development Department. Approximately 10% of the Department's time should be dedicated to engaging with business and organizational partners within the targeted sectors.
- **KPIs:** Number of entries to CRM representing network of key actors and ambassadors (ensuring coverage across all targeted sectors)

b. **Action:** Continue to improve engagement with local business. Continue an on-going dialogue with existing companies in the City of Maple Ridge in both one-on-one and focus group formats. Businesses generally prefer that the City approach them and come to them than vice versa. The goal of this ongoing dialogue is to share experiences, support mutual understanding, identify areas for improving the local business environment, and to identify potential roles of industry and local businesses in marketing and outreach. (See BRE Implementation Plan)

- **Timeline:** Immediate and Ongoing
- **Priority Level:** Very High
- **Responsible Parties:** Economic Development Department
- Please see BRE implementation plan for further details

- c. **Action:** Coordinate with the local real estate and development community. Establish and maintain an understanding of who the developers are in the region, what they do and what they look for in sites and in a community. Consider this when developing planning efforts and overall economic development goals.
- Coordinate with the National Association of Industrial and Office Properties (NAIOP) to connect with developers who can assist the City to better understand what attracts and what deters developers.
  - Coordination with NAIOP also opens the opportunity to potentially meet developers who are interested in developing land in the City.
  - For development and real estate community contacts that are interested in developing land in the City, organize their contact information in a centralized contact listing and consider the use of a Dynamics 365 drip marketing campaign to automate marketing outreach to these parties.<sup>88</sup>
  - **Timeline:** 1 year and ongoing
  - **Priority Level:** High
  - **Responsible Parties:** Economic Development Department
  - **Cost Estimate:** At least 8 hours of Economic Development Staff time per quarter.
  - **KPIs:** Number of current contacts in the development and real estate communities, Development permits, Construction starts
- d. **Action:** Engage with local property owners in the downtown area to communicate and seek alignment on community goals for economic and business development. This involves better understanding who the property owners are (for example, does the owner actively manage their property, or is the property held in a trust? Are the owners active members of the Maple Ridge community?) Be mindful of real estate values that are likely to rise in the downtown area and retail gentrification. Collect ownership information, contact information, and begin to develop relationships with
- **Timeline:** Immediate: Collect ownership and contact information of local property owners in the downtown area (6-9 months). Medium Term: Develop relationships with local property owners and work to establish alignment on community goals for economic and business development (2-3 years)
  - **Priority Level:** Medium
  - **Responsible Parties:** Economic Development Department, Bylaw, Licensing and Community Safety (Business Licensing)

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<sup>88</sup> <https://community.dynamics.com/blogs/post/?postid=51e41be8-dbafe4e41-92da-fb831e2afa11>

- **Cost Estimate:** Dependent upon the availability of information, at least 16 hours of Economic Development Staff time to collect ownership and contact information. At least 2-4 hours of Economic Development Staff time per quarter for relationship building.
  - **KPIs:** CRM entries for downtown property owners, number of conversations per year with each property owner
- e. **Action:** Inventory and continue to foster relationships with institutional partners in the regional economic development ecosystem. As a city closely connected to a larger region, it is important that Maple Ridge understand who its partners are in the regional economic development ecosystem, leverage these relationships, and communicate to businesses and potential investors that locating in Maple Ridge provides them with access to a variety of supportive regional partners. The City should develop key messages and advocacy points for different types of regional partners and create consistent messaging that various City staff can use when engaging with regional partners. Regional partners with which to foster relationships include:
- Post-secondary institutions such as the University of British Columbia (UBC), British Columbia Institute of Technology (BCIT), Simon Fraser University (SFU), University of the Fraser Valley (UFV), and Kwantlen Polytechnic University (KPU).
  - Innovation centres and programs including but not limited to Innovate BC,<sup>89</sup> the Regional Innovation Ecosystems in British Columbia,<sup>90</sup> the BC Centre for Agritech Innovation at SFU<sup>91</sup>, the BC Food Hub Network<sup>92</sup>, Food and Beverage Innovation Centre (FBIC), DIGITAL Technology Supercluster in Vancouver<sup>93</sup>, the Zero Emissions Innovation Centre<sup>94</sup>, British Columbia Centre for Innovation and Clean Energy (CICE)<sup>95</sup>, Creative BC<sup>96</sup>, and DigiBC<sup>97</sup>.
  - Business advocacy organizations, such as the Ridge Meadows Chamber of Commerce and BC Chamber of Commerce.
  - **Timeline:** Immediate and Ongoing
  - **Priority Level:** High

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<sup>89</sup> <https://www.innovatebc.ca/>

<sup>90</sup> <https://www.canada.ca/en/pacific-economic-development/services/funding/regional-innovation-ecosystems.html>

<sup>91</sup> <https://www.sfu.ca/agritech-innovation.html>

<sup>92</sup> <https://www2.gov.bc.ca/gov/content/industry/agriculture-seafood/innovation-and-technology>

<sup>93</sup> <https://www.digitalsupercluster.ca/>

<sup>94</sup> <https://zeic.ca/>

<sup>95</sup> <https://cice.ca/>

<sup>96</sup> <https://creativebc.com/>

<sup>97</sup> <https://digibc.org/>

- **Responsible Parties:** Economic Development Department, Intergovernmental Affairs
  - **Cost Estimate:** This is a core function of the Economic Development Department – to be determined by responsible parties. At a minimum, allocate 16 hours per quarter for fostering relationships with various institutional partners.
  - **KPIs:** Number of engagements with regional partners
- f. **Action:** Continue to regularly engage with Kwantlen and Katzie First Nations partners on regional economic development. The Kwantlen and Katzie First Nations communities are key partners and contribute to workforce development and industry innovation. Regularly meet with these communities for mutual understanding of one another's key economic development goals and priorities, to discuss current economic development plans and initiatives, and to identify opportunities for collaboration. Focus on holding less formal and smaller, project-oriented engagements that prioritize being present and building trust. Communications should always include the Intergovernmental Affairs Manager. (Please see the Maple Ridge Intergovernmental Strategy for more guidance).
- **Timeline:** Immediate and Ongoing
  - **Responsible Parties:** Intergovernmental Affairs, Economic Development Department
  - **KPIs:** Regular communications and engagements, number of collaborative initiatives
- g. **Action:** Continue to regularly engage with Kwantlen and Katzie First Nations partners on regional economic development. The Kwantlen and Katzie First Nations communities are key partners and contribute to workforce development and industry innovation. Regularly meet with these communities for mutual understanding of one another's key economic development goals and priorities, to discuss current economic development plans and initiatives, and to identify opportunities for collaboration. Focus on holding less formal and smaller, project-oriented engagements that prioritize being present and building trust. Communications should always include the Intergovernmental Affairs Manager. (Please see the Maple Ridge Intergovernmental Strategy for more guidance).
- **Timeline:** Immediate and Ongoing
  - **Priority Level:** Very High
  - **Responsible Parties:** Intergovernmental Affairs, Economic Development Department
  - **Cost Estimate:** To be determined by Intergovernmental Affairs in collaboration with Economic Development Department. EBP recommends a minimum of 4-8

hours of Economic Development staff time per quarter for engagement on topics of economic development.

- **KPIs:** Regular communications and engagements, number of collaborative initiatives

## Community Outreach & Public Relations

Whether accurate or exaggerated, community perceptions – including perceptions of safety, cleanliness, and other quality of life factors in some areas around the City – directly impact the economic success of businesses in Maple Ridge. On one hand, perceptions of safety may impact residents' decisions on whether or not to visit certain areas of the City, meaning that they may not patron businesses in those areas. On the other hand, perceptions of safety may impact local businesses' decisions about their hours of operation, or about how much to invest in physical aspects of their business out of fear of repeated vandalism or theft. Similarly, potential investors may choose to locate elsewhere due to negative community perceptions. Due to the impact that community perceptions have on business activity and economic development in Maple Ridge, the City should prioritize the development of strategies that build trust and positively influence residents' and businesses' perceptions of quality of life factors in the community.

### 11. Proactively Influence Positive Community Perceptions

- a. **Action:** Coordinate with or collect data from regional or provincial data partners to maintain accurate crime data on the City and region. Readily having this data available may assist in countering false narratives of community safety. Similarly, the Economic Development Department can readily provide this data to interested investors. In British Columbia, the following agencies and dashboards collect and/or report out crime data:
  - Ministry of Justice, Public Services Division
  - Uniform Crime Reporting Survey
  - DataBC
  - BC Stats
  - BC Corrections
  - Police Forces such as the Vancouver Police Department and RCMP
  - **Timeline:** 2-3 years
  - **Priority Level:** Medium-Low
  - **Responsible Parties:** City Council, Police Department, Strategic Development, Communications, and Public Engagement Department, Economic Development Department

- **Cost Estimate:** Economic Development Department staff should dedicate approximately 4-8 work hours each quarter to coordinate with other communities in the Lower Mainland and other partners.
  - **KPIs:** more fulsome data set that allows for more detailed reporting and for more nuanced comparison to data from other jurisdictions
- b. **Action:** Improve visibility of safety efforts in the City of Maple Ridge. This may involve closer partnerships with community care and service providers like the CSO program – promote greater marketing about the good that this program has done through a strategic communications plan, publications, editorials, or other marketing materials & define regional partners to assist in these communications.
- **Timeline:** 2-3 years
  - **Priority Level:** Medium
  - **Responsible Parties:** City Council, Strategic Development, Communications, and Public Engagement Department, Economic Development Department
  - **Cost Estimate:** The cost of this effort should be jointly developed between City Council, Strategic Development, Communications, and Public Engagement Department, the Economic Development Department, and other relevant departments. At a minimum, this action requires Economic Development department staff time of approximately 4 hours monthly to coordinate with other departments and to help to develop or update a marketing or messaging budget geared toward this specific initiative.<sup>98</sup>
  - **KPIs:** Citizen Satisfaction Survey results
- c. **Action:** Proactively improve communications and public relations related to public safety:
- Seek alignment throughout City government on outward messaging as it relates to public safety. Focus on fostering a constructive and positive message.
  - On social media, a City representative should proactively get ahead of or fact check misinformed or misrepresentative commentary about the community. While there is some strategic risk in this in that efforts to change a narrative can sometimes backfire, the City should seek to be an active participant in conversations that are happening around issues of public safety, as unchecked misinformation may have negative consequences for local business activity. Economic Development should have an embedded communications resource that assists the Strategic Development, Communications, and Public Engagement Department in strategic communications.

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<sup>98</sup> Guidance on costs for different types of marketing initiatives and strategies can be found here: <https://emulent.com/blog/marketing-cost-cheat-sheet-how-much-marketing-costs-if-done-correctly/>



- **Timeline:** 1-2 years
  - **Priority Level:** Medium-Low
  - **Responsible Parties:** Strategic Development, Communications, and Public Engagement Department, City Council, Economic Development Department
  - **Cost Estimate:** This is a core responsibility for the City's Strategic Development, Communications, and Public Engagement Department. The Economic Development Department should dedicate approximately 4 hours per month to coordinate with other City departments on this topic.
  - **KPIs:** Citizen Satisfaction Survey results
- d. **Action:** Regularly coordinate with BIA and Chambers to address needs and issues of downtown businesses, and to improve communications about services that are available to businesses downtown. Work with these organizations to help businesses in the downtown area to take advantage of programs including the Façade Improvement Program to improve visual aspects of buildings, public spaces, and infrastructure.
- If not already, consider including local businesses within the Citizen Satisfaction Survey
  - **Timeline:** 1-2 years
  - **Priority Level:** Medium
  - **Responsible Parties:** Economic Development Department, BIA, Chambers
  - **Cost Estimate:** To be determined by responsible parties. EBP recommends at minimum a monthly check-in or meeting with both the BIA and Chambers, requiring approximately 2-4 hours of Economic Development Department staff time.
  - **KPIs:** reported business satisfaction

## 12. Continue to Support Community and Placemaking Events that Generate Economic Activity in Town Centre

- a. **Action:** Allocate resources to support opportunities for economic and business activity at place-making events that draw residents downtown and encourage public gathering. Events centered around community gathering generate economic activity, as visitors will tend to patron nearby restaurants, retailers, and other businesses. The City should also consider investor hosting at community events, which involves organizing and facilitating visits, meetings with City Council and members of the local business and development communities, and experiences for potential investors to showcase the advantages to investing in Maple Ridge. The City may consider developing an investor hosting playbook

for organizing and executing investor hosting. Events that are authentic to Maple Ridge that the City may consider for investor hosting include but are not limited to Our Neck of the Woods and other music events, Innovation Challenges, and farmers' markets or craft markets showcasing local makers.

- **Timeline:** 2-3 years
- **Priority Level:** Medium-Low
- **Responsible Parties:** City Council, Economic Development Department
- **Cost Estimate:** The cost of supporting business activity or investor hosting at place-making events is highly variable dependent on the event itself and the type of investor the City wishes to host. Investor hosting may require multiple days of Economic Development Department staff time while the investor is in town, and may take several weeks of staff time for planning purposes. For investor hosting, the City should consider the following potential costs:
  - Identifying the respective focus or priorities of specific interested investors
  - Identifying what to show an interested investor based off of their priorities
  - Organizing business or site tours
  - Planning logistics such as hotels, meals, and transportation
- **KPIs:** Increase in local business revenues during these events versus typical activity, Number of potential investors hosted at these events

## Infrastructure and Business Supports

Strong infrastructure and market access are critical drivers of investment decisions. To attract businesses, Maple Ridge must proactively assess and communicate the value of its transportation networks, utilities, and available sites. Ensuring investment-ready land, improving relationships with service providers, and expanding transportation options will enhance the City's economic appeal. By strengthening these foundational assets, Maple Ridge can position itself as a competitive and well-equipped destination for business growth.

### 13. Maintain Inventory of Physical Assets

- a. **Action:** Maintain inventory of physical assets and communicate the value of municipal and regional infrastructure & its value for attracting investment. Transportation infrastructure is key for enabling trade and access to markets east, west, south, and across the Pacific Ocean. The City should develop a deep understanding of how the region's infrastructure connects Maple Ridge businesses to major markets across North America, as well as to major shipping and transportation routes around the globe. Having a deep understanding of the region's transportation assets and how these support the

needs of Maple Ridge businesses (e.g., market access, facilitating trade and the flow of goods and services) is critical for communicating the City's value proposition to potential investors. It is also important that public-facing data related to zoning of land, available sites, and infrastructure in datasets and tools such as the GIS Site Selector Tool are consistent with internal data sources. (See "Physical Asset Inventory" section)

- **Timeline:** 3 months and ongoing. Review and update every 6-12 months.
- **Priority Level:** Very High
- **Responsible Parties:** Economic Development Department, City Council
- **Cost Estimate:** Approximately 40 hours of Economic Development Department staff time for development. Approximately 16 hours for each update.
- **KPIs:** Ability to communicate the value and role of certain assets in the region to economic development and various key industries, Sites identified for priority sectors (at least 1 site for each key industry)

## 14. Proactively Create Investment-Ready Sites

- a. **Action:** Develop (or update) a site readiness checklist for available sites in the City. Development and ultimately investment goes where infrastructure (e.g., utilities, transportation network) already exists – it's critical that the City have plans and infrastructure in place and already built so that sites are investment ready. The City should develop a site readiness checklist that includes specific site requirements that apply to the City's key sectors. Sites can then be evaluated against this checklist to see if they are ready for investment. A site readiness checklist should include the following<sup>99</sup>:
  - Utilities: Is the site adequately serviced by water, electric, natural gas, sanitary, and telecommunications utilities?
  - Transportation and Market Access: How accessible is the nearest highway, rail service, and airport from the site?
  - Zoning and Land Use: Does zoning match the intended use of the site?
  - Business Climate: Understanding of Property Taxes, Corporate Income Tax Rate, and Regulatory Environment associated with the site
  - **Timeline:** 3 months and ongoing. Review and update every 6-12 months.
  - **Priority Level:** Very High
  - **Responsible Parties:** Economic Development Department, Planning and Building Department

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<sup>99</sup> <https://siteselectorsguild.com/wp-content/uploads/2024/09/Best-Practices-in-Site-Readiness.pdf>

- **Cost Estimate:** Approximately 32 hours of staff time for development. Approximately 12 hours per review.
  - **KPIs:** Percentage of industrial sites that are investment-ready, Development permits, Construction starts. Readily apply checklist of specific site requirements for each target industry to available sites as an evaluation tool.
- b. **Action:** Continue to implement existing recommendations for improving Industrial Areas, and utility service and transportation networks that service industrial and employment centres in Maple Ridge, as found in the City's Industrial Impact Analysis, Sanitary Master Plan, Stormwater ISMPs, Strategic Transportation Plan, Community Improvement Plan, and the upcoming 256 North Plan. The recommendations presented in these plans directly impact site readiness and investment attraction to Maple Ridge.
- **Timeline:** Immediate and Ongoing (Refer to Associated Plans)
  - **Priority Level:** High
  - **Responsible Parties:** Refer to Associated Plans
  - **Cost Estimate:** Refer to Associated Plans
  - **KPIs:** Refer to Associated Plans
- c. **Action:** Continue to improve working relationships with utilities providers to (1) advocate on behalf of local businesses for possible service provision (2) explore opportunities for improved servicing. Coordinate with utilities providers on common goals, and explore ways in which they could directly support economic development.
- Telecommunications: Improve coordination & communications with Telecommunications providers and federal decision-makers. Companies to engage include TELUS and Rogers. Ask companies about their telecommunications gaps and needs. Improve understanding on fibre optic infrastructure, including areas in the community that are already served.
  - Sanitary and Stormwater: Continue to improve working relationship with Metro Vancouver, and explore communications with the neighbouring Water and Sewer Commission (WSC) jointly operated by the City of Mission and City of Abbotsford. While Ruskin lies just outside of the Metro Vancouver service boundaries, water infrastructure from the WSC is in place very near to Ruskin. The City should explore all possibilities of improving water servicing in Ruskin, which could possibly include collaboration with the neighbouring water servicer.
  - Natural Gas: Continue to build a working relationship with FortisBC.
  - Electricity: Continue to develop working relationship with BC Hydro.

- **Timeline:** Immediate and Ongoing (6-12 months). The City should aim to meet with utilities providers 2 to 4 times per year to discuss the needs of Maple Ridge businesses and explore opportunities for improvement.
  - **Priority Level:** Medium
  - **Responsible Parties:** Economic Development Department, Intergovernmental Affairs
  - **Cost Estimates:** At least 8 hours of staff time per quarter.
  - **KPIs:** Percentage of all industrial sites that are fully serviced by utilities
- d. **Action:** Continue to foster working relationship with TransLink. Work with the agency to explore possibilities for service improvement or expansion that will support business activity, for example, building additional bus shelters or improved transit service to industrial areas and employment centres.
- **Timeline:** Immediate and Ongoing (6-12 months). The City should aim to meet with TransLink 2 to 4 times per year to explore opportunities for better meeting the transportation needs of Maple Ridge residents and businesses.
  - **Priority Level:** Medium
  - **Responsible Parties:** Economic Development Department, Intergovernmental Affairs
  - **Cost Estimates:** At least 4 hours of staff time per quarter.
  - **KPIs:** TransLink ridership to locations with start and end points in Maple Ridge, number of residents that live within 1 km of a TransLink transit stop

## 15. Prioritize the establishment of a reliable hotel in the City of Maple Ridge

- e. **Action:** Continue to build momentum on the establishment of a reliable, business class hotel in the City of Maple Ridge. Many of the City's key sectors and activities, including Film and tourism, rely on access to nearby accommodations. Similarly, when tourists and corporate visitors associated with the City's Industrial Areas or Business Parks, local government, or post-secondaries come to Maple Ridge and stay in a hotel outside of the City, this leads to additional economic leakages, as these visitors are more likely to spend money at restaurants, retail, and entertainment outside of the City. The City has already conducted a market analysis for a hotel – next steps may include:
- Data collection: understand tourism trends in the City and region, Maple Ridge demographic and economic trends. Include economic data specific to sectors that rely on access to a hotel, such as the Film industry.
  - Marketing Materials: Develop or update hotel investment marketing materials, which may include Maple Ridge data on demographic and economic trends,

visitors statistics and other drivers of hotel demand, and available incentives for development.

- Site readiness for hotel development: Develop a site-readiness checklist specific to the development of a hotel, which includes that land is properly zoned, serviced by utilities and transportation infrastructure, etc. This checklist will help with identifying prime hotel locations.
- Develop a list of Prime Hotel Locations: Continue to identify locations around the City with high potential for hotel development. Understand the costs and timeframes associated with bringing land to market.
- Directly engage with members of the hotel industry. The City should prioritize regular and focused engagements with different members of the hotel industry to support the development of a hotel in Maple Ridge. This will likely include engagements with hotel owners, operators, and hotel brands. Track connections in the City's CRM system.
- Offer assistance or incentives for a hotel: Consider offering development or tax incentives, including expedited permitting for hotel developers. The City should also consider engaging with financial advisors to explore new solutions to offer to potential hotels.
- **Timeline:** Engage with members of the hotel industry immediately and at least once every 3 months. Collect data, develop marketing materials, and develop a site-readiness checklist within 6-9 months. Develop a list of prime hotel locations within 9-12 months and update every 3-6 months.
- **Priority Level:** High
- **Responsible Parties:** Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department, City Council, SLT
- **Cost Estimate:** Cost estimate is dependent on many variables, including scope of the project. Responsible parties should dedicate at least 8-12 staff hours per quarter for engagement with members of the hotel industry. The City may consider enlisting outside support for conducting an additional hotel feasibility study, which would help in data collection, developing a list of prime hotel locations, and further identifying key contacts to with whom to engage within the hotel industry: for a medium-sized hotel in a competitive urban market, the cost of a feasibility study can range between \$27,000 and \$70,000 CAD. Smaller hotels or ones in less competitive markets may cost around \$27,000 CAD.<sup>100</sup>

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<sup>100</sup> <https://www.proven.partners/blog/hospitality-feasibility-study>



- **KPIs:** Number of recipients of marketing materials, meetings with connections in the hotel industry, total number of potential hotel locations that are investment-ready, business inquiries

## Monitoring and Evaluation

In addition to key performance indicators outlined throughout the actions presented in this plan, the following is a list of metrics for the City to maintain and track overtime, as they are indicators of economic growth and may indicate success of investment attraction efforts. These metrics should be collected and tracked on an annual basis, as possible.

**Table 35. Monitoring and Evaluation Criteria**

Metric	Description
<b>Economic</b>	
<b>GDP</b>	Measure of Economic Production
<b>Business Confidence Index</b>	Survey Measure of Confidence of business owners
<b>Labour Force Participation Rate</b>	Percentage of Labour Force Working or Actively Looking for Work (participating in the labour force)
<b>Employment in Thousands</b>	How Many People are Actively Working (Job Growth)
<b>Unemployment Rate</b>	What percentage of the Labour Force is not working
<b>Number of Businesses</b>	Total number of Businesses by Employee Size
<b>Top Growing Industries</b>	What Sectors have Experienced the Greatest Growth in New Businesses
<b>Employment by Sector and Occupation</b>	Employment in each sector, particularly in the priority sectors, and employment by occupation
<b>Median Household Income</b>	Median Income of Households
<b>Per Capita Income</b>	Total income in the City divided by population
<b>Median Income by Sector (Provincial)</b>	Median Income for each sector, particularly in the targeted sectors
<b>Income Distribution</b>	Distribution of Income
<b>Labour Force Educational Attainment</b>	Percentage of the Labour Force without high school, with high school & post-secondary education
<b>Real Estate</b>	
<b>Property Tax Base</b>	Overall Property Tax (Residential & Non-Residential)
<b>Office Vacancy Rate</b>	Vacancy Rate for Office Buildings
<b>Office Average Gross Rent per Sq. Ft.</b>	Average Gross Rent for Office Buildings
<b>Residential - Average Rent</b>	Average Rent for Apartments
<b>Residential - Average Rent (Built from 2005-Present)</b>	Average Rent for Newer Apartments (more reflective of new developments)
<b>Residential - Vacancy Rate</b>	Vacancy Rate for Apartment Buildings
<b>Residential Building Permit Values</b>	Value of Residential Building Permits
<b>Non-Residential Building Permit Values</b>	Value of Commercial, Industrial, Institutional Building Permits
<b>Building Permits Issued</b>	Number of Building Permits Issued
<b>Average Single Family Home Price</b>	Average price for a single-family home

Metric	Description
<b>Housing Starts - Single Family</b>	New housing construction starts (single detached, townhomes, rowhouses, and condos)
<b>Housing Starts - Apartment Rentals</b>	New Multi-unit Apartment Housing Starts
<b>Population</b>	
<b>Population Estimates</b>	Estimates of the population
<b>Population Growth</b>	Year over year growth in population
<b>Population Forecasts</b>	Forecasted population growth 2-5 years in the future
<b>Net Natural Increase</b>	Net increase from births and deaths
<b>Average Age</b>	Average age of residents in the City
<b>Public School Enrollment</b>	Enrollment in public elementary, middle, and high schools in the City
<b>Taxes</b>	
<b>Property Tax Mill Rate</b>	The amount of tax payable per dollar of the assessed value of a property, particularly for non-residential properties.
<b>Share of Residential Assessments</b>	The percentage of total municipal property assessments represented by residential property.

## Costed Implementation Plan (CIP) Summary Table

The following table summarizes the details provided in the Strategic Action Plan and Costed Implementation Plan.

**Table 36. Costed Implementation Plan**

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
1a	Regularly engage with City Council and the Senior Leadership Team on the importance of industrial and commercial investment for supporting high-value job growth and building a larger tax base.	Immediate and Ongoing. Once every quarter.	Very High	Economic Development Department, City Council, Senior Leadership Team, Planning and Building Department	Approximately 16 person hours of preparatory time for Economic Development. Approximately 1 hour of preparatory time for members of City Council and SLT. Approximately 1 hour of discussion for all meeting attendees.	Council and the SLT are informed and engaged participants in investment attraction discussions.
1b	Ensure that key functions are properly assigned and resourced within the Economic Development Department and other City departments whose work relates to investment attraction.	Immediate and Ongoing (Within 12 months and Ongoing)	Very High	Economic Development Department, City Council, Senior Leadership Team, Planning and Building Department	These are some of the core functions of the Economic Development Department staff as they relate to investment attraction.	Economic Development Department has the necessary staff and tools to fulfill core functions. Ability and capacity to collect and track metrics in the Monitoring and Evaluation section (see Monitoring and Evaluation Metrics).

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
2a	Establish clear channels for interdepartmental communication	Immediate and Ongoing. At a regular cadence to be determined by the Director of Economic Development.	Very High	Economic Development Department & Other City Departments	Economic Development to allocate at least 4 to 8 hours per month of staff time to engage with other City departments on topics of economic development and investment attraction.	City staff are informed and engaged participants in investment attraction discussions.
2b	Develop standards for interdepartmental information and data sharing.	Immediate and Ongoing. Engage in an interdepartmental workshop 2 times per year.	High	Economic Development Department & Other City Departments	Approximately 4 to 8 hours of preparatory time for participants to: 1) Inventory current data resources and processes, 2) Identify gaps in departmental needs, 3) Consider opportunities for addressing gaps in needs.  2 hours per participant for meeting or workshop.	improved ability to share data between departments, facilitate or enhance collaboration between departments

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
3a	Prioritize resources to adequately support the City's Concierge Program	1 year and Ongoing	High	City Council, Senior Leadership Team, Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department, Human Resources Department	To be determined by responsible parties. The cost to implement this action is equal to the cost of one of more staff positions to serve as Concierge. Please note that costs will vary according to the assignment of and roles of the Concierge.	Number of Business Inquiries, Average time to process business permits, number of businesses successfully assisted
3b	Regularly engage with local businesses and industry to understand challenges and opportunities for improvement when it comes to improving the user experience of permitting and regulatory processes.	See BRE Implementation Plan	Very High	See BRE Implementation Plan	See BRE Implementation Plan	See BRE Implementation Plan



Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
4a	Advocate for the adoption of NAICS codes throughout the City and communicate the value of using NAICS codes.	6 months and Ongoing	High	Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department	<p>Approximately 4-8 individual staff hours of preparatory time for Economic Development Department and other City Departments to understand:</p> <ol style="list-style-type: none"> <li>1) The importance of NAICS Codes,</li> <li>2) How to work with NAICS Codes,</li> <li>3) Where within City processes NAICS Codes should be used</li> </ol> <p>As is required, time to engage with City Council and SLT on the importance and use of NAICS Codes. This would require an estimated 8 hours of staff time from Economic Development. Refined cost and staff time estimates to be determined by responsible parties.</p>	<p>Number of one-on-one meetings with key stakeholders – meetings with departments or leadership teams to address concerns and encourage adoption</p> <p>Percentage of relevant city departments using NAICS – Track the adoption rate across different City departments.</p>

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
4b	Institute the use of NAICS codes throughout the City	1 year and Ongoing	High	Economic Development Department, Planning and Building Department, and Bylaw, Licensing and Community Safety Department	Dependent upon the findings of Action 4a. Costs may include the resources needed for collecting industry data from existing businesses, resources to regularly collect industry data from future businesses, and resources for coordination with provincial offices (e.g., Provincial Assessor's Office or the Ministry of Revenue) or other partners who may also collect or use NAICS codes for Maple Ridge businesses.	1) Use of NAICS in economic reports & analyses – Track how often NAICS-based data is used in sector analysis and investment strategies, 2) Percentage of new business licenses issued with NAICS codes – Ensure all new business licenses include NAICS classifications, 3) Percentage of existing business licenses re-classified using NAICS
4c	Continue to collect and organize data that is relevant for site selectors and other location professionals	6 months and Ongoing (collect and update data on an annual or semiannual basis)	High	Economic Development Department	Core function of Economic Development Department. Updating Monitoring and Evaluation metrics will take a minimum of 16 hours of staff time.	Improved ability to provide data to interested investors or site selectors. Enhanced response time to site selector requests for information.

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
4d	Continue to prioritize maintenance of City datasets and outward facing tools that support investment attraction and use interdepartmental data.	Immediate and Ongoing (update 2-4 times annually)	High	Economic Development Department, Information Technology Department, Planning and Building Department	Cost of hosting for different platforms or data sources. Approximately 4 to 8 hours of work hours to update data within each individual outward facing tool. Refined cost and staff time estimates to be determined by responsible parties.	Number of website visits to publicly available online tools, length of time to provide data to inquiring investors
5a	As a core function of the Economic Development Department, continue to determine how to enhance competitiveness within the region, and coordinate efforts at the regional, provincial, and federal levels to better support investment in targeted sectors.	Immediate and Ongoing (quarterly, semiannual, or annual meetings with Economic Development partners, as needed)	High	Economic Development Department, Manager of Intergovernmental Affairs	Approximately 8 work hours of scheduling and preparatory time per engagement. 1 to 2 hours of work hours per engagement.	Number of annual meetings with regional economic development partners, Reported business satisfaction over time

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
6a	Continue to define clear value propositions for the City and for its Key Sectors	6 months. Revisit on an annual basis.	High	Economic Development Department	An estimated 40-60 hours of staff time involving developing value proposition statements for key sectors and getting buy-in from Council and other City Departments as may be needed or required.	Number of business inquiries in target sectors
6b	Develop updated marketing materials for target sectors	6-9 months	High	Economic Development Department	The cost for this action may vary. We estimate that it will require at least 40 hours of Economic Development Department staff time per sector to develop content, and at least 40 hours of graphic design work per sector to develop updated marketing materials for each sector. Refined staff time and cost estimates to be refined by responsible parties.	Number of businesses that marketing materials are distributed to, number of business inquiries
7a	Conduct user testing and gather feedback and recommendations from businesses about ease of use.	1 year	Medium	Economic Development Department, Information Technology Department, Bylaw, Licensing and Community Safety (Business Licensing)	Approximately 40 hours for workshop design, scheduling, and results.	Businesses are engaged and interested in participating in user testing. Businesses speak freely about website user experience and provide constructive recommendations.

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
7b	Implement recommendations from user testing.	2 years	Medium	Economic Development Department, Information Technology Department, Bylaw, Licensing and Community Safety (Business Licensing)	Dependent on the findings of Action 7a. This could include work time from Economic Development and Other Involved Departments to clarify or update information relevant to businesses, and could involve IT work time to update the City website. Refined staff time and cost estimates are to be refined by responsible parties.	Visitors to City Website (specifically "Build & Do Business"), Business Licence Applications, Business Inquiries
8a	Determine what content should be highlighted on the investment attraction micro site.	1-2 years	Medium	Economic Development Department, Information Technology Department	Approximately 40 to 80 work hours for reviewing web content, project development and management, and perhaps developing an RFP.	A clear plan for content to highlight on the investment attraction micro site, Number of interested vendors for an RFP to develop micro site
8b	Contract a vendor to develop and launch a new investment attraction micro site.	2-3 years. The contracted working period for the development of a microsite to take approximately 3-6 months.	Medium	Economic Development Department, Information Technology Department, Invest Vancouver and other regional partners	Approximately \$70,000 CAD. Potential to receive matching funds from grants, such as those issued through the CanExport Community Investments Program from Global Affairs Canada	Number of visitors to investment attraction micro site, business inquiries

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
9a	Prioritize attendance at key conferences within the City's targeted industries, and regularly follow up with business contacts made at conferences.	2 years	High	Economic Development Department, Intergovernmental Affairs, Invest Vancouver and other regional partners	<p>The cost of this action will vary depending on location, duration, registration cost, and needed preparatory and post-attendance time by conference. Refined staff time and cost estimates are to be refined by the Economic Development Department and the Director of Economic Development. The cost of conference attendance may include:</p> <p>1) Hotels, meals, travel, and staff time associated with conference attendance.</p> <p>2) Costs associated with planning, registration, preparing for attendance, etc.</p> <p>3) Costs associated with follow up of conference attendance, such as organizing notes, information, entering contacts into the CRM system, and follow up communications.</p>	<p>Number of conferences attended per year (by sector), number of new connections made per year, number of connections that the City actively communicates with each year (aim to communicate with each connection at least once a year), track the number of potential investors at each level of lead generation: prospect, lead, qualified lead, investor</p>



Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
10a	Identify and engage with key actors and ambassadors within targeted sectors	Immediate and Ongoing	Very High	Economic Development Department	This is a core function of the Economic Development Department. Approximately 10% of the Department's time should be dedicated to engaging with business and organizational partners within the targeted sectors.	Number of entries to CRM representing network of key actors and ambassadors (ensuring coverage across all targeted sectors)
10b	Continue to improve engagement with local business	Immediate and Ongoing	Very High	Economic Development Department	see BRE implementation plan	see BRE implementation plan
10c	Coordinate with the local real estate and development community.	1 year and ongoing	High	Economic Development Department	At least 8 hours of Economic Development Staff time per quarter.	Number of current contacts in the development and real estate communities, Development permits, Construction starts

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
10d	Engage with local property owners in the downtown area to communicate and seek alignment on community goals for economic and business development.	<p>Immediate: Collect ownership and contact information of local property owners in the downtown area (6-9 months).</p> <p>Medium Term: Develop relationships with local property owners and work to establish alignment on community goals for economic and business development (2-3 years)</p>	Medium	Economic Development Department, Bylaw, Licensing and Community Safety (Business Licensing)	Dependent upon the availability of information, at least 16 hours of Economic Development Staff time to collect ownership and contact information. At least 2-4 hours of Economic Development Staff time per quarter for relationship building.	CRM entries for downtown property owners, number of conversations per year with each property owner

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
10e	Inventory and continue to foster relationships with institutional partners in the regional economic development ecosystem.	Immediate and Ongoing	High	Economic Development Department, Intergovernmental Affairs	This is a core function of the Economic Development Department – to be determined by responsible parties. At a minimum, allocate 16 hours per quarter for fostering relationships with various institutional partners.	Number of engagements with regional partners
10f	Continue to regularly engage with Kwantlen and Katzie First Nations partners on regional economic development.	Immediate and Ongoing	Very High	Intergovernmental Affairs, Economic Development Department	To be determined by Intergovernmental Affairs in collaboration with Economic Development Department. EBP recommends a minimum of 4-8 hours of Economic Development staff time per quarter for engagement on topics of economic development.	Regular communications and engagements, number of collaborative initiatives
10g	Establish a connection with BC Assessment.	1 year and Ongoing	Medium-High	Economic Development Department, Planning and Building Department	At least 4 hours of staff time per quarter	Local business assessments improved as a result of communications with BC Assessment.

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
11a	Coordinate with or collect data from regional or provincial data partners to maintain accurate crime data on the City and region.	2-3 years	Medium-Low	City Council, Police Department, Strategic Development, Communications, and Public Engagement Department, Economic Development Department	Economic Development Department staff should dedicate approximately 4-8 work hours each quarter to coordinate with other communities in the Lower Mainland and other partners.  The cost of this effort should be jointly developed between City Council, Strategic Development, Communications, and Public Engagement Department, the Economic Development Department, and other relevant departments.	more fulsome data set that allows for more detailed reporting and for more nuanced comparison to data from other jurisdictions
11b	Improve visibility of safety efforts in the City of Maple Ridge.	2-3 years	Medium	City Council, Strategic Development, Communications, and Public Engagement Department, Economic Development Department	At a minimum, this action requires Economic Development department staff time of approximately 4 hours monthly to coordinate with other departments and to help to develop or update a marketing or messaging budget geared toward this specific initiative.	Citizen Satisfaction Survey results

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
11c	Proactively improve communications and public relations related to public safety	1-2 years	Medium-Low	Strategic Development, Communications, and Public Engagement Department, City Council, Economic Development Department	This is a core responsibility for the City's Strategic Development, Communications, and Public Engagement Department. The Economic Development Department should dedicate approximately 4 hours per month to coordinate with other City departments on this topic.	Citizen Satisfaction Survey results
11d	Regularly coordinate with BIA and Chambers to address needs and issues of downtown businesses, and to improve communications about services that are available to businesses downtown.	1-2 years	Medium	Economic Development Department, BIA, Chambers	To be determined by responsible parties. EBP recommends at minimum a monthly check-in or meeting with both the BIA and Chambers, requiring approximately 2-4 hours of Economic Development Department staff time.	reported business satisfaction

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
12a	Allocate resources to support opportunities for economic and business activity at place-making events that draw residents downtown and encourage public gathering.	2-3 years	Medium-Low	City Council, Economic Development Department	<p>The cost of supporting business activity or investor hosting at place-making events is highly variable dependent on the event itself and the type of investor the City wishes to host. Investor hosting may require multiple days of Economic Development Department staff time while the investor is in town, and may take several weeks of staff time for planning purposes. For investor hosting, the City should consider the following potential costs:</p> <ol style="list-style-type: none"> <li>1) Identifying the respective focus or priorities of specific interested investors,</li> <li>2) Identifying what to show an interested investor based off of their priorities,</li> <li>3) Organizing business or site tours,</li> <li>4) Planning logistics such as hotels, meals, and transportation</li> </ol>	<p>Increase in local business revenues during these events versus typical activity, Number of potential investors hosted at these events</p>



Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
13a	Maintain inventory of physical assets and communicate the value of municipal and regional infrastructure & its value for attracting investment.	3 months and ongoing. Review and update every 6-12 months.	Very High	Economic Development Department, City Council	Approximately 40 hours of Economic Development Department staff time for development. Approximately 16 hours for each update.	Ability to communicate the value and role of certain assets in the region to economic development and various key industries, Sites identified for priority sectors (at least 1 site for each key industry)
14a	Develop (or update) a site readiness checklist for available sites in the City. A site readiness checklist should include utilities, transportation and market access, zoning and land use, and the business climate.	3 months and ongoing. Review and update every 6-12 months.	Very High	Economic Development Department, Planning and Building Department	Approximately 32 hours of staff time for development. Approximately 12 hours per review.	Percentage of industrial sites that are investment-ready, Development permits, Construction starts. Readily apply checklist of specific site requirements for each target industry to available sites as an evaluation tool.

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
14b	Continue to implement existing recommendations for improving Industrial Areas, and utility service and transportation networks that service industrial and employment centres in Maple Ridge, as found in the City's Industrial Impact Analysis, Sanitary Master Plan, Stormwater ISMPs, Strategic Transportation Plan, Community Improvement Plan, and the upcoming 256 North Plan.	Immediate and Ongoing (Refer to Associated Plans)	High	Refer to Associated Plans	Refer to Associated Plans	Refer to Associated Plans
14c	Continue to improve working relationships with utilities providers to (1) advocate on behalf of local businesses for possible service provision (2) explore opportunities for improved servicing.	Immediate and Ongoing (6-12 months). The City should aim to meet with utilities providers 2 to 4 times per year to	Medium	Economic Development Department, Intergovernmental Affairs	At least 8 hours of staff time per quarter.	Percentage of all industrial sites that are fully serviced by utilities

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
14d	Continue to foster working relationship with TransLink.	discuss the needs of Maple Ridge businesses and explore opportunities for improvement.  Immediate and Ongoing (6-12 months). The City should aim to meet with TransLink 2 to 4 times per year to explore opportunities for better meeting the transportation needs of Maple Ridge residents and businesses.	Medium	Economic Development Department, Intergovernmental Affairs	At least 4 hours of staff time per quarter.	TransLink ridership to locations with start and end points in Maple Ridge, number of residents that live within 1 km of a TransLink transit stop

Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
15a	Continue to build momentum on the establishment of a reliable, business class hotel in the City of Maple Ridge.	Engage with members of the hotel industry immediately and at least once every 3 months. Collect data, develop marketing materials, and develop a site-readiness checklist within 6-9 months. Develop a list of prime hotel locations within 9-12 months and update every 3-6 months.	High	Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department, City Council, SLT	Cost estimate is dependent on many variables, including scope of the project. Responsible parties should dedicate at least 8-12 staff hours per quarter for engagement with members of the hotel industry. The City may consider enlisting outside support for conducting an additional hotel feasibility study, which would help in data collection, developing a list of prime hotel locations, and further identifying key contacts to engage with within the hotel industry: for a medium-sized hotel in a competitive urban market, the cost of a feasibility study can range between \$27,000 and \$70,000 CAD. Smaller hotels or ones in less competitive markets may cost around \$27,000 CAD.	Number of recipients of marketing materials, meetings with connections in the hotel industry, total number of potential hotel locations that are investment-ready, business inquiries

## Appendix 1. Additional Resources on Investment Attraction Best Practices

### Investment Attraction

[“Steps to Creating an Investment Attraction Strategy Business Attraction Toolkit for B.C. Communities”](#), British Columbia Ministry of Jobs, Tourism, and Skills Training

[“So, You Had an Investment Attraction Strategy. What Now?”](#), *Site Selectors Guild*.

[“Next practices for investment attraction in the UK, Europe, Africa and the Middle East”](#), fDi Intelligence

[“Best Practices in Investment Promotion”](#), OECD

### Talent Attraction

[“Live, Work, and Play: Attracting and Retaining Tomorrow’s Talent”](#), IEDC, Economic Development Research Partners

# Business Retention and Expansion Strategy

CITY OF MAPLE RIDGE

MAY 2025



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EBP 



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## 1. Executive Summary

The City of Maple Ridge, one of British Columbia's fastest-growing communities, has developed a comprehensive Business Retention and Expansion (BRE) Strategy to strengthen its economic base and support local businesses. This strategy, developed by EBP in parallel with the City's Investment Attraction Strategy, is grounded in data analysis, stakeholder engagement, and best practices to enhance Maple Ridge's business environment and position the City for sustainable growth.

### 1. Current Landscape and Key Findings

Maple Ridge's Economic Development department has taken steps toward structured BRE activities, including a CRM system and annual business visits. However, current outreach efforts remain largely ad-hoc. Stakeholder engagement highlighted the need for a more systematic and proactive BRE approach, stronger interdepartmental coordination, and improved relationships with community partners such as the Chamber of Commerce, utility providers, and educational institutions.

Key strengths include the City's affordability, young and growing population, scenic environment, and emerging sectors such as advanced manufacturing and film. Yet, businesses face several critical challenges: limited available industrial land, slow permitting processes, transportation congestion, insufficient temporary accommodations, and a lack of skilled trades and post-secondary education options within city limits.

### 2. Opportunities and Recommendations

The BRE Strategy identifies multiple areas for improvement:

- **Workforce Development:** Leverage partnerships with institutions like UBC, BCIT, and JIBC to support school-to-job pipelines, and advocate for a post-secondary campus in Maple Ridge.
- **Permitting & Governance:** Streamline permitting and licensing processes, improve transparency, and promote internal collaboration.
- **Communication & Outreach:** Expand digital presence, establish consistent social media strategies, and increase awareness of City services.
- **Targeted Support:** Customize support for different business segments (e.g., large firms, small entrepreneurs, high-growth startups), and emphasize the importance of concierge-style services and business walk programs.
- **Partnerships:** Strengthen coordination with the Chamber, local utilities, and other regional stakeholders to share responsibilities and align goals.

- **Future-Proofing:** Prepare for macroeconomic shifts such as trade disruptions, remote work trends, and climate-resilient industries by adapting local policies and support mechanisms.

### 3. Implementation and Resourcing

The report includes a detailed implementation plan emphasizing internal process improvement, the use of CRMs to manage business relationships, and ongoing performance monitoring. It proposes a segmented BRE model tailored to “Elephants” (large firms), “Mice” (small stable businesses), and “Gazelles” (high-growth firms), each with distinct needs. The strategy also recommends establishing task forces and enhancing feedback loops with businesses and community groups to ensure that efforts are data-driven and outcomes-focused.

### 4. Conclusion

The BRE Strategy provides a roadmap for Maple Ridge to become a more responsive, supportive, and competitive business environment. Through targeted actions, strategic partnerships, and sustained investment in infrastructure and workforce, the City can foster a thriving local economy that benefits all residents and businesses.

## 2. Introduction

The City of Maple Ridge, situated in British Columbia's Lower Mainland, is one of Metro Vancouver's fastest-growing communities, with a current population exceeding 106,000 and projections estimating growth to 150,000 within the next 15 years. To support this expansion and enhance the quality of life for its residents, the City is committed to strengthening its economy by attracting investments and expanding support to existing businesses.

The Business Retention and Expansion (BRE) Strategy contained in this document covers the work performed by EBP, a multi-national firm that specializes in economic development and investment attraction. This work includes data analyses, engagement with key groups and businesses in the local and regional economic development ecosystem, and a robust implementation plan to support BRE activities in the City. This Business Retention and Expansion Strategy was developed in parallel with a separate Economic Development initiative, the Investment Attraction Strategy. EBP has developed both strategies and refers to the Investment Attraction Strategy in different places in this report. The research team engaged with private sector representatives, local government, community groups, and other members of the regional economic development ecosystem in interviews and focus groups, gathering key insights into

the City's business environment and the dynamics of and opportunities for improving business support services in the City.

The analyses and research enabled the EBP team to evaluate the City's overall strengths and weaknesses, and the opportunities and threats that the City faces in supporting and retaining current businesses. Similarly, the EBP team evaluated political, economic, and social factors that may impact the City's ability to support BRE activities.

### 3. Environmental Scan

This task aimed to assess the current state of BRE in the City of Maple Ridge and comparable geographies to inform the development of a forward-looking BRE strategy and implementation plan. As the city continues to grow, this plan will support retention and expansion of the local business community.

Figure 1 presents an overview of the Maple Ridge Economic Development department's current practices laid out on a framework of BRE best practices. These practices, sourced from EBP's expertise as well as guidance from British Columbia and the International Economic Development Council (IEDC), serve as a starting point for Maple Ridge to systematize touchpoints with its business community and community partners.

#### 1. In-House BRE Activities

According to its Economic Development Strategy (2021), Maple Ridge's BRE program is "presently focused on a Customer Relationship Management (CRM) system to efficiently manage business relationships, and an annual business visitation program."<sup>1</sup> The practices in this report centre largely on BRE activities and partnerships that are more proactive in general.

As the City nurtures different BRE activities, it is important that the Economic Development department considers the following tenets:

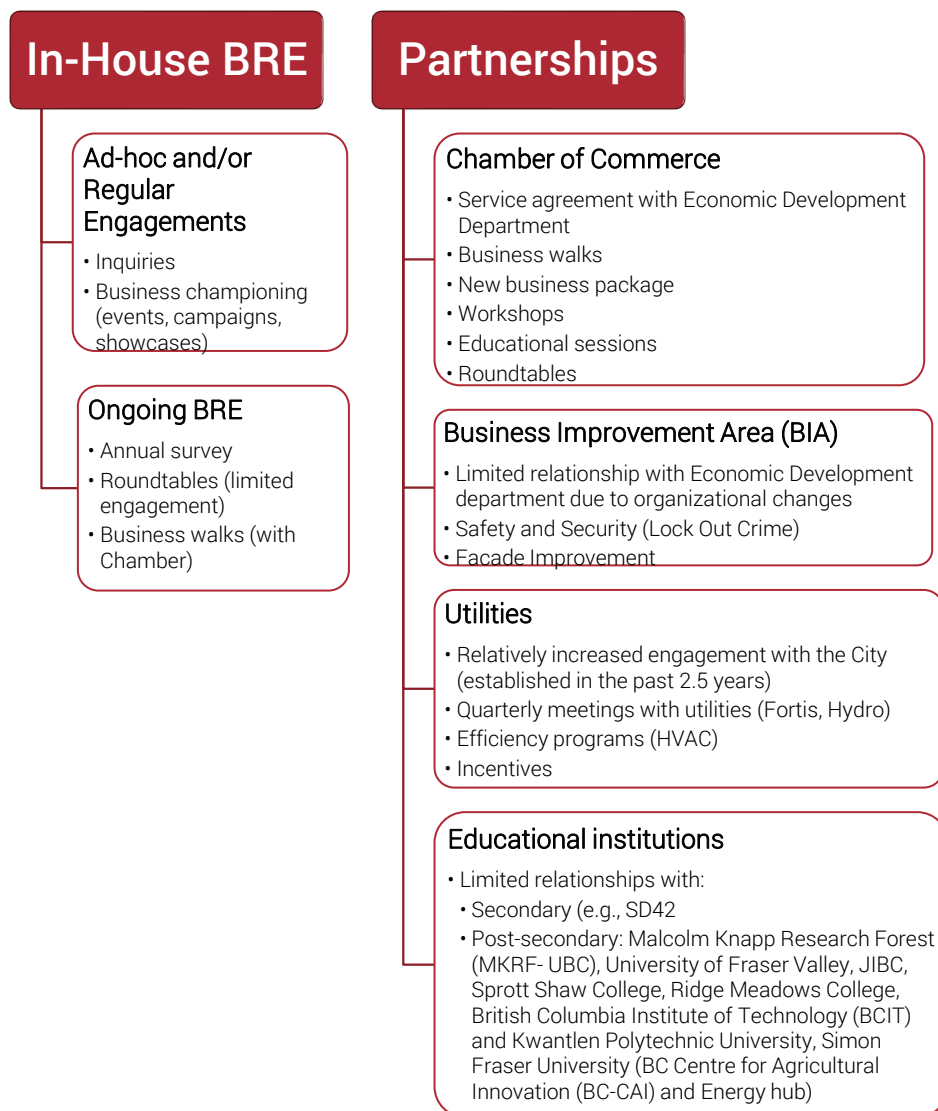
**BRE is an ongoing, perennial effort.** While annual events like networking mixers or annual surveys are useful, BRE is best thought of as an ongoing, multi-pronged effort to build relationships with the community's businesses. The Economic Development department could elevate its impact on the local business ecosystem by systematizing existing practices (e.g., focus groups, business walks, in-depth facility visits, and marketing) that are currently ad-hoc.

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<sup>1</sup> City of Maple Ridge Economic Development Strategy (2021).

**BRE is a team sport.** Economic development is just one of multiple entities supporting BRE efforts in the community. To that end, it is essential that Economic Development department engage its partners and position itself as a central player that helps coordinate partners' programs. This is especially important given the Chamber of Commerce's BRE activities. Not only does this avoid duplicated efforts and save labour and resources, but it also improves the business ecosystem's interconnectedness. This strategic planning effort offers a moment for the team to rethink its current activities and how partners can assume or transfer existing or new responsibilities in a way that plays to partners' strengths.

Figure 1 Summary of Maple Ridge's Current BRE Practices



## Ad-hoc outreach

Maple Ridge Economic Development offers ad-hoc support to businesses but understands the need for more consistency within the BRE program. For example, the team addresses ad-hoc inquiries from businesses as they arise and there are multiple input and data entry points in the City of Maple Ridge. Inquiry response is one of the primary interfaces for businesses. The Economic Development department does not have its own dedicated Facebook or LinkedIn, but the team does repost content from the City of Maple Ridge, the Ridge Meadows Chamber of Commerce, and Downtown Maple Ridge Business Improvement Association. The Economic Development department and its partners currently engage in the following:

- Announcements about new businesses
- Grand openings and ribbon-cutting events
- Business spotlights and success stories
- Choose MR -shop local campaign (such as 'Choose Maple Ridge – Support Local' campaign<sup>2</sup>)
- Opportunities for local businesses (e.g., grants, technical support services, educational resources)
- Tariff response efforts (new and upcoming)<sup>3</sup>

## 2. Economic Development Partnerships

BRE is a team sport, and Economic Development plays a coordinating role among various players in a complex ecosystem (Table 1). The Economic Development department does not have the time or resources to tackle every possible component of BRE, which means that collaboration is essential as well as prioritization.

Among other partnerships, reconciliation with First Nations and Indigenous communities should remain a priority. Some strategies that other municipalities have undertaken toward reconciliation include:

- The City of Regina, SK, has implemented an Indigenous procurement policy aimed at fostering economic reconciliation. This policy sets a target for at least 20% of the total value of city procurement contracts to be awarded to Indigenous vendors. Regina's efforts are guided by an Indigenous Procurement Advisory Committee.<sup>4</sup>

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<sup>2</sup> [Choose Maple Ridge - Support Local | Maple Ridge, BC](#)

<sup>3</sup> [Tariff Response Resources | Maple Ridge, BC](#)

<sup>4</sup> [City of Regina | Tenders & Procurement](#)



- The City of Montreal, QC, has developed a 2020-2025 Strategy for Reconciliation with Indigenous Peoples, which outlines seven key objectives. Among these is the establishment of a government-to-government relationship within city institutions, recognizing Indigenous governments as key partners in regional dialogue. The strategy also acknowledges the vital role of Indigenous civil society and organizations in decision-making and problem-solving on urban issues that affect their communities.<sup>5</sup>
- The City of Edmonton, AB, has strengthened its relationship with the Enoch Cree Nation by signing a Memorandum of Understanding, reflecting their commitment to collaboration as neighbouring communities. This partnership has facilitated joint initiatives in areas such as recreation, land use planning, and public transit, contributing to regional prosperity and community well-being.<sup>6</sup>

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<sup>5</sup> [2020-2025 Strategy for Reconciliation with Indigenous Peoples | Ville de Montréal](#)

<sup>6</sup> [Relationship with Enoch Cree Nation | City of Edmonton](#)

Table 1. BRE Partners

	Economic Development Department	Rest of City	Metro Vancouver Regional District (MVRD) Govt	Chamber	Utilities	Educational Institutions	Business Leaders	Downtown Maple Ridge Business Improvement Association (DMBRIA)
Manage the program	X							
Serve as an advisor to the program				X	X	X	X	X
Financially support the program	X	X		X				
Conduct outreach	X	X		X		X		X
Evaluate outreach feedback	X			X		X		
Problem-solve	X	X	X	X	X	X		X
Identify subject-matter experts	X		X	X		X	X	
Provide technical assistance		X	X	X	X	X		
Ensure program sustainability	X	X						
Promote the program	X	X		X	X	X	X	X
Advocate for the program	X			X			X	X
Recruit participants for the program	X			X		X	X	X

## Chambers of Commerce

Chambers of Commerce are a critical fixture in economic development ecosystems. In Maple Ridge, the Economic Development department has a service agreement with the Chamber of Commerce to delineate roles for each organization, which work in tandem and can overlap. The Chamber conducts business walks, conducts outreach to new businesses to orient them to resources, hosts workshops and educational sessions to support local businesses, and organizes roundtable to discuss emergent issues in the business community.

To maximize effectiveness, it is essential that both organizations maintain open lines of communication, regularly share insights from their respective engagements, and jointly plan initiatives where appropriate. By leveraging each other's strengths and relationships, they can present a unified front to the business community, foster stronger relationships with local enterprises, and more efficiently respond to emerging economic challenges and opportunities.

## Utilities

Utility providers play a unique role in BRE because they share an interest with the Economic Development department in maximizing its customer base without incurring large costs to access new geographies. In other words, utility companies often prioritize maximizing their customer base using its existing infrastructure.

Given this alignment of interests, the Economic Development team can collaborate with utility firms to improve the business climate. Utility firms in the Maple Ridge region already independently do BRE-like activity, such as efficiency programs, HVAC replacement, and other incentives. This offers an opportunity for the Economic Development department to help coordinate efforts and find new ways to collaborate.

The Economic Development department should foster an ongoing relationship with utilities to find symbiotic opportunities. The team should explore possible collaborations with initial conversations about mutual interests and needs. The Economic Development department's current relationship with utilities is generally ad-hoc, but it is a good sign that utilities have collaborated with them for specific issues.

Task Forces BRE task forces, also called committees, offer another way to build buy-in, better distribute activities, and gain strategic support. More generally, task forces create a feedback loop from stakeholders to Economic Development and serve as another touchpoint for Economic Development. There are various types of task forces, for example to discuss and help solve issues related to workforce, data analysis, communications/marketing, or special initiatives. The most common and important task force is the Executive Task Force, which generally comprises

stakeholders from across the BRE landscape, e.g., trade associations, chambers, educational institutions, workforce development organizations. These task forces can identify and execute tasks to support the Economic Development department. Maple Ridge does not currently have any such committees or task forces.

### Educational Institutions

Maple Ridge is supported by a range of post-secondary institutions, including the Justice Institute of BC (JIBC), UBC Research Forest, BCIT, Douglas College (via WorkBC), and the University of the Fraser Valley (UFV). However, the absence of a dedicated, full-scale campus within city boundaries presents a barrier to equitable, consistent, and community-centered access to higher education for residents. In 2024 and early 2025 considerable work was done to build a collaboration with key partners to encourage location of a post-secondary institution in Maple Ridge. Establishing a collaboration model is now completed and will continue to be maintained. Future work will focus on connecting the post-secondary engagement and collaboration to investment attraction needs regarding the future labour force. While the Economic Development department regularly engages with Malcolm Knapp Research Forest (UBC), BCIT, JIBC, and Simon Fraser University (SFU), there are more opportunities for future collaboration amid new enthusiasm. For example, School District 42 and JIBC have new leadership interested in school-to-job programs and collaborative engagement with local businesses by UBC. There is also interest in Research Forest development opportunities of, for example, meeting spaces, commercial kitchens, and business incubator spaces. Such opportunities can be explored further while the City waits for a feasibility analysis from the government of British Columbia and determine next steps towards establishing a post-secondary institution campus in Maple Ridge.

These new initiatives—and especially a potential institution in Maple Ridge—present key opportunities for BRE collaboration.

## 3. State of Business Data Collection Activities

Maple Ridge does not have a live centralized database of active businesses. Its current lists (the Business Licence Directory and Business Finder app) present challenges to effective BRE outreach and data collection:

- **Partial lists.** Current lists require voluntary opt-in, meaning that these lists are not exhaustive. The lists also exclude non-residential licences (contractors, film productions, etc.) not registered in Maple Ridge. Similarly, Fraser Valley Inter-Municipal Business Licence (IMBL) holders domiciled outside Maple Ridge are also excluded.

- **Esoteric industry categories.** Maple Ridge's current business information does not use NAICS categories, making definitions unclear and noncomparable with data from peers or across the country.
- **Intermittently accurate.** The information for these lists is compiled in November before licences expire every year on December 31. Because late notices extend into late February and March, the data is not yet "useful" until April or later.

The above issues are substantial challenges for effective BRE. In the long term, the Economic Development team, in conjunction with Bylaw, Licensing and Community Safety, should explore how to establish a streamlined system that will enable the City to better track its economic progress using universal terminology, which allows for benchmarking with peers and within national classification systems such as North American Industry Classification System (NAICS).

#### 4. Provincial Programs

The Government of British Columbia publishes diverse [BRE resources and templates](#).

- Performance Measurement Toolkit
  - Includes a library of possible indicators
- Excel-based Dashboard and Workbook Templates
- Annual Report Template
- [Funding Finder Tool](#) and funding [program list](#) to support cities' Economic Development departments.

The City of Maple Ridge can greatly benefit from these resources.

## 5. BRE Activities in Comparable Geographies in the Region

Geography	Region	Notes	In-House BRE	In-House Entrepreneurship	Partnerships
<b>Airdrie (City)</b>	Alberta	Proximate to Calgary, Suburban market, has similar sectors as Maple Ridge	<b>New business outreach</b> <ul style="list-style-type: none"> <li>Welcome emails</li> <li>In-person visit</li> <li>Social media for new/re-opening businesses</li> </ul> <b>Ad-hoc</b> <ul style="list-style-type: none"> <li>Downtown events</li> <li>Grant opportunities</li> <li>Inquiry response</li> <li>Marketing (FB, IG, LinkedIn)</li> </ul> <b>Downtown Revitalization</b> <ul style="list-style-type: none"> <li>Geographically targeted IA/BRE</li> </ul>	<b>Airdrie Entrepreneurship Common</b> <b>Economic Development Team-led partnership</b> <ul style="list-style-type: none"> <li>Discover</li> <li>SMARTstart</li> <li>AdvanceSMART</li> <li>Mentor Meetup</li> <li>The Cashflow Canvas</li> </ul> <b>Ad-hoc</b> <ul style="list-style-type: none"> <li>Workshops (e.g., two about leasing)</li> </ul> <b>Ongoing Entrepreneurship</b> <ul style="list-style-type: none"> <li>Business breakfast</li> </ul>	<b>Chamber</b> <ul style="list-style-type: none"> <li>Workshops</li> <li>Luncheons</li> <li>Networking events</li> <li>Annual trade show</li> <li>Economic Development liaison on Board of Directors</li> </ul> <b>City</b> <ul style="list-style-type: none"> <li>Annual business breakfast</li> <li>Programming</li> </ul>
<b>Bellingham (City)</b>	Washington, USA	Suburban market with proximity to nature; redeveloping areas (e.g., waterfront) to attract business	<b>New business outreach/support</b> <ul style="list-style-type: none"> <li>The Small Business Development Center (SBDC) connects businesses with a Certified Business Advisor who will help create a business plan, develop financial</li> </ul>	<ul style="list-style-type: none"> <li>Free of charge 1:1 business consultation</li> <li><a href="#">SCORE</a> connects businesses with a retired business professional who knows the ins and</li> </ul>	<b>Expedited Permitting</b> Faster permitting options for green building projects; free technical assistance and support is also available. <b>Financing support</b> A variety of local, state and federal options for funding businesses. <sup>9</sup>

<sup>9</sup> [Home | Small Business Development Center | Western Washington University](#)



Geography	Region	Notes	In-House BRE	In-House Entrepreneurship	Partnerships
			forecasts, obtain financing and more – all at no cost. <sup>7</sup>	outs of running a small business. <sup>8</sup>	
<b>Boulder (City)</b>	Colorado, USA	Suburban market with proximity to Denver and nature	<p>The city, under its BRE strategic plan:</p> <ul style="list-style-type: none"> <li>Continues to proactively analyze trends in market forces to shape its activities, plans and policies regarding local business and business retention.</li> <li>Considers the projected needs of businesses and their respective employees, such as commercial and office space, when planning for transportation infrastructure, programs and housing.</li> <li>Conducts analysis of market trends and policies about affordable business space and a diverse employment base.<sup>10</sup></li> <li>Works with the local business community and residents to make sure the city's regulations and development review processes provide flexibility.</li> </ul>		
<b>Chilliwack (City)</b>	British Columbia	Local competitor in Fraser Valley / Lower Mainland of British Columbia		Online learning platform (The Chamber Academy) for businesses to access affordable workshops and educational resources.	The <b>Chamber of Commerce</b> helps local businesses thrive by offering valuable financial support resources. The <b>Downtown Chilliwack Business Improvement Association, (DCBIA)</b> , in partnership with other communities, has a Health Benefits Program that offers small companies an

<sup>7</sup> [Home | Small Business Development Center | Western Washington University](#)

<sup>8</sup> [Bellingham | SCORE](#)

<sup>10</sup> [Boulder Valley Comprehensive Plan | City of Boulder](#)

Geography	Region	Notes	In-House BRE	In-House Entrepreneurship	Partnerships
					opportunity for pooled benefits that may otherwise not be affordable.
<b>Coquitlam (City)</b>	British Columbia	Local competitor in Metro Vancouver	<p>Conduct a survey of local businesses on a bi-annual basis to identify areas of success and areas of concern and pro-actively address issues.</p> <p>Develop an inventory of business support and incentive programs and prepare a marketing portal as part of the City's Economic Development website for local and prospective businesses.</p>	Work with the local business community to develop 'ready for market' programs and support services for export-oriented businesses.	<p>Work with the local hotel and hospitality industry to explore the viability of establishing a <b>Coquitlam Destination Marketing Organization</b></p> <p>Support the establishment of new <b>Business Improvement Areas</b> for various employment clusters by leveraging our relationship with the Tri-Cities Chamber of Commerce.</p>
<b>Langley (Township)</b>	British Columbia	Local competitor in Metro Vancouver			Greater Langley Chamber of Commerce – NextGen Business Network, Women's Business Network <sup>11</sup>

<sup>11</sup> [Langley City Business Resource Booklet by LangleyCity - Issuu](#)

Geography	Region	Notes	In-House BRE	In-House Entrepreneurship	Partnerships
Pickering (City)	Ontario	Proximate to Toronto, similar population and economy	The <a href="#">Small Business Hub</a> provides entrepreneurs items to borrow as they start up and grow. This includes a Grand Opening Banner (intended only for the Grand Opening date); assistance with product photography; and items to support business tradeshow, conferences and events.	Red Ribbon Cutting Ceremony ensuring invitations are extended to Mayor, Councillors and local stakeholders	<p>The <a href="#">Business Advisory Centre Durham (BACD)</a> free services support small business and include:</p> <ul style="list-style-type: none"> <li>• Business Advisory Services</li> <li>• Business Training</li> <li>• Mentoring &amp; Workshops</li> <li>• Entrepreneurship Funding Programs</li> <li>• Networking Opportunities</li> </ul> <p><b>Durham Business Showcase event</b> provides an opportunity for businesses to share their products and services with municipal staff and other public sector partners</p>
Strathcona (County)	Alberta	Proximity to Edmonton and natural environment (e.g., a	Strathcona County conducted a business survey to identify the needs and challenges faced by local businesses.	Business Learning Seminars (e.g. Negotiating Commercial Leases and Renewals)	Each year, businesses in the region are invited to attend the Supply Chain Forum - an event connecting attendees with many of the

Geography	Region	Notes	In-House BRE	In-House Entrepreneurship	Partnerships
		national park that must be accessed via the municipality)	Led promotions, council visitations, marketing campaigns, publications (including annual survey results), start-up workshops, events like Savour Strathcona to increase visibility of local food vendors, agricultural producers and artists, marketing support, and mentorship programs		region's major industrial companies. The 2023 forum was held jointly by the City of Fort Saskatchewan, Sturgeon County, and Strathcona County, with support from the Sherwood Park & District Chamber of Commerce.

## 4. Relevant Groups Engagement

The City of Maple Ridge's initial outreach and engagement with key groups have helped to build a strong foundational understanding of the Maple Ridge business environment and strengthen business connections and will directly inform the Business Retention and Expansion strategy. These efforts have also provided valuable insights into the ongoing health and success of local businesses.

Different groups have varying perspectives on the advantages and challenges of operating in Maple Ridge. While concerns such as traffic congestion and inadequate infrastructure create obstacles for businesses, Maple Ridge also offers significant benefits. Its affordability, access to a younger talent pool, and a growing sense of optimism contribute to its attractiveness for business growth and retention.

Some key findings with recommended next steps are summarized in Table 2. It is important to note this section covers "what we heard" in the field and they may not fully reflect ongoing effort of the City to address different challenges.

Table 2. Key Findings and Opportunities for Maple Ridge

Challenge faced by the City	Resulting Implications for Local Businesses	Opportunities for the City Leadership
Infrastructure Gaps	Businesses must invest heavily in basic amenities like water and sewer services, leading to increased costs.	Streamline communication, implement modular infrastructure solutions, and ensure leadership support for growth-focused policies
Limited Industrial Land	Businesses struggle to find suitable locations for operations and expansion, restricting economic growth.	Identify and rezone land for industrial use and explore mixed-use developments to optimize available space
Transportation Connectivity Issues	Congestion, particularly on Lougheed Highway and Golden Ears Bridge, creates logistical challenges and increases transportation costs.	Advocate for regional transportation improvements and explore mobility solutions to ease congestion
Slow Permitting and Approval Processes	Lengthy development approval and business licensing timelines discourage investment, delay	Streamline approval processes by enhancing interdepartmental coordination

	project completion, and put government grants at risk.	Better support and improve awareness of concierge services within the Economic Development department to keep businesses informed through the process
Insufficient Temporary Accommodations	Lack of hotels and short-term rentals impacts industries like film and construction, making it harder to attract workers.	Encourage hotel and short-term rentals development through incentives and partnerships with private developers
Lagging Commercial Development	Residents travel outside the City for amenities and services, reducing local consumer spending and business viability.	Support local business growth by attracting retail and service-sector investments to enhance commercial offerings, such as celebrating 'Shop Local Week'
Historical Lack of Interdepartmental Collaboration	Inefficient processes and bureaucratic delays worsen business development challenges, causing frustration among business owners.	Strengthen Economic Development team's role in interdepartmental working groups to improve communication and accelerate decision-making

## 1. Initial Outreach Engagement

EBP worked closely with the City to identify key partners for engagement, including local businesses and City leadership. In close collaboration with the Economic Development department, we identified critical groups such as local companies, Indigenous communities, and business associations, recognizing their essential roles in the successful implementation of the BRE Plan.

The Economic Development team scheduled engagements with local businesses and community leaders for the EBP field visit. Unfortunately, due to logistical constraints, we did not have an opportunity to meet with the First Nations communities; however, we strongly encourage the City to continue prioritizing these communities and to make it a consistent practice to involve them. The City intends to continue fostering and strengthening these relationships, particularly those established or strengthened through this initiative, ensuring sustained collaboration and support for the long-term success of Maple Ridge's business community and the effective implementation of the BRE Plan.



## 2. Relevant Group Engagement from Field Visit

The objective of the field visit was to gain insight into the needs and experiences of both existing businesses and potential investors while identifying early opportunities, legacy challenges, and discrepancies between perceptions and data realities. This exercise provided a deeper understanding of the current business ecosystem, key opportunities, and any perceived barriers to growth within the community. Table 3 summarizes the relevant groups the team engaged with during the field visit, which took place January 21-23, 2025.

Table 3. Groups Engaged During Field Visit

Focus Group 1	Focus Group 2	Focus Group 3
Meadowridge School	Haney Builders Supply	Business Improvement Association (BIA)
Sanscorp Products Ltd.	Therapeutic Collective	Chambers of Commerce
Meadowridge School	Supreme Structural Transport	
University of British Columbia (UBC)	JLL	
Avison Young	Vanspec	
Partap Forest Products	Switchback Brewing	
Production Playground (A division of Owen Holdings Ltd.)	BC Hydro	
Beedie Development Group	Mainland West	
Orion Constructions	Haney Builders Supply	
Meridian Farm Market	Colliers	
Jewel WFC	JIBC	
Invest Vancouver	JEDI	
	Advanced Self Storage	

- Themes explored with local businesses/ companies included:
  - Location decision – Why is your business in Maple Ridge?
  - Local regulatory processes & business supports – your experiences working in and with the City
  - Land, infrastructure, and utilities in and around Maple Ridge
  - Talent and workforce development
- Themes explored with City Business Officials included:
  - Members' businesses, goals, and challenges
  - Business Improvement Association (BIA) and Chamber roles, goals, and challenges

- The future of the Maple Ridge business environment

### 3. Findings

#### 1. Perceived Wins

##### *Location*

- The City provides access to nature and the environment, promoting a sense of stewardship and encouraging residents to appreciate and protect these natural resources.

##### *Expanding Resources' Inventory*

- Land granted by the province to UBC led to the development of the Malcolm Knapp Research Forest, which is currently developing a welcome and education centre, which will also serve as a destination building with event space.
- The Golden Ears Bridge is a vital transportation and logistics asset. Chilliwack has successfully attracted investment due to its proximity to Highway 1, highlighting a similar opportunity for Maple Ridge.
- Despite rising costs, Maple Ridge still offers a strong value proposition, as industrial land south of the Fraser River is significantly more expensive.
- Maple Ridge stands out for its talent and population density, affordability, and access to nature.
- Younger workers are moving to Maple Ridge, supported by an affordable housing market. This gives the City the opportunity to retain younger talent in the community by helping them identify more job opportunities in the City.

##### *Lower/ Competitive Property Tax and Other Costs*

- Maple Ridge was selected for growth in the lumber and manufacturing sectors over Surrey and Abbotsford due to its lower property taxes and less restrictive land use policies. Businesses prioritized affordability to avoid being priced out of more expensive markets.
- Affordability remains one of Maple Ridge's key advantages, attracting many individuals from Vancouver and Burnaby, particularly younger professionals. Housing availability is a major selling point, drawing both residents and businesses to the area.
- For example, we learned during in-person interviews in the field that Meadowridge School considers Maple Ridge an ideal location for staff to raise families, which helps attract

high-quality applicants. Additionally, the private school brings in 40 to 70 families each year who relocate specifically for its strong educational opportunities.

### *Growing Opportunities to Explore*

During field engagement, participants recognized that the City has the potential to contribute to the green economy and explore further opportunities.

Younger generations increasingly prefer to shop locally, a trend that has contributed to the success of Chilliwack's development. The area has effectively balanced big-box retailers with the charm of local businesses, attracting young consumers. Maple Ridge can leverage this trend by fostering a similar environment that supports both large and small businesses.

### *Growing Optimism and Positive Momentum*

Most of the people noted that the City has been great to work with, fostering close collaboration with businesses and neighbouring communities. The business community is noting a shift in the City recently to being more responsive and flexible, which needs to continue. There is optimism that the new leadership will streamline the permitting process, making it more efficient.

Businesses have experienced a strong commitment from the City, with a proactive approach to engagement. One company described their experience as the best they've had with any city—officials met with them quickly, identified key contacts, and established regular meetings with clear timelines. This efficient coordination was especially crucial for a large, high-priority project. Ensuring all relevant departments, such as planning and fire services, are aligned early in the process remains essential.

There is also a strong appetite for building relationships and connections within the business community. Economic Development has played a key role in fostering conversations to attract film investment that have been highly successful, generating excitement for the future. Such activity provides evidence to the business community that there is a cultural and practical shift taking place with the City becoming more responsive and business friendly.

## *2. Perceived Challenges*

### *Resource Limitations*

- Maple Ridge is struggling to meet the infrastructure needs of businesses in certain areas of the City. Many local businesses must invest heavily to access basic amenities such as water and sewer services. In Ruskin, for example, some services are provided on an on-call basis, leading to increased insurance rates.

- The limited availability of industrial land further complicates business operations in Maple Ridge. The City is low on industrial land inventories, making it increasingly difficult for businesses to establish and expand their operations.
- Transportation connectivity remains a challenge in certain parts of the City, particularly access via the Golden Ears Bridge due to congestion on Lougheed Highway.
- Despite having a fair inventory supply of industrial lands in the City, there is a perception that there is a lack of industrial land that makes it harder to do business in the City, as Maple Ridge's industrial land inventory is among the smallest in the region, ranking just above First Nations land.
- Commercial development is lagging population growth, leading many residents who move to Maple Ridge to travel outside the City for amenities and recreational opportunities. The City needs more retail shopping options and 'Saturday entertainment'.

### *Gaps in Service Provision*

- The Silver Valley neighbourhood has limited services for residents.
- Business deals are being lost due to a lack of available accommodation.
- Temporary accommodation, such as hotels, are insufficient to meet the needs of the film industry and other sectors, particularly for union workers on contract. As a result, many struggle to find suitable lodging within the City or near their work sites.

*"Can we arrange for the people involved in the show to stay at a hotel just 10 minutes from the site? This is a key deciding factor for a major film opportunity that was considering our location."*

### *Restrictive Regulatory & Permitting Processes*

The permitting process was extensively discussed in focus group discussions. The number of permits required, along with the lengthy approval process for each, presents a significant barrier to entry and business operations in the City. Additionally, the City has a limited number of permitted dump sites, which some businesses rely on for their operations.

In certain cases, the permitting and business licencing process has taken much longer than what was initially estimated by the City. As a result, businesses have faced escalating costs while waiting for approval, with inflation adding to the problem as well.

What makes the situation even more frustrating is that similar businesses in other cities, which applied later, received their permits before those in Maple Ridge. Due to these inefficiencies, Maple Ridge has gained a negative reputation among other cities for its slow permitting such as development approvals processes.

There is significant competition in the region, making Maple Ridge's lengthy permitting and approval process even less attractive for new or expansion development projects. For example, an investor expressed interest in building a hotel in the City but ultimately decided not to proceed due to the complexities of the permitting process.

*"It took 2.5 years for a company to complete its relocation, significantly discouraging the business owner from pursuing any larger developments. The standard process should have taken no more than six months. Frustrated by the delays, the business owner ultimately decided to scale back plans."*

*"Complications with building permits caused major setbacks, preventing us from moving forward with our intended work."*

Some also noted that these delays are not unique to Maple Ridge but are resource- and time-intensive across the province, averaging 1.5 years. However, this is still significantly longer compared to Alberta, where the process takes only six months. Businesses also noted that the bylaws and procedures between the two provinces appear very similar, but it is more a matter of philosophy/culture than any other factor. In Maple Ridge, a historical lack of collaboration among different departments has further exacerbated the issue. The lengthy approval timelines not only result in higher costs for businesses but also put government grants at risk if a submitted permit does not proceed as planned.

Land developers and construction professionals emphasized that they carefully select the communities they work in, with ease of doing business being a major deciding factor.

### *Lack of Skilled Trade Workers*

Maple Ridge lacks higher education institutions and trade/ technical schools, which limits local workforce development.

- In industries like film, there is a need for stronger collaboration between tradespeople and business professionals.
- The lumber and manufacturing sectors rely heavily on workers from Mission, Abbotsford, and Surrey, with little to no local workforce participation.
- The hospitality sector, particularly in roles like cooks and food service, has faced staffing challenges, often requiring workers from outside the City.

### *Other Issues*

- Recent changes within the City management, including changes in human resources and organizational changes, may have disrupted continuity in planning and decision-making processes.
- Many businesses are unaware of the services the City offers, particularly in three key areas: safety, business revitalization, and promotion.
- Despite being well-positioned to address core challenges around homelessness and safety, the City continues to struggle with a persistent negative perception regarding public safety.
- Maple Ridge has pioneered innovative programs to improve community care and address homelessness, yet it does not receive due recognition from the province or other municipalities. While jurisdictions have adopted these approaches, they have not acknowledged Maple Ridge's leadership in this space.

## 4. Recommendations Shared During Group Engagement

### *Enhance Communication and Collaboration*

Maple Ridge can learn from Langley's proactive and customer-focused approach, where city services work in coordination to foster a strong interdepartmental team culture. Streamlining communication between businesses, developers, and city officials would improve efficiency and encourage investment.

The City should also improve communication regarding the future of the Haney Mall site by actively exploring potential redevelopment opportunities. Additionally, commercial landowners could take a more proactive role in informing the community about plans for vacant storefronts, helping to generate public interest and engagement.

More broadly, the City needs to enhance communication about ongoing initiatives, including safety measures and business activities, to correct misinformation and build confidence in the community.

### *Adopt a Pro-Growth Mindset*

For Maple Ridge to support long-term growth, engineers should play an active role in decision-making to develop innovative, modular housing solutions that address the City's unique challenges. Implementing strong decision-making framework—backed by strong top-down support for pro-growth policies—will be critical in attracting and retaining businesses. A more modular and adaptive approach to planning and development would help Maple Ridge better



prioritize infrastructure investments, addressing longstanding issues such as limited industrial land, transportation bottlenecks, and service accessibility.

### *Strengthen Public-Private Partnerships*

Establishing structured partnerships between developers, engineers, and the City would help Maple Ridge address its pressing infrastructure challenges, particularly in areas like Silver Valley and Ruskin, where businesses face high costs for basic utilities. By fostering collaboration, the City can develop more effective and sustainable solutions for infrastructure expansion, industrial land availability, and transportation improvements, ultimately creating a more business-friendly environment.

### *Expand Education and Workforce Development*

Introducing a trade or technical school in Maple Ridge would strengthen workforce development, building on the foundation provided by local high schools. This would build on the foundation already provided by local high schools and help create a pipeline of skilled workers.

### *Improve Business Support Services*

The Economic Development department should be the primary point of contact for businesses looking to establish or expand in Maple Ridge. Raising awareness of the "Business Ready" initiative would help increase support for new and existing businesses. The department has the potential to adopt various BRE practices such as concierge services for new and existing businesses in the City.

### *Invest in Infrastructure*

To support proactive growth, Maple Ridge must make significant upgrades to its roads, water systems, bridges, and recreational facilities. The City currently has one of the smallest industrial land inventories in the region. Expanding industrial land and improving infrastructure are critical to attracting and retaining businesses.

Additionally, Maple Ridge urgently needs a hotel to accommodate business travellers, film industry professionals, and other visitors. The redevelopment of Haney Place Mall should also be a priority, particularly as the site will serve as the terminus for the new Bus Rapid Transit (BRT) line connecting to the SkyTrain in Langley. As one of the first impressions for visitors, transforming this space into a vibrant, well-utilized hub would be beneficial for economic development and the City's image.

Investing in commercial spaces that support small businesses, such as breweries and local retail, is also a key opportunity. The establishment of Patch Brewery is being viewed as a huge

win for the City and has generated demand for such establishments, reinforcing the need for strategic infrastructure investments that create a more dynamic business environment in Maple Ridge.

### *Enhance Transportation Connectivity*

Connecting the Golden Ears Bridge via Abernethy Way extension to 256 Street would unlock significant industrial development potential, as the area has large tracts of land available. Strengthening transportation infrastructure will be key to facilitating long-term economic growth and regional connectivity.

## 5. Future Trends

Maple Ridge Economic Development Department's Business Retention & Expansion (BRE) strategy should be tailored not only to the City's current economic ecosystem but also to future needs. Economic data analysis provides a high-level understanding of Maple Ridge's current industries. Qualitative data from business leaders provide deeper insight into emergent trends that are not as easily captured in Census data.

This report provides an overview of Maple Ridge's businesses and discusses sector-level trends at the local, regional, provincial, national, and international scales that will shape Maple Ridge's business needs. It aims to inform the City's BRE strategy to resiliently navigate economic shifts.

Highlights:

- **Supply chain volatility & manufacturing risks:** Trade policy and supply chain shifts threaten Maple Ridge's industrial production. While advanced manufacturing (e.g., metal products and machinery) shows resilience, wood product manufacturing faces risks due to U.S. tariffs and recent sawmill closures, highlighting the need for targeted BRE support.
- **Workforce shifts & remote work:** With 18.8% of the workforce working remotely, Maple Ridge can leverage trends like remote work and home-based businesses.<sup>12</sup> However, that is a risk, as there is growing pressure by employers to have employees return to the office. BRE strategies should focus on supporting non-traditional business models through investments in high-speed internet, affordable housing, and quality of life.

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<sup>12</sup> Environics estimates via Esri Canada. Derived from Statistics Canada census data.

- **Quality of life:** Maple Ridge's amenities, outdoor features, and recreation are crucial for attracting and retaining talent. A strong quality of life supports workforce retention and, in turn, local business growth, especially as remote work becomes more prevalent.
- **Retail trade remains a large employer:** Retail in Maple Ridge is sensitive to consumer spending shifts, particularly amid inflation. Targeted retention strategies for key retail sectors and expansion opportunities for big-box and specialty stores can help retain local spending. Focus retail development/ investment to the Town Centre area to complement and support other initiatives in the area, including enhanced transit service.
- **Healthcare is a large stable employment sector:** Healthcare remains a major employer, driven by an aging population and steady hiring demand, making it a key sector for economic stability.
- **Professional, Scientific, & Technical Services are a significant but not dominant employer:** Professional, Scientific, and Technical Services are a sizable sector with potential for growth, especially with available commercial land and possible connections to post-secondary education pipelines.

## 1. Maple Ridge's Business Community

Maple Ridge's labour force comprises nearly 58,000 individuals and has strong market and workforce access as a part of the Metro Vancouver region. As of 2021, the City's residents had a workforce participation rate of 67.6%, above British Columbia's (65.2%) and Canada's rate (65.3%), which may in part speak to the City's slightly younger-than-average population. The City itself has a population of 105,000 but has access to a population of 3,200,000 within a 1-hour driving distance.

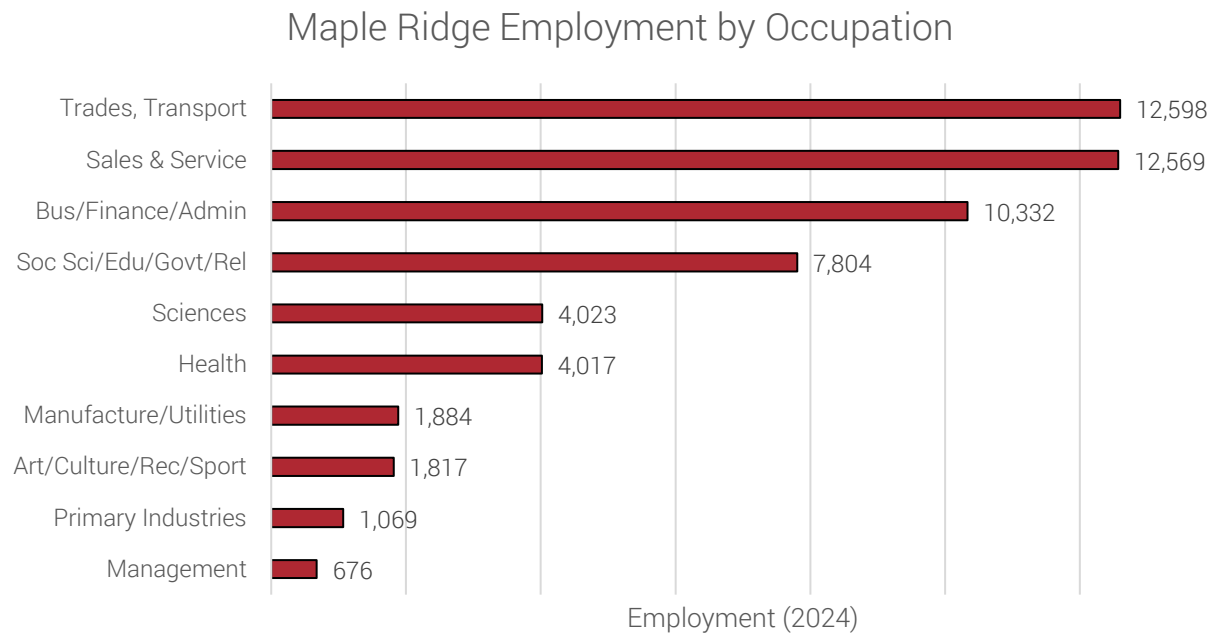
Around 13.9% (8,000 workers) of Maple Ridge's workforce are self-employed, which is on par with national average.<sup>13</sup> Approximately 41.5% of the workforce have "white-collar" jobs, while 27.4% have "blue-collar" jobs and 31.1% service jobs.<sup>14</sup> This roughly aligns with the workforce's educational attainment. In the City, 6.3% of the population has no certificate, diploma, or degree; 26.9% have a high school diploma or equivalency certificate; 11.1% hold an apprenticeship, trade certificate, or diploma; 25.5% have a university bachelor's degree or higher; 29.4% have a combined non-university certificate or diploma and university education below the bachelor level.

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<sup>13</sup> Environics estimates via Esri Canada. Derived from Statistics Canada census data; [Trading Economics](#)

<sup>14</sup> Ibid.

Figure 2. Maple Ridge Employment by Occupation

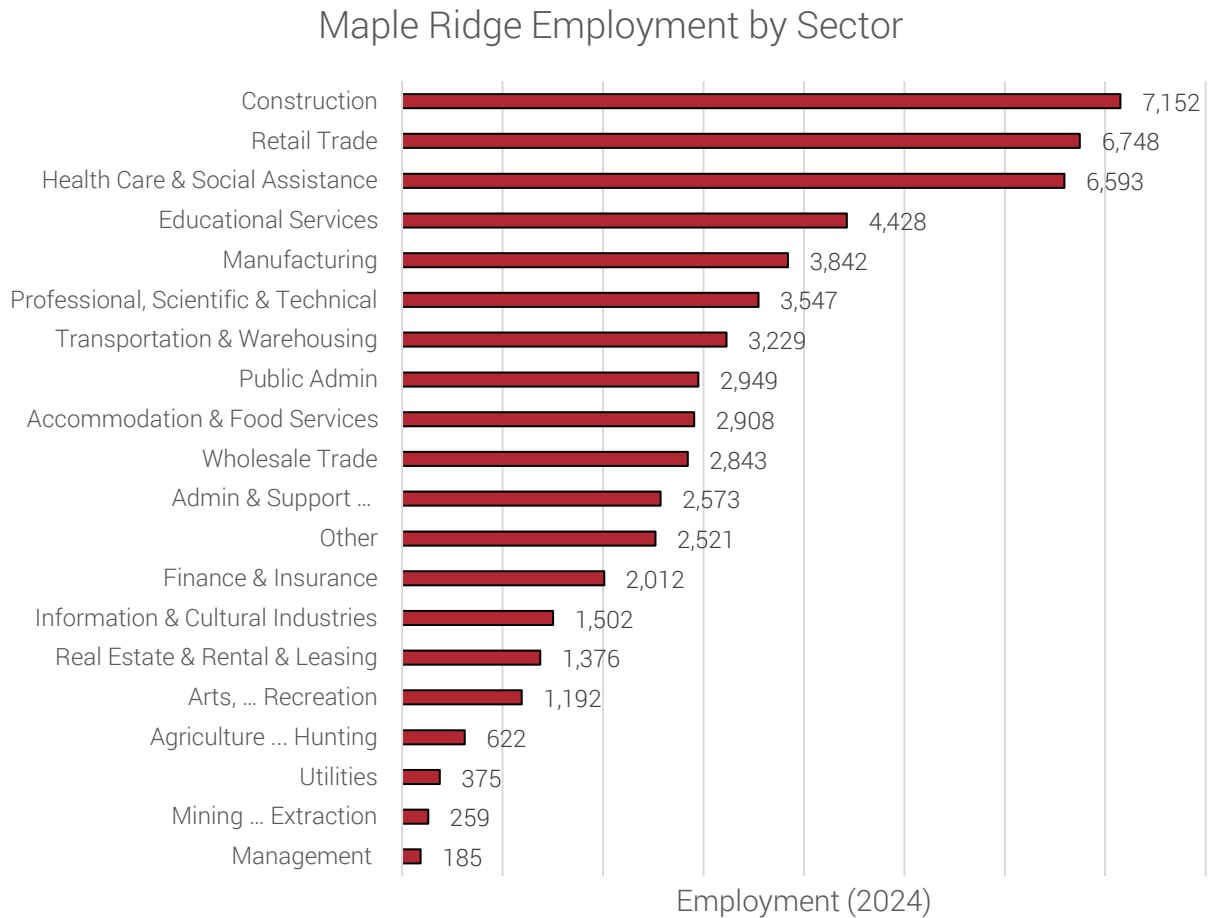


Source: EBP Analysis of Environics estimates via Esri Canada. Derived from Statistics Canada census data.

Occupational data tells a similar story to industry employment data (Figure 2). A plurality of Maple Ridge's labour force works in trades & transport and sales & service. These occupations best align with industries including construction, transportation & warehousing, retail trade. The third largest occupation category in Maple Ridge is business, finance, and administration, which is best reflected by the professional services industry.

Maple Ridge's largest employment sector remains construction (7,200 workers), although shift share analysis showed that this sector had the greatest negative local competitive effect, in that this sector's employment underperformed relative to national and industry-level expectations (see Competitive Benchmarking section in the Investment Attraction companion report). Nonetheless, Maple Ridge's concentration of construction workers still suggests robust activity in the City (Figure 3).

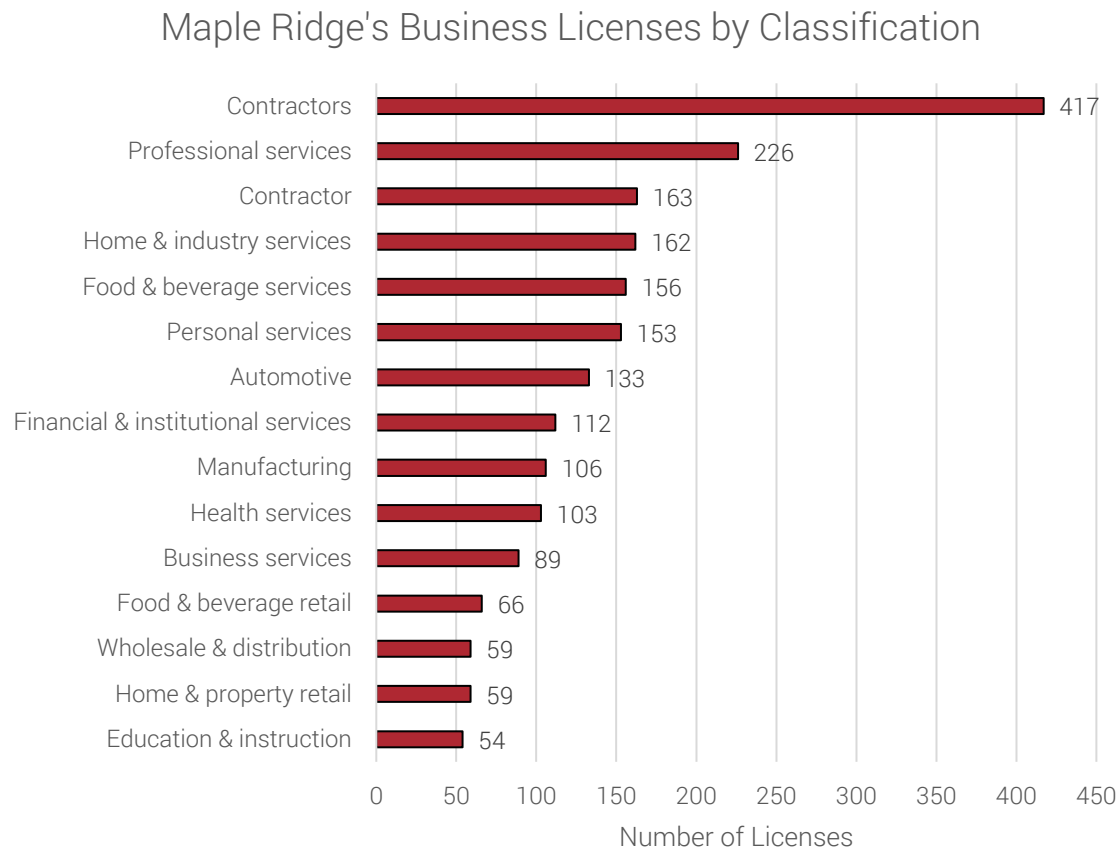
Figure 3. Maple Ridge Employment by Sector



Source: EBP Analysis of Environics estimates via Esri Canada. Derived from Statistics Canada census data.

In terms of employment, construction is closely followed by retail trade (6,700 workers) and healthcare (6,600 workers), and more distantly by educational services (4,400 workers), manufacturing (3,800 workers), and professional, scientific, and technical services (3,500 workers). All these sectors exhibit local competitiveness, which reflects Maple Ridge's above-average employment growth in recent years. Manufacturing's local competitiveness is particularly noteworthy, especially given recent manufacturing developments not yet reflected in historical public data.

Figure 4. Maple Ridge's Business Licences by Classification



Source: EBP Analysis of Maple Ridge business licence data via Business Finder.

## 2. Key Trends

The following selected trends are important to consider within the context of Maple Ridge's local BRE program.

### 1. Local Trends

**Workforce housing needs.** Maple Ridge faces a growing need for affordable housing to house its workforce and attract talent. Addressing this need is essential for business retention and workforce stability, particularly as remote work becomes more prevalent.

**Room for expansion.** About 56% of the City's commercially zoned lands are developed while 11% are vacant and 33% are under-utilized, particularly in the Town Centre and Lougheed Highway areas.



There is an opportunity to leverage vacant and underutilized commercial land in the Town Centre. Supportive policies and zoning could accommodate both business expansion and new residential development, contributing to Maple Ridge's overall BRE goals.

**Competitive edge in manufacturing.** Maple Ridge's manufacturing sector, particularly advanced manufacturing, remains competitive despite broader industry challenges. The increasing demand for skilled trades and technological advancements ensures the sector's continued resilience and growth. Notably, there is a growing provincial interest in developing the prefabricated, mass timber, and modular housing industry, further strengthening its prospects.

**Educational institutions.** A local post-secondary education campus could provide valuable support to businesses in Maple Ridge, particularly in the professional services sector. There is growing interest in exploring such opportunities. Additionally, given the City's strong construction industry, there is a clear need for a trade/ technical school to help develop a skilled workforce.

**Robust film industry.** While not captured in employment and business licence data, film industry activity in Maple Ridge is critical to retaining and expanding local businesses that serve the film industry.

## 2. Regional Trends

**Federal interest in trade relationships across the Pacific Ocean.** Maple Ridge is strategically positioned within the jurisdiction of the Port of Vancouver, allowing it to benefit from federal efforts to increase trade activity across the Pacific. This opportunity aligns with growing interest in transportation and multi-modal logistics, supporting potential growth in the warehousing and distribution sectors.

## 3. National Trends

**Supply chain and trade policy disruptions.** Volatile U.S. trade policies and reshoring efforts are driving manufacturers to diversify or shorten their supply chains to reduce risks. This shift creates opportunities for Maple Ridge to attract manufacturers seeking stable, local alternatives and reliable infrastructure.

**Technological disruptions.** Manufacturers are increasingly adopting new technologies like automation and artificial intelligence to improve efficiency and manage costs amid inflation and an aging workforce. This technological shift is reshaping the industry, with AI playing a key role in optimizing processes and asset management, presenting opportunities for local businesses to innovate and stay competitive.

### 3. Implications for Maple Ridge's BRE

#### 4. Home-Based Business

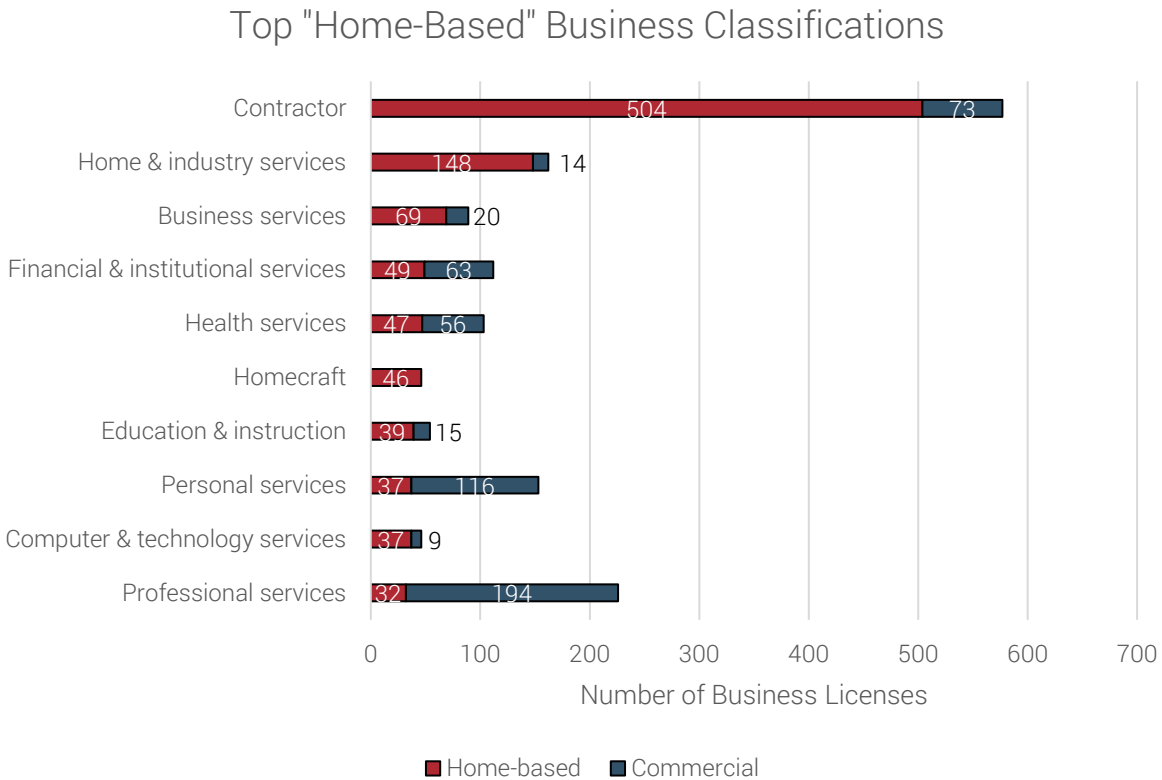
Home-based businesses are another key segment of Maple Ridge's local economy. According to Maple Ridge's business licence data on Business Finder, 1,358 (nearly half) of the City's 2,739 licences are classified as home-based.<sup>15</sup>

Home-based businesses, however, should not be conflated with WFH. For example, most contractor and agricultural business licenses are classified as home-based, but these sectors' activities are typically in-person and geographically bound. That said, home-based businesses and WFH businesses will likely necessitate a similar BRE strategy, i.e., focus groups and individual outreach rather than site visits and business walks, as there may be no brick-and-mortar location.

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<sup>15</sup> Business Finder data, provided by Maple Ridge Economic Development

Figure 5. Maple Ridge's Top Home-Based Licence Categories



Source: EBP Analysis of Maple Ridge business licence data via Business Finder.

## 5. Brick-and-Mortar Businesses

Maple Ridge's business community also includes many non-home-based businesses. Professional services, food & beverage services, personal services, and retail-related businesses are the most numerous, but these businesses likely have small numbers of employees.

By contrast, sectors like manufacturing and healthcare have fewer business licences but far more employees, indicating they are larger businesses. It is also noteworthy that shift share analysis suggests that both these sectors have local competitive advantages.

*Professional, scientific, and technical services*

With 3,500 workers, the professional services sector is the sixth largest sector in Maple Ridge. Its closest corresponding business licence category has the largest number of commercial business licences.<sup>16</sup>

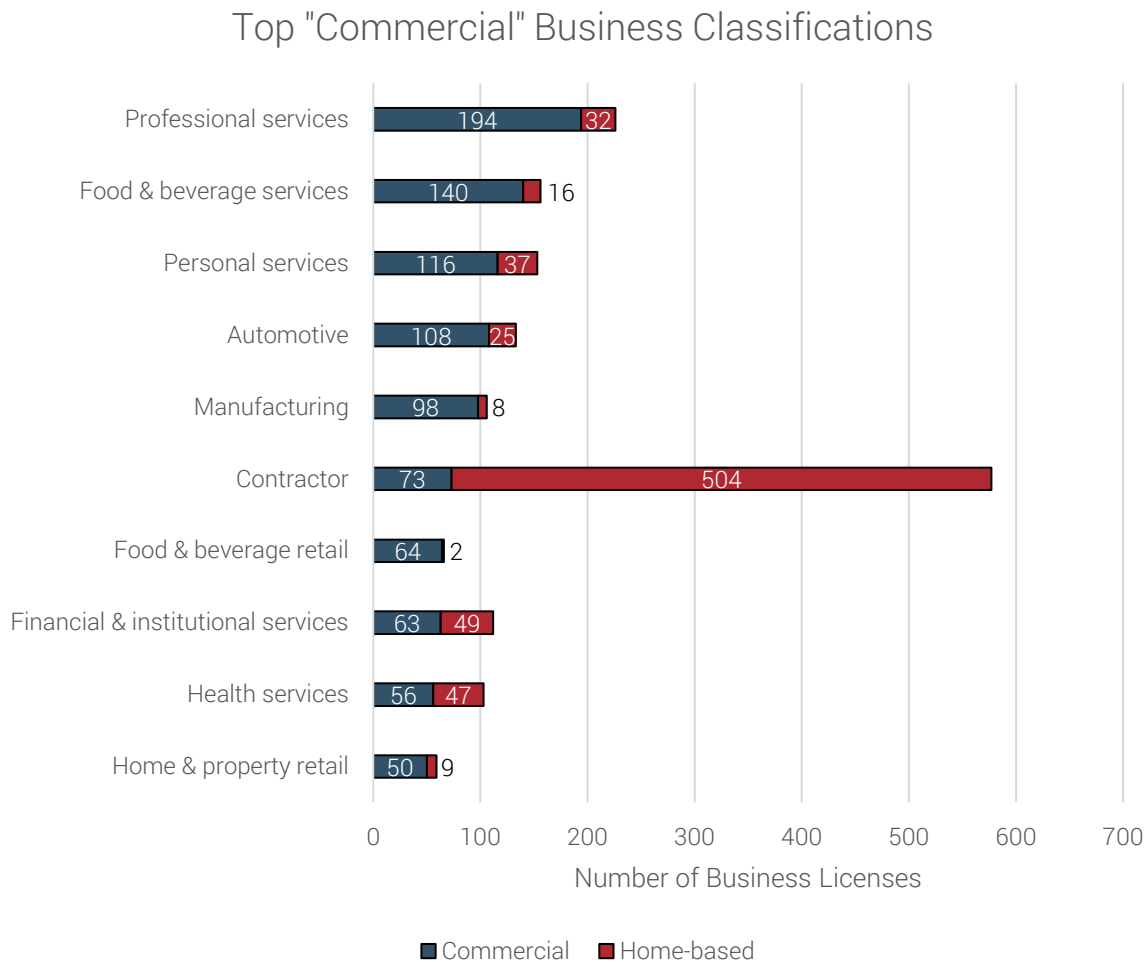
This sector captures a wide variety of economic activity and subsectors. **Engineering services** remain competitive through innovation and serve both renewable and traditional energy markets, with smaller firms thriving regionally. Meanwhile, **management consulting** has experienced revenue fluctuations, with demand surging post-pandemic for restructuring and risk management, particularly in major provinces. **Environmental consulting** is growing as stricter regulations and sustainability concerns drive demand, especially in construction and energy. Finally, **IT consulting** in Canada is especially important as the country sees increasing activity cloud-based services and networking infrastructure.

All these subsectors share a need for skilled talent pipelines (post-secondary education is often required) and office space availability. Maple Ridge's Official Community Plan indicates that there is room for opportunity on both issues. Maple Ridge is exploring post-secondary institution options to better service this sector's businesses.

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<sup>16</sup> EBP is unfortunately unable to crosswalk license and employment data because Maple Ridge's business license categorization scheme does not align with traditional NAICS codes.

Figure 6. Maple Ridge's Top Commerical (not Home-Based) Licence Categories

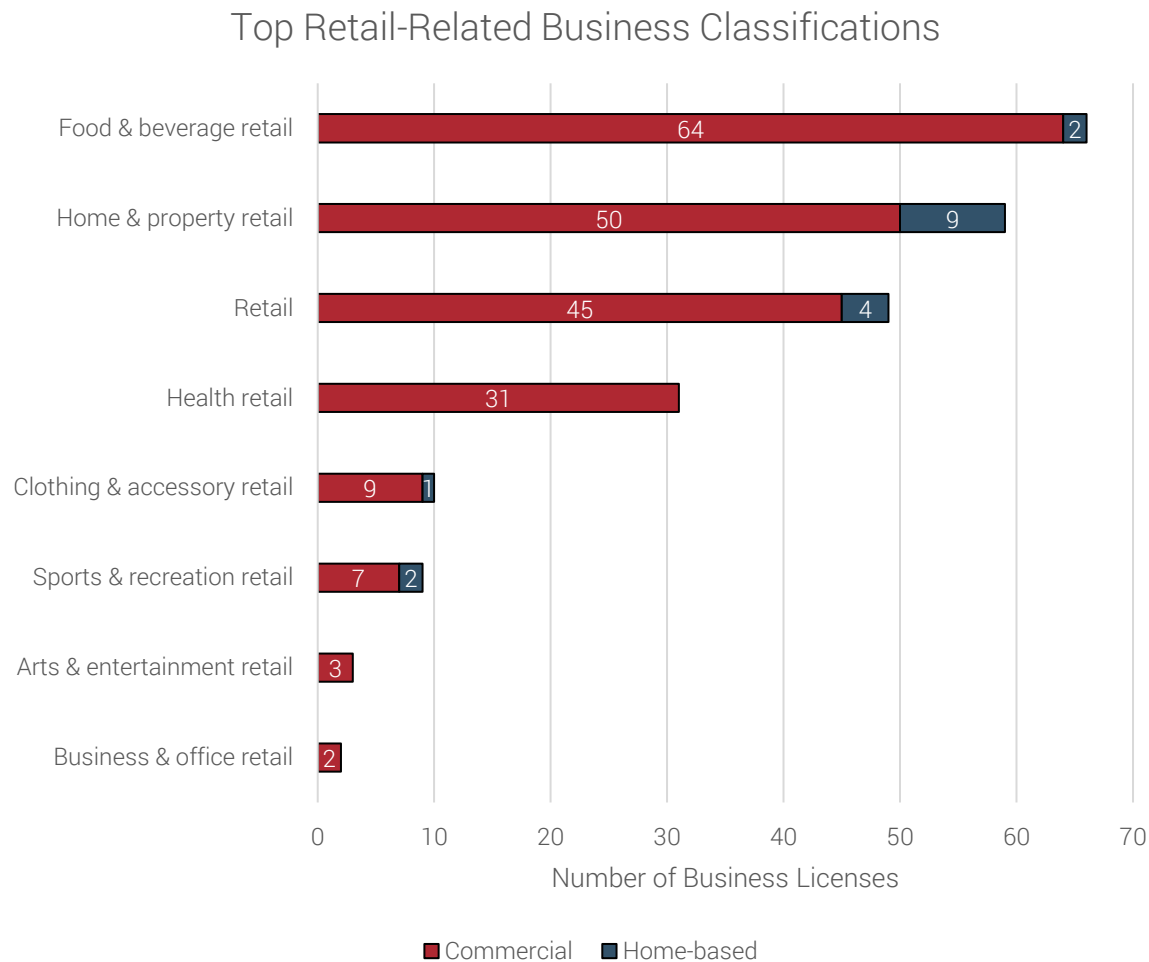


Source: EBP Analysis of Maple Ridge business licence data via Business Finder.

### *Accommodation & food services and retail*

Maple Ridge's accommodation and food services sector is best reflected in its food & beverage services licences. With 6,700 workers, retail is the second largest employment sector while accommodation and food services are the ninth largest employer sector (2,900 workers). In terms of business licences, food & beverage services have the second largest number of commercial business licences. A majority of the 62 licences in food & beverage retail are convenience stores, supermarkets, and liquor stores while nearly all food & beverage services are restaurants.

Figure 7. Maple Ridge's Top Retail-Related Licence Categories



Source: EBP Analysis of Maple Ridge business licence data via Business Finder.

The retail industry is typically more sensitive to consumer behaviour changes and economic volatility. That said, Maple Ridge's Retail Market Analysis suggests that the City has the consumer demographics and disposable income to meet unmet retail demand in the City. The study found that most people are shopping for essentials in Maple Ridge, but leave for non-essential "destination" shopping, to such shopping districts as Willowbrook in Langley Township, Langley City Centre and Coquitlam City Centre. This suggests that there is unmet demand for big-name retailers (e.g., Costco, Home Depot, Best Buy) and a revitalized downtown.<sup>17</sup>

The report highlighted retention opportunities for grocery, drug store, personal services, electronics, books/media, toys/hobbies, and family entertainment. The report also identified

<sup>17</sup> City of Maple Ridge, "[Retail Market Analysis](#)" (Dec 2023)



attraction opportunities for mall-type spending (e.g., health & beauty, accessories, specialty retail, sporting goods, full-services restaurant, and clothing/shoes/jewellery). Building on this report and changing trade patterns in response to U.S. tariffs, there is a growing opportunity to promote 'buy local / buy Canadian' movement to spur more local activity.

### *Manufacturing*

Manufacturing stands out as Maple Ridge's fifth largest employment sector (3,800 workers) despite having a smaller number of business licences than the above sectors, suggesting that manufacturing facilities have a high concentration of employment. With an average hourly wage of \$31.00 in 2021, the manufacturing sector boasts strong wages.

While Maple Ridge's business licence data does not specify the subsector for a plurality of licences, the most numerous subcategories for which there is data include wood products, food and beverage, metal, sawmill, and machinery & equipment.

The economic outlook for manufacturing is complicated by inflation and high costs. While efficiency through technologies show promise for cutting costs and scaling operations, manufacturers commonly cite prohibitive costs and uncertainty.<sup>18</sup> There are persistent challenges related to inflation and supply chain disruptions, especially amid US-Canada trade policy volatility. Sudden and uncertain changes in tariff policy will have mixed effects across subsectors and will likely cool business investment in the near term.

Economic outlooks vary for Maple Ridge's manufacturing subsectors:

- **Wood products** – Trade policy has large implications, as the US imports account for nearly half the Canadian wood industry's revenue. Building on tariff rate increases on Canadian softwood lumber in 2025, the current US administration has taken additional steps to increase the US's domestic supply of lumber.<sup>19</sup> Citing these tariffs as well as rising costs and reduced demand after a 2021-2022 housing construction boom, major sawmill and wood production companies (e.g., West Fraser, Canfor) closed mills in Canada.<sup>20</sup> These negative outlooks have critical implications for Maple Ridge's sizeable number of wood product and sawmill, shake, and shingle manufacturers, which together account for at least 27.6% of the City's manufacturing business licences.

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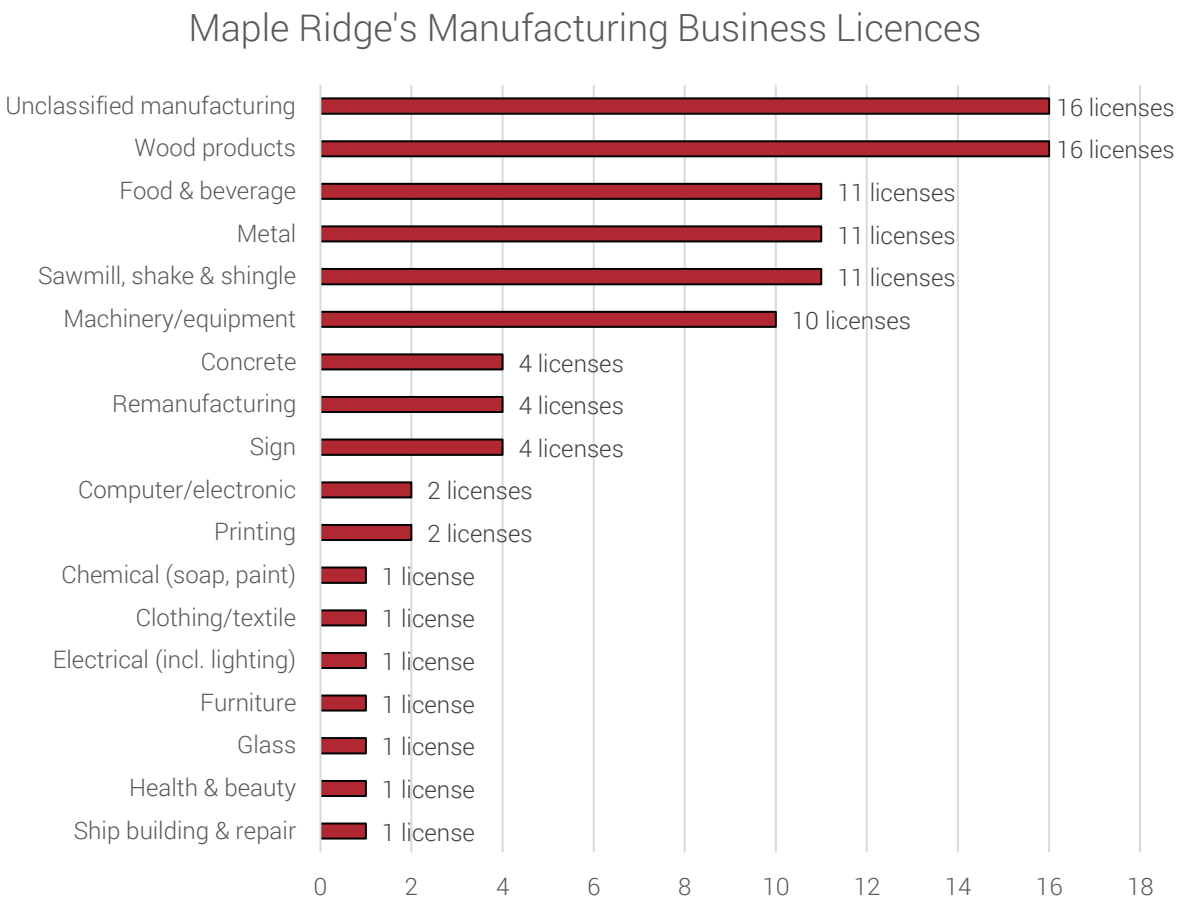
<sup>18</sup> *Canadian Manufacturing*, "[2025 Advanced Manufacturing Outlook](#)"

<sup>19</sup> National Association of Home Builders, "[US Nearly Doubles Canadian Lumber Tariffs](#)" (Aug 2024); *New York Times*, "[Economic Upheaval and Plans to Retaliate: Canada Braces for Tariffs](#)" (Mar 2025)

<sup>20</sup> IBISWorld, "[Sawmills & Wood Production in Canada - Market Research Report \(2015-2030\)](#)"; IBISWorld, "[Wood Panelling Manufacturing in Canada - Market Research Report \(2015-2030\)](#)"

- **Food & beverage manufacturing** – Food & beverage manufacturing is one of Maple Ridge's largest subsectors. It is expected to improve but remain "lean" amid a decrease in consumer spending (particularly for alcohol) and persistent inflation.<sup>21</sup>
- **Metal manufacturing and machinery & equipment manufacturing** – Due to improvements in production techniques and efficiency technologies (e.g., prototyping, 3D modelling, laser cutting, and automation), metal product and machinery & equipment manufacturing have performed well despite market volatility. These improvements are expected to drive revenue growth.<sup>22</sup> This may bode well for Maple Ridge's 11 metal manufacturers and 10 machinery & equipment manufacturers, depending on their operations.

Figure 8. Maple Ridge's Manufacturing Business Licences



Source: EBP Analysis of Maple Ridge business licence data via Business Finder.

<sup>21</sup> [Food in Canada](#); For more on outlook, see also [BC Food & Beverage](#).

<sup>22</sup> *Canadian Manufacturing*, "2025 Advanced Manufacturing Outlook"; IBISWorld, "Structural Metal Product Manufacturing in Canada - Market Research Report (2015-2030)"

### *Healthcare and social assistance*

Healthcare is Maple Ridge's third largest employment sector (6,600 workers). This suggests that healthcare businesses have a high concentration of employment. This aligns with hiring studies conducted in 2022 and 2023 that showed healthcare as the top hirer across multiple quarters. Top hirers included Provincial Health Services Authority, Fraser Health Authority, and Prime Health Ltd. The Ridge Meadows Hospital, operated by Fraser Health, is a particularly large single employer.

The outlook for healthcare employment in Canada is positive, given expectations of an aging population with greater healthcare needs. At the same time, these expectations could be tempered by the fact that Maple Ridge's healthcare sector has slightly under-average employment concentration in comparison with Canada at large, and that the City has a slightly younger average population.

## 6. Work-from-Home

**BRE strategies have shifted in recent years to capture the benefits of remote business.**

Economic development organizations have invested in campaigns, for example, to target early-career workers looking to relocate from traditional employment hubs like Vancouver back to their suburban hometowns. Some jurisdictions even offer moving expense stipends and homebuying assistance to reattract workers.<sup>23</sup>

**An estimated 18.8% (10,900 individuals) of Maple Ridge's labour force works from home.**<sup>24</sup> Maple Ridge's work-from-home (WFH) population is contextualized by a rise in remote work and digital-based professions across North America. As of November 2023, about 20% of work hours in Canada were performed from home.<sup>25</sup>

**A strong quality-of-life value proposition is critical to retaining workers and supporting the local businesses on whom they depend.** Although supporting large brick-and-mortar employers remains important, strategies must balance this approach with support for live/work/play initiatives to attract and retain local talent that stimulate local business. In other words, BRE is not just about business logistics and permitting, but also supporting a high quality of life:

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<sup>23</sup> Consult IEDC's "[Live, Work, and Play: Attracting and Retaining Tomorrow's Talent](#)" for examples of how EDOs have integrate quality-of-life initiatives into BRE strategies. The report outlines marketing collateral, microsites, and remote worker attraction programs.

<sup>24</sup> Environics estimates via Esri Canada. Derived from Statistics Canada census data.

<sup>25</sup> Statistics Canada, "[Working from home in Canada](#)" (Jan 2024)

- **Lower cost of living** is the most important cited reason for workers' decisions of where to relocate, according to Development Counsellors International research in 2024.<sup>26</sup> The next cited reasons are housing costs, safety/crime rates, housing availability, quality healthcare, and ability to live/work/play in location without a long commute. Maple Ridge's efforts to increase its housing stock will play an important role in providing a variety of housing options and keeping the cost of living competitive.
  - Maple Ridge's Housing Needs Report showed very low rental vacancy and a need for denser rental unit developments for middle income households, possibly along the Lougheed transit corridor.<sup>27</sup>
- **Quality of life** plays an essential role in business and talent attraction and retention. Perceptions of safety and crime are a critical component and underscore the importance of Maple Ridge's façade improvement and initiatives to address property theft.
  - Many residents and workers also value the ability to access local amenities, including popular natural spaces and recreational destinations like Golden Ears Provincial Park.<sup>28</sup>
- **Reliable high-speed internet** is a key determinant of where WFH workers choose to locate, as many remote business models require good connectivity.

## 4. Conclusion

Manufacturing merits its own targeted BRE approach because of the City's demonstrated competitive edge. As a sector that provides high-wage, stable employment and can drive innovation, Maple Ridge should bolster support for manufacturing businesses. These companies are often key employers in the region and their growth tends to have a ripple effect across other sectors, including transportation, logistics, and skilled trades.

BRE support should focus on initiatives that help companies modernize and adopt new technologies like automation and AI, workforce development to ensure a skilled labour pool, and infrastructure improvements to support logistics and supply chain needs. Supporting manufacturers with access to capital, training programs, and incentive programs for technology adoption will help drive long-term growth and secure high-value jobs in the city.

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<sup>26</sup> DCI, "Talent Wars" (May 2024)

<sup>27</sup> City of Maple Ridge, "[2024 Housing Needs Report](#)" (Sep 2024)

<sup>28</sup> The importance of lifestyle and amenities is reflected in the City's value proposition in the 2021 Economic Development Strategy.

The quality of life plays an important role in Maple Ridge's economic development strategy. As remote work continues to be a significant and important form of work, the City's natural and built amenities, safety, and overall living environment are key factors in attracting and retaining both talent and businesses. Supporting quality-of-life initiatives, such as affordable housing, green spaces, quality schools, amenities, and community infrastructure, will enhance Maple Ridge's appeal as a place to live and work—particularly for remote workers who can bring their spending power to local businesses.

While retail and healthcare may not be primary targets for investment attraction, they remain major employers in Maple Ridge, providing stable jobs and supporting the local economy. Retail, despite its sensitivity to consumer spending shifts, provides a significant number of low-to-middle-skill jobs, while healthcare remains a cornerstone sector driven by sustained demand for services and institutional stability. These sectors are crucial for the local workforce and should continue to receive BRE support to ensure they remain stable and grow where possible.

EBP's BRE programming suggestions build off these findings and are spelled out in detail in the Implementation and Resourcing Plan.

## 6. Implementation and Resourcing Plan

The City of Maple Ridge is growing, and its business community plays a key role in driving economic growth. To support both existing and new businesses—regardless of size or sector—the City must adopt proactive approaches to Business Retention and Expansion (BRE). Before implementing specific BRE initiatives, it is essential to define what BRE means for Maple Ridge and establish the broader goals of the program. Given the City's strong presence of retail-focused and local businesses, targeted support from the City could help these businesses achieve positive and impactful successes. A BRE program should:

- Assist businesses in capitalizing on opportunities to enhance their growth potential using a wide range of business support services
- Eliminate or reduce local challenges that hinder businesses overall prosperity and growth
- Improve the attractiveness and competitiveness of the market area
- Foster resilience within the local economy

A well-designed, systematic, and sustainable BRE program consists of four interconnected components, mirroring the principles of effective private-sector customer service models. These include:

- Establishing strong **relationships with businesses**
- Delivering comprehensive **business support services**
- Maintaining ongoing **business engagement and follow-ups**
- Implementing effective **business knowledge management such as CRMs**

This section serves as an implementation plan for the City of Maple Ridge's BRE efforts. This plan provides:

- **A framework** for identifying and supporting businesses at different stages of growth
- **Guidance on relationship management** with key stakeholders such as industry associations, workforce development organizations, and utility providers
- **Actionable strategies** for outreach and engagement
- **Processes for tracking and measuring success**, ensuring accountability and continuous improvement

### 1. Internal BRE Structure & Processes

Streamlining internal BRE programs and processes is as important as developing public-facing relationships with the business community. Having a robust internal structure would make the program more resilient, efficient, and organized.



There are some best practices that City of Maple Ridge can consider for its BRE program:

- The City should conduct an in-house exercise to define clear goals / objectives that the City is trying to achieve under this program. This can take the form of an annual work plan to help maintain focus, set achievable goals, and allocate resources appropriately such as a financial support program for home-based businesses.
- For an effective plan to be implemented, goals should be Specific, Measurable, Achievable, Relevant, and Time-Bound (SMART). Regardless of the framework used, it is crucial to avoid the pitfall of merely checking boxes and instead maintain a strong focus on execution and tangible outcomes. See Appendix 1: Economic Development BRE Action Checklist for a sample action list that the Economic Development department can consider implementing.
- The City has immense potential to leverage its local strengths by focusing on strengthening the small business community rather than seeking Big Box Retailers. Insights from on-site focus group conversations indicate that many residents prefer to shop locally and want to see homegrown businesses thrive.
- The City should develop its own toolkit to understand the level of support it can provide to BRE activities in the community. The toolbox can cover:
  - Marketing efforts designed for new business outreach: Currently, the economic development department does not have its own dedicated Facebook or LinkedIn page, which can result in messages being unclear or getting overshadowed by other communications.
  - Support with land and facility needs: This is further expanded in the Asset Inventory document prepared by EBP for the Investment Attraction project and is applicable to BRE efforts as well.
  - Assistance with permitting and licensing processes: While the City is piloting a concierge service, there is room for improvement and better marketing.
  - Identify different business support services that the City can offer such as financial support, tax incentives, workforce development through training and retraining programs, access to technology resources, etc.
  - Provide guidance on reducing energy costs and ensuring compliance with environmental regulations. This knowledge bank can comprise programs available from local utilities as well as from the province.

*"You can't manage what you can't measure" – Peter Drucker*

## 1. Organizational Leadership

The BRE team is responsible for program sustainability, entrepreneurial development, business recruitment, community development, and workforce development. As the team is expected to wear multiple hats, effective and strong program management is critical to the success of BRE initiatives. The program manager should be a senior-level economic development or allied professional affiliated with the sponsoring organization. The ideal program manager should possess excellent communication and organizational skills, along with experience in front-line economic development and/or private-sector sales. They should be adept at using technology, particularly CRM systems, and managing multiple priorities simultaneously. Since BRE managers often need to identify and address program deficiencies, they must also have the confidence and decisiveness to make tough decisions when necessary.

## 2. Organizational/Team Structure

An ideal BRE team for the City is described in Figure 9.

Figure 9. BRE Team Organizational Chart



### *Job Descriptions – Economic Development Officer (EDO)*

When writing job descriptions, succinctly and clearly convey your human resource needs. Job descriptions should have the primary responsibilities and functions (experience and education) required for the job, the salary and benefits offered, and the date to begin. Although much of the search process to fill a position is done through professional networks, defining and communicating your needs is an important first step.

**Sample Job Description for Maple Ridge - Economic Development Officer (EDO)**

Education: Bachelor's degree required

Experience:

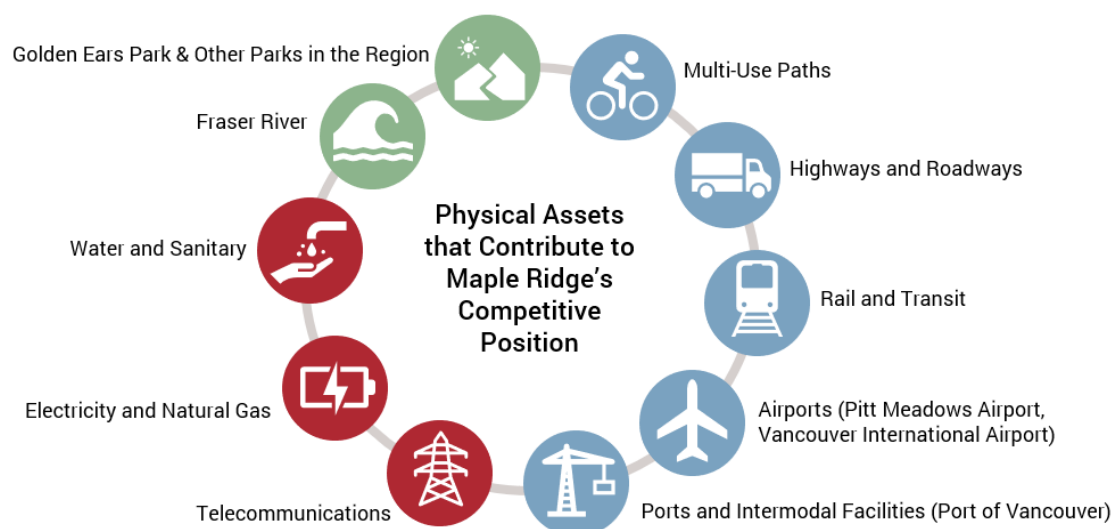
- Economic development exposure; experience with promoting economic development activities.
- Prior understanding of economic development needs in a constantly changing environment.
- Builds and maintains strong working relationships with departmental staff, municipal employees, local business owners, and the public.
- Tracks project progress, prepares reports, and ensures successful completion.
- Strong public speaking and written communication skills.
- Diplomatic internal and external communications skills; political savvy.

Source: Adapted from IEDC's "Managing Economic Development Organizations" Manual

### 3. Identify Key Assets and Resource Partners

To develop strategies suited to local conditions, the City of Maple Ridge must first gain a comprehensive understanding of its key economic assets. EBP's work on creating an Asset Inventory under the Investment Attraction project will serve as an initial step in helping the City identify its core strengths, resource capacity, and the level of service it can offer (Figure 10).

Figure 10. Maple Ridge's Physical Assets Scan



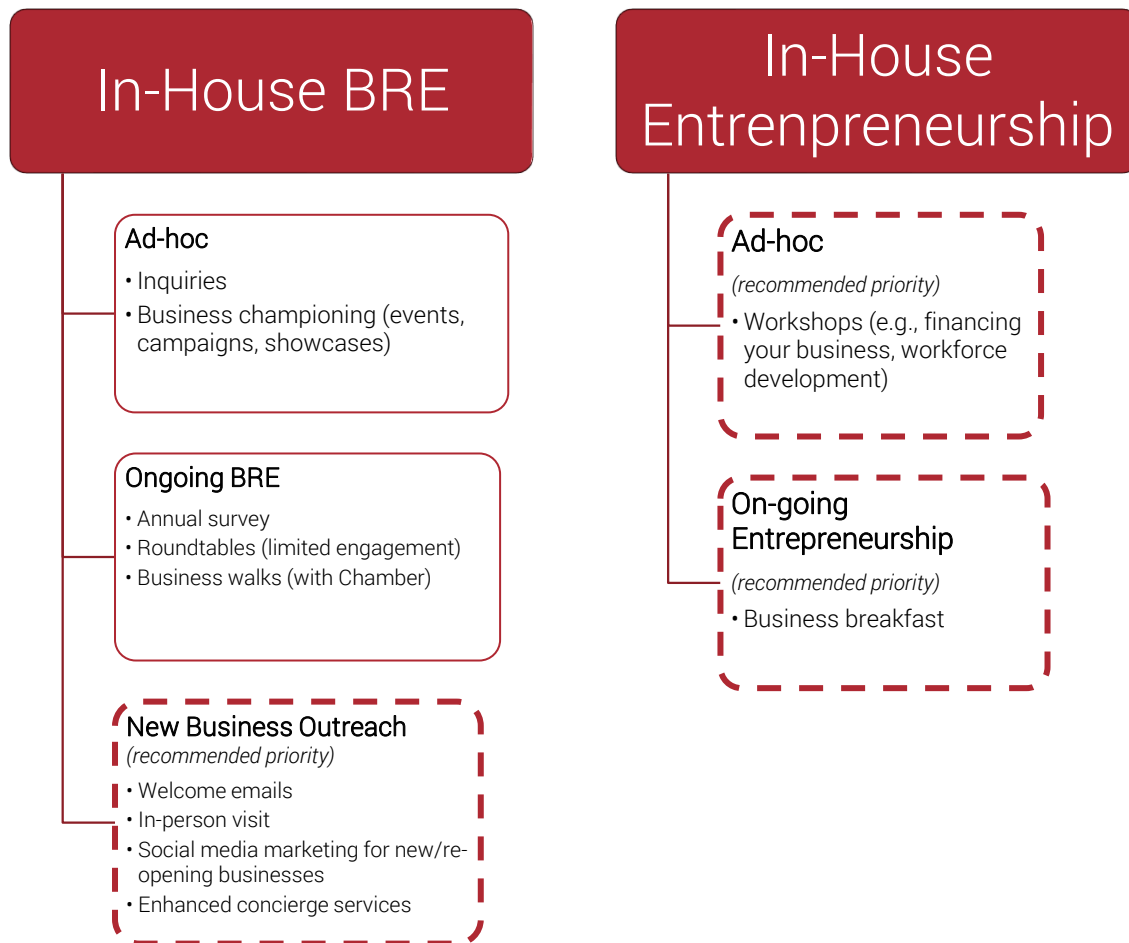
### Resource Partners

In most cases, valuable resource partners already exist in the form of community, economic, and workforce development organizations that independently serve businesses at the city, regional, or provincial levels. Rather than duplicating efforts, the goal is to collaborate with these organizations and leverage their technical expertise within the BRE initiative.

## 4. Current and Recommended In-house BRE and Entrepreneurship Programs

Figure 11 presents an overview of Maple Ridge's Economic Development's current practices as well as toolkit for recommended BRE activities. These recommendations, discussed in greater detail below, serve as a starting point from which this Implementation Plan can evolve.

Figure 11. Summary of Maple Ridge's Current and Recommended Internal BRE Practices



### Ad-hoc outreach

Maple Ridge Economic Development offers ad-hoc support to businesses. For example, the team addresses ad-hoc inquiries from businesses as they arise. The team intends to improve their CRM that minimizes multiple channels of manual input. Inquiry response is one of the primary interfaces for businesses, so while revisiting CRM functionality, Economic Development department should consider the following functions:

- **Clear handoff** - The system should enable a clear handoff process such as the one presented in the sample inquiry hand-off protocol below.
- **Response time standards** – The system should enable real-time monitoring of response time (e.g. 24, 36, or 48 hours, depending on the type of inquiry).

- **Report generation** – Useful report functions include average response times, inquiry source, types of businesses making inquiries, types of business needs, types of interventions, and outcomes related to business expansion, job creation, or investment.
- **Task automation** – Ability to automate time-intensive, repetitive tasks such as regularly planned outreach, thank you notes, and internal and external reminders.
  - Maple Ridge's business data system (based on permits and licences) is currently fragmented, which would not be solved by a CRM.

### Sample Inquiry Hand Off Protocol

1. **Entry into CRM** – Every inquiry from an online inquiry form is logged in the CRM with relevant details. The inquiry field could include:
  - a. business name,
  - b. sector/industry (NAICS is ideal but may not be possible given current data),
  - c. issue type (e.g., permitting/regulatory, expansion support, incentives and grants, workforce, infrastructure/utilities, local supplier or partner connections, general).
  - d. Inquiry priority level (e.g., urgent, routine, informational).
2. **Assignment & Notification** – The inquiry is assigned to a team member, who receives an automated notification. Assignment to a team member can be manually managed by an inquiry supervisor or could be assigned automatically based on the inquiry's topic.
3. **Acknowledgment** – The assignee must confirm receipt (either by clicking "Accept" in the CRM, replying to an internal notification, or another trackable method).
4. **Follow-Up** – The assignee responds within the defined timeframe (24-48 hours) or provides a status update if resolution takes longer.

Social media can also play a role in ad-hoc BRE. The Economic Development department does not have its own dedicated Facebook or LinkedIn, but the team does repost content from the City of Maple Ridge, the Ridge Meadows Chamber of Commerce, and Downtown Maple Ridge Business Improvement Association.

Whether on social media or emails or websites, ad-hoc communications should be responsive to real-time events. Economic Development and its partners currently engage in the following:

- Announcements about new businesses
- Grand openings and ribbon-cutting events
- Business spotlights and success stories
- Shop-local campaigns
- Local product showcases



- Opportunities for local businesses (e.g., grants, technical support services, educational resources)

### *New business outreach and in-house entrepreneurship (recommended)*

The BRE team can draw upon numerous tools to interface with the business community, with each tool having their own advantages and shortcomings. Because these methods can yield different answers, they can be used in tandem—they are not always either/or. For detailed guidance, we recommend [British Columbia Economic Development Association's BRE guide](#).

## 5. Program Methodology/ Approach

Maple Ridge Economic Development program currently uses a geographic approach, but it is important to segment your business community in other meaningful ways. Using the available business lists, Economic Development department should distinguish three broad groups of businesses—Elephants, Mice, and Gazelles. A one-size-fits-all approach won't maximize impact. These groups can also be divided by sector, geography, or thematic issue.

## A Segmented Approach to BRE

### Elephants (Large, Stable Firms)

- Common needs:
  - Predictable regulatory environment
  - Workforce pipeline and talent attraction
  - Infrastructure and utilities reliability
  - Expansion and reinvestment support
- BRE approach:
  - Regular executive outreach and site visits (facility tours)
  - Customized workforce development programs
  - Advocacy on policy and infrastructure needs
  - Proactive problem-solving for business retention

### Mice (Small, Entrepreneurial Firms that Stay Small)

- Common needs:
  - Access to local networks and resources
  - Low-cost business support services
  - Zoning and permitting assistance
  - Visibility in the local economy
- BRE approach:
  - Streamlined permitting and regulatory guidance
  - Small business grants or microloan programs
  - Marketing and promotion (e.g., shop local campaigns)
  - Business networking and mentorship connections

### Gazelles (High-Growth Entrepreneurial Firms)

- Common needs:
  - a. Access to capital and incentives
  - b. Scalable workforce solutions
  - c. Strong innovation ecosystem
  - d. Fast-track regulatory approvals
- BRE approach:
  - a. Introductions to investors and funding opportunities
  - b. Growth-oriented incentives (e.g., tax breaks, grants)
  - c. Rapid response permitting and site selection assistance
  - d. Support for R&D partnerships with universities or incubators

## 6. Communication and Digital Strategy

As part of its work plan, the City of Maple Ridge should actively promote the BRE program to the broader community. During field engagement, many groups noted significant changes occurring within the City. While optimism is growing, few businesses are aware of the available support services or how to access them, particularly in terms of business expansion opportunities that

can eventually increase retention. Strengthening outreach and communication efforts will help bridge this gap and ensure businesses can fully leverage the City's resources.

The program should deliver a consistent message about its benefits by partnering with local media outlets and leveraging communication channels established by other stakeholders. Organizations such as utility providers and educational institutions often run outreach initiatives targeting local businesses, presenting an opportunity to expand the reach and impact of the BRE program.

## 7. Monitoring and Evaluation Tools

The Economic Development department should continually monitor and evaluate the activities undertaken as part of this Implementation Plan. Monitoring and evaluation activities enable process improvement and also provides information to demonstrate to stakeholders that the department's resources are being used wisely and reassure partners that their efforts are having a meaningful impact.

Monitoring and evaluation begin by identifying metrics and measures of interest to the City and stakeholders. These metrics often include number of jobs, personal income, number of businesses, property values, investment in plants and equipment. They should incorporate and reflect the goals and values established by the City's economic development strategy as well as this BRE plan.

The team should regularly collect and maintain relevant data and information that can be used to evaluate progress toward established goals. The International Economic Development Council recommends the use of both process-based and outcome-based measurements. Examples of each type are presented below. Both types of measurements are of value on their own, however together, process-based and outcome-based measurements allow the team to connect their efforts to outcomes, i.e., the number of interactions per job created or number of active workforce development program partners and investment in workforce development.

### Examples of Process-Based Evaluation Measures

- Number of interactions with businesses across touchpoints
- Action items (partner referrals) generated
- Type of action items (partner referrals) generated
- Resolution of action items (partner referrals)—open, closed, unable to close
- Number of active program partners
- Programs, policies, or strategies developed because of the BRE program

## 2. External BRE Structure and Processes

### 8. Best Practices

Across all the touchpoints with relevant groups and business community (summarized in Table 4), consider these key principles:

- **Solicit candor.** Consider how the meeting format (e.g., number of participants, participants' relationships, prompts/questions, location) impact participation and

#### Examples of Outcome-Based Evaluation Measures

- Job creation
- Increased tax base
- Investment in talent/workforce
- Creation of affordable housing
- Creation of childcare centres
- Educational attainment of workers
- Export dollars/expansion of exporters
- Investment in technology
- Technology transfer
- Economic diversification
- Quality of jobs

honesty. There is no one-size-fits-all. Choose the most appropriate BRE tool/touchpoint to meet different expectations to put them at ease and make them feel heard and advocated for.

- **Build trust.** Building rapport is a long-term endeavour. Assuring confidentiality around sensitive information is essential to building trust over time. Tailor the Economic Development team's value proposition to the business's need. Businesses might not initially be receptive to BRE activities. They could be skeptical of its value, so it is important to demonstrate Economic Development's value to an individual business.
- **Be action oriented.** Businesses need to see that these calls and meetings yield results. The Economic Development department should identify next steps from each of these touchpoints and flag businesses for a follow-up (typically every 6 months or 1 year, depending on the business's needs). A good CRM should support these efforts to automate messages and reminders for outreach.

- **Be mindful of partners' interests.** Economic Development should identify possible points of friction or possible worries among partners. Be aware of who does what in the community and avoid making partners feel forgotten, usurped or threatened, e.g., they may have a system or process or goal that they may not want to give up or change. Consider how these interests can be aligned to play to partners' strengths.
- **Mark your calendar.** Always generate a to-do reminder from interactions. There should always be a reminder or event on the calendar. For example, if a company mentions they might undertake a new project in a couple months, create a reminder to ask how it's going. When you have a call or meeting with a business, flag them for follow-up in 6 months or a year. The CRM can be helpful for setting these reminders.
- **Tailor efforts.** Segment your high-value business community – downtown, at-home businesses, large vs small, startups vs mature firms. Also consider segmenting by target sector or geographic region. Outreach to these segments can overlap.

Table 4. Recommended Business Retention and Expansion Activities

Touchpoint	Description	Duration/ Frequency	Who	Advantages	Drawbacks
<b>In-person Facility Visits</b>	<b>Objective:</b> discuss business strategy (e.g., workforce, supply chain, markets), and discuss opportunities and threats	<b>Duration:</b> ≤1hr- tour of facility and organic discussion <b>Frequency:</b> 1-2 times per year for approx. 5 large target businesses	<b>Business:</b> 1 high- value targeted firm (ideally decision- making representative) <b>Economic Development department:</b> 1-2 economic developers (can include local gov't official or other partner)	<b>Build rapport.</b> One-on- one meetings build rapport <b>On-the-ground understanding</b> of a target business/ industry	<b>Time-consuming.</b> Facility tours can be time intensive. In- person facility visits are by far the most expensive tool.
<b>Business Walks</b>	<b>Objective:</b> Identify common opportunities and challenges. <b>Advance notice.</b> Be sure to notify businesses and strategize outreach tactic.  <b>“Business walks” vs “in-person facility visits.”</b> Walks focus on breadth (covering	<b>Duration:</b> 2- 4hrs (across 1- 2 days) <b>Frequency:</b> Once per year for 3-4 business regions	<b>Businesses:</b> Many businesses in a target geography <b>Economic Development department:</b> 1-3 team members, possibly two groups. Include partners (Chamber, City, Utilities)	<b>Presence.</b> Economic Development shows it is present in the community <b>On-the-ground.</b> The team can see businesspeople in their place of business <b>Cost-effective.</b> The team can cover much ground, introduce contacts, and leave information.	<b>Limited feedback</b> Some business may not have thought about what they want to say, and they may be busy with clients.

Touchpoint	Description	Duration/ Frequency	Who	Advantages	Drawbacks
	ground) while in-person visits focus on depth (just a handful of large employers)  British Columbia has widely cited guidance for BRE walks: <a href="#">read more</a> .				
<b>Focus Groups</b>	<b>Objective:</b> Identify collective challenges and opportunities and corresponding program/policy solutions.	<b>Duration:</b> ≤2hr <b>Frequency:</b> Once per year for 3-4 groups	<b>Businesses:</b> 4-6 businesses with a common theme (e.g., firm size, target industry, opportunities or threat) <b>Economic Development department:</b> 1 experienced facilitator	<b>Cost-effective</b> use of time <b>Flexibility:</b> In-person or virtual <b>Synergistic feedback</b> from businesses	<b>Marginalization:</b> Some businesspeople may be more reticent in a large group, esp. with competitors.
<b>Stakeholder Events</b>	<b>Objective:</b> Get face time with businesses and build rapport.	<b>Duration:</b> varies <b>Frequency:</b> varies	<b>Economic Development department:</b> 1-2 team members to	<b>Presence.</b> Economic Development shows it is present in the community	<b>Third-party.</b> Economic Development may be a third party in these



Touchpoint	Description	Duration/ Frequency	Who	Advantages	Drawbacks
	<p>Facilitate networking and resource-sharing</p> <p>Be sure to have the full calendar of community events held by the Chamber, City, etc. Economic Development should be present.</p>		cover events. The team can liaison with BIA and Chambers of Commerce to execute these events.	<b>Cost-effective.</b> The team can cover much ground, introduce contacts, and leave information.	spaces and may not be a priority for people.
<b>Social media</b>	<p><b>Duration:</b> 1-2 posts <b>Frequency:</b> weekly or monthly <b>Objective:</b> Disseminate information, promote opportunities, and keep Economic Development relevant</p> <p>Set up a regular posting calendar while also addressing ad-hoc topics as they emerge.</p>		<b>Economic Development department:</b> 1-2 team members to cover events.	<p><b>Low-cost.</b> Social media posts can reach a large audience with low effort.</p> <p><b>Open forum.</b> Social media offers another way for community members to voice thoughts.</p>	<b>Partial engagement.</b> Social media platforms' audiences vary. Some businesses may rely on social media, but others may not at all. This highlights the importance of having strategic redundancies.

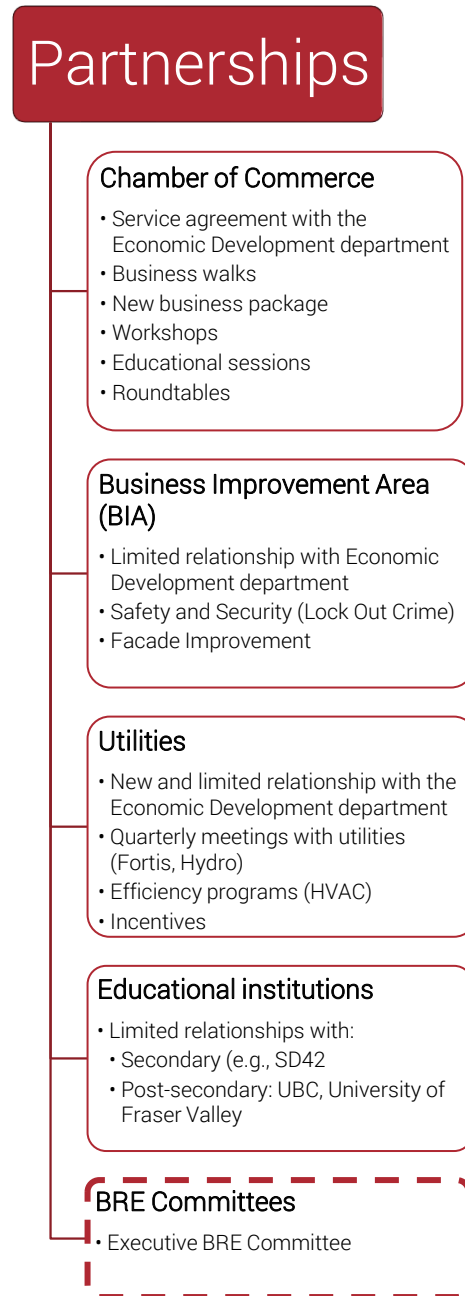
Touchpoint	Description	Duration/ Frequency	Who	Advantages	Drawbacks
<b>Digital communication</b>	<p><b>Duration:</b> 1</p> <p><b>Frequency:</b> weekly or monthly, or every other month</p> <p><b>Objective:</b> Disseminate information to various target audiences (by sector, geography, size, etc.)</p> <p>Yearly BRE surveys can also be sent by email or distributed via QR code.</p>		<p><b>Economic Development department:</b> 1 team member to manage or regulation all communications. The team should be coordinated on all digital touchpoints to avoid duplicated or inefficient efforts.</p>	<p><b>Low-cost.</b> Social media posts can reach a large audience with fairly low effort.</p> <p><b>Open forum.</b> Social media offers another way for community members to voice thoughts.</p>	<p><b>Limited engagement.</b> Sending too many or overly lengthy emails/surveys will reduce engagement. Be targeted in</p>
<b>Inquiry response</b>	<p><b>Duration:</b> varies</p> <p><b>Frequency:</b> as needed</p> <p><b>Objective:</b> Address particular issues and make businesses feel supported</p> <p>Always flag businesses for follow-ups months after an issue is resolved.</p>		<p><b>Economic Development department:</b> 1 team member to manage and route inquiries to the best contact.</p>	<p><b>Direct support.</b> Businesses feel supported.</p>	<p><b>Reactive, not proactive.</b> It's always better to proactively identify and address issues</p>

Touchpoint	Description	Duration/ Frequency	Who	Advantages	Drawbacks

## 9. Current and Recommended External BRE Partnerships

Figure 12 presents an overview of Maple Ridge Economic Development's partnerships.

Figure 12. Summary of Maple Ridge's Current and Recommended External BRE Partnerships



## 10. Economic Development Partnerships

Partnerships are central to BRE and Economic Development plays a coordinating role among various players in a complex ecosystem (**Error! Reference source not found.**). Economic Development does not have the time or resources to tackle every component of BRE, which means that collaboration is essential, and some prioritization is required.

Table 5. Actors and Roles in the Economic Development Ecosystem

	EDOs	Local Govt	Metro Vancouver Regional District (MVRD) Govt	Chamber	Utilities	Educational Institutions	Individual Business Leaders
Manage the program	✓			✓	✓		
Serve as an advisor to the program	✓	✓	✓	✓	✓		✓
Financially support the program	✓	✓	✓	✓	✓		
Conduct outreach	✓	✓	✓	✓	✓		
Evaluate outreach feedback	✓			✓	✓	✓	
Problem-solve	✓	✓	✓	✓	✓		✓
Identify subject-matter experts	✓			✓	✓	✓	✓
Provide technical assistance	✓			✓	✓	✓	✓
Ensure program sustainability	✓	✓	✓	✓	✓	✓	✓
Promote the program	✓	✓	✓	✓	✓	✓	✓
Advocate for the program	✓	✓	✓	✓	✓	✓	✓

Recruit participants for the program	✓			✓	✓	✓	✓
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### *Utilities*

**Improvement financing.** Utility companies can acquire capital improvement financing, which can in turn be disbursed via loans or grants to small-to-medium customers that otherwise cannot afford improvements.

**Site preparedness.** Utilities are also key partners to upgrading infrastructure for available sites. It is important that Economic Development communicate with utilities to identify high-potential sites that utilities can invest in. Utility companies can also provide up-to-date marketing material and site-level detail. The Economic Development department can also facilitate business expansion (or new investments) by liaising with utilities to ensure the expanding business's needs are met.

### *Committees (recommended)*

BRE committees (or task force) offer another way to build buy-in, better distribute activities, and gain strategic support. There are various types of committees. The most common and important committee is the Executive Committee.

**Executive Committees** can comprise representatives from local business or trade associations, utilities (e.g., Fortis), community foundations, workforce development organizations, the Chamber, universities or trade schools. This can also include organizations such as Invest Vancouver and IVAC. Involving diverse stakeholders helps give them a reason to engage with the Economic Development department and build trust. The purpose of the Executive Committee is to provide strategic direction validated by community members and to take on some of the BRE work. For example, committee members can support outreach, developing marketing materials, providing subject matter expertise for inquiries, and developing process improvements. Committee members also serve as Economic Development ambassadors to all businesses they interact with.

**Special-purpose committees (aka taskforces)** can be formed with more particular purposes, such as workforce issues, communications/outreach, economic data analysis, major project, or special issues.

**Role of Economic Development.** The Economic Development representative on the committee can serve as a facilitator but should ensure the Committee members largely steer themselves, both to give them a stake but also to avoid overcommitting Economic Development resources. Committee members find value in supporting their community while having an opportunity to promote their own business.

### *Other collaboration*

**Facilitating creative solutions.** There are many ways the Economic Development department can facilitate solutions among partners. For example, the team can contact local educational institutions or workforce organizations to set up a satellite program/courses in the local library about business taxes or business planning. The team can similarly facilitate a business-to-business initiative for a community warehousing space, other incubator-like spaces, or pop-ups for nascent businesses. Cities also encourage “beer or food walks” initiatives to foster business sales and increase footfall. In short, the team can identify collective needs among businesses and facilitate a possible solution that other stakeholders then manage.

**One-stop shop.** The Economic Development department should serve as a one-stop shop, having at hand the knowledge and resources businesses need to succeed. As a central connector, all roads should lead to Economic Development — offering guidance on permitting, business establishment, financing options, and continuity planning. The team should be able to quickly route businesses to diverse resources:

- Federal/provincial/local and intergovernmental programs
- Utilities, Chambers, and all other partners.
- Workforce
- Taxes
- Permitting

**Local business champion.** The Economic Development department should continue to actively promote local businesses and events online, amplifying their visibility and fostering community engagement. Beyond resources and promotion, the team should champion economic development by recognizing and appreciating businesses through dedicated events, reinforcing Economic Development’s commitment to a thriving business ecosystem. Economic Development can always ask its partners how the team can help them.



## Appendix 1: Economic Development BRE Action Checklist

This non-exhaustive checklist serves as a starting point to implement EBP's recommendations.

### Quick Wins

☐ Brainstorm to list potential "quick wins" to enhance Economic Development team's impact and improve the business ecosystem.

☐ Ensure that the City's website serves as a one-stop shop for BRE. What quick additions or fixes can be made?

### Ongoing BRE Activities

☐ Using the City's business finder directory, identify key segments that merit specific attention.

List the firms for each category and mark levels of priority

☐ "Elephants," "mice," and "gazelles" (mark the highest priority firms in terms of employment or economic contribution)

☐ Target sectors (e.g., film)

☐ Geographic areas (Town Centre, Silver Valley, etc.)

☐ Determine the cadence, content, metrics, and managers/partners of recommended BRE activities. Set up a recurring calendar (or excel sheet or CRM) to prompt follow-up. Consider the following activities:

☐ Facility visits (one-on-one) with ~5 priority firms – recommended 1-2 times a year

☐ Business walks by geography – recommended once a year for 3-4 regions.

☐ Focus groups – recommended once a year for 3-4 groups of 4-6 businesses grouped by size, sector, issue area, etc.

☐ Downtown revitalization – Consider how general BRE dovetail or complement downtown efforts. Consider how to consolidate activities and improve efficiency.

☐ Review the inquiry response system to align with BRE activities metrics and content

☐ Clarify process for inquiry response management (inquiry assignment, standards for response time)

### Partnerships

☐ Pursue ongoing relationships with utilities

☐ Develop Economic Development's pitch for collaboration with utilities

☐ Conduct outreach to utilities to ask about their needs and pitch possible points of collaboration (efficiency improvement funding, site preparedness, response to inquiries)

☐ Explore the creation of an Executive Committee

☐ Establish a purpose for the Executive Committee

☐ Identify possible partners and conduct stakeholder analysis (interests, concerns)

☐ Clarify process for inquiry response management (inquiry assignment, standards for response time)

City of Maple Ridge Investment Attraction Strategy - Costed Implementation Plan

The following table contains the Costed Implementation Plan for the Investment Attraction Strategy developed for the City of Maple Ridge. Further details for Actions can be found in text in the section, "Strategic Action Plan and Costed Implementation Plan" in the Investment Attraction Strategy Report Document.

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Vision and Operations in City Government	1. Lead the City in developing a unified vision for Economic Development and Investment Attraction.	1a	Regularly engage with City Council and the Senior Leadership Team on the importance of industrial and commercial investment for supporting high-value job growth and building a larger tax base.	Immediate and Ongoing. Once every quarter.	Very High	Economic Development Department, City Council, Senior Leadership Team, Planning and Building Department	Approximately 16 person hours of preparatory time for Economic Development. Approximately 1 hour of preparatory time for members of City Council and SLT. Approximately 1 hour of discussion for all meeting attendees.	Council and the SLT are informed and engaged participants in investment attraction discussions.
Vision and Operations in City Government	1. Lead the City in developing a unified vision for Economic Development and Investment Attraction.	1b	Ensure that key functions are properly assigned and resourced within the Economic Development Department and other City departments whose work relates to investment attraction.	Immediate and Ongoing (Within 12 months and Ongoing)	Very High	Economic Development Department, City Council, Senior Leadership Team, Planning and Building Department	These are some of the core functions of the Economic Development Department staff as they relate to investment attraction.	Economic Development Department has the necessary staff and tools to fulfill core functions. Ability and capacity to collect and track metrics in the Monitoring and Evaluation section (see Monitoring and Evaluation Metrics).
Vision and Operations in City Government	2. Prioritize focused and targeted interdepartmental communications.	2a	Establish clear channels for interdepartmental communication	Immediate and Ongoing. At a regular cadence to be determined by the Director of Economic Development.	Very High	Economic Development Department & Other City Departments	Economic Development to allocate at least 4 to 8 hours per month of staff time to engage with other City departments on topics of economic development and investment attraction.	City staff are informed and engaged participants in investment attraction discussions.
Vision and Operations in City Government	2. Prioritize focused and targeted interdepartmental communications.	2b	Develop standards for interdepartmental information and data sharing.	Immediate and Ongoing. Engage in an interdepartmental workshop 2 times per year.	High	Economic Development Department & Other City Departments	Approximately 4 to 8 hours of preparatory time for participants to: 1) Inventory current data resources and processes, 2) Identify gaps in departmental needs, 3) Consider opportunities for addressing gaps in needs.  2 hours per participant for meeting or workshop.	improved ability to share data between departments, facilitate or enhance collaboration between departments

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Vision and Operations in City Government	3. Continue to streamline processes to improve permitting and regulatory timelines and user experiences.	3a	Prioritize resources to adequately support the City’s Concierge Program	1 year and Ongoing	High	City Council, Senior Leadership Team, Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department, Human Resources Department	To be determined by responsible parties. The cost to implement this action is equal to the cost of one of more staff positions to serve as Concierge. Please note that costs will vary according to the assignment of and roles of the Concierge.	Number of Business Inquiries, Average time to process business permits, number of businesses successfully assisted
Vision and Operations in City Government	3. Continue to streamline processes to improve permitting and regulatory timelines and user experiences.	3b	Regularly engage with local businesses and industry to understand challenges and opportunities for improvement when it comes to improving the user experience of permitting and regulatory processes.	See BRE Implementation Plan	Very High	See BRE Implementation Plan	See BRE Implementation Plan	See BRE Implementation Plan
Vision and Operations in City Government	4. Adopt Data Collection and Dissemination Best Practices	4a	Advocate for the adoption of NAICS codes throughout the City and communicate the value of using NAICS codes.	6 months and Ongoing	High	Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department	<p>Approximately 4-8 individual staff hours of preparatory time for Economic Development Department and other City Departments to understand:</p> <p>1) The importance of NAICS Codes, 2) How to work with NAICS Codes, 3) Where within City processes NAICS Codes should be used</p> <p>As is required, time to engage with City Council and SLT on the importance and use of NAICS Codes. This would require an estimated 8 hours of staff time from Economic Development. Refined cost and staff time estimates to be determined by responsible parties.</p>	<p>Number of one-on-one meetings with key stakeholders – meetings with departments or leadership teams to address concerns and encourage adoption</p> <p>Percentage of relevant city departments using NAICS – Track the adoption rate across different City departments.</p>

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Vision and Operations in City Government	4. Adopt Data Collection and Dissemination Best Practices	4b	Institute the use of NAICS codes throughout the City	1 year and Ongoing	High	Economic Development Department, Planning and Building Department, and Bylaw, Licensing and Community Safety Department	Dependent upon the findings of Action 4a. Costs may include the resources needed for collecting industry data from existing businesses, resources to regularly collect industry data from future businesses, and resources for coordination with provincial offices (e.g., Provincial Assessor’s Office or the Ministry of Revenue) or other partners who may also collect or use NAICS codes for Maple Ridge businesses.	1) Use of NAICS in economic reports & analyses – Track how often NAICS-based data is used in sector analysis and investment strategies, 2) Percentage of new business licenses issued with NAICS codes – Ensure all new business licenses include NAICS classifications, 3) Percentage of existing business licenses re-classified using NAICS
Vision and Operations in City Government	4. Adopt Data Collection and Dissemination Best Practices	4c	Continue to collect and organize data that is relevant for site selectors and other location professionals	6 months and Ongoing (collect and update data on an annual or semiannual basis)	High	Economic Development Department	Core function of Economic Development Department. Updating Monitoring and Evaluation metrics will take a minimum of 16 hours of staff time.	Improved ability to provide data to interested investors or site selectors. Enhanced response time to site selector requests for information.
Vision and Operations in City Government	4. Adopt Data Collection and Dissemination Best Practices	4d	Continue to prioritize maintenance of City datasets and outward facing tools that support investment attraction and use interdepartmental data.	Immediate and Ongoing (update 2-4 times annually)	High	Economic Development Department, Information Technology Department, Planning and Building Department	Cost of hosting for different platforms or data sources. Approximately 4 to 8 hours of work hours to update data within each individual outward facing tool. Refined cost and staff time estimates to be determined by responsible parties.	Number of website visits to publicly available online tools, length of time to provide data to inquiring investors
Vision and Operations in City Government	5. Advocate for Policy that Supports Economic Development in Maple Ridge	5a	As a core function of the Economic Development Department, continue to determine how to enhance competitiveness within the region, and coordinate efforts at the regional, provincial, and federal levels to better support investment in targeted sectors.	Immediate and Ongoing (quarterly, semiannual, or annual meetings with Economic Development partners, as needed)	High	Economic Development Department, Manager of Intergovernmental Affairs	Approximately 8 work hours of scheduling and preparatory time per engagement. 1 to 2 hours of work hours per engagement.	Number of annual meetings with regional economic development partners, Reported business satisfaction over time

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Branding and Marketing	6. Develop updated marketing materials for investment attraction in key sectors	6a	Continue to define clear value propositions for the City and for its Key Sectors	6 months. Revisit on an annual basis.	High	Economic Development Department	An estimated 40-60 hours of staff time involving developing value proposition statements for key sectors and getting buy-in from Council and other City Departments as may be needed or required.	Number of business inquiries in target sectors
Branding and Marketing	6. Develop updated marketing materials for investment attraction in key sectors	6b	Develop updated marketing materials for target sectors	6-9 months	High	Economic Development Department	The cost for this action may vary. We estimate that it will require at least 40 hours of Economic Development Department staff time per sector to develop content, and at least 40 hours of graphic design work per sector to develop updated marketing materials for each sector. Refined staff time and cost estimates to be refined by responsible parties.	Number of businesses that marketing materials are distributed to, number of business inquiries
Branding and Marketing	7. Improve wayfinding on the City of Maple Ridge Website	7a	Conduct user testing and gather feedback and recommendations from businesses about ease of use.	1 year	Medium	Economic Development Department, Information Technology Department, Bylaw, Licensing and Community Safety (Business Licensing)	Approximately 40 hours for workshop design, scheduling, and results.	Businesses are engaged and interested in participating in user testing. Businesses speak freely about website user experience and provide constructive recommendations.
Branding and Marketing	7. Improve wayfinding on the City of Maple Ridge Website	7b	Implement recommendations from user testing.	2 years	Medium	Economic Development Department, Information Technology Department, Bylaw, Licensing and Community Safety (Business Licensing)	Dependent on the findings of Action 7a. This could include work time from Economic Development and Other Involved Departments to clarify or update information relevant to businesses, and could involve IT work time to update the City website. Refined staff time and cost estimates are to be refined by responsible parties.	Visitors to City Website (specifically “Build & Do Business”), Business License Applications, Business Inquiries
Branding and Marketing	8. Develop dedicated web content for investment attraction	8a	Determine what content should be highlighted on the investment attraction micro site.	1-2 years	Medium	Economic Development Department, Information Technology Department	Approximately 40 to 80 work hours for reviewing web content, project development and management, and perhaps developing an RFP.	A clear plan for content to highlight on the investment attraction micro site, Number of interested vendors for an RFP to develop micro site

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Branding and Marketing	8. Develop dedicated web content for investment attraction	8b	Contract a vendor to develop and launch a new investment attraction micro site.	2-3 years. The contracted working period for the development of a microsite to take approximately 3-6 months.	Medium	Economic Development Department, Information Technology Department, Invest Vancouver and other regional partners	Approximately \$70,000 CAD. Potential to receive matching funds from grants, such as those issued through the CanExport Community Investments Program from Global Affairs Canada	Number of visitors to investment attraction micro site, business inquiries
Networking and Communications	9. Prioritize Industry Networking	9a	Prioritize attendance at key conferences within the City’s targeted industries, and regularly follow up with business contacts made at conferences.	2 years	High	Economic Development Department, Intergovernmental Affairs, Invest Vancouver and other regional partners	The cost of this action will vary depending on location, duration, registration cost, and needed preparatory and post-attendance time by conference. Refined staff time and cost estimates are to be refined by the Economic Development Department and the Direct of Economic Development. The cost of conference attendance may include: 1) Hotels, meals, travel, and staff time associated with conference attendance. 2) Costs associated with planning, registration, preparing for attendance, etc. 3) Costs associated with follow up of conference attendance, such as organizing notes, information, entering contacts into the CRM system, and follow up communications.	Number of conferences attended per year (by sector), number of new connections made per year, number of connections that the City actively communicates with each year (aim to communicate with each connection at least once a year), track the number of potential investors at each level of lead generation: prospect, lead, qualified lead, investor
Networking and Communications	10. Continue to Engage with Local Businesses & Partners in the Regional Economic Development Ecosystem	10a	Identify and engage with key actors and ambassadors within targeted sectors	Immediate and Ongoing	Very High	Economic Development Department	This is a core function of the Economic Development Department. Approximately 10% of the Department’s time should be dedicated to engaging with business and organizational partners within the targeted sectors.	Number of entries to CRM representing network of key actors and ambassadors (ensuring coverage across all targeted sectors)

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Networking and Communications	10. Continue to Engage with Local Businesses & Partners in the Regional Economic Development Ecosystem	10b	Continue to improve engagement with local business	Immediate and Ongoing	Very High	Economic Development Department	see BRE implementation plan	see BRE implementation plan
Networking and Communications	10. Continue to Engage with Local Businesses & Partners in the Regional Economic Development Ecosystem	10c	Coordinate with the local real estate and development community.	1 year and ongoing	High	Economic Development Department	At least 8 hours of Economic Development Staff time per quarter.	Number of current contacts in the development and real estate communities, Development permits, Construction starts
Networking and Communications	10. Continue to Engage with Local Businesses & Partners in the Regional Economic Development Ecosystem	10d	Engage with local property owners in the downtown area to communicate and seek alignment on community goals for economic and business development.	Immediate: Collect ownership and contact information of local property owners in the downtown area (6-9 months). Medium Term: Develop relationships with local property owners and work to establish alignment on community goals for economic and business development (2-3 years)	Medium	Economic Development Department, Bylaw, Licensing and Community Safety (Business Licensing)	Dependent upon the availability of information, at least 16 hours of Economic Development Staff time to collect ownership and contact information. At least 2-4 hours of Economic Development Staff time per quarter for relationship building.	CRM entries for downtown property owners, number of conversations per year with each property owner
Networking and Communications	10. Continue to Engage with Local Businesses & Partners in the Regional Economic Development Ecosystem	10e	Inventory and continue to foster relationships with institutional partners in the regional economic development ecosystem.	Immediate and Ongoing	High	Economic Development Department, Intergovernmental Affairs	This is a core function of the Economic Development Department – to be determined by responsible parties. At a minimum, allocate 16 hours per quarter for fostering relationships with various institutional partners.	Number of engagements with regional partners



Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Networking and Communications	10. Continue to Engage with Local Businesses & Partners in the Regional Economic Development Ecosystem	10f	Continue to regularly engage with Kwantlen and Katzie First Nations partners on regional economic development.	Immediate and Ongoing	Very High	Intergovernmental Affairs, Economic Development Department	To be determined by Intergovernmental Affairs in collaboration with Economic Development Department. EBP recommends a minimum of 4-8 hours of Economic Development staff time per quarter for engagement on topics of economic development.	Regular communications and engagements, number of collaborative initiatives
Networking and Communications	10. Continue to Engage with Local Businesses & Partners in the Regional Economic Development Ecosystem	10g	Establish a connection with BC Assessment.	1 year and Ongoing	Medium-High	Economic Development Department, Planning and Building Department	At least 4 hours of staff time per quarter	Local business assessments improved as a result of communications with BC Assessment.
Community Outreach & Public Relations	11. Proactively Influence Positive Community Perceptions	11a	Coordinate with or collect data from regional or provincial data partners to maintain accurate crime data on the City and region.	2-3 years	Medium-Low	City Council, Police Department, Strategic Development, Communications, and Public Engagement Department, Economic Development Department	Economic Development Department staff should dedicate approximately 4-8 work hours each quarter to coordinate with other communities in the Lower Mainland and other partners.	more fulsome data set that allows for more detailed reporting and for more nuanced comparison to data from other jurisdictions
Community Outreach & Public Relations	11. Proactively Influence Positive Community Perceptions	11b	Improve visibility of safety efforts in the City of Maple Ridge.	2-3 years	Medium	City Council, Strategic Development, Communications, and Public Engagement Department, Economic Development Department	The cost of this effort should be jointly developed between City Council, Strategic Development, Communications, and Public Engagement Department, the Economic Development Department, and other relevant departments. At a minimum, this action requires Economic Development department staff time of approximately 4 hours monthly to coordinate with other departments and to help to develop or update a marketing or messaging budget geared toward this specific initiative.	Citizen Satisfaction Survey results

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Community Outreach & Public Relations	11. Proactively Influence Positive Community Perceptions	11c	Proactively improve communications and public relations related to public safety	1-2 years	Medium-Low	Strategic Development, Communications, and Public Engagement Department, City Council, Economic Development Department	This is a core responsibility for the City's Strategic Development, Communications, and Public Engagement Department. The Economic Development Department should dedicate approximately 4 hours per month to coordinate with other City departments on this topic.	Citizen Satisfaction Survey results
Community Outreach & Public Relations	11. Proactively Influence Positive Community Perceptions	11d	Regularly coordinate with BIA and Chambers to address needs and issues of downtown businesses, and to improve communications about services that are available to businesses downtown.	1-2 years	Medium	Economic Development Department, BIA, Chambers	To be determined by responsible parties. EBP recommends at minimum a monthly check-in or meeting with both the BIA and Chambers, requiring approximately 2-4 hours of Economic Development Department staff time.	reported business satisfaction

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Community Outreach & Public Relations	12. Continue to Support Community and Placemaking Events that Generate Economic Activity in Town Centre	12a	Allocate resources to support opportunities for economic and business activity at place-making events that draw residents downtown and encourage public gathering.	2-3 years	Medium-Low	City Council, Economic Development Department	The cost of supporting business activity or investor hosting at place-making events is highly variable dependent on the event itself and the type of investor the City wishes to host. Investor hosting may require multiple days of Economic Development Department staff time while the investor is in town, and may take several weeks of staff time for planning purposes. For investor hosting, the City should consider the following potential costs: 1) Identifying the respective focus or priorities of specific interested investors, 2) Identifying what to show an interested investor based off of their priorities, 3) Organizing business or site tours, 4) Planning logistics such as hotels, meals, and transportation	Increase in local business revenues during these events versus typical activity, Number of potential investors hosted at these events
Infrastructure and Business Supports	13. Maintain Inventory of Physical Assets	13a	Maintain inventory of physical assets and communicate the value of municipal and regional infrastructure & its value for attracting investment.	3 months and ongoing. Review and update every 6-12 months.	Very High	Economic Development Department, City Council	Approximately 40 hours of Economic Development Department staff time for development. Approximately 16 hours for each update.	Ability to communicate the value and role of certain assets in the region to economic development and various key industries, Sites identified for priority sectors (at least 1 site for each key industry)
Infrastructure and Business Supports	14. Proactively Create Investment-Ready Sites	14a	Develop (or update) a site readiness checklist for available sites in the City. A site readiness checklist should include utilities, transportation and market access, zoning and land use, and the business climate.	3 months and ongoing. Review and update every 6-12 months.	Very High	Economic Development Department, Planning and Building Department	Approximately 32 hours of staff time for development. Approximately 12 hours per review.	Percentage of industrial sites that are investment-ready, Development permits, Construction starts. Readily apply checklist of specific site requirements for each target industry to available sites as an evaluation tool.

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Infrastructure and Business Supports	14. Proactively Create Investment-Ready Sites	14b	Continue to implement existing recommendations for improving Industrial Areas, and utility service and transportation networks that service industrial and employment centres in Maple Ridge, as found in the City’s Industrial Impact Analysis, Sanitary Master Plan, Stormwater ISMPs, Strategic Transportation Plan, Community Improvement Plan, and the upcoming 256 North Plan.	Immediate and Ongoing (Refer to Associated Plans)	High	Refer to Associated Plans	Refer to Associated Plans	Refer to Associated Plans
Infrastructure and Business Supports	14. Proactively Create Investment-Ready Sites	14c	Continue to improve working relationships with utilities providers to (1) advocate on behalf of local businesses for possible service provision (2) explore opportunities for improved servicing.	Immediate and Ongoing (6-12 months). The City should aim to meet with utilities providers 2 to 4 times per year to discuss the needs of Maple Ridge businesses and explore opportunities for improvement.	Medium	Economic Development Department, Intergovernmental Affairs	At least 8 hours of staff time per quarter.	Percentage of all industrial sites that are fully serviced by utilities
Infrastructure and Business Supports	14. Proactively Create Investment-Ready Sites	14d	Continue to foster working relationship with TransLink.	Immediate and Ongoing (6-12 months). The City should aim to meet with TransLink 2 to 4 times per year to explore opportunities for better meeting the transportation needs of Maple Ridge residents and businesses.	Medium	Economic Development Department, Intergovernmental Affairs	At least 4 hours of staff time per quarter.	TransLink ridership to locations with start and end points in Maple Ridge, number of residents that live within 1 km of a TransLink transit stop

Recommendation Area	Recommendation	Action Number	Action	Timeline	Priority Level	Responsible Parties	Cost Estimate	KPIs
Infrastructure and Business Supports	15. Prioritize the establishment of a business class hotel in the City of Maple Ridge	15a	Continue to build momentum on the establishment of a reliable, business class hotel in the City of Maple Ridge.	Engage with members of the hotel industry immediately and at least once every 3 months. Collect data, develop marketing materials, and develop a site-readiness checklist within 6-9 months. Develop a list of prime hotel locations within 9-12 months and update every 3-6 months.	High	Economic Development Department, Planning and Building Department, Bylaw, Licensing and Community Safety Department, City Council, SLT	Cost estimate is dependent on many variables, including scope of the project. Responsible parties should dedicate at least 8-12 staff hours per quarter for engagement with members of the hotel industry. The City may consider enlisting outside support for conducting an additional hotel feasibility study, which would help in data collection, developing a list of prime hotel locations, and further identifying key contacts to engage with within the hotel industry: for a medium-sized hotel in a competitive urban market, the cost of a feasibility study can range between \$27,000 and \$70,000 CAD. Smaller hotels or ones in less competitive markets may cost around \$27,000 CAD.	Number of recipients of marketing materials, meetings with connections in the hotel industry, total number of potential hotel locations that are investment-ready, business inquiries

## **2024-228-RZ, 12011 223 Street**

### ***Zone Amending Bylaw No. 8034-2025***

#### **Recommendations:**

**THAT *Zone Amending Bylaw No. 8034-2025* be given first, second and third reading; and**

**THAT staff be directed to work with the applicant to address the outstanding terms and conditions as outlined in the Staff report dated May 20, 2025, and any other as identified by Council, prior to recommending bylaw adoption.**

#### **Report Purpose and Summary Statement:**

*Zoning Amending Bylaw No. 8034-2025* pertaining to the subject property, located at 12011 223 Street, is proposed for Council's consideration. This amendment seeks to adjust the density provision and make some minor changes to the applicable zone to facilitate the future construction of the proposed six-storey mixed-use building for Phase 2 of the ERA development.

#### **Previous Council Action:**

N/A

#### **Proposed Variance(s):**

N/A

#### **Strategic Alignment:**

Liveable Community

#### **Communications:**

Pursuant to Section 467 of the *Local Government Act*, a public notice of the first reading was published on May 16, 2025, with a subsequent publication scheduled on May 23, 2025, in the local newspaper. Notices were mailed out to area residents.

#### **Development Requirements Consequences:**

Terms and conditions are outlined in the report.

**To:** Mayor and Council

**File number:** 2024-228-RZ

**2024-228-RZ, 12011 223 Street**  
***Zone Amending Bylaw No. 8034-2025***

**BACKGROUND:**

Applicant:	Richard Berstein
Legal Description:	Lot A District Lot 399 GROUP 1 New Westminster District Plan EPP118505
OCP Designation:	
Existing:	<i>TCCOMM (Town Centre Commercial)</i>
Proposed:	<i>TCCOMM (Town Centre Commercial)</i>
Within Urban Area Boundary:	Yes
Area Plan:	Town Centre
OCP Major Corridor:	Yes
Zoning:	
Existing:	CD-1-17
Proposed:	CD-1-17
Surrounding Uses:	
North:	Use: Vacant Zone: RM-2 (Medium Density Apartment Residential)
South:	Designation: <i>Low-Rise Apartment</i> Use: Commercial Zone: Town Centre Commercial
East:	Designation: <i>Town Centre Commercial</i> Use: Commercial and Single Detached Residential Zone: CD-1-17
West:	Designation: <i>Town Centre Commercial</i> Use: Mixed Use Zone: CD-1-17 Designation: <i>Town Centre Commercial</i>
Existing Use of Property:	Commercial, Parking Lot, Vacant
Proposed Use of Property:	Mixed Use



Phase 2 Site Area:	4,209.33 m <sup>2</sup>
Phase 2 Net Site Area After	3,760.84 m <sup>2</sup>
Dedication:	
Proposed Vehicular Access:	223 Street
Servicing Requirement:	Urban Standard
Flood Plain:	No
Fraser Sewer Area:	Yes

## **BACKGROUND:**

### **Site Characteristics:**

The subject property is located at the intersection of the Dewdney Trunk Road and 223 Street. The site is proposed to be subdivided into two lots. The subject development is for the southern portion of the site with a total gross site area of 4,209.33 m<sup>2</sup> (Attachments A, B, and C).

The subject property is located in a transitional area of the downtown core. It is bounded to the north by Brown Avenue, across from which is an RM-2 (Medium Density Apartment Residential) zoned property. To the south lies Dewdney Trunk Road, with single-storey commercial buildings (zoned C-3, Town Centre Commercial) situated on the opposite side. To the east is 223 Street, across from which is a mix of single-storey commercial buildings and single-detached residential homes that are zoned either CD-1-17 (Comprehensive Development) or C-3 (Town Centre Commercial). To the west, the site is adjacent to the Phase 1 four-storey mixed-use building of the master-planned ERA Project.

The site is currently occupied by single-storey commercial buildings and a surface parking lot. It is generally flat and largely devoid of vegetation, except for a few trees along the eastern property line and a small, landscaped area within the parking lot. The applicant has submitted an arborist report which recommends appropriate tree replacement measures.

## **ANALYSIS:**

### **Project Description:**

The proposed development is Phase 2 of a seven-phase development called the ERA Project. The current proposal involves subdividing the subject site into two lots, separating Phase 2 from Phase 3, and facilitating the development of a six-storey, mixed-use building on the southern portion of the site for Phase 2. The proposed building will consist of 120 residential units and 7 ground floor commercial units. The residential entrance lobby will be accessed from 223 Street, while the commercial units will face Dewdney Trunk Road (Attachment D).

Vehicular access to the development will be provided via 223 Street. The commercial parking stalls, long-term commercial bicycle storage, loading area, and garbage pick-up zone are situated in a semi-concealed ground-level parking garage in the northwest quadrant of the building. The residential parking stalls, storage rooms, and long-term residential bicycle storage are located in a two-level parking garage (Attachment D).

To the north of the subject site, at the Phase 2 boundary, is a public east-west pedestrian corridor, which began in Phase 1 to the west and will continue through the subject site and into future phases of the ERA Project (Attachment E). This project continues the master-planned ERA Project, following Council's guidelines set during the pre-zoning of the seven-phase site (Attachment F).

Any subdivision layout provided at this stage is strictly preliminary and is subject to change prior to approval by the Approving Officer (Attachment G).

### **Discussion:**

As stated, the current proposed development represents Phase 2 of the larger ERA Project, which comprises a total of seven (7) phases (Attachment E). The site was rezoned to a comprehensive development zone (CD-1-17) back in 2021 to allow the development of a four-storey mixed-use building on the southern portion of the subject site.

Under the previously issued development permit (2021-586-DP), Phases 2 and 3 consisted of four primarily residential buildings. Phase 2 (along Dewdney Trunk Road) consisted of a four-storey residential use and commercial retail units along the south side of the building along Dewdney Trunk Road. It also included a two-storey clubhouse toward the middle of the subject site. Both Phases 2 and 3 were proposed to be located over a two-level basement parking garage.

The current application proposes some amendments to the Comprehensive Development zone (Attachment H) to facilitate the development of a six-storey mixed-use residential and commercial building, excluding a clubhouse for Phase 2 (Attachments D and E). There is an associated subdivision application to this rezoning application to subdivide the subject site into two parcels, separating the development of Phase 2 from Phase 3 (Attachment G).

### **Planning Analysis:**

This application was received in August 2024 and is subject to the new combined Development Procedures, which were adopted by Council on July 25, 2023, under *Development Procedures Amending Bylaw No. 7931-2023*.

In compliance with Bill 44 and the *Local Government Act Section 464*, a Public Hearing is prohibited from being held for this application since it consistent with the City's Official Community Plan (OCP), and more than 50% of the project is for residential use. Notice of Council consideration of first reading of the Zone Amending Bylaw was published on May 16, 2025, in the Maple Ridge Pitt Meadows News and is scheduled to be published again on May 23, 2025 and notices were mailed to area residents.

## Official Community Plan:

The development site is located within the Town Centre Area Plan, in the Downtown West precinct and is currently designated *Town Centre Commercial* (Attachment C). The current *Town Centre Commercial* designation supports the proposed development under the Comprehensive Development (CD-1-17) zone, as amended (Attachment H).

## Zoning Bylaw:

As noted the proposed development is part of a larger, multi-phase development. *Zone Amending Bylaw No. 7336-2017* for the CD-1-17 (Comprehensive Development) zone received final reading by Council on July 13, 2021. The CD-1-17 zone was structured to provide maximum development flexibility, with few restrictions for regulations such as setbacks, lot coverage, and building height, thereby enabling the project to be developed over multiple phases and be in compliance with the Master Plan for the project. However, each phase is still be required to obtain a separate development permit and lot consolidation or lot subdivision as the work progresses.

*Bylaw No. 7336-2017* was also crafted to allow for bonus densities in exchange for additional amenities. The density is expressed in terms of Floor Space Ratio (FSR) in three components:

- The **base density**, which is the FSR granted to the developer outright before any bonuses, is incorporated into the project;
- The **bonus density** is for the provision of underground parking and building height; and
- The **additional bonus density** is for providing all the amenity offerings indicated for each phase.

The proposed development is a six-storey mixed-use building with a total FSR of 2.36, covering an area of 8,889 m<sup>2</sup> (95,677 sq.ft.). The development complies with both the base and bonus density provisions outlined in the zoning regulations. However, it does not fully meet all the amenity bonus density (i.e., additional bonus density) requirements specified in the CD zone. Under the current CD zone regulations, the proposed development without a clubhouse would be eligible for a 2.24 FSR. However, since the proposed 2.36 FSR density is higher than the current allowed FSR, a text amendment to the CD zone is necessary. The amended CD Bylaw allows a maximum density of 2.54 FSR through both the base and bonus density provisions. The proposed development with a density of 2.36 FSR complies with this regulation, as it falls within the allowed density (Attachment H).

Table 1 outlines the density provision mechanism under the CD-1-17 as well as for the amended zone, along with what has been proposed by the developer.

**Table 1. Current and Proposed Density**

<b>Comprehensive Development – 1- 17 Zone</b>				
<b>Phase</b>	<b>Base Density</b>	<b>Bonus Density</b>	<b>Additional Bonus Density</b>	<b>Maximum Density</b>
<b>Phase 2</b>	1.0 FSR	Combined total 1.3 FSR:	Combined total 0.3 FSR:	2.65
		0.34 FSR for 100% of the required parking within a parking structure or an underground parking	Continuation of the east-west landscaped pedestrian way	
		0.3 FSR for each storey over the second (max 0.9)	Minimum four electric vehicle charging stations	
			Compliance with any BC Energy Step Code	
			Sustainability elements	
			A Clubhouse (if not provided in another phase)	
<b>Comprehensive Development – 1- 17 Zone as Amended</b>				
<b>Phase</b>	<b>Base Density</b>	<b>Bonus Density</b>	<b>Additional Bonus Density</b>	<b>Maximum Density</b>
<b>Phase 2</b>	1.0 FSR	Combined total 1.54 FSR:	Combined total 0.3 FSR:	2.84
		0.34 FSR for 100% of the required parking within a parking structure or an underground parking	Minimum seven electric vehicle charging stations	
		0.3 FSR for each storey over the second (max 1.2)	Compliance with any BC Energy Step Code	
			Sustainability elements	
			A Clubhouse (if not provided in another phase)	

The CD-1-17 zone requires a minimum of four electric vehicle charging stations for Phase 2. The number of residential units in the subject development proposal has increased from the number of units in the previously approved Development Permit (2021-586-DP) from 69 units to 120 units. Accordingly, the amended Zoning Bylaw requires a minimum of seven electric vehicle charging stations for the subject development proposal. No further amendments have been applied to the approved Comprehensive Development Zone. The subject proposal complies with the remaining of the CD zone requirements.

## Off-Street Parking and Loading Bylaw:

The development is located within the Central Business District area. The applicable vehicle and bicycle parking rates per the requirements of the *Maple Ridge Off-Street Parking and Loading Bylaw No. 4350-1990* are outlined in Table 2. The subject proposal meets the parking requirements (Attachment I).

**Table 2. Parking Requirements**

<b>Parking Space Requirement</b>			
<b>Category</b>	<b>Current Bylaw – Applicable Rate – CBD Rate</b>	<b>Current Bylaw – Required</b>	<b>Proposed</b>
<b>Mixed-Use Residential Component</b>	Bachelor = 0.8 space/unit 1 bedroom = 0.9 spaces/unit Each additional bedroom = 0.1 space/unit	117	132
	Visitor = 0.05 spaces/unit	6	6
<b>Small Retail Units (under 300 m<sup>2</sup>)</b>	1 space / 100 m <sup>2</sup> GFA	10	10
<b>Total</b>		<b>133</b>	<b>148</b>
<b>Small Car</b>	10% of the parking stalls	15	15
<b>Accessible Stalls</b>	Between 126 and 200 stalls	3	5
<b>Bicycle Parking Space Requirement</b>			
<b>Category</b>	<b>Current Bylaw – Applicable Rate</b>	<b>Current Bylaw – Required</b>	<b>Proposed</b>
<b>Residential Long Term</b>	1 space/4 units	30	30
<b>Residential Short Term</b>	6 spaces/20 units	36	36
<b>Commercial Long Term</b>	1 space/750 m <sup>2</sup> GFA	2	2
<b>Commercial Short Term</b>	6 spaces/1500 m <sup>2</sup> GFA	4	6
18 short-term bicycle spaces are proposed in addition to the required short-term bicycle spaces in the amenity spaces.			

As per the Maple Ridge Off-Street Parking and Loading Bylaw No. 4350-1990, each parking space provided for residential use, excluding visitor stalls, will be provided with roughed-in infrastructure capable of providing electric-vehicle Level 2 charging. In addition, a minimum of 50% of the required visitor parking spaces will be provided with roughed-in infrastructure capable of providing Level 2 charging.

A minimum of 10% of the parking spaces for the commercial uses will be provided with roughed-in infrastructure capable of providing Level 2 charging.

As per the requirement of the CD Bylaw, a minimum of seven electric vehicle charging stations is proposed for the subject development proposal.

### **Development Permits:**

Pursuant to Section 8.11 of the OCP, a Town Centre Development Permit application is required for all multi-unit residential, flexible mixed-use and commercial development located in the Town Centre. The proposed development is located within the Town Centre Area Plan's Downtown West Precinct.

There is a registered No-Build Covenant on the subject property establishing Form, Character and Landscaping Guidelines (Attachment H) applicable to all of the proposed seven phases of the ERA Project, that must be considered in conjunction with the Guidelines for the Downtown West Precinct and the Guidelines for Green Building Techniques.

The project architect has submitted a checklist for an analysis of the compliance of the proposed development with the ERA Guidelines (Attachment J).

The following is a brief description and assessment of the proposal's compliance with the applicable Downtown West Precinct Key Guideline Concepts (Attachment K):

#### Town Centre Development Permit Area – Downtown West Precinct - Key Guideline Concepts:

1. "Provide a gateway to the Town Centre. New development should promote the Downtown West Precinct as a Town Centre gateway for eastbound travelers along Dewdney Trunk Road. A gateway element should have strong visual presence that features landmark structures, landscape elements, welcome signage, public art and/or enhanced views to the mountains, while maintaining architectural quality and character of associated new development. This concept applies mainly to properties located on Lougheed Highway and Dewdney Trunk Road."
  - *Staff Comment: The proposed development is part of a phased development that will potentially include high-rise and landmark buildings. This development features a stepped-back design on the second floor, with landscaped planters along the second-floor facade, overlooking Dewdney Trunk Road, 223 Street, and the south-north fire lane access road. The overall design and quality of the proposed materials of the facade align with the area's overall character and vision (Attachment D).*
2. "Create a pedestrian-oriented, boutique-style shopping district. New development in the Downtown West Precinct should foster a pedestrian-oriented, boutique-style shopping and employment environment amongst diverse residential buildings. A building's form and mass should support a strong pedestrian-oriented urban realm and should help to define the street and sidewalk areas as active public spaces. Taller buildings (greater than 5 stories) should be stepped back in a podium style to blend with low-rise (3-5 storey buildings) and provide a more ground-oriented feel."

- *Staff Comment: The proposed development supports a pedestrian-oriented urban realm through stepped-back building design and the provision of building setbacks on ground level for commercial units (Attachment D). The stepped-back building design provides a more ground-oriented feel, and commercial setbacks on the ground level can potentially incorporate seating areas adjacent to sidewalks.*
3. "Enhance the quality, character and vibrancy of the Town Centre. New development should promote the quality, character and vibrancy of the urban environment. Colours should be fairly consistent, and materials of sustainable quality. All new commercial, multi-family and mixed-use buildings should create an attractive appearance to the street."
- *Staff Comment: The proposed development generally enhances the quality, character, and vibrancy of the emerging mixed-use residential and commercial area along Dewdney Trunk Road (Attachment D).*
4. "Reference traditional architectural styles. New development in the Downtown West Precinct should reference more traditional architectural styles, that include materials such as brick and wood. A cohesive building style should be maintained, ensuring new buildings have consistent architectural and urban design setbacks, form, mass and height throughout the precinct."
- *Staff comment: The proposed development generally maintains a cohesive building style within the area. Overall, the development proposes cultured stone for the commercial units, and cementitious panels for the residential levels. Some of the cementitious panels will feature a simulated wood finish (Attachment D).*
5. "Capitalize on important views. New development should capitalize on important mountain and/or river views. Existing streets and buildings should maintain and enhance these views."
- *Staff comment: The proposed development does not negatively impact the existing street corridor's mountain views.*
6. "Provide public outdoor space. New developments should include attractive, functional public outdoor spaces, where appropriate and feasible. Outdoor spaces should be designed to accommodate a wide use of activities, incorporate universal access, reduce vandalism, and increase safety."
- *Staff comment: The subject development is one of the seven phases to be carried out under the ERA Project (Attachment D). Phase 4 will involve the creation of an open plaza and neighbourhood park, along with the installation of public art features. The current phase includes the continuation of the east-west public walkway with landscape features and provides a connection for the public to engage with.*



7. "Provide climate appropriate landscaping and green features. New development should provide landscape elements that reinforce the urban character and vibrancy of the Town Centre. Landscape elements should enrich the pedestrian-friendly character of streets in the precinct, moderate the internal building climate, help manage stormwater on site, and reference the architectural quality of new buildings. Where feasible, mature trees should be retained, vegetation suitable for the Maple Ridge climate should be planted, and green roofs and walls should be considered."
  - *Staff comment: The subject development proposes planting where possible along walkways, connections, and amenity gathering spaces, including the rooftop. Perimeter planters are also proposed on the second floor, wrapping around the west and east corners (Attachment E).*
8. "Maintain street interconnectivity. New development should maintain street interconnectivity and the traditional use of the lane as a service street and secondary vehicular and pedestrian thoroughway. Where feasible, parking requirements should be accommodated underground."
  - *Staff Comment: The subject development maintains street connectivity by providing one vehicular access from 223 Street, along with the continuation of the public east-west pedestrian corridor on the north side. The pedestrian corridor, commenced in Phase 1 (to the west), will continue through the subject site and all future phases of the ERA Project. Parking stalls are provided in a semi-concealed, ground-level parking garage and two levels of underground parking (Attachments D and E).*

### **Advisory Design Panel:**

The application was reviewed by the Advisory Design Panel during its meeting on March 12, 2025. The ADP's resolution and comments, and the applicant's responses are appended to this report (Attachment L).

A detailed description of the development's form and character will be included in a future Development Permit report to Council for consideration.

### **Development Information Meeting:**

In accordance with Council Policy 6.20, a Development Information Meeting (DIM) hosted by the applicant was held at Maple Ridge Library on April 8, 2025, and was attended by 22 people and three representatives on behalf of the applicant. A summary of the main comments and discussions with the attendees at the DIM was provided by the applicant and is appended to this report (Attachment M).

The notification for the DIM included a mail-out, newspaper advertisements, and notice on the development signs posted on the property that provided the contact information for the developer and information on the development.

### Conditions to be Met Prior to Adoption:

Staff have advised the applicant that adoption of the Zone Amending Bylaw will not be recommended unless the following conditions, and any others that Council identifies, are met:

1. Registration of the following:

- a) A Rezoning Servicing Agreement as a Restrictive Covenant and receipt of the security, as outlined in the Agreement.

The following servicing upgrades will be required through the Rezoning Servicing Agreement:

- Road dedication as required to meet the design criteria of the *Subdivision and Development Bylaw No. 4800-1993*.
- Utility servicing as required to meet the design criteria of the *Subdivision and Development Bylaw No. 4800-1993*.
- Frontage upgrades to the applicable road standard.
- Transportation improvements required by the City's Transportation division.

- b) A Restrictive Covenant for the Geotechnical Report which addresses the suitability of the subject property for the proposed development;

- c) A Statutory Right-of-Way plan and agreement for the east-west public pedestrian corridor;

- d) A Restrictive Covenant for protecting the Visitor Parking; and,

- e) A Restrictive Covenant for Stormwater Management.

2. Approval from the Ministry of Transportation and Transit;

3. Road dedication, as required;

4. Submission of a Letter of Credit from the developer, in the amount of 130% of the Class B cost estimate, to secure the transportation improvement measures required by the Transportation Department. The Letter of Credit must be reviewed and approved by the Transportation Department;

5. Removal of existing buildings;

6. A Water Supply System Construction Permit from Fraser Health Authority is required by the *Drinking Water Protection Act* and must be obtained before the construction, installation, alteration, or extension of a water supply system;

7. A Professional Engineer's certification that adequate water quantity for domestic and fire protection purposes can be provided;

8. If the Director of Waste Management from the Ministry of Environment and Parks determines that a site investigation is required based on the submitted Site Disclosure Statement, a rezoning, development, or development variance permit cannot be approved until a release is obtained for the subject property;

9. In addition to the Ministry of Environment Site Disclosure Statement, a disclosure statement must be submitted by a Professional Engineer advising whether there is any evidence of underground fuel storage tanks on the subject property. If so, a Stage 1 Site Investigation Report is required to ensure that the subject property is not a contaminated site; and
10. That a voluntary contribution, in the amount of \$672,000.00 (\$5,600.00 x 120 units) be provided in keeping with the Council Policy 6.31 with regards to Community Amenity Contributions.

## **Internal/ External Referrals:**

### **Ministry of Transportation and Transit:**

As the subject property is located within 800 m of Loughheed Highway, a referral has been sent to the Ministry of Transportation and Transit.

Ministry approval of the Zone Amending Bylaw will be required as a condition of final reading. At this time, the Ministry has granted preliminary approval of the development application.

## **CONCLUSION:**

It is recommended that first, second and third readings be given to *Zone Amending Bylaw No. 8034-2025*.

"Maryam Lotfi"

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Prepared by: Maryam Lotfi, Planner 2

### **Attachments:**

- (A) Location Map
- (B) Arial Photo
- (C) OCP and Zoning Map
- (D) Architectural Plans
- (E) Landscape Plans
- (F) No-Build Covenant
- (G) Preliminary Subdivision Plan
- (H) *Zone Amending Bylaw No. 8034-2025*
- (I) Revised Architectural Data Sheet
- (J) ERA Design Guidelines Checklist
- (K) Development Permit Design Guidelines Checklist
- (L) Advisory Design Panel Review Comments
- (M) Development Information Meeting Comments

## Report Approval Details

Document Title:	2024-228-RZ, 12011 223 Street, CD-1-17.docx
Attachments:	<ul style="list-style-type: none"><li>- Attachment A - Location Map.pdf</li><li>- Attachment B - Aerial Photo.pdf</li><li>- Attachment C - OCP and Zoning Map.pdf</li><li>- Attachment D - Architectural Plans.pdf</li><li>- Attachment E - Landscape Plans .pdf</li><li>- Attachment F - No-Build Covenant.pdf</li><li>- Attachment G - Preliminary Subdivision Plan.pdf</li><li>- Attachment H - Zone Amending Bylaw No. 8034-2025.pdf</li><li>- Attachment I - Revised Architectural Data Sheet .pdf</li><li>- Attachment J - ERA Design Guidelines Checklist.pdf</li><li>- Attachment K - Development Permit Design Guidelines Checklist.pdf</li><li>- Attachment L - Advisory Design Panel Review Comments.pdf</li><li>- Attachment M - Development Information Meeting Comments .pdf</li></ul>
Final Approval Date:	May 13, 2025

This report and all of its attachments were approved and signed as outlined below:

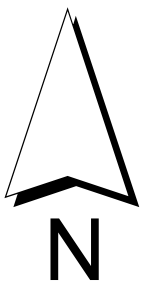
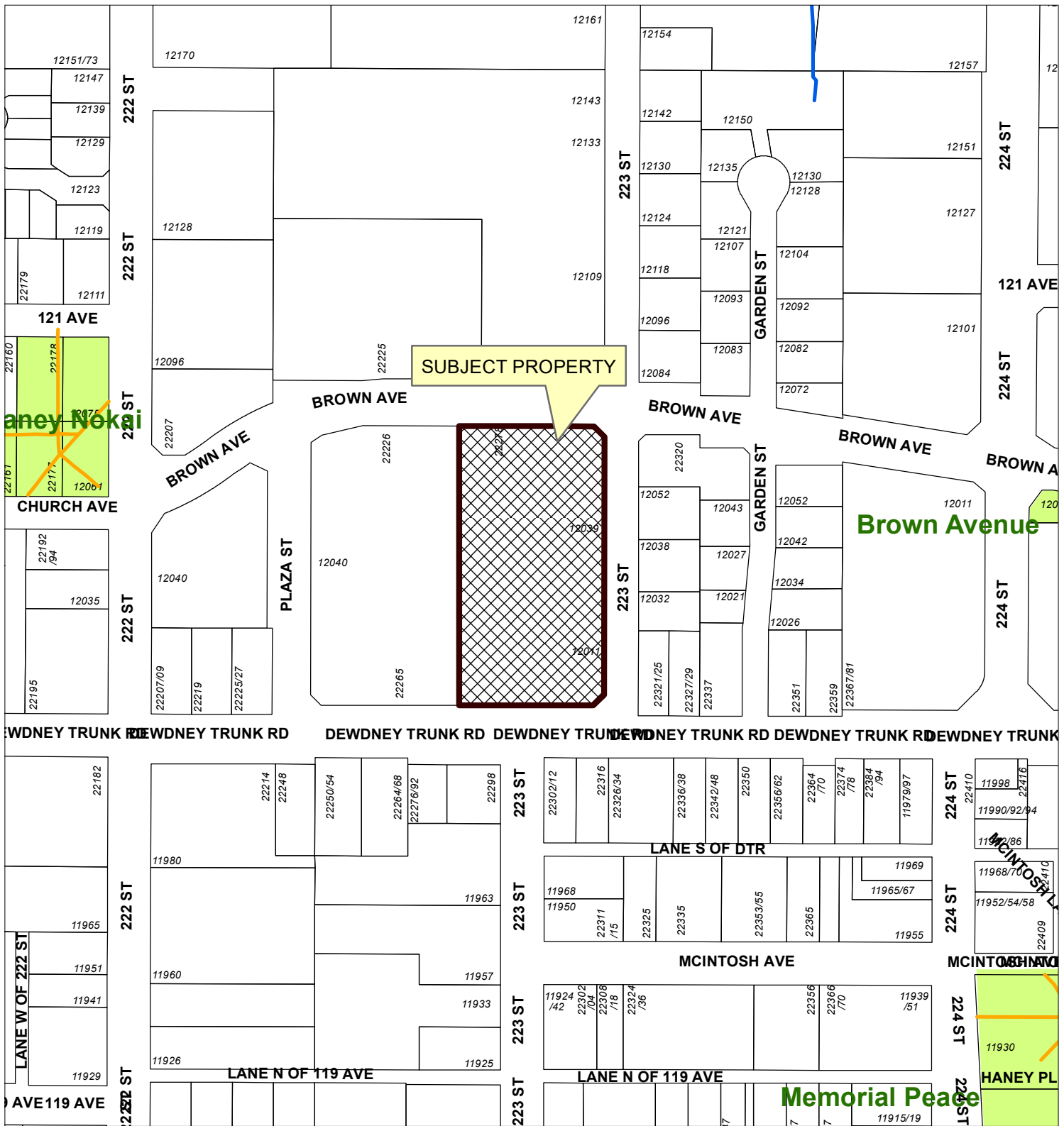
David Purcell-Chung, Manager of Development and Environmental Services

Hasib Nadvi, Deputy Director of Planning and Building





James Stiver, Director of Planning and Building

Carolyn Mushata, Director of Legislative Services and Corporate Officer

Scott Hartman, Chief Administrative Officer



## Legend

-  Stream
-  Existing Trails
-  Active Applications (RZ/SD/DP/VP)
-  Municipal Park

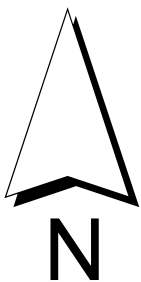
## 12011 223 STREET LOCATION MAP

PLANNING DEPARTMENT

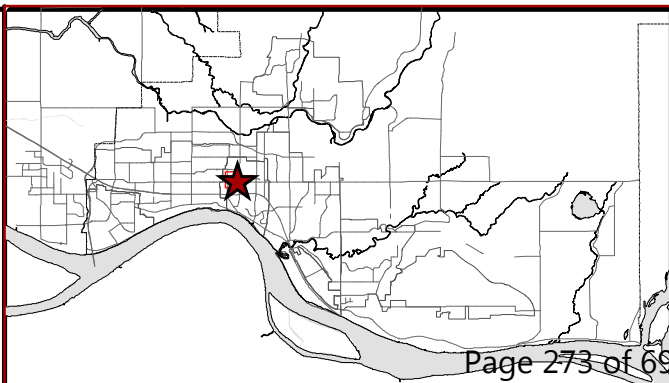


# Maple Ridge





Scale: 1:2,500



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## 12011 223 STREET AERIAL PHOTO

PLANNING DEPARTMENT

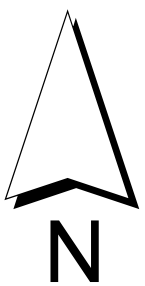
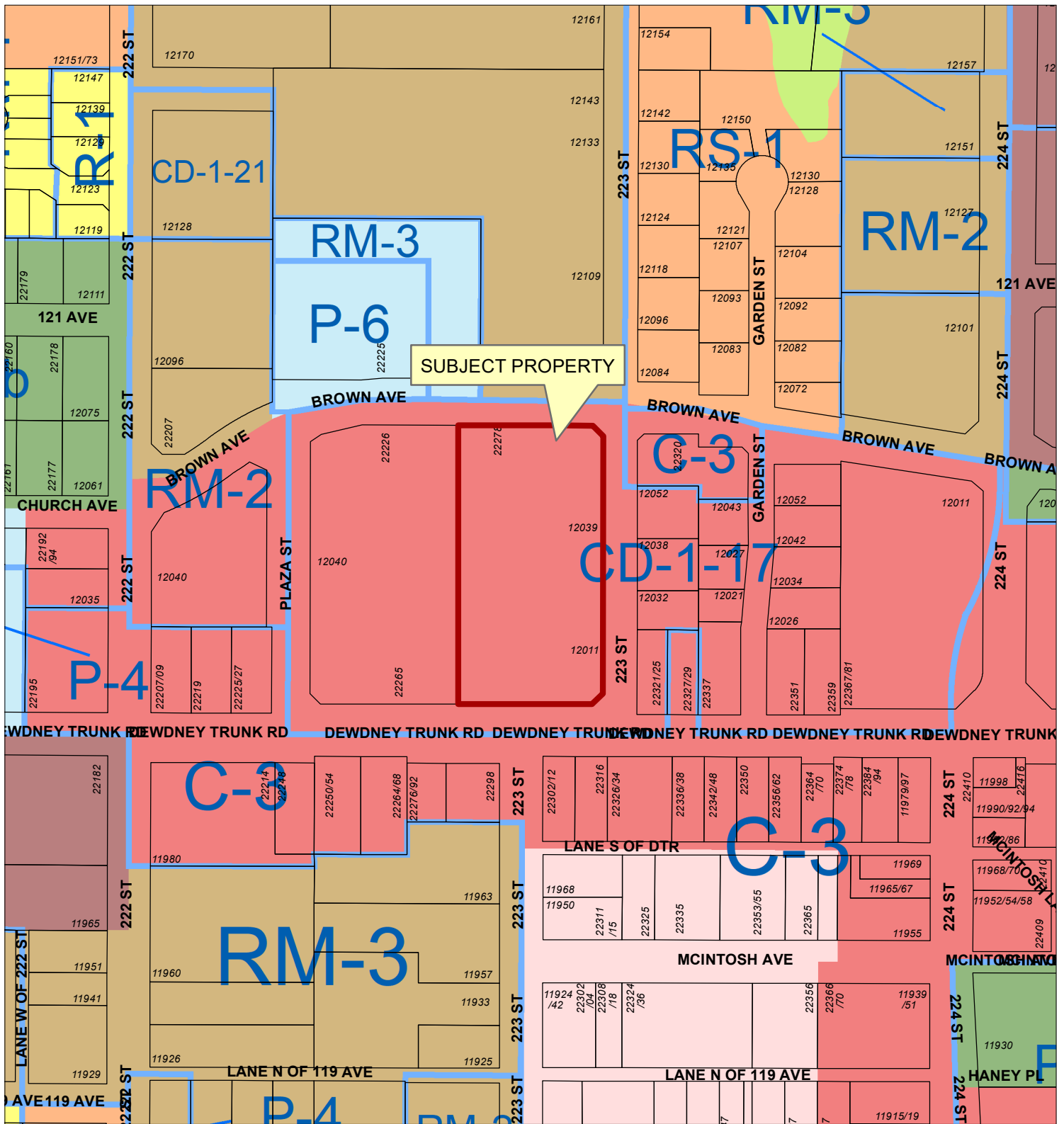


# Maple Ridge

FILE: 2024-228-RZ  
DATE: Apr 4, 2025

BY: DM





Scale: 1:2,500

### Legend

- |                                |                        |
|--------------------------------|------------------------|
| Zoning Boundaries              | Flexible Mixed-Use     |
| Single-Family Residential      | Town Centre Commercial |
| Ground-Ori... Multi-Family     | Institutional          |
| Medium and High-Rise Apartment | Conservation           |
| Low-Rise Apartment             | Park                   |

## 12011 223 STREET OCP & ZONING MAP

PLANNING DEPARTMENT



# Maple Ridge

FILE: 2024-228-RZ  
DATE: Apr 4, 2025

BY: DM



ERA 2

DEWDNEY TRUNK & 223 ST. MAPLE RIDGE, BC  
MIXED USE DEVELOPMENT

PHASE 2 DEVELOPMENT PERMIT RESUBMISSION  
TO CITY OF MAPLE RIDGE

April 14, 2025



**OWNER**  
487559 B.C. LTD. (on behlf of  
JOHNSTON MEIER INSURANCE  
AGENCIES GROUP)

22367 DEWDNEY TRUNK ROAD,  
MAPLE RIDGE, B.C., V2X 3J4  
CONTACT: TOM MEIER  
TEL: (604) 467-4184  
EMAIL: TOM.MEIER@JMINS.COM

**DEVELOPMENT MANAGER**  
WILLIAMS MANAGEMENT CORP.

#300 - 225 WEST 8th AVE.  
VANCOUVER BC, V5Y 1N3  
CONTACT: PAUL WILLIAMS  
TEL: (604) 644-9939  
EMAIL: PAUL@WILLIAMSMANAGEMENT.CA

**LANDSCAPE & ARBORIST**  
DURANTE KREUK LTD. (DKL)  
LANDSCAPE ARCHITECTS INC.

SUITE 102-1613 W. 5TH AVE., VANCOUVER,  
BC, V6J 1N5  
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EMAIL: DYLAN@DKL.BC.CA  
EMAIL: JTAYLOR@DKL.BC.CA

**STRUCTURAL**  
WEILER SMITH BOWERS (WSB)  
CONSULTING STRUCTURAL ENGINEERS

118-3855 HENNING DR., BURNABY,  
BC, V5C 6N3  
CONTACT: DARRYL J. BOWERS  
CONTACT: LAWRENCE CHAN  
TEL: (604) 294 - 3753  
EMAIL: BOWERS@WSB-ENG.COM  
EMAIL: LCHAN@WSB-ENG.COM

**CODE CONSULTANTS**  
GHL CONSULTANTS LTD.

800 - 700 W PENDER ST., VANCOUVER,  
BC, V6C 1G8  
CONTACT: FRANKIE VICTOR  
CONTACT: MATT TURCO  
TEL: (604) 689- 4449  
EMAIL: FV@GHL.CA  
EMAIL: MT@GHL.CA

**MECHANICAL**  
INTEGRAL GROUP

SUITE 180 - 200 GRANVILLE STREET,  
VANCOUVER, BC, V6C 1S4  
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CONTACT: ALEX WONG  
TEL: (604) 687- 1800  
EMAIL: GMcDONALD@INTEGRALGROUP.COM  
EMAIL: VRAJU@INTEGRALGROUP.COM

**ELECTRICAL**  
NEMETZ (S/A) & ASSOCIATES LTD.

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BC, V6J 1N3  
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CONTACT: BRIAN LORETTE  
TEL: (604) 736 6562  
EMAIL: GARY@NEMETZ.COM  
EMAIL: BRIANLORETTE@NEMETZ.COM

**ARCHITECTURAL**  
CHRIS DIKEAKOS  
ARCHITECTS INC.  
  
1635 W BROADWAY, VANCOUVER,  
BC, V6J 1W9  
CONTACT: RICHARD BERNSTEIN  
CONTACT: PHILIP POLOTSKY  
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EMAIL: PHILIP.P@DIKEAKOS.COM

**CIVIL**  
McELHANNEY LTD.

SUITE 100-8837 201 STREET,  
LANGELY, BC, V2Y 0C8  
CONTACT: MIKE CANNING  
TEL: (604) 424 4867  
E: MCANNING@MCELHANNEY.COM

**GEOTECHNICAL**  
GEOPACIFIC CONSULTANTS LTD.

1779 W. 75th AVE, VANCOUVER,  
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EMAIL: DKOKAN@GEOPACIFIC.CA

**ENVIRONMENTAL**  
McELHANNEY LTD.  
  
SUITE 100-8837 201 STREET, LANGELY,  
BC, V2Y 0C8  
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TEL: (778) 688-7024  
EMAIL: GLIN@MCELHANNEY.COM

**SURVEY**  
MCELHANNEY ASSOCIATES  
LAND SURVEYING LTD.  
  
SUITE 2300 CENTRAL CITY TOWER,  
13450 - 102 AVE, SURREY, BC, V3T 5X3  
CONTACT: PAUL BARTLETT  
TEL: (604) 424-4824  
EMAIL: PBARTLETT@MCELHANNEY.COM

**TRAFFIC**  
BUNT & ASSOCIATES  
ENGINEERING

SUITE 1550 - 1050 WEST PENDER  
STREET, VANCOUVER, BC, V6E 3S7  
CONTACT: JAMES B.LEE  
TEL: (604) 685-6427  
EMAIL: JLEE@BUNTENG.COM

**WASTE MANAGEMENT**  
TARGET ZERO WASTE

1567 COVE ROAD, NORTH VANCOUVER,  
BC, V7G 1S4  
CONTACT: JEFF LEVITT  
TEL: (604) 688-7024  
EMAIL: JEFF@TARGETZEROWASTE.COM

ARCHITECTURAL DRAWINGS INDEX

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A004	TOPOGRAPHICAL SURVEY PLAN
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A101	FIRE TRUCK ACCESS PLAN
A102	SITE ROOF PLAN
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DATES OF ISSUE

NO. OF REVISIONS		DATE
01	Phase 1 & 2 DP Submission	December 16, 2021
02	Phase 2 DP PAM Submission	January 23, 2024
03	DP Resubmission	July 12, 2024
04	DP Resubmission	December 20, 2024





	GROSS SITE AREAS:		ROAD DEDICATION AREAS:		NET SITE AREAS (used to calculate FSR):	
	SQ. FT.	SQ. M.	SQ. FT.	SQ. M.	SQ. FT.	SQ. M.
Phase 2 - South	45,308.80	4,209.33	4,827.60	448.49	40,481.20	3,760.84

SETBACKS	PHASE BOUNDARY	223 STREET	DEWDNEY TRUNK ROAD	NORTH SOUTH MEWS
Required	N/A	N/A	N/A	N/A
Proposed	14' - 5 1/4"	Residential: between 10'-2"	8'-0"	29'-6"
		Commercial: 8'-0"	Residential: between 3.10m	8.99m
	4.40m	Commercial: 1.83m	1.83m	

SUMMARY OF BUILDING PROPOSED (SF)

BLDG	LEVELS	NUMBER OF RES. UNITS	TOTAL GFA (SF)	COMMERCIAL GFA (SF)	RESIDENTIAL GFA (SF)	TOTAL EXCLUSION (SF)	TOTAL INDOOR AMENITY EXCLUSION (SF)	TOTAL NET AREA (SF)	RESIDENTIAL FSR	COMMERCIAL FSR	TOTAL FSR BY PHASE	MAX FSR ALLOWABLE (PER REZONING)	ALLOWABLE NET GFA (SF)
PH 2 - LOW RISE	6	120	119,683	9,627	110,056	18,782	5,224	95,677	2.13	0.24	2.36	2.65	107,275.18
			11,119	894	10,224	1,745	485	8,889					9,966

L1 BUILDING FOOTPRINT	SQ. FT.	SQ. M.	SITE COVERAGE	66%	TOTAL NET PH2 UNUSED AREA +/-	SQ. FT.	SQ. M.
	26,231	2,437				95,677	8,889
						-11,586.18	-1,077

PHASE 2 - PARKING (Reference bylaw : City of Maple Ridge Bylaw No.4350-1990 )												
LEVEL		L1	P1	P2					PROVIDED		TOTAL REQ'D	+/-
Residential		0	66	66					132		117	15
Visitor		0	6	0					6		8	
Commercial		10	0	0					10		10	
TOTAL									148		133	
Small Stall Count (10% of total)		1	7	7					15		13	
Accessible Stall Count		1	1	3					5		4	
EV Stall Count		0	7	0					7	*For residential use	7	

Note : Resident Rental Parking Ratio is proposed at 0.8 stalls/unit (bachelor); 0.9 stalls/unit (1 bedroom); 0.1 stalls/unit (additional bedroom)  
Note : Visitor Parking Ratio is 0.1 stall / unit - where on-street supply is available (Reference bylaw No. 4350-1190 - Section 10.2).  
Note : Commercial Parking Ratio is 1 stall /100m<sup>2</sup> GFA for small Retail units (Section 10.3).  
Note : On Street parking number is per civil drawings and calculations. The number is not included in total provided stalls number.  
Note : In all CD Zones, each parking space provided for Residential use, excluding Visitor parking spaces, shall be provided with roughed-in Level 2 infrastructure (Schedule F - 7663-2020).  
Note : A minimum of seven (7) electric vehicle charging stations will be provided for residential apartment use.

Amenity (not included for FSR)		SQ. FT.	SQ. M.
Indoor Amenities		5,224	485
Outdoor Amenities (L1 Courtyard & Rooftop Amenity)		6,299	585

Common Open Areas (not included for FSR)		SQ. FT.	SQ. M.
L1 Common Open Area		11,761	1,093
Private Balconies & Patios		12,800	1,189
Rooftop Outdoor Amenity		3,466	322

**Adaptable Units**  
Note : There are no adaptable units considered in the current total unit numbers.

PHASE 2 - Bike Storage	Short Term Provided Bike Storage		Long Term Provided Bike Storage	
	RESI	COMM	RESI	COMM
P2	0	0	0	0
P1	36	2	30	2
L1	0	0	0	0
Total	36	2	30	2

\*Note : There are no Bylaw requirements for personal storage lockers  
Maple Ridge Long-Term Bicycle Parking requirement = 1 space per 4 units  
Maple Ridge Short-Term Bicycle Parking requirement = 6 spaces per 20 units  
Commercial Long-Term Bicycle Parking requirement = 1 space per 750m<sup>2</sup>  
Commercial Short-Term Bicycle Parking requirement = 6 spaces for every 1500m<sup>2</sup>  
\*Charging receptacles will be provided for 10% of long-term bike lockers

REQUIRED BIKE STORAGE	PROVIDED BIKE STORAGE
30	30
36	36
2	2
6	6

PHASE 2 - AREA BREAKDOWN			
Area Use	Name	Area	Area (m2)

AMENITY			
AMENITY	AMENITY	5224 ft²	485 m²
		5224 ft²	485 m²

COMMON			
COMMON	COMMON AREA	12546 ft²	1166 m²
COMMON	RESIDENTIAL & COMMERCIAL SHARED SERVICE CORRIDOR	825 ft²	77 m²
COMMON	STAIR A	1060 ft²	99 m²
COMMON	STAIR B	1121 ft²	104 m²
COMMON	STAIR C	170 ft²	16 m²
COMMON	STAIR D	1228 ft²	114 m²
		16951 ft²	1575 m²

COMMON LOBBY			
COMMON LOBBY	FILING/RECORDS	53 ft²	5 m²
COMMON LOBBY	JANITOR	47 ft²	4 m²
COMMON LOBBY	MAIL & PARCEL	221 ft²	21 m²
COMMON LOBBY	RENTAL ADMIN	202 ft²	19 m²
COMMON LOBBY	RES. ENTRY LOBBY	954 ft²	89 m²
		1477 ft²	137 m²

OUTDOOR AMENITY			
OUTDOOR AMENITY	COMMUNAL GARDEN OUTDOOR AMENITY	3466 ft²	322 m²
OUTDOOR AMENITY	OUTDOOR AMENITY	2833 ft²	263 m²
		6299 ft²	585 m²

RENTAL			
RENTAL	1 BED	13357 ft²	1241 m²
RENTAL	1 BED + DEN	17897 ft²	1663 m²
RENTAL	2 BED	30972 ft²	2877 m²
RENTAL	2 BED + DEN	4370 ft²	406 m²
RENTAL	3 BED	19452 ft²	1807 m²
		86048 ft²	7994 m²

RETAIL			
RETAIL	CRU #1	1721 ft²	160 m²
RETAIL	CRU #2	716 ft²	66 m²
RETAIL	CRU #3	806 ft²	75 m²
RETAIL	CRU #4	566 ft²	53 m²
RETAIL	CRU #5	2568 ft²	239 m²
RETAIL	CRU #6	1228 ft²	114 m²
RETAIL	CRU #7	1196 ft²	111 m²
		8801 ft²	818 m²

RETAIL CORRIDOR			
RETAIL CORRIDOR	RESIDENTIAL AND COMMERCIAL SHARED SERVICE CORRIDOR	826 ft²	77 m²
		826 ft²	77 m²

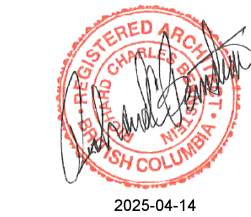
SERVICES			
SERVICES	COMMERCIAL REFUSE	457 ft²	42 m²
SERVICES	RESIDENTIAL REFUSE	550 ft²	51 m²
SERVICES	SERVICES	321 ft²	30 m²
		1329 ft²	123 m²
Grand total		126954 ft²	11794 m²

DENSITY CALCULATIONS

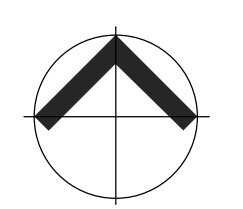
PHASE 2 LOW RISE	Units		120		Studio		0 units		0%																														
	Total GFA Area (SF)		110,056		1 Bed		25 units		21%																														
	Total GFA Area (SM)		10,224		1 Bed + den		30 units		25%																														
					2 Bed		40 units		33%																														
					2 bed + den		5 units		4%																														
						3 bed		20 units		17%																													
						459-574		575-660		738-780		870		943-977																									
		Unit Size Range ( SF)																																					
				GROSS FLOOR AREA (SF)		GROSS FLOOR AREA (SM)		UNITS		STUDIO		1BR		1BR+DEN		2BR		2BR+DEN		3BR		CORRIDOR + SERVICES AREA EXCLUSION (SF)		CORRIDOR + SERVICES AREA EXCLUSION (SM)		AREA EXCLUSION INDOOR (SF)		AMENITY AREA EXCLUSION INDOOR (SM)		AMENITY AREA EXCLUSION OUTDOOR (SF)		AMENITY AREA EXCLUSION OUTDOOR (SM)		NET FLOOR AREA (SF)		NET FLOOR AREA (SM)		EFFICIENCY	
Level		9,627		894																																			
L1 Commercial																																							
L1 Res.		8,896		826		0		0		0		0		0		0		0		0		3,672		341		5,224		485		2,833		263		0		0			
2		20,232		1,880		24		0		5		6		8		1		4		4		3,022		281		0		0		0		0		17,210		1,599		85.1%	
3		20,232		1,880		24		0		5		6		8		1		4		4		3,022		281		0		0		0		0		17,210		1,599		85.1%	
4		20,232		1,880		24		0		5		6		8		1		4		4		3,022		281		0		0		0		0		17,210		1,599		85.1%	
5		20,232		1,880		24		0		5		6		8		1		4		4		3,022		281		0		0		0		0		17,210		1,599		85.1%	
6		20,232		1,880		24		0		5		6		8		1		4		4		3,022		281		0		0		0		0		17,210		1,599		85.1%	
Roof																																							
Totals		110,056		10,224		120		0		25		30		40		5		20		20		18,782		1,745		5,224		485		6,299		585		95,677		8,889		82.9%	

PARKING CALCULATIONS

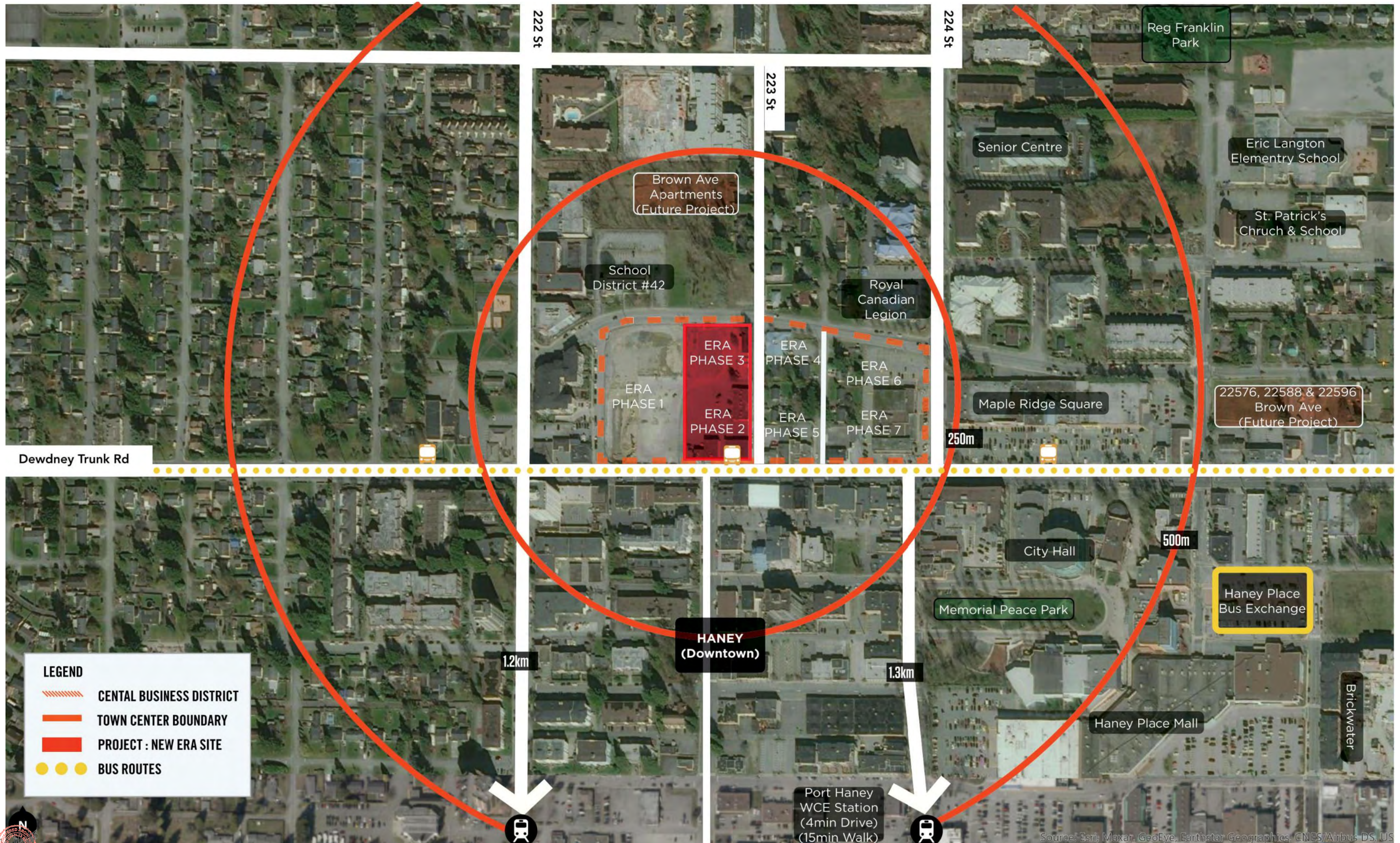
RESIDENT PARKING RATIO (# Stalls per unit)	0.80	0.9	0.9	1	1	1.1						REQUIRED SUB TOTAL:		STALLS PROVIDED:
# RESIDENT PARKING STALLS REQUIRED	0	23	27	40	5	22						117		128
# VISITOR PARKING STALLS REQUIRED												6		6
TOTAL # RESIDENTIAL PARKING STALLS REQUIRED												123		134
TOTAL # COMMERCIAL PARKING STALLS REQUIRED (1 per 100m <sup>2</sup> )												9		9
TOTAL # PARKING STALLS REQUIRED												132		143
BICYCLE PARKING STALLS REQ'D (SPACES)												74		74





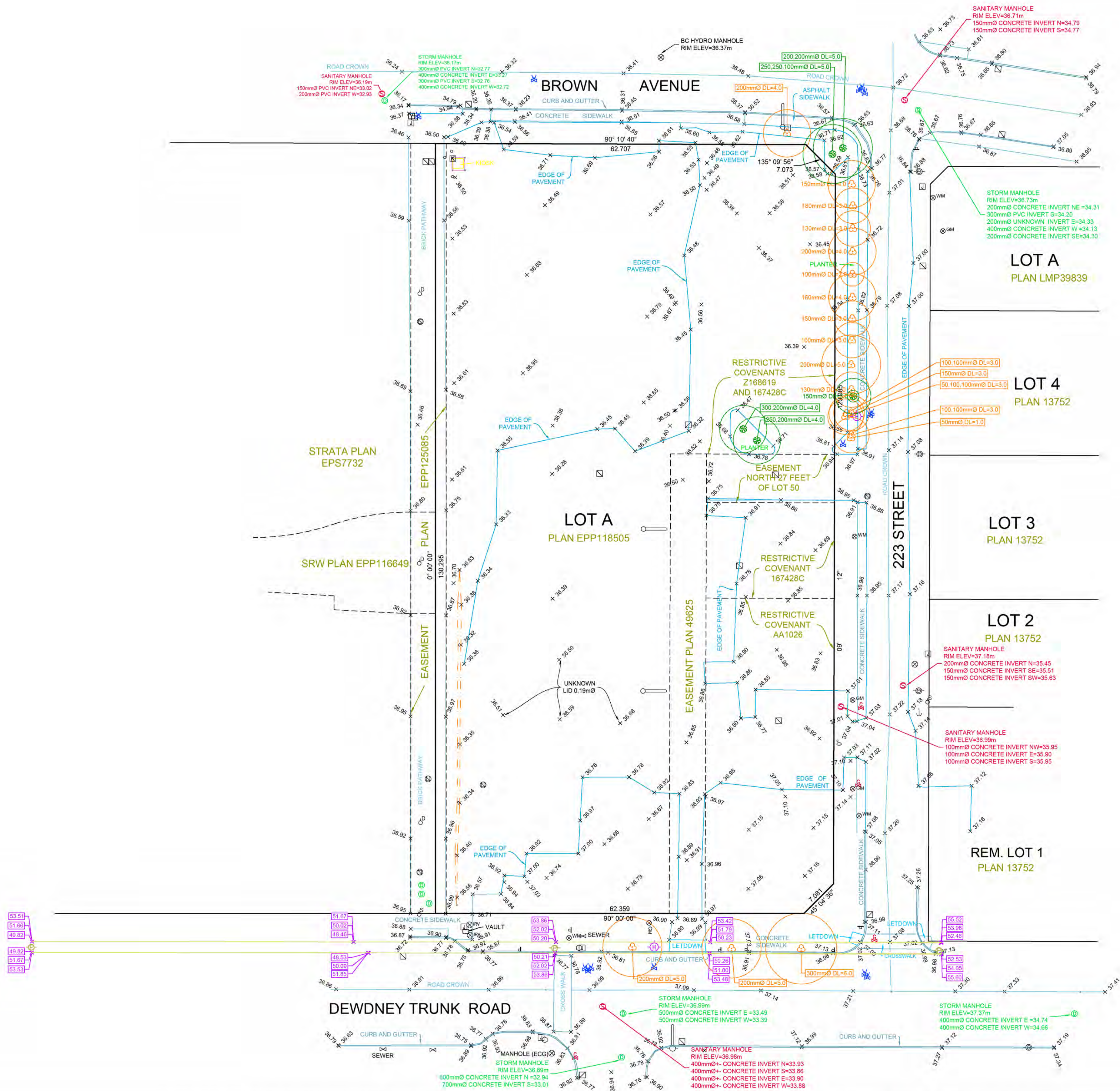








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LEGEND

- +55.24 DENOTES SPOT ELEVATION
- m DENOTES METRE(S)
- m DENOTES MILLIMETRE(S)
- o BOLLARD
- o CATCH BASIN - MANHOLE
- o CATCH BASIN (TOP INLET)
- o FIRE HYDRANT
- o GUY WIRE
- o JUNCTION BOX
- o LAMP STANDARD
- o MANHOLE
- o MANHOLE - SANITARY
- o MANHOLE - STORM
- o MANHOLE - TELEPHONE
- o METER - GAS
- o METER - WATER
- o MONITORING WELL
- o POLE - HYDRO
- o POLE - HYDRO/TELEPHONE POLE
- o POLE WITH LIGHT
- o POLE WITH PILASTER
- o POLE WITH TRANSFORMER
- o STORM INSPECTION CHAMBER
- o SANITARY INSPECTION CHAMBER
- o SIGN POST
- o TRAFFIC COUNTER
- o TREE - CONIFEROUS
- o TREE - DECIDUOUS
- o TREE DIAMETER (mm)
- o TREE DRIP LINE (m)
- o VALVE
- o VALVE - GAS
- o VALVE - IRRIGATION
- o VALVE - WATER

NOTES:

- DISTANCES ARE IN METRES
- BEARINGS ARE DERIVED FROM PLAN EPP118505
- ELEVATIONS ARE GEODETIC, IN METRES
- DERIVED FROM INTEGRATED MONUMENT NO. 85H0739
- ELEVATION = 36.982m (CVD286VRO)
- SITUATED IN THE DISTRICT OF MAPLE RIDGE IN THE CENTRE OF THE INTERSECTION OF 222 STREET AND 119 AVENUE

LEGAL DESCRIPTION:

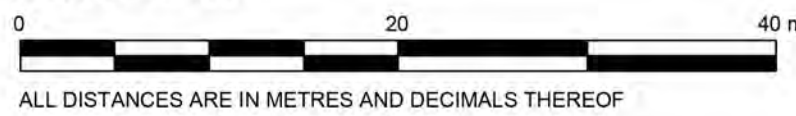
PID: 031-743-684

LOT A  
DISTRICT LOT 399, GROUP 1,  
NEW WESTMINSTER DISTRICT,  
PLAN EPP118505

Rev	Date	Description	Survey	Drawn	App'd
1	2024-07-19	ADDED OVERHEAD WIRES ALONG DEWDNEY TRUNK ROAD	SA/AH	EA	JS
0	2024-05-06	TOPOGRAPHIC SURVEY PLAN UPDATE	SA/AH	EA	JS

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SCALE 1 : 400



ALL DISTANCES ARE IN METRES AND DECIMALS THEREOF

ORIGINAL DWG SIZE: ANSI D (22" x 34")  
ALL DIMENSIONS ARE IN METRES

**McElhanney**  
McElhanney Associates Land Surveying Ltd.

Suite 2300  
13450 - 102 Avenue  
Surrey BC  
Canada V3T 5X3  
Tel 604 596 0391

Approved Sealed

487559 BC Ltd C/O Johnston Meier Insurance Agencies  
22367 Dewdney Trunk Rd. Maple Ridge, BC.

**TOPOGRAPHIC SURVEY PLAN  
OF LOT A, EPPP118505 (PHASE 2)  
DEWDNEY TRUNK ROAD  
MAPLE RIDGE, B.C.**

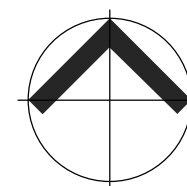
Drawing No.

**V-17**

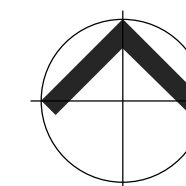
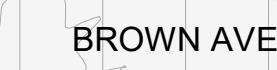
Project Number  
2112-63658-01

Rev.  
1

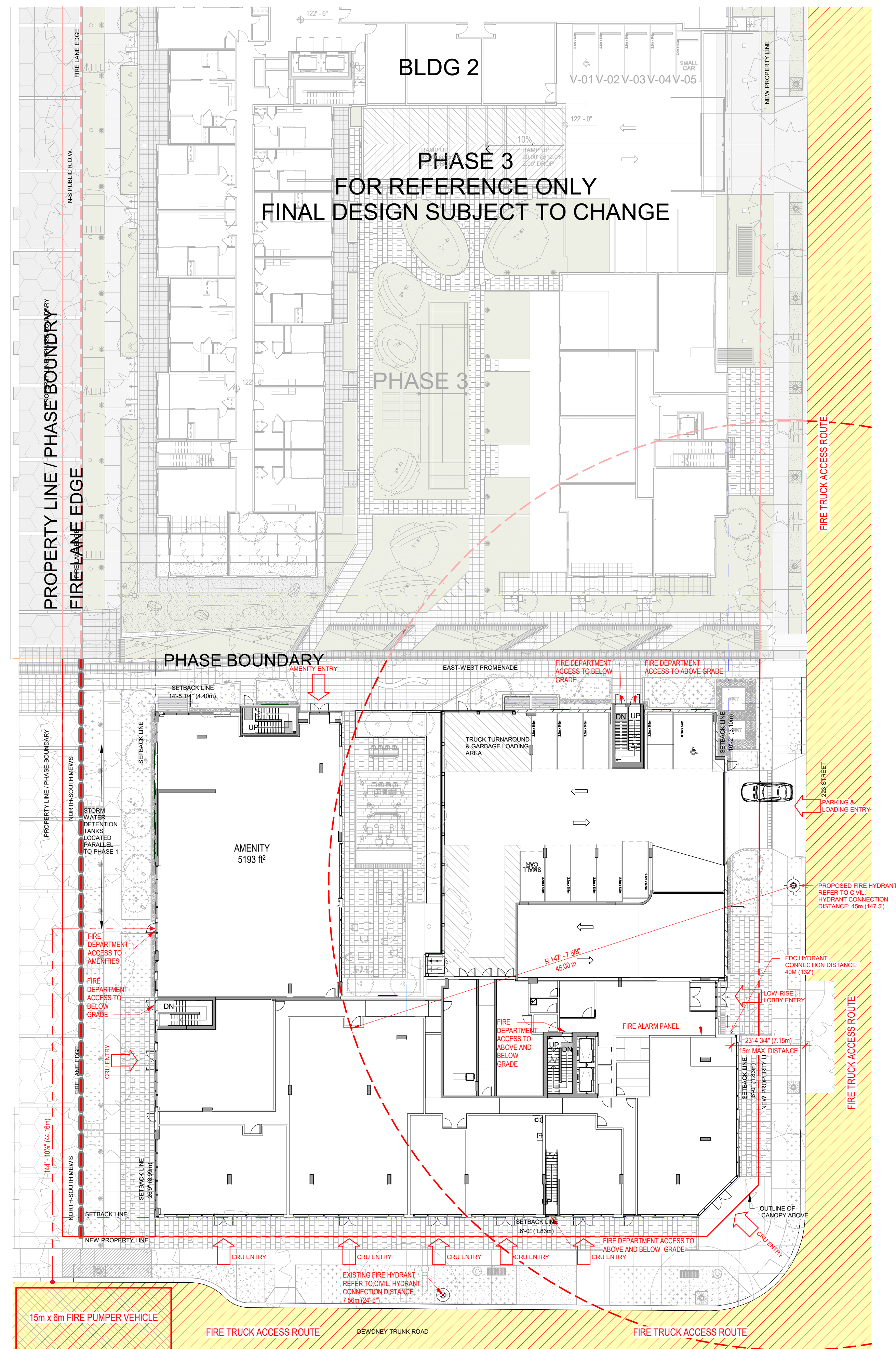
DESTROY ALL PRINTS BEARING PREVIOUS REVISION



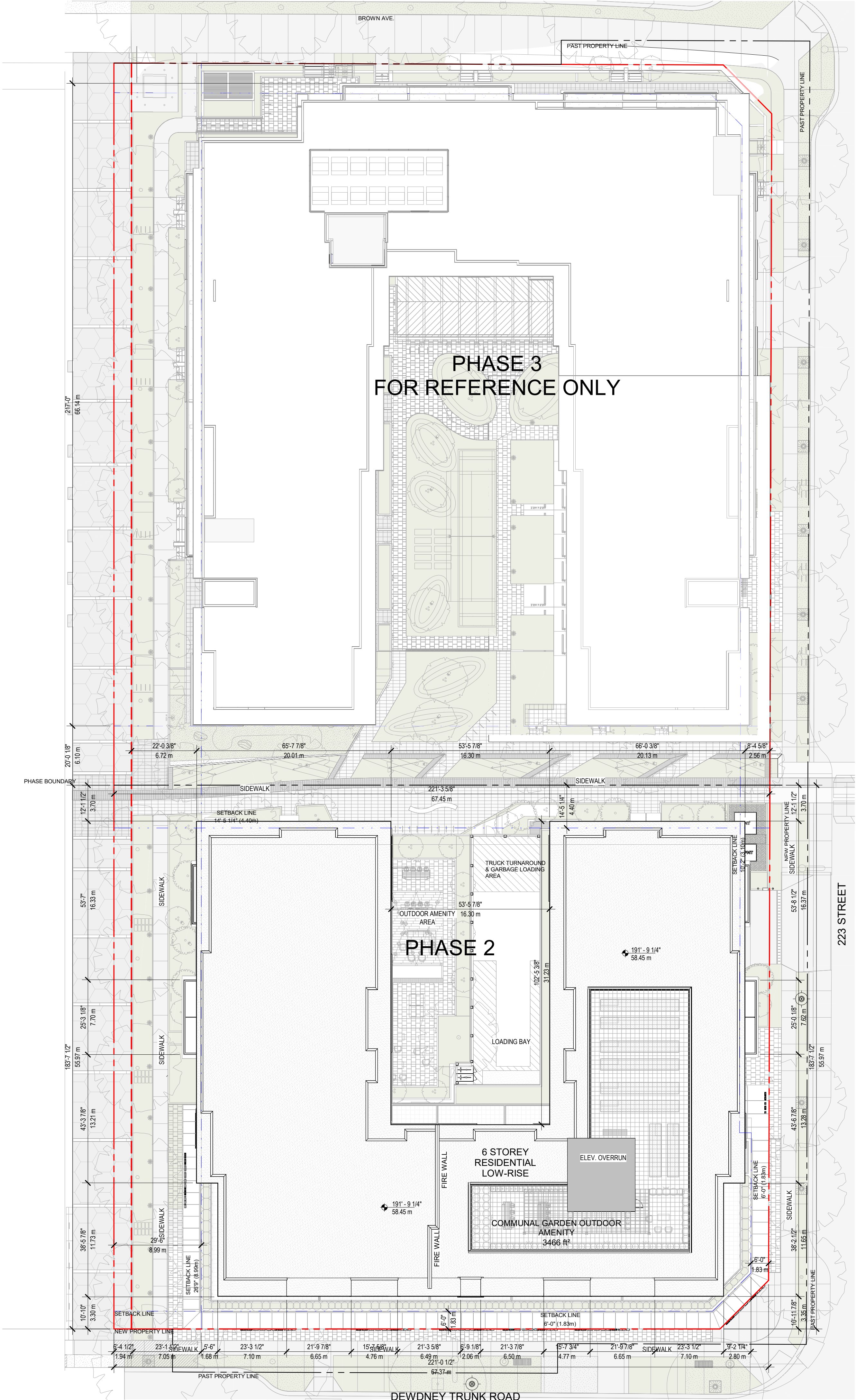
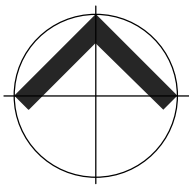






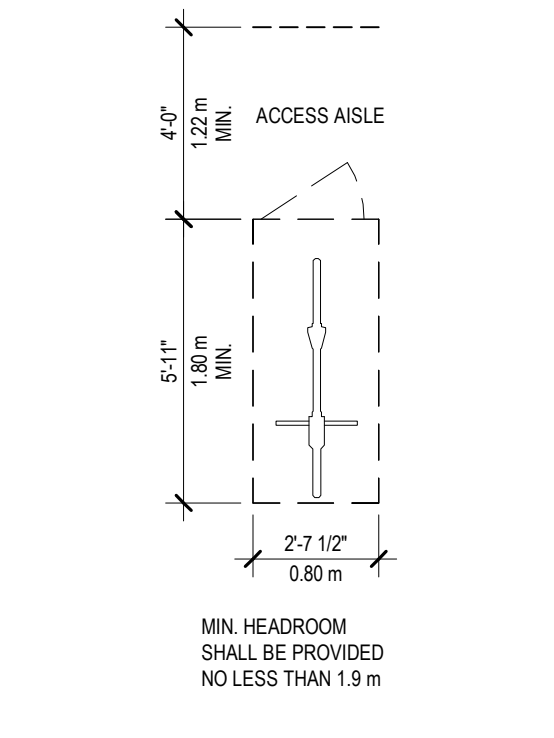




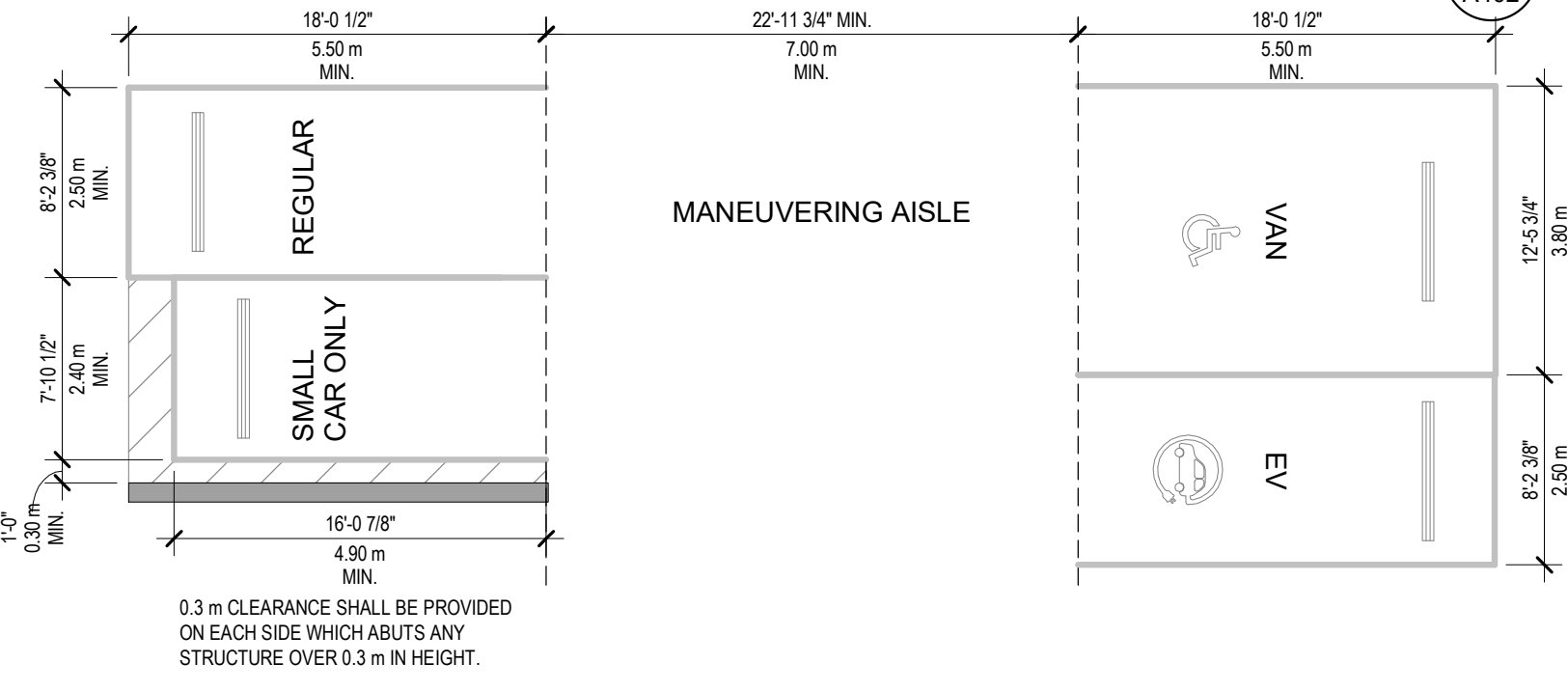




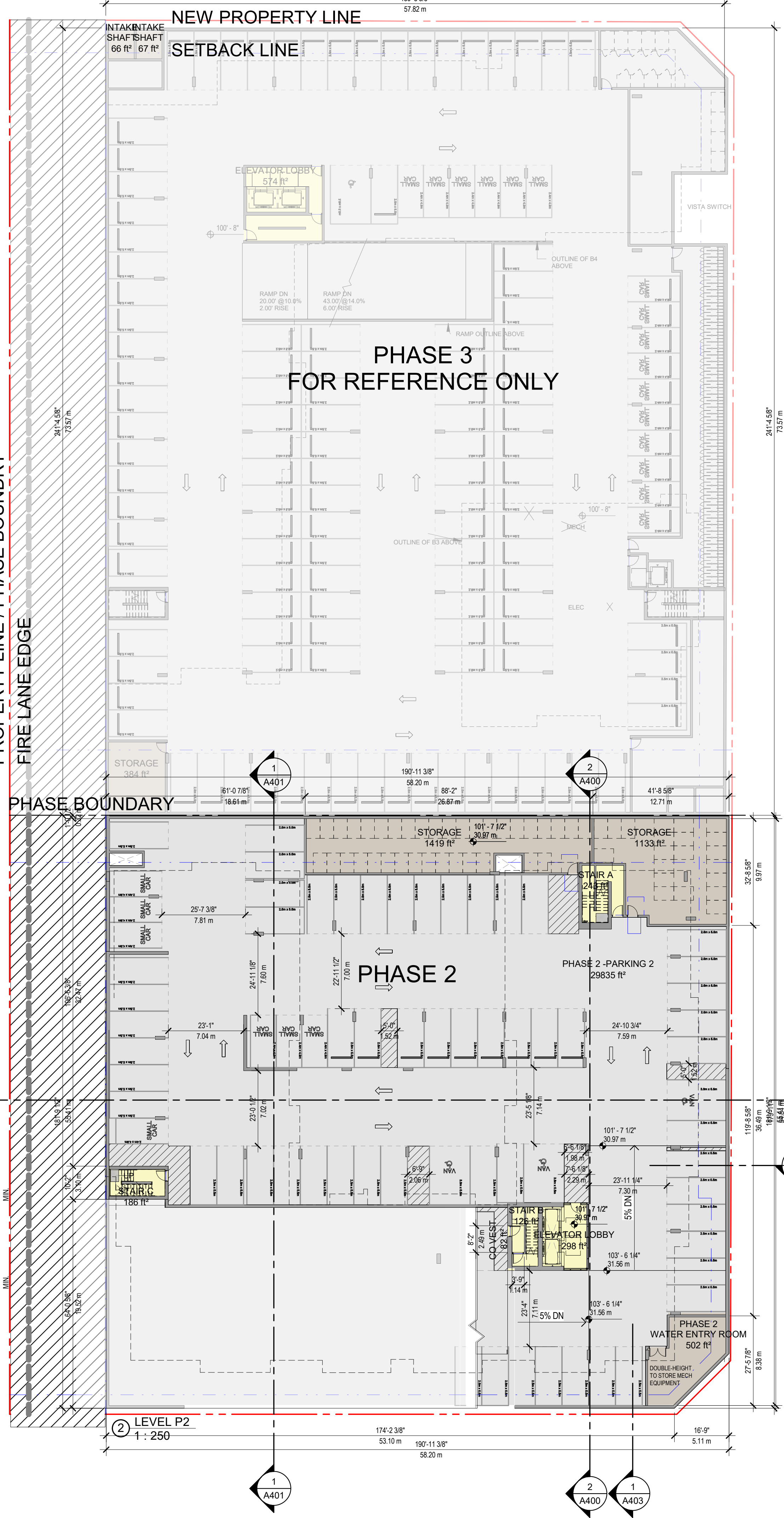
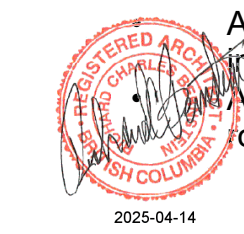
LONG-TERM BICYCLE STORAGE REQUIREMENTS



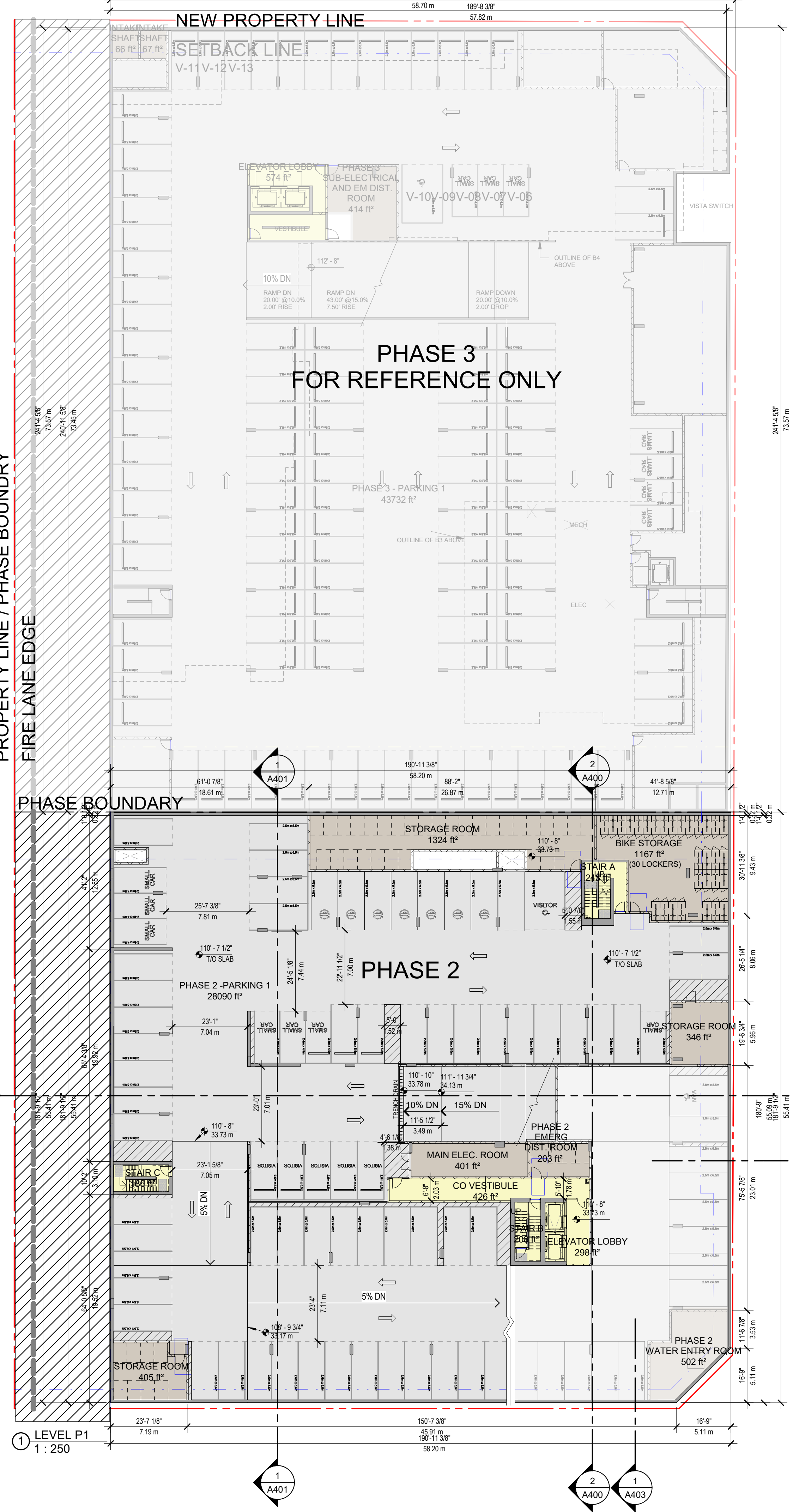
OFF-STREET PARKING SPACE REQUIREMENTS



- As per the City of Maple Ridge *Off-Street Parking and Loading Bylaw NO. 4350 – 1990*:
- Each parking space provided for residential use, excluding visitor stalls, will be provided with roughed-in infrastructure capable of providing Level 2 charging.
- A minimum of 50% of required visitor parking spaces will be provided with roughed-in infrastructure capable of providing Level 2 charging.
- A minimum of 10% of the parking spaces for commercial use will each be provided with roughed-in infrastructure capable of providing Level 2 charging.



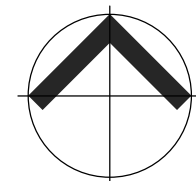
**PARKING LEVEL P2 & P1**  
SCALE: As indicated



**PH 2 - RE-ISSUED FOR DP**  
April 14, 2025

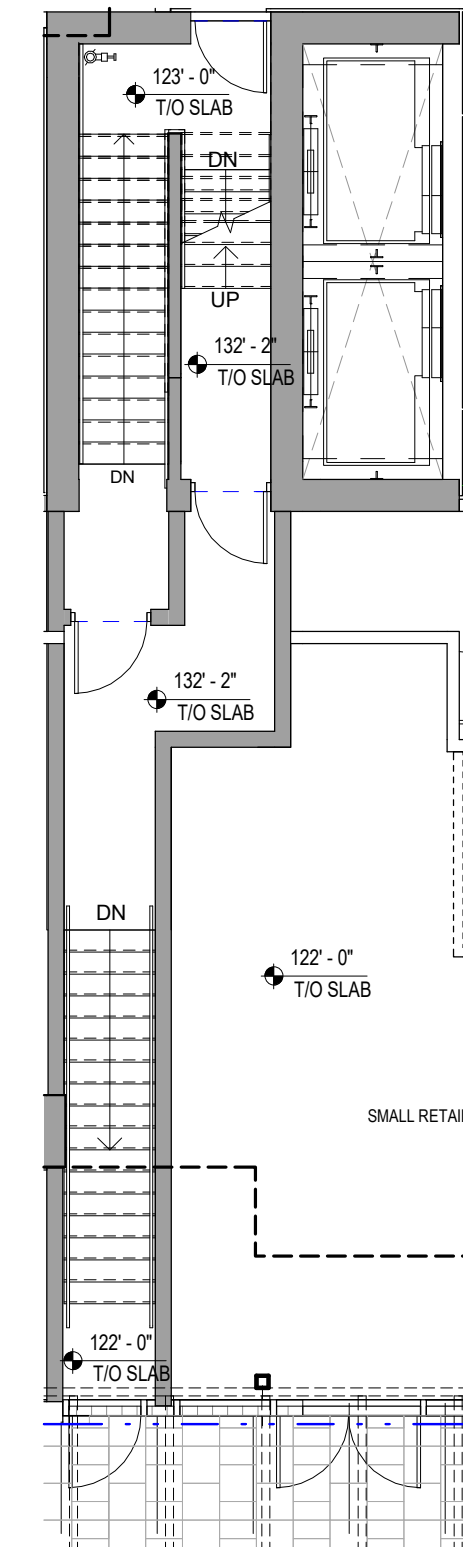
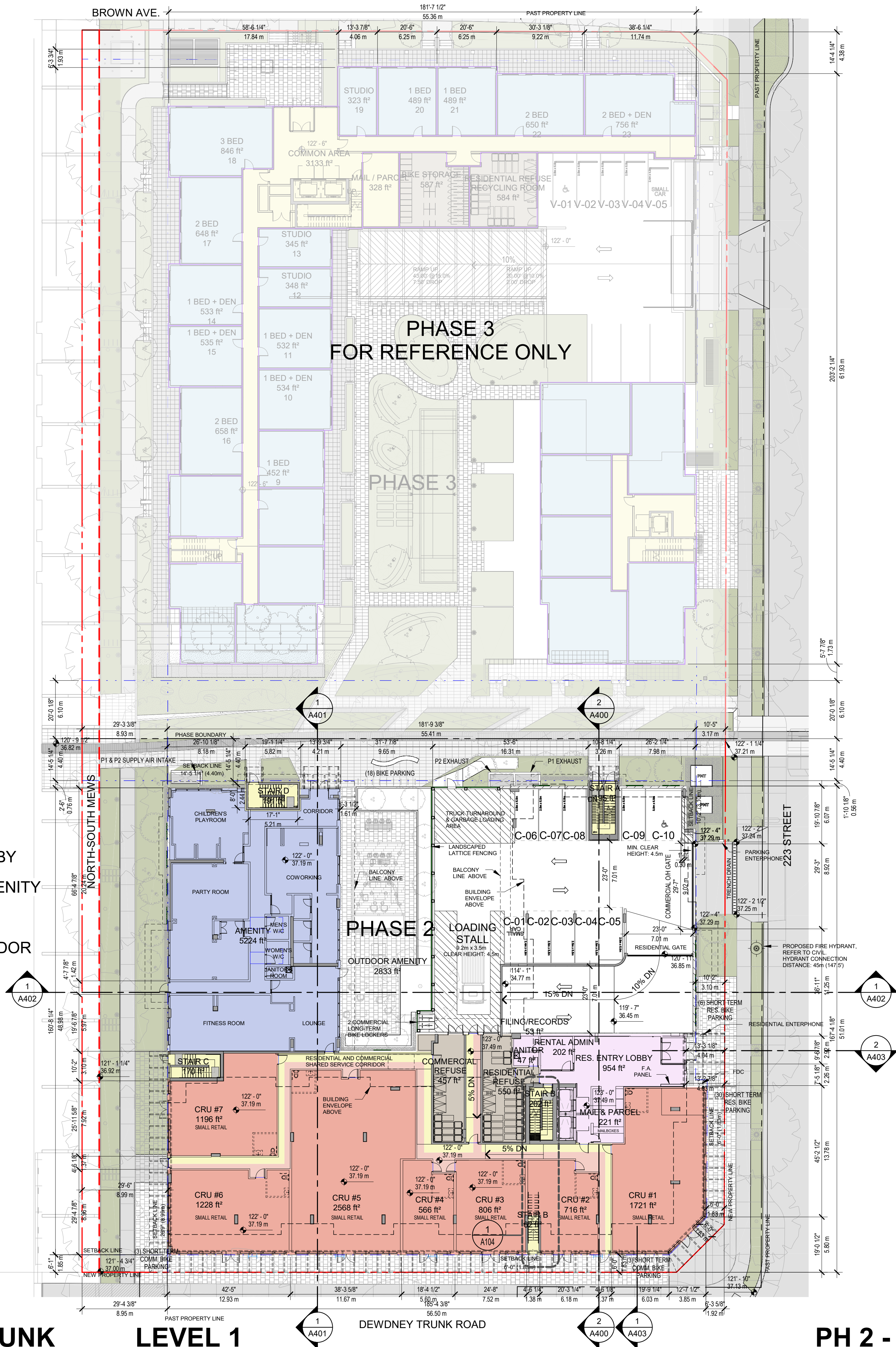


- As per the City of Maple Ridge *Off-Street Parking and Loading Bylaw NO. 4350 – 1990*:
- Each parking space provided for residential use, excluding visitor stalls, will be provided with roughed-in infrastructure capable of providing Level 2 charging.
- A minimum of 50% of required visitor parking spaces will be provided with roughed-in infrastructure capable of providing Level 2 charging.
- A minimum of 10% of the parking spaces for commercial use will each be provided with roughed-in infrastructure capable of providing Level 2 charging.



**AREAS**

- AMENITY
- COMMON
- COMMON LOBBY
- OUTDOOR AMENITY
- RETAIL
- RETAIL CORRIDOR
- SERVICES

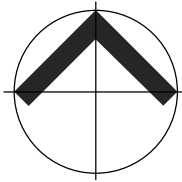
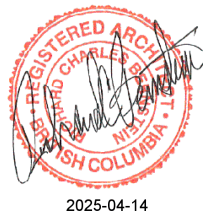


① ENLARGED EXIT STAIR B PLAN  
1/8" = 1'-0"

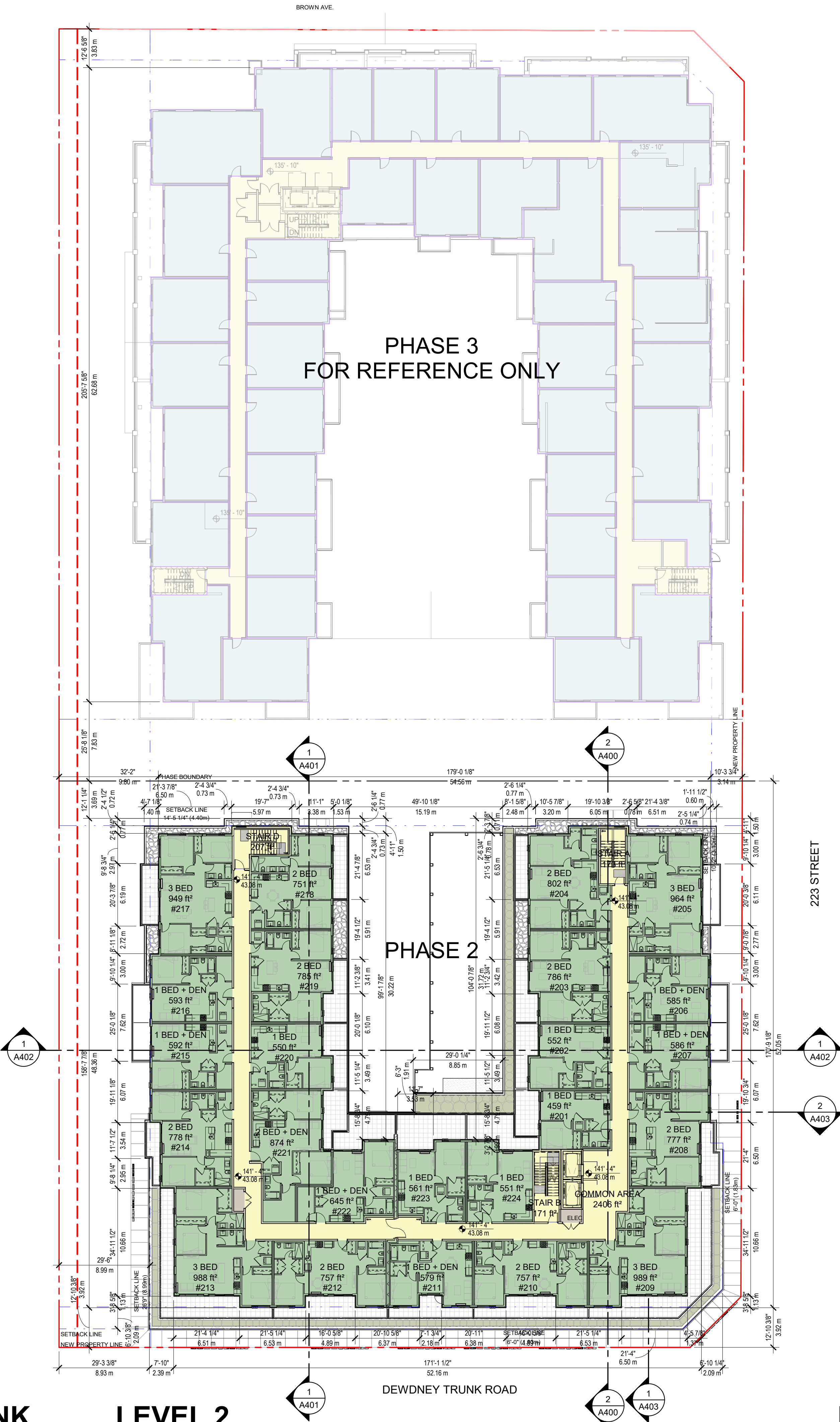
PHASE 2 - L1 AREA BREAKDOWN	
Name	Area
AMENITY	5224 ft <sup>2</sup>
COMMON	5224 ft <sup>2</sup>
RESIDENTIAL & COMMERCIAL SHARED SERVICE CORRIDOR	825 ft <sup>2</sup>
STAIR A	195 ft <sup>2</sup>
STAIR B	264 ft <sup>2</sup>
STAIR C	170 ft <sup>2</sup>
STAIR D	191 ft <sup>2</sup>
COMMON LOBBY	1645 ft <sup>2</sup>
FILING/RECORDS	53 ft <sup>2</sup>
JANITOR	47 ft <sup>2</sup>
MAIL & PARCEL	221 ft <sup>2</sup>
RENTAL ADMIN	202 ft <sup>2</sup>
RES. ENTRY LOBBY	954 ft <sup>2</sup>
OUTDOOR AMENITY	1477 ft <sup>2</sup>
OUTDOOR AMENITY	2833 ft <sup>2</sup>
RETAIL	2833 ft <sup>2</sup>
CRU #1	1721 ft <sup>2</sup>
CRU #2	716 ft <sup>2</sup>
CRU #3	806 ft <sup>2</sup>
CRU #4	566 ft <sup>2</sup>
CRU #5	2568 ft <sup>2</sup>
CRU #6	1228 ft <sup>2</sup>
CRU #7	1196 ft <sup>2</sup>
RETAIL CORRIDOR	8801 ft <sup>2</sup>
RESIDENTIAL AND COMMERCIAL SHARED SERVICE CORRIDOR	826 ft <sup>2</sup>
SERVICES	826 ft <sup>2</sup>
COMMERCIAL REFUSE	457 ft <sup>2</sup>
RESIDENTIAL REFUSE	550 ft <sup>2</sup>
Grand total	1008 ft <sup>2</sup>
	21813 ft <sup>2</sup>

PHASE 2 - L1 PARKING			
Level	TYPE	Count	Description
COMMERCIAL			
B1 - L1	COMMERCIAL	8	REGULAR
B1 - L1	COMMERCIAL	1	SMALL CAR
B1 - L1	COMMERCIAL	1	VAN ACCESSIBLE
Grand total: 10			



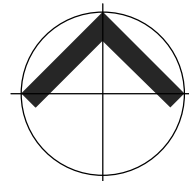
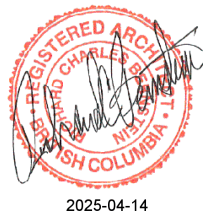


- AREAS**
- COMMON
  - RENTAL
  - SERVICES



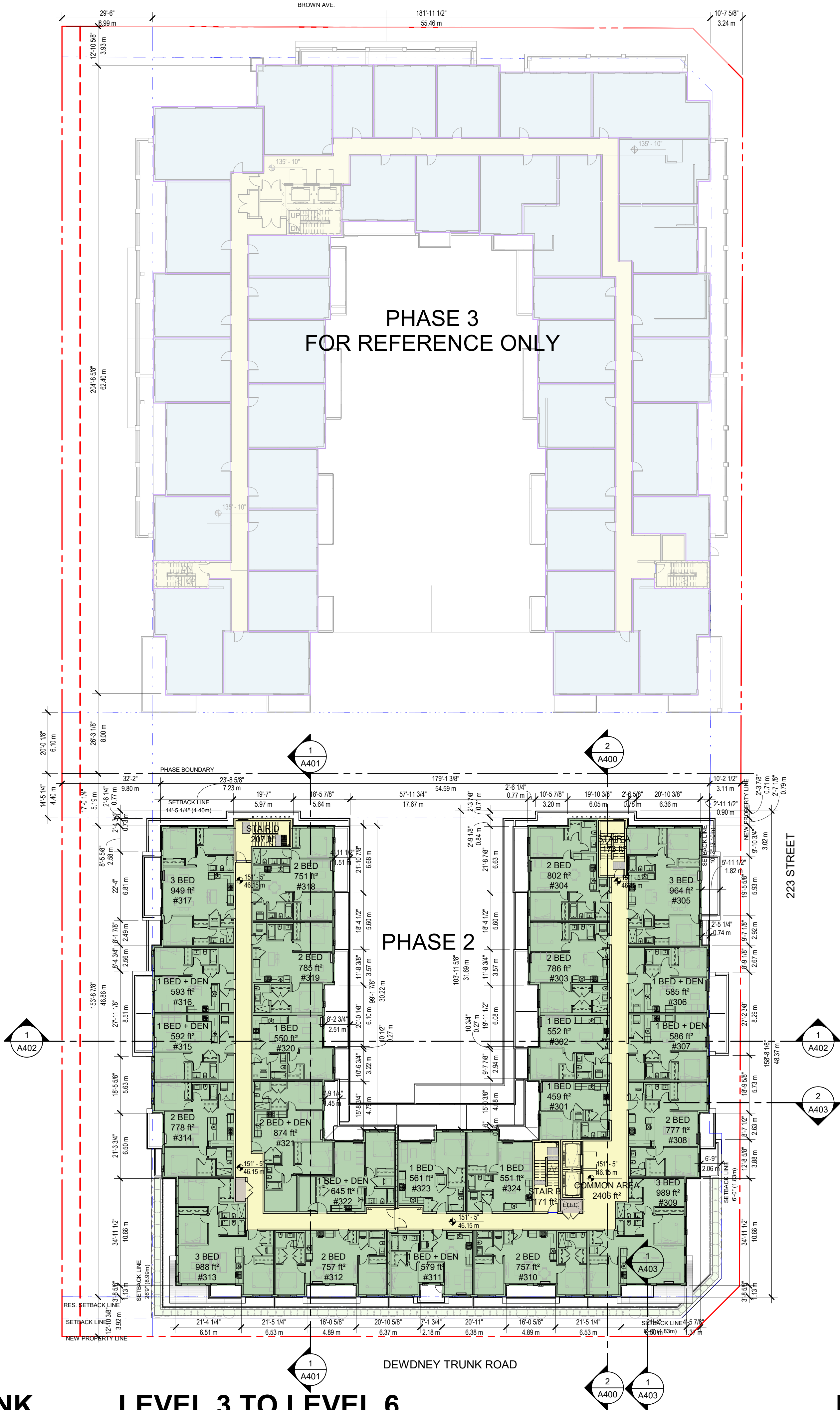
PHASE 2 - LEVEL 2 AREA BREAKDOWN					
Name	Count	Area (SF)	Total Area	Total Area (m²)	Area Ratio
COMMON					
COMMON AREA	1	2406 ft²	2406 ft²	224 m²	12%
STAIR A	1	173 ft²	173 ft²	16 m²	1%
STAIR B	1	171 ft²	171 ft²	16 m²	1%
STAIR D	1	207 ft²	207 ft²	19 m²	1%
COMMON: 4			2958 ft²	275 m²	15%
RENTAL					
1 BED	5	459 ft² ... 561 ft²	2671 ft²	248 m²	13%
1 BED + DEN	6	579 ft² ... 645 ft²	3579 ft²	333 m²	18%
2 BED	8	751 ft² ... 802 ft²	6194 ft²	575 m²	31%
2 BED + DEN	1	874 ft²	874 ft²	81 m²	4%
3 BED	4	949 ft² ... 989 ft²	3890 ft²	361 m²	19%
RENTAL: 24			17210 ft²	1599 m²	85%
SERVICES					
SERVICES: 2	2	31 ft² ... 34 ft²	64 ft²	6 m²	0%
SERVICES: 2			64 ft²	6 m²	0%
Grand total: 30			20232 ft²	1880 m²	100%
TOTAL BALCONY SF @ L2: 3,600 ft²					





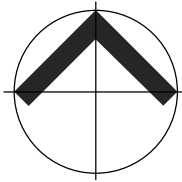
**AREAS**

- COMMON
- RENTAL
- SERVICES



PHASE 2 - TYP. LEVEL AREA BREAKDOWN					
	Count	Area (SF)	Total Area	Total Area (m²)	Area Ratio
COMMON					
COMMON AREA	5	2406 ft²	12030 ft²	1118 m²	12%
STAIR A	5	173 ft²	865 ft²	80 m²	1%
STAIR B	5	171 ft²	857 ft²	80 m²	1%
STAIR D	5	207 ft²	1037 ft²	96 m²	1%
COMMON: 20			14789 ft²	1374 m²	15%
RENTAL					
1 BED	25	459 ft² ... 561 ft²	13357 ft²	1241 m²	13%
1 BED + DEN	30	579 ft² ... 645 ft²	17897 ft²	1663 m²	18%
2 BED	40	751 ft² ... 802 ft²	30972 ft²	2877 m²	31%
2 BED + DEN	5	874 ft²	4370 ft²	406 m²	4%
3 BED	20	949 ft² ... 989 ft²	19452 ft²	1807 m²	19%
RENTAL: 120			86048 ft²	7994 m²	85%
SERVICES					
SERVICES	10	31 ft² ... 34 ft²	321 ft²	30 m²	0%
SERVICES: 10			321 ft²	30 m²	0%
Grand total: 150			101158 ft²	9398 m²	100 %

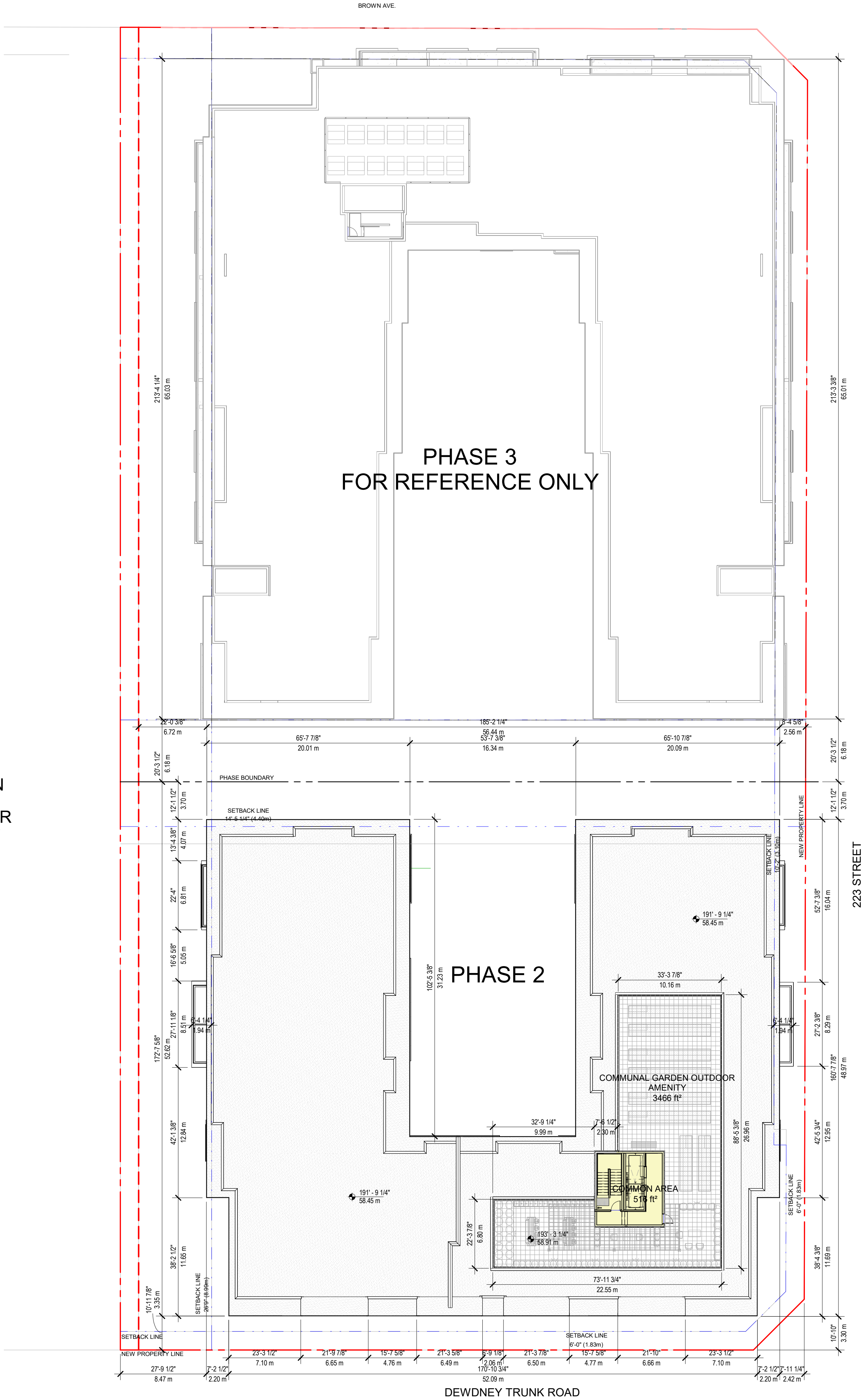




**AREAS**

COMMON

OUTDOOR AMENITY



PHASE 2 - ROOF LEVEL AREA BREAKDOWN					
	Count	Area (SF)	Total Area	Total Area (m²)	Area Ratio
COMMON					
COMMON AREA	1	516 ft²	516 ft²	48 m²	13%
COMMON: 1			516 ft²	48 m²	13%
OUTDOOR AMENITY					
COMMUNAL GARDEN OUTDOOR AMENITY	1	3466 ft²	3466 ft²	322 m²	87%
OUTDOOR AMENITY: 1			3466 ft²	322 m²	87%
Grand total: 2			3982 ft²	370 m²	100%





CHILD PLAY ROOM



GOLF SIMULATOR ROOM



FITNESS / GYM



PARTY ROOM / LOUNGE





MARCH / SEPTEMBER 21st



10:00 am



12:00 pm



2:00 pm



4:00 pm

JUNE 21st



10:00 am



12:00 pm



2:00 pm

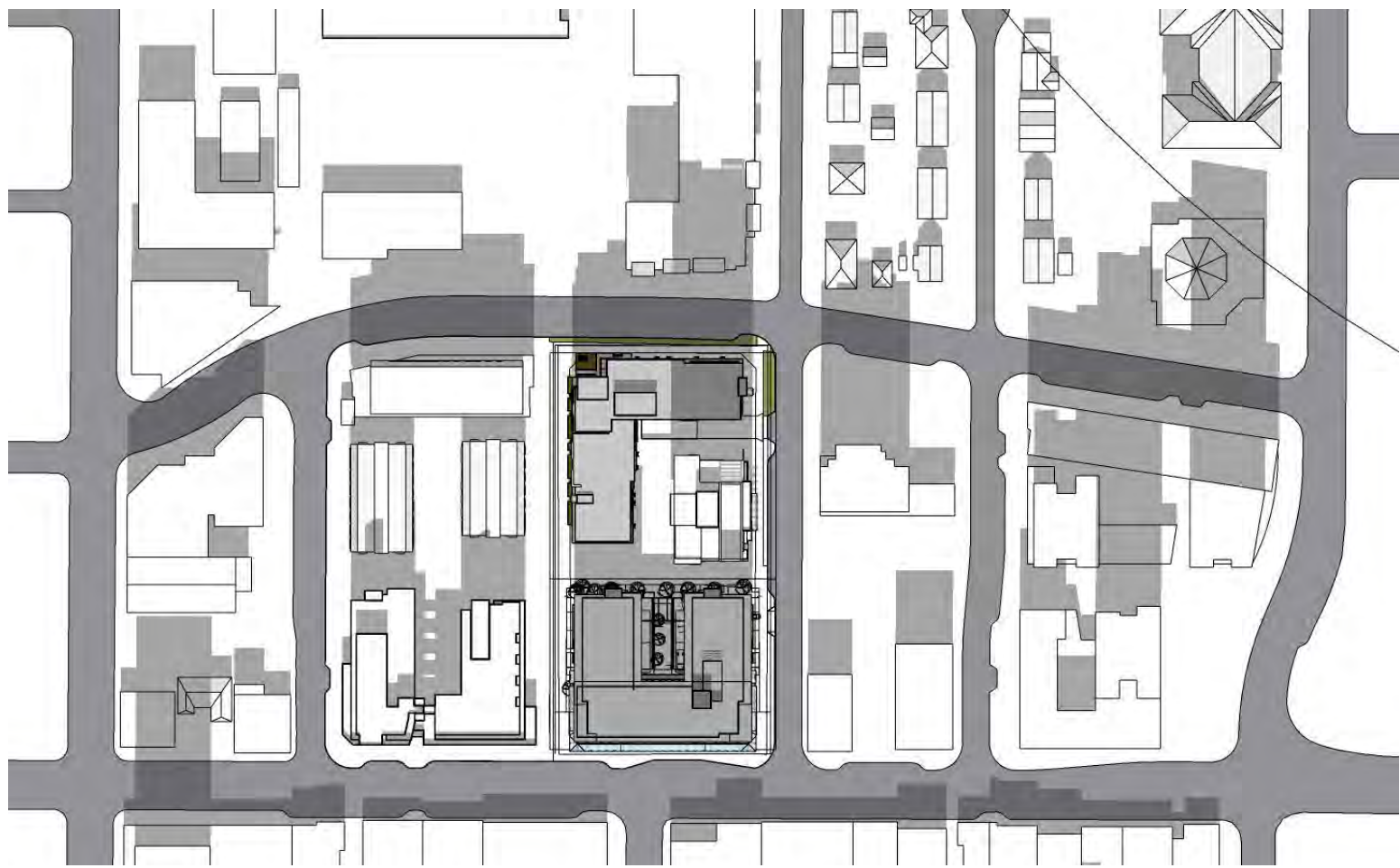


4:00 pm

DECEMBER 21st



10:00 am



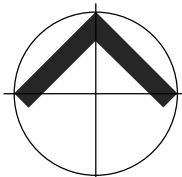
12:00 pm



2:00 pm



4:00 pm







**MATERIALS LEGEND**

CN-01	ARCHITECTURAL FINISH CONCRETE - CLEAR SEALED
CN-02	PAINTED CONCRETE - COLOUR 2
CP-01	FIBRE CEMENT PANEL - CHARCOAL
CP-02	FIBRE CEMENT PANEL - BROWN
CP-03	FIBRE CEMENT PANEL - CHARCOAL GREY
GL-01	LAMINATED BALCONY GLAZING
GL-02	DOUBLE GLAZED CLEAR VISION CURTAIN WALL
GL-03	DOUBLE GLAZED CLEAR VISION WINDOW WALL

**MATERIALS LEGEND**

MC-01	METAL CLADDING - WARM WHITE
MC-02	METAL CLADDING (FEATURE) : COPPER COLOUR
ML-01	MECHANICAL LOUVER - COLOUR TBD
MP-01	METAL PANEL - SIMULATED WOOD GRAIN
PS-01	PLANT SCREEN - WIRE MESH / CLIMBING PLANTS
SP-01	GLASS SPANDREL - CHARCOAL
SS-01	STEEL AND GLASS CANOPY WITH SUPPORTS
ST-01	CULTURED STONE CLADDING









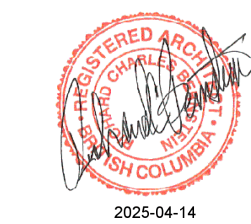


**MATERIALS LEGEND**

CN-01	ARCHITECTURAL FINISH CONCRETE - CLEAR SEALED
CN-02	PAINTED CONCRETE - COLOUR 2
CP-01	FIBRE CEMENT PANEL - CHARCOAL
CP-02	FIBRE CEMENT PANEL - BROWN
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SS-01	STEEL AND GLASS CANOPY WITH SUPPORTS
ST-01	CULTURED STONE CLADDING





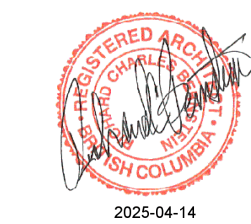


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ST-01	CULTURED STONE CLADDING



























2025-04-14





















- 1 VISION GLASS - CLEAR  
GUARDIAN 6mm CRYSTAL GRAY TEMPERED 1/2"  
VITRO SOLARBAN 60 (#3) 6mm CLEAR
- 2 VISION GLASS - LIGHT GRAY  
VITRO 6mm SOLARCGRAY TEMPERED 1/2"  
VITRO SOLARBAN 60 (#3) 6mm CLEAR
- 3 SPANDREL PANEL - BLACK CHARCOAL  
(From Starline Windows Color Options)
- 4 WINDOW FRAME/ MULLION - ALUMINUM SILVER  
(From Starline Windows Color Options)
- 5 METAL PANEL - RUSTED METAL  
COLOR TO MATCH AB097
- 6 COPPER  
(Clubhouse Business Centre)
- 7 METAL PANEL - WHITE  
COLOR TO MATCH 3D 323 WHITE
- 8 METAL PANEL - WARM GREY METALIC  
(BALCONY SLAB COVER / MECHANICAL ENCLOSURE /  
BALCONY HANDRAIL )
- 9 FIBER CEMENT PANEL - COLOR 1  
WARM CHARCOAL GREY
- 10 FIBER CEMENT PANEL - COLOR 2  
LIGHT GREY
- 11 FIBER CEMENT PANEL - COLOR 3  
BROWN
- 12 WINDOW FRAME - VINYL DARK CHARCOAL  
(From Starline Windows Color Options)
- 13 METAL PANEL - SIMULATED WOOD GRAIN  
COLOR : DARK CHERRY
- 14 STONE CLADDING - THIN STONE  
COLOR : TONES OF GREY
- 15 GLULAM POSTS (GLUE LAMINATED TIMBER)  
COLOR : NATURAL (Clubhouse West facade)
- 16 ARCHITECTURAL FINISH CONCRETE - WHITE  
COLOR : BENJAMIN MOORE SNOW WHITE (OC-66)
- 17 GRAVEL BALLASTED ROOF  
2 TONES OF GREY & WHITE





NEW ERA

MAPLE RIDGE | PHASES 2 & 3

Maple Ridge , BC

OWNER

CLIENT

487559 B.C. Ltd. (on behalf of JOHNSTON MEIER INSURANCE AGENCIES GROUP)

CONTACT

TOM MEIER

22367 Dewdney Trunk Road, Maple Ridge, B.C., V2X 3J4

TEL: (604) 467-4184

EM: tom.meier@jmins.com

DRAWING LIST

LANDSCAPE COVERSHEET (NTS)

L-0.0    Legends & Notes

LANDSCAPE PLANS (SCALE = 1:100)

L-1.0    Landscape Context Plan (1:500)

L-1.1    Ground Level

L-1.1.1 Interim Condition - Ground Level

L-1.2    Podium Level

L-1.3    Roof Level

L-2.1    Ground Level Planting

L-3.1    Landscape Details

L-3.2    Landscape Details

L-3.3    Landscape Details

NOTES

<div>General Notes</div> <div><div>1. Refer to architectural drawings for all walls and stair layout and elevations, unless otherwise noted.</div><div>2. Refer to electrical drawings for all final landscape lighting layout and specifications.</div><div>3. Refer to architectural and mechanical drawings for all drain locations and rim elevations.</div></div>	<div>Planting Notes</div> <div><div>1. All plants / planting specs to be per current edition of Canadian Landscape Standards (CLS).</div><div>2. Plant selection subject to availability at the time of planting.</div><div>3. Contractor shall source specified plant material and only after area of search has been exhausted will substitutions be considered.</div><div>4. All trees to be staked in accordance with CLS Standards.</div><div>5. All plants to be sourced from nurseries certified free of P. ramorum.</div><div>6. Plant sizes and related container classes are specified according to the CLS current edition. For container classes #3 and smaller, plant sizes shall be as shown in the plant list and the standard; for all other plants, both plant size and container calls shall be as shown in the plant list. Specifically, when the plant list calls for #5 class containers, these shall be as defined in the CLS.</div><div>7. All trees to have minimum 10 cubic meters of growing medium unless otherwise specified.</div><div>8. Taxus x media plants to be male specimens only (Re: toxicity)</div></div>
<div>Irrigation Notes</div> <div><div>1. All 'Soft Landscape Areas' are to be irrigated with a high efficiency design/built irrigation system to IIA&amp;C Standards,, complete with Rain and Wind Sensor.</div><div>2. The irrigation system design and installation shall be in accordance with the Irrigation Industry of BC Standards and Guidelines.</div><div>3. System design and installation shall take into account elevation differences, sun orientation and other factors affecting zoning and operation of the system to minimized evapotranspiration and wind lost.</div><div>4. System design shall provide for uniform complete 'Head to Head' coverage of all lawns and planted areas.</div><div>5. Contractor shall be responsible to provide SHOP DRAWINGS a minimum of 3 weeks prior to installation of any irrigation for review and approval.</div><div>6. Should the contractor proceed without approval, any additional modifications to the irrigation systems, as directed by the Landscape Architect shall be at at contractors cost.</div><div>7. Lawns shall be irrigated on separate zones from planted areas.</div><div>8. Controller shall be located in mechanical room.</div><div>9. PRIOR to Substantial Performance, contractor shall provide a maintenance data and Operation instruction manual containing operational information for all operating components, cleaning and lubrication schedules, overhaul/adjustment schedule.</div><div>10. Record Drawings: Submit with the operating and maintenance manuals a reproducible copy of the AS-BUILT condition of the system.</div><div>11. Contractor shall instruct a designated representative of the Owner in the complete operating and maintenance procedures for the irrigation system, including winterizing for the fist time with the designated representative observing.</div><div>12. All piping shall be class 200.</div><div>13. Use GSR Schedule 40 PVC designed for solvent welding to PVC pipe except where valves, risers, etc., require threaded joints.</div><div>14. Provide sleeves under all hard surfaces and as required through walls. If under vehicular paving, cast iron piping required.</div><div>15. Solenoid valves shall be first quality, compatible with the controller selected.</div><div>16. Valve boxes shall be reinforced plastic boxes manufactured specifically for landscape irrigation, complete with captive lock bolt cover, sized to suit valves and other components with adequate room for operation and maintenance.</div></div>	<div>Soil Preparation and Placement Notes</div> <div><div>1. All growing medium placed on project to meet or exceed BCNTA and B.C Landscape Standards latest edition.</div><div>2. Submit sieve analysis by an approved independent soil testing laboratory for each type of growing medium being used on the project PRIOR to placement for review and approval. Clearly identify source and type for each. Resubmit as required until growing medium is approved. Provide one composite sample of each type of proposed growing medium for each different application within the project. minimum 1 litre physical sample.</div><div>3. Submittals shall be made at least seven (7) days before</div><div>4. Contractor shall not move or work growing medium or additives when they are excessively wet, extremely dry, or frozen or in any manner which will adversely affect growing medium structure. Growing medium whose structure has been destroyed by handling under these conditions will be rejected. Growing medium shall not be handled in wet or frozen conditions.</div><div>5. Slab drainage shall be 19mm (3/4") diameter drain gravel free from any silt and clay as shown in details.</div><div>6. Place growing medium, except structural to required finish grades and minimum depths as detailed, unless shown otherwise.</div></div>
<div>Product and Material Notes</div> <div><div>1. All materials to be as specified or pre-approved equivalent.</div><div>2. All material and products to be installed per manufacturer's specifications.</div></div>	

16    APR 14, 2025    Re-issued for DP

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01    Apr 26, 2021    Preliminary City Consult

NO.:    DATE:    ITEM:

Revisions:

durante  
kreuk

Durante Kreuk Ltd.

102 - 1637 West 5th Avenue

Vancouver BC V6J 1N5

t: 604 684 4611

f: 604 684 0577

www.dkl.bc.ca

Project:

NEW ERA

MAPLE RIDGE

MAPLE RIDGE, BC

Drawn by:    JBT/MP

Checked by:    DC

Date:    April 14, 2025

Scale:    NTS

Drawing Title:

Landscape Coversheet -

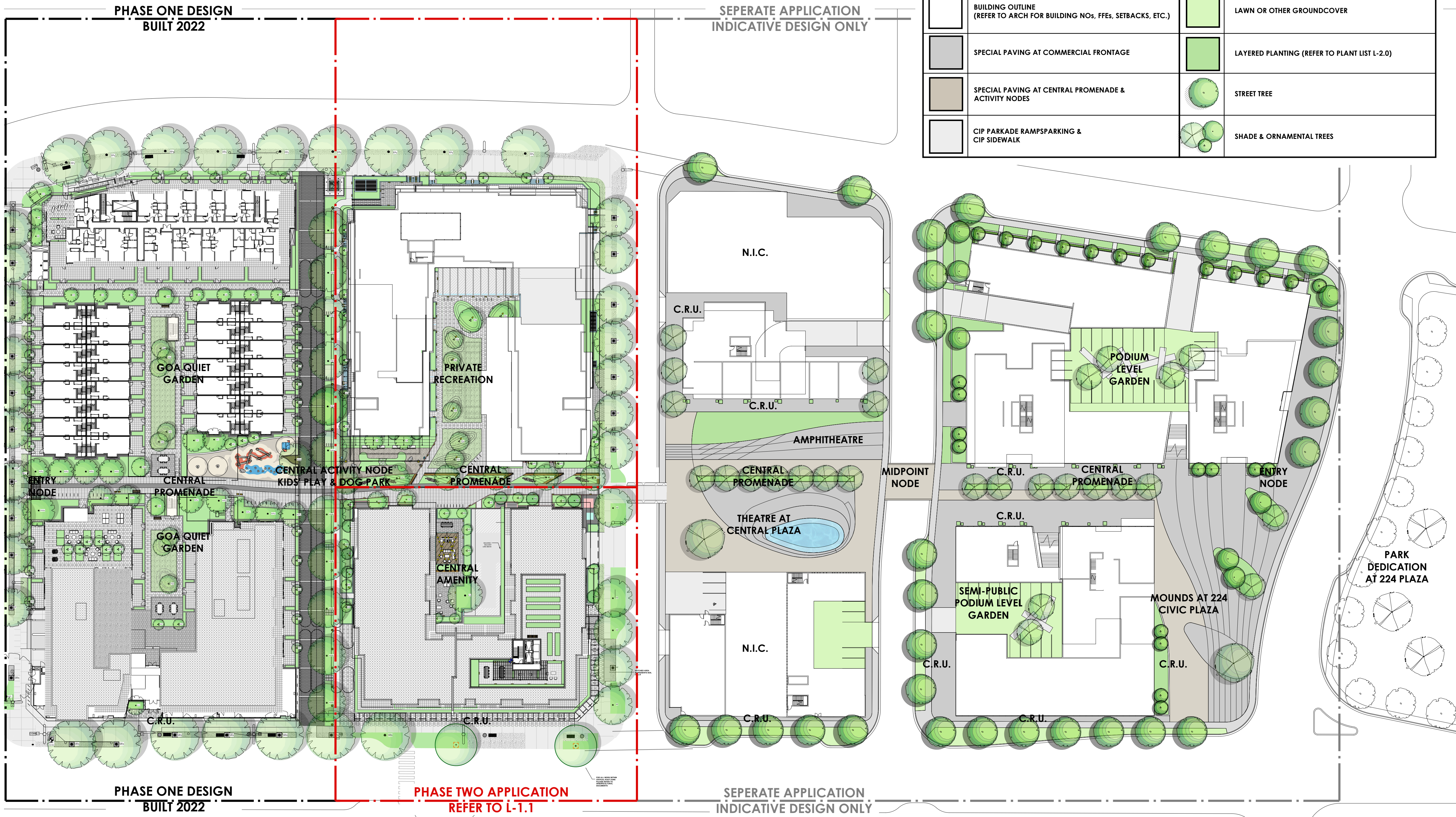
Legend & Notes

DK Project No.:    21029

Sheet No.:

L-0.0





THEMATIC IMAGES - PUBLIC REALM

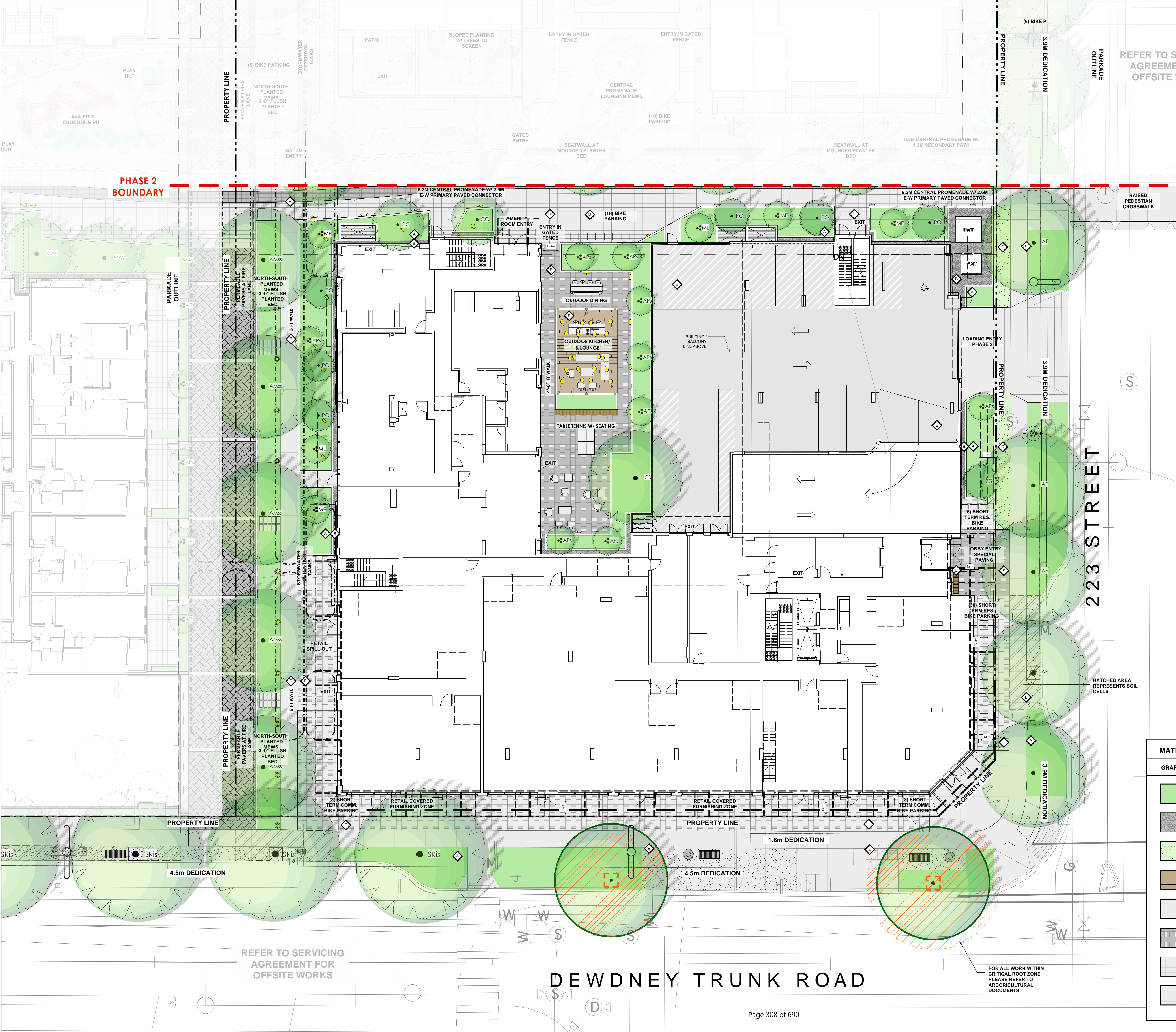
THEMATIC IMAGES - PODIUM AMENITY

DK Project No.: 21029

Sheet No.:

L-1.0





D02

SAN1

PROJECT N.

16	APR 14, 2025	Re-issued for DP
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Project:

## NEW ERA

MAPLE RIDGE

MAPLE RIDGE, BC

Drawn by: JBT/MP

Checked by: DC

Date: April 14, 2025

Scale: 1:150

Drawing Title:

### Landscape Plan - Level 1 Materials

DK Project No.: 21029

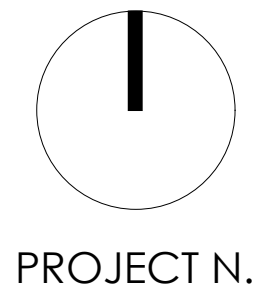
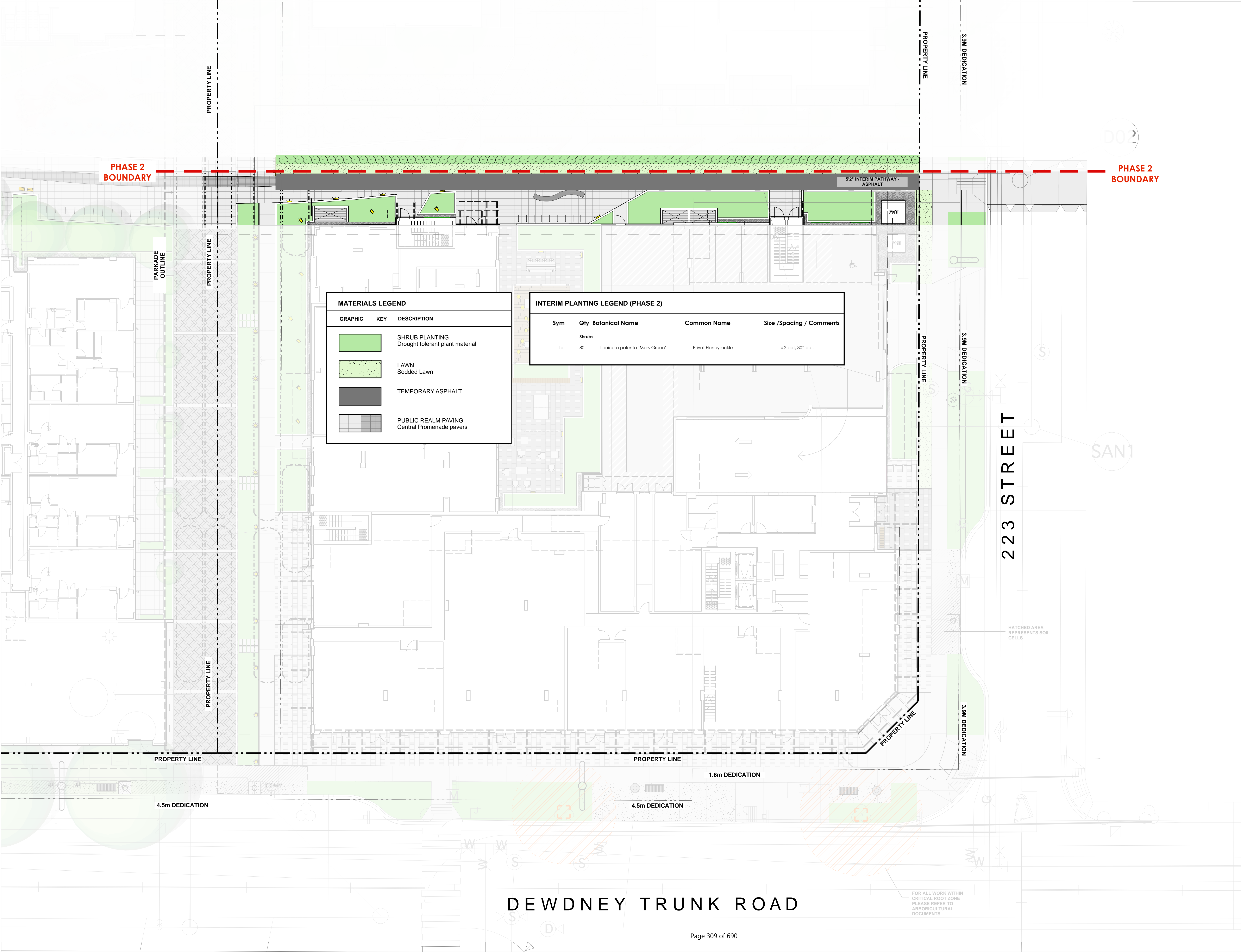
Sheet No.:

# L-1.1

MATERIALS LEGEND		
GRAPHIC	KEY	DESCRIPTION
	A	SHRUB PLANTING Drought tolerant plant material
	B	BUFFER MATERIAL River Rock
	C	LAWN Sodded Lawn
	D	SEATING Wood Benches
	E	CIP CONCRETE PAVING Broom finished & exposed aggregate
	F	SPECIAL PAVING Contrast material at Entries & Paths
	G	PATIO PAVING Hydropressed slabs
	H	PUBLIC REALM PAVING Central Promenade pavers

FOR ALL WORK WITHIN  
CRITICAL ROOT ZONE  
PLEASE REFER TO  
ARBORICULTURAL  
DOCUMENTS





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102 - 1637 West 5th Avenue  
Vancouver BC V6J 1N5  
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Project:

# NEW ERA

MAPLE RIDGE

MAPLE RIDGE, BC

Drawn by: JBT/MP

Checked by: DC

Date: April 14, 2025

Scale: 1:150

Drawing Title:

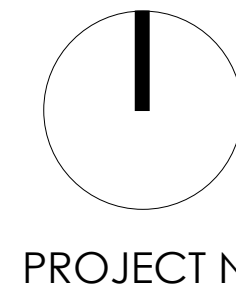
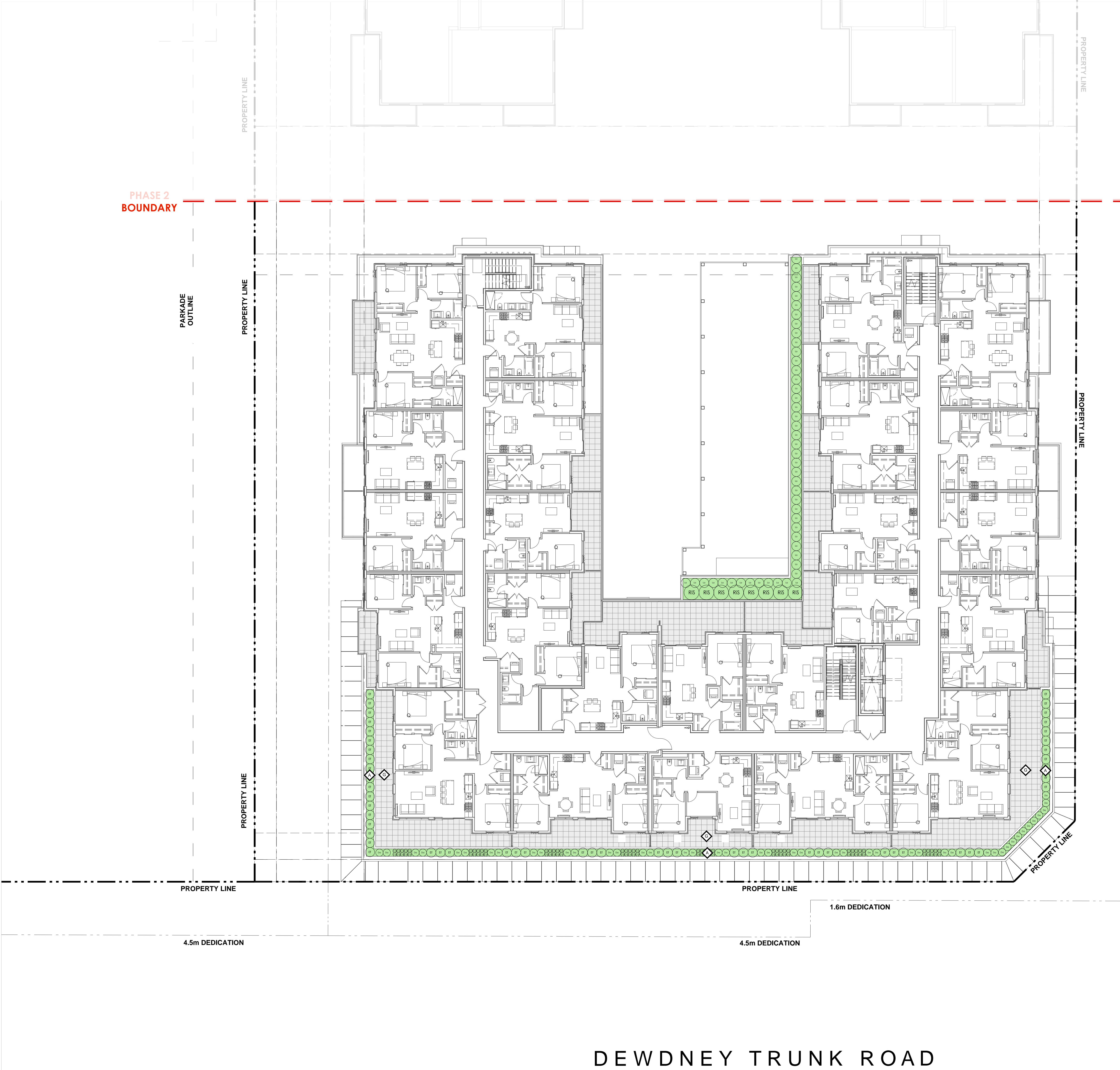
## Interim Condition - Ground Level

DK Project No.: 21029

Sheet No.:

# L-1.1.1





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Project:

# NEW ERA MAPLE RIDGE

MAPLE RIDGE, BC

Drawn by: JBT/MP

Checked by: DC

Date: April 14, 2025

Scale: 1:150







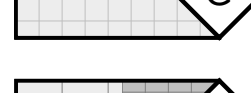

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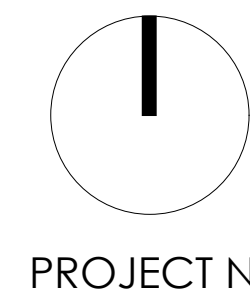
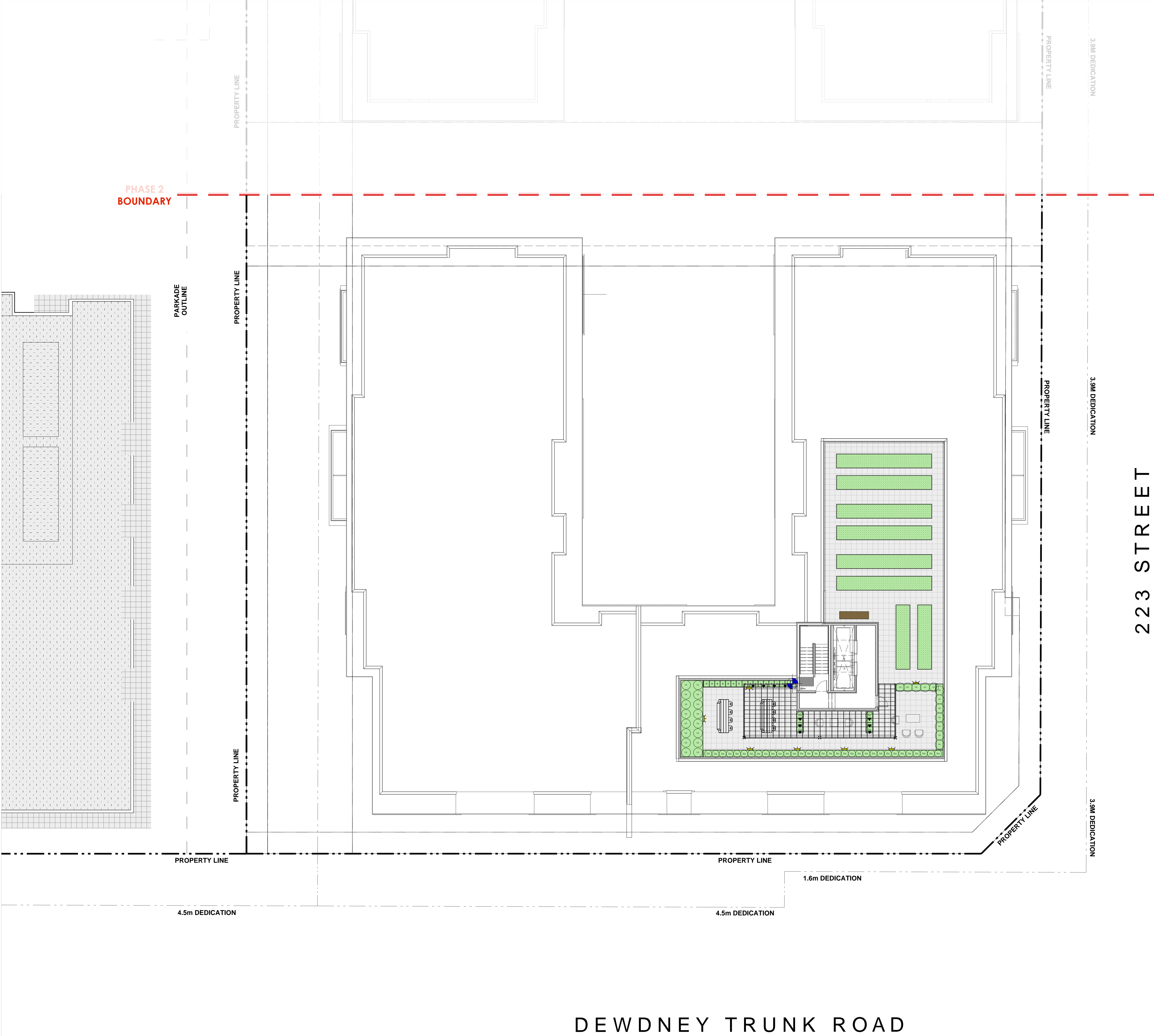
## Landscape Plan - Level 2 Materials

DK Project No.: 21029

Sheet No.:

# L-1.2

MATERIALS LEGEND		
GRAPHIC	KEY	DESCRIPTION
	A	SHRUB PLANTING Drought tolerant plant material
	B	URBAN AGRICULTURE PLANTER
	C	COLOURED GRAVEL At Dog Run & Patterned Roofs
	D	SEATING Wood Benches
	E	CIP CONCRETE PAVING Broom finished & exposed aggregate
	F	SPECIAL PAVING Contrast material at Entries & Paths
	G	PATIO PAVING Hydropressed slabs
	H	PUBLIC REALM PAVING Central Promenade pavers



16	APR 14, 2025	Re-issued for DP
15	APR 07, 2025	Issue for Review
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03	Sep 30, 2021	DP Pre-Application
02	Jun 10, 2021	DP Pre-Application
01	Apr 26, 2021	Preliminary City Consult
NO.:		DATE:
Revisions:		ITEM:



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Project:

**NEW ERA**  
**MAPLE RIDGE**

Maple Ridge, BC

Drawn by:

JBT/MP

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1:150

Drawing Title:


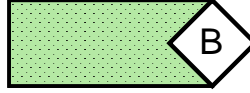




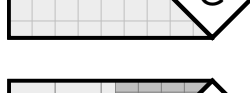
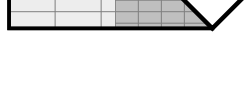
**Landscape Plan -**  
**Level 7 (Roof) Materials**

DK Project No.:

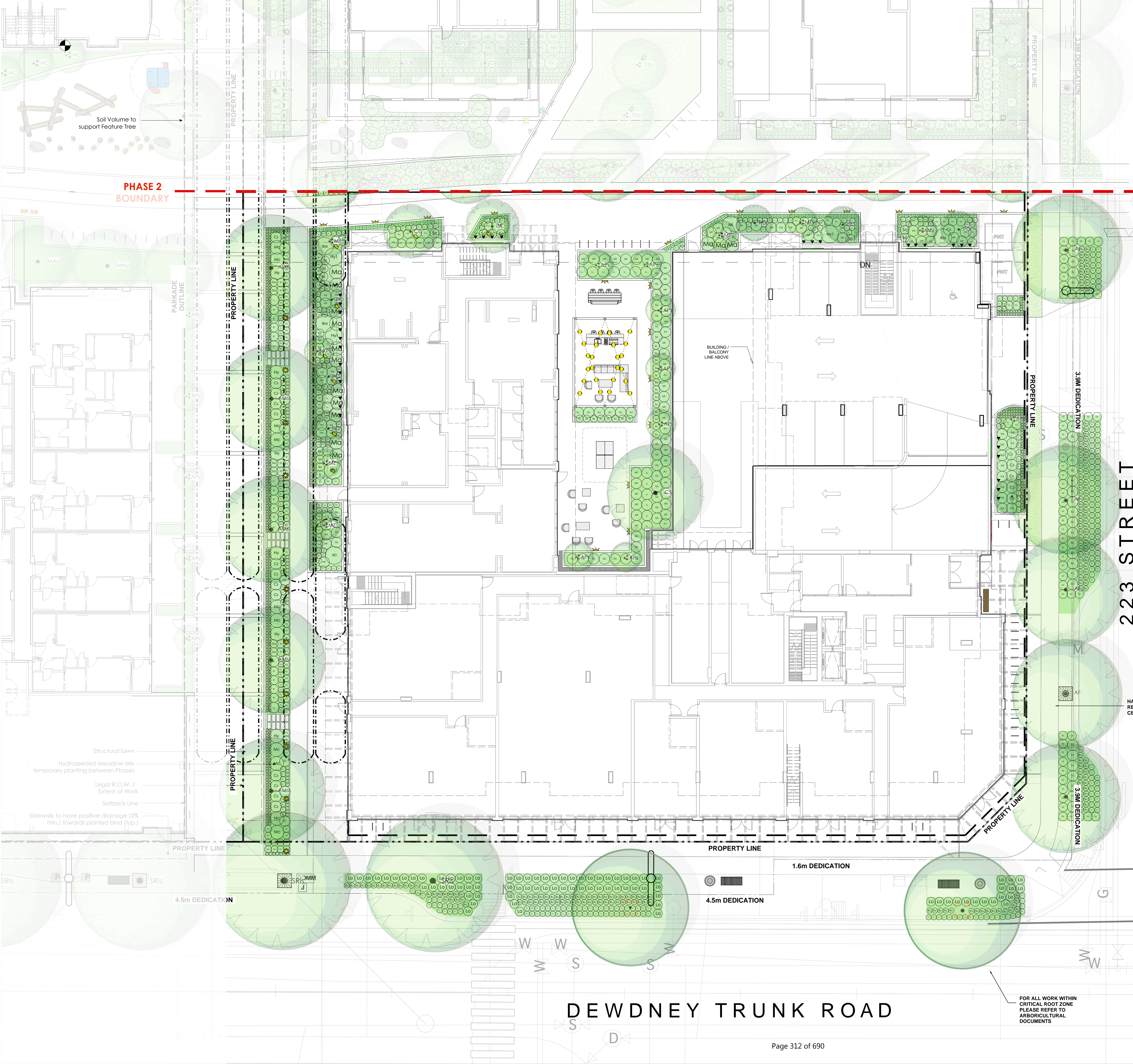
**21029**

Sheet No.:

**L-1.3**

MATERIALS LEGEND		
GRAPHIC	KEY	DESCRIPTION
	A	SHRUB PLANTING Drought tolerant plant material
	B	URBAN AGRICULTURE PLANTER
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PLANTING LEGEND (PHASE 2)				
Sym	Qty	Botanical Name	Common Name	Size
<b>Trees</b>				
AM5	5	Acer mayabei 'State Street'	Morton Miyabe Maple	6cm cal. B&B
APs	9	Acer palmatum 'Green'	Green Japanese Maple	#20 pot. B&B, Multi-stem
CS	1	Calopha speciosa	Northern Calopha	6cm cal. B&B
CC	2	Cornus controversa 'June Snow'	June Snow Dogwood	6cm cal. B&B
ME	6	Magnolia x brooklynensis 'Elizbeth'	Magnolia 'Elizbeth'	#20 pot. B&B, Multi-stem
PO	7	Picea amurica	Siberian Spruce	3.0m ht., B&B, Multi-stem
<b>Shrubs</b>				
+	147	Taxus x media 'Hicksii' (MALE ONLY)	Yew Hedge (MALE)	4' ht., B&B, RB-to-RB
Al	11	Aucuba japonica	Spotted Japanese Laurel	#2 pot. 36" o.c.
Ca	21	Cornus sanguinea 'Midwinter Fire'	Blood Wiggled Dogwood	#2 pot. 36" o.c.
Da	84	Daphne x transatlantica 'Blafo'	Eternal Fragrance Daphne	#2 pot. 24" o.c.
El	73	Eunymia fortunei 'Emerald n' Gold'	Wintercreeper	#2 pot. 30" o.c.
Eu	142	Euphorbia characias subsp. wulfenii	Mediterranean Spurge	#1 pot. 18" o.c.
Fa	25	Fatsia japonica	Japanese Aralia	#2 pot. 36" o.c.
Gs	30	Gaultheria shallon	Salal	#1 pot. 18" o.c.
Hy	10	Hydrangea arborescens 'Annabelle'	Smooth Hydrangea	#3 pot. 60" o.c.
Lo	215	Lonicera pleata 'Moss Green'	Privet Honeysuckle	#2 pot. 36" o.c.
Ma	14	Mahonia aquifolium	Oregon Grape Holly	#3 pot. 48" o.c.
MT	56	Mattuccio struthiopteris	Ostrich Fern	#2 pot. 36" o.c.
Pm	19	Pinus mugo 'Candlen' Gold'	Dwarf Mountain Pine	#2 pot. 30" o.c.
Rk	7	Ribes sanguineum	Red Flowering Currant	#2 pot. 36" o.c.
RD	1	Rhododendron 'Davies'	Davies' Rhodo	#3 pot. 60" o.c.
Rm	3	Rhododendron macrophyllum	Pacific Rhodo	#3 pot. 60" o.c.
Rh	14	Rhododendron 'Ramapo'	Ramapo Rhododendron	#2 pot. 30" o.c.
Sc	28	Sarcococca hookeriana var 'humilis'	Sweet Box	#1 pot. 30" o.c.
Va	31	Viburnum davidii	David Viburnum	#2 pot. 36" o.c.
<b>Perennials, Grasses &amp; Groundcovers</b>				
An	62	Anemone x hybrida 'Andrea Atkinson'	Andrea's Windflower	#1 pot. 18" o.c.
As	86	Astilbe japonica 'Deutchland'	Hybrid Japonica	#2 pot. 18" o.c.
Kf	71	Calamagrostis acutiflora 'Karl Foerster'	Karl's Feather Reed Grass	#2 pot. 18" o.c.
C	46	Carex marshallii 'Ice Dance'	Ice Dance Sedge	#1 pot. 12" o.c.
Ca	103	Carex oshimensis 'Evergold'	Japanese Sedge	#1 pot. 12" o.c.
De	35	Deschampsia cespitosa 'Goldtau'	Goldtau Tufted Hair Grass	#1 pot. 24" o.c.
Ec	34	Echinacea purpurea 'Magnus'	Coneflower Magnus	#2 pot. 36" o.c.
Ga	184	Gallium odoratum	Sweet Woodruff	4" pot. 12" o.c.
G	410	Geranium sanguineum	Bloody Cranesbill	4" pot. 12" o.c.
Ha	76	Hakonechloa macro 'Aureola'	Japanese Forest Grass	#1 pot. 24" o.c.
Ho	14	Hosta 'Victory'	Plantain Lily	#2 pot. 24" o.c.
Jh	2	Juniperus horizontalis 'Blue Chip'	Blue Chip Creeping Juniper	#2 pot. 48" o.c.
Ml	2	Miscanthus sinensis 'Gracillimus'	Maiden Grass	#3 pot. 60" o.c.
Mc	10	Miscanthus sinensis 'Cascade'	Cascade Maiden Grass	#3 pot. 60" o.c.
Ne	7	Nepeta x faassenii 'Six Hills Giant'	Catmint Six Hills Giant	#1 pot. 18" o.c.
P	142	Potentilla fruticosa	Three-leaved cinquefoil	#1 pot. 12" o.c.
Po	66	Polydichum minus	Western Sword Fern	#1 pot. 18" o.c.
Ru	219	Rudbeckia hirta	Black Eyed Susan	#1 pot. 24" o.c.
Sa	16	Salix purpurea 'Nana'	Dwarf Basket Willow	#2 pot. 30" o.c.
▼	41	Lonicera x brownii 'Dropmore Scarlet'	Dropmore Scarlet Honeysuckle	#3 pot. 3 leads min.
<b>PLANTING LEGEND (OFF SITE)</b>				
<b>Trees</b>				
SRs	2	DEWDNEY TRUNK ROAD: (PH1 REV) Syringa reticulata 'Ivory Silk'	Japanese Tree Lilac	7cm Cal / B&B / Street Tree
223 STREET:				
AF	5	Acer x Freemanii 'Autumn Blaze'	Freeman Maple	7cm Cal / B&B / Street Tree

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Project:  
**NEW ERA**  
**MAPLE RIDGE**

MAPLE RIDGE, BC

Drawn by: JBT/MP

Checked by: DC

Date: April 14, 2025

Scale: 1:150

Drawing Title:  
**Planting Plan -**  
**Ground Level**

DK Project No.: 21029

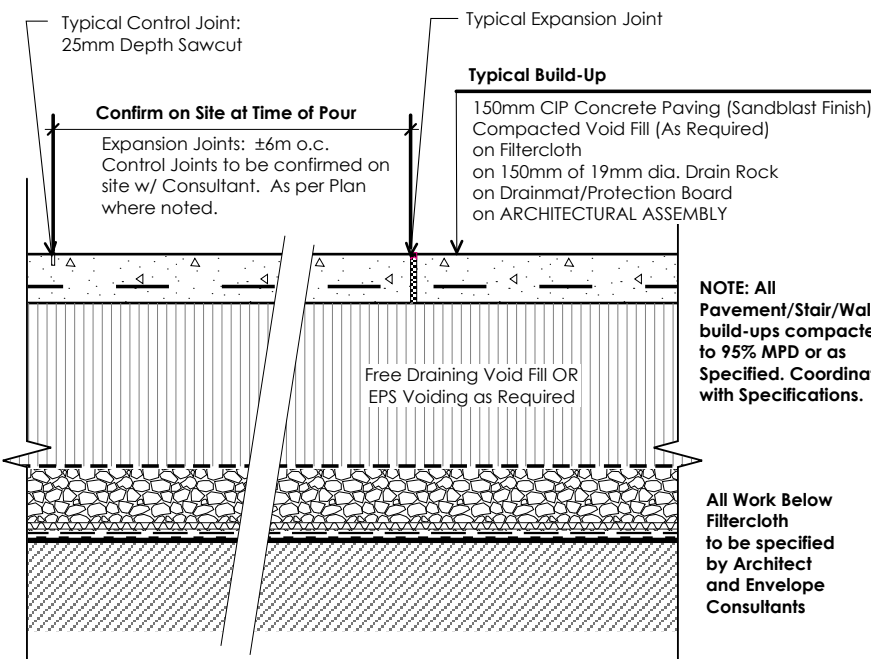
Sheet No.: L-2.1

**PLANNING NOTE:**  
Landscaping is subject to review or change for location and type of trees and other suitable planting, including grass, once Offsite Servicing Agreement Plans are submitted and accepted by the City.



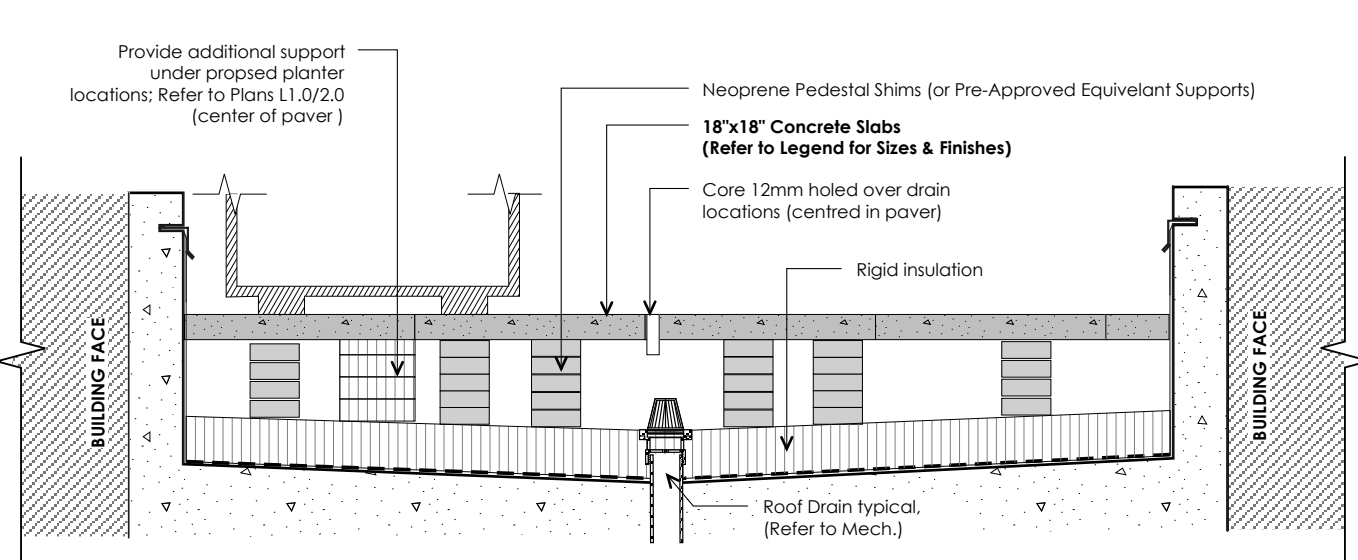
**GENERAL CONCRETE WORKS NOTES:**  
1. All concrete details shown on landscape drawings for design purposes only.  
2. All concrete work on slab to be built off arch, drawings.  
3. All reinforcement as shown on structural drawings.

**GENERAL NOTES:**  
1. Confirm all "Special Tooling", Expansion Joint and Control Joint Layout on site w/Consultant.  
2. Remove all "soft spots" in subgrade to Consultant approval and to Specifications Section 02210.  
3. Place E's at all locations where slab meets/drobs with walls or stairs or any "permanent" adjacent work not of this contract. Max. 6m o.c. unless shown otherwise. Refer to Layout Plan L-1.  
4. Refer to L-1 Layout Materials and Grading Plan for specific concrete finish treatments. "Medium Sandblast Finish" preferred. Tool all edges.  
5. Reinforcing where shown 10M Bar at 300mm o.c. both ways. Slab thickness to 150mm C/P Concrete.

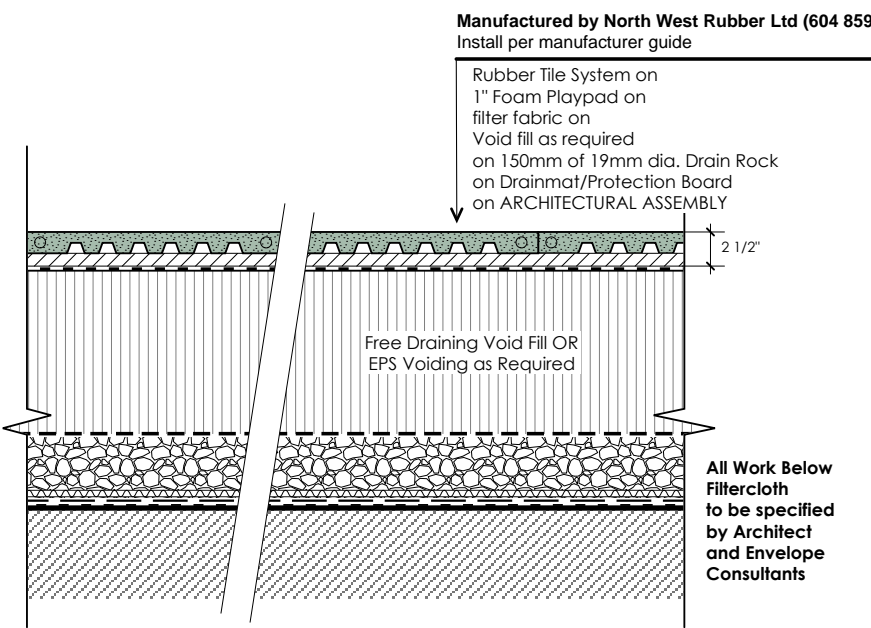


**Ld-01 L1.1 CIP Concrete On-Slab**  
Scale: 1:10

**GENERAL NOTES:**  
1. Installation must be complete in accordance with unit paver products specifications.  
2. Use of buffer pads is mandatory.  
3. Pre-approved equivalent pedestals or shims may be used as alternate.  
4. Provide additional support in centre of pavers below all proposed planter locations.

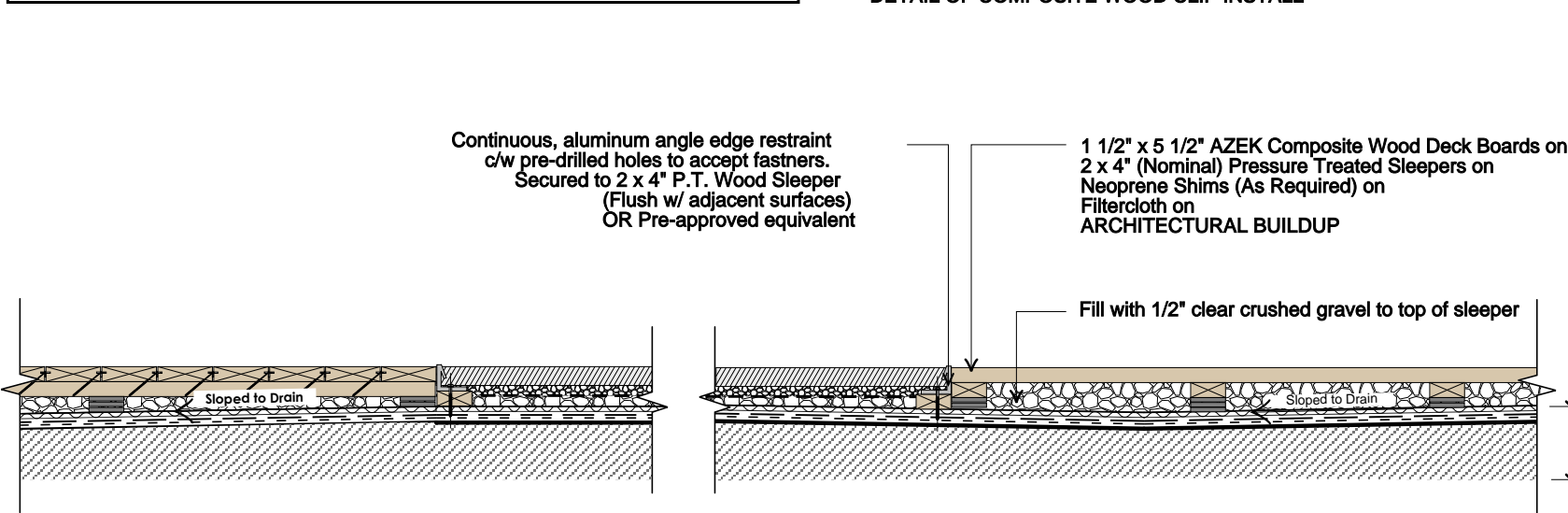


**Ld-02 L1.1 Unit Pavers on Pedestal System (at Level 7)**  
Scale: 1:10



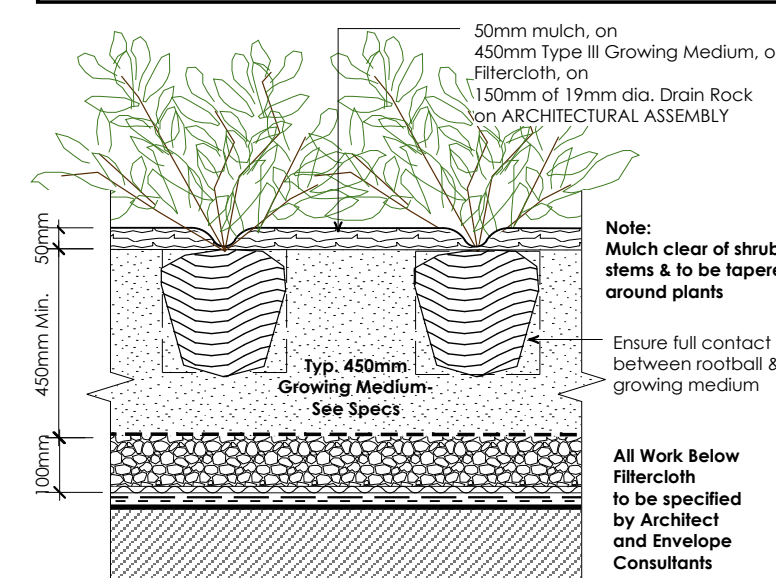
**Ld-04 L1.1 Rubber Tile Play Surfacing On Slab**  
Scale: 1:10

**GENERAL NOTES:**  
1. CUTTING AND DRILLING: Premium carbide tipped saw blades to be used for cutting, of composite material. Use Brad point drill bits or faster bits to provide cleaner holes. Pre-drilling is required. Pre-drill all holes with appropriately sized drill bit for your screw size.  
2. FASTENERS: Use only 305 or higher grade stainless steel fasteners to fasten composite wood decking. Use hidden deck fasteners by AZEK manufacturer or fasten the deck boards to pressure treated joists. Available through <http://azekco.com>

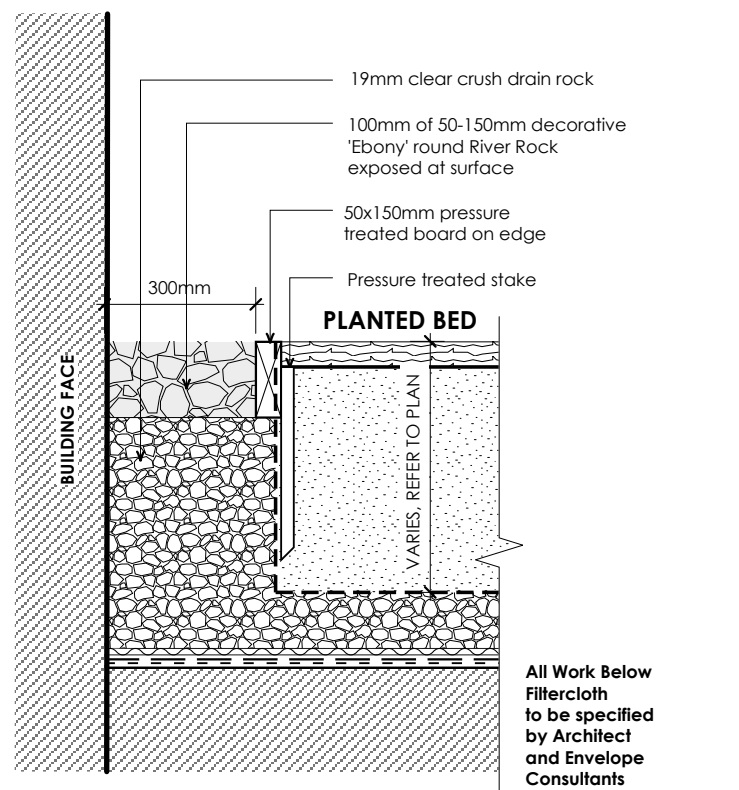


**Ld-05 L1.1 Composite Wood Decking On Waterproof**  
Scale: 1:10

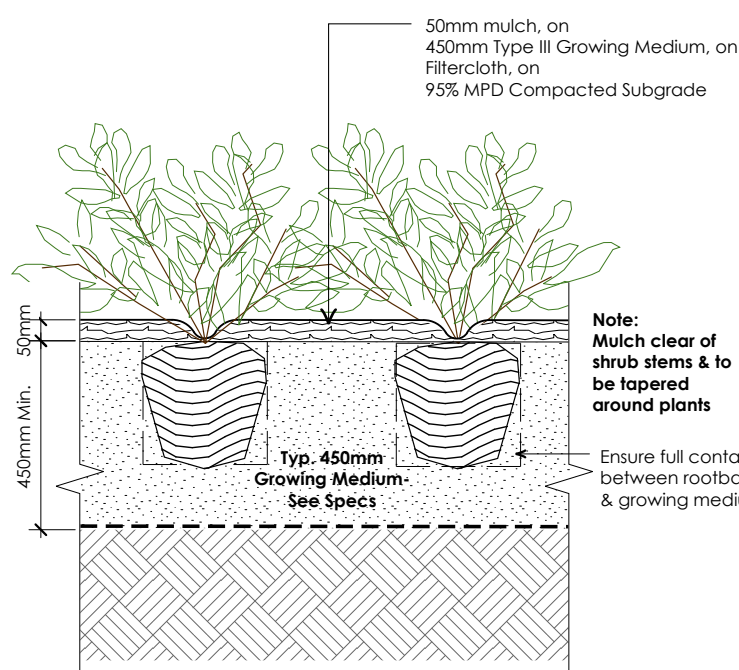
**GENERAL NOTES:**  
1. Plant to be planted with top of rootball level with finish grade.  
2. Composted bark mulch to be kept at least 2" away from stems.  
3. Prune any broken or damaged branches and double leaders using approved pruning tools and standard I.S.A. pruning practices, see specifications.  
4. A representative area of each plant species is to be laid out and approved by landscape architect prior to planting.



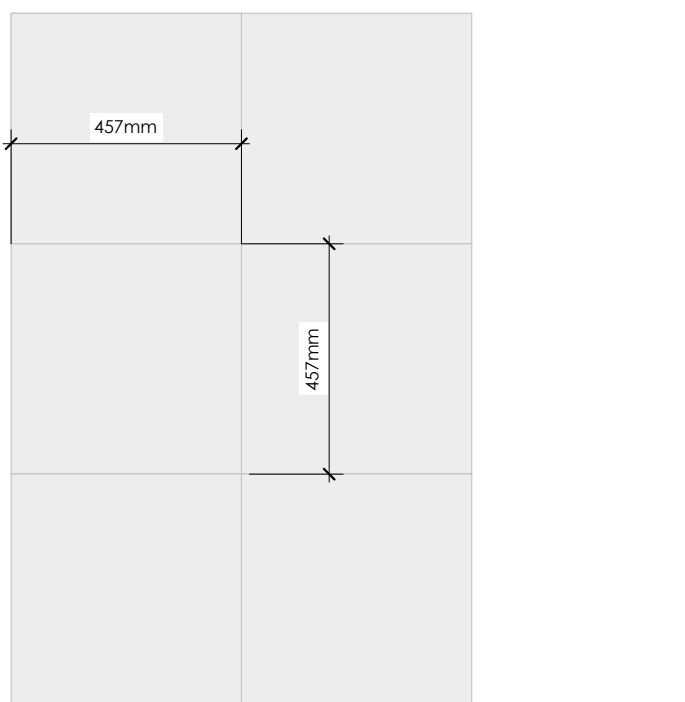
**Ld-06 L1.1 Typical Shrub Planting On-Slab / In-Planter**  
Scale: 1:10



**Ld-04 L1.1 Typical Drip Strip (in planting bed)**  
Scale: 1:10



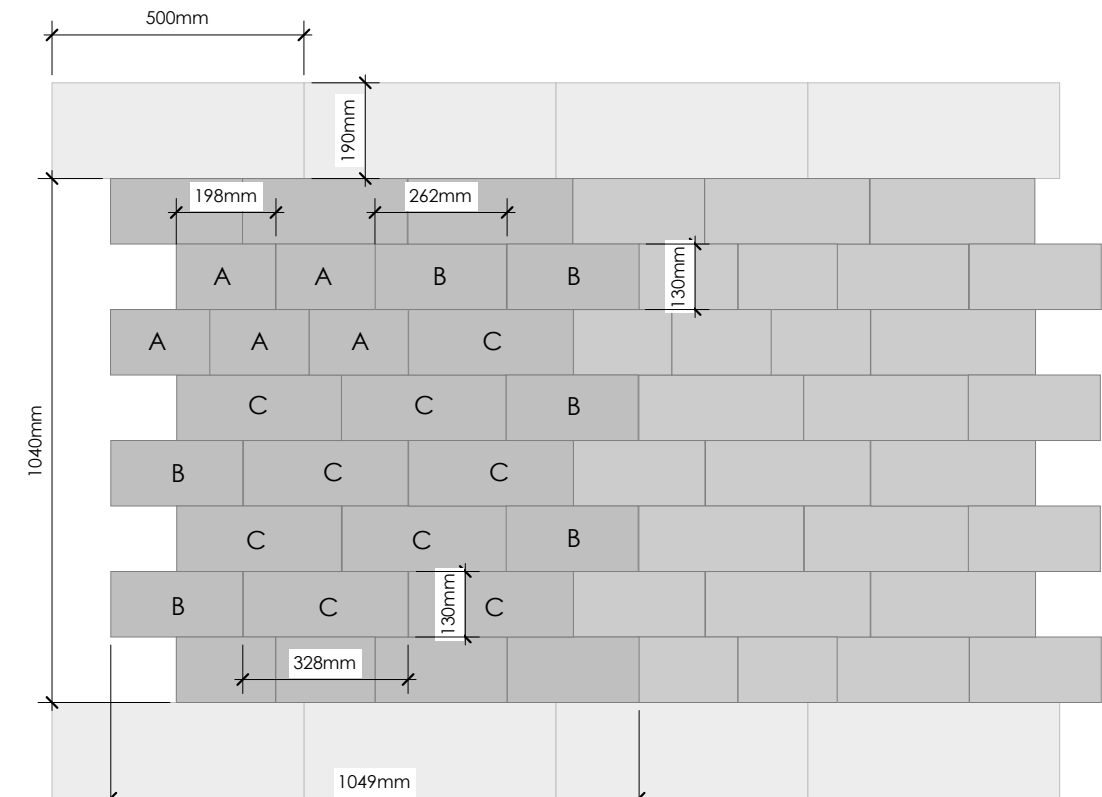
**Ld-07 L1.1 Typical Shrub Planting On-Grade**  
Scale: 1:10



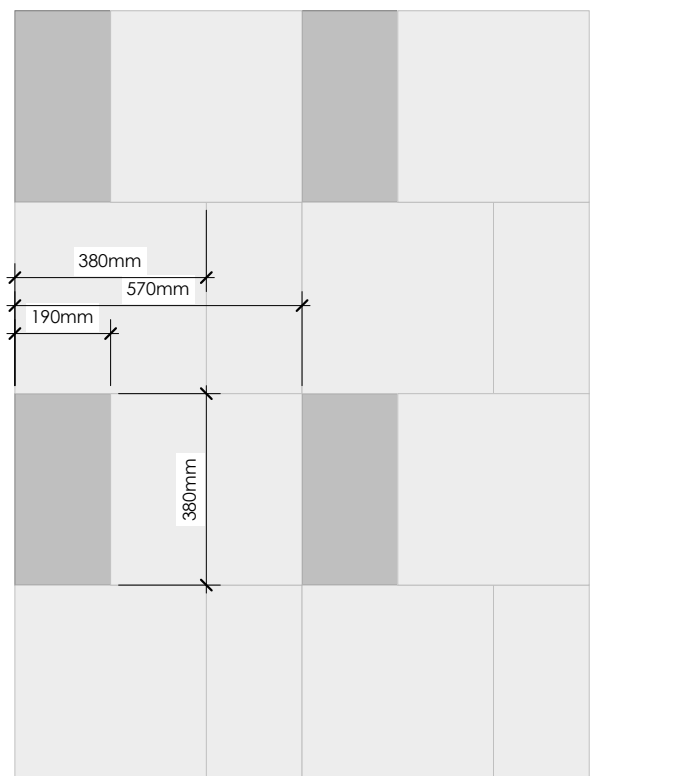
**Ld-08 L1.1 Concrete Slab Layout - At Rooftop**  
Scale: 1:10



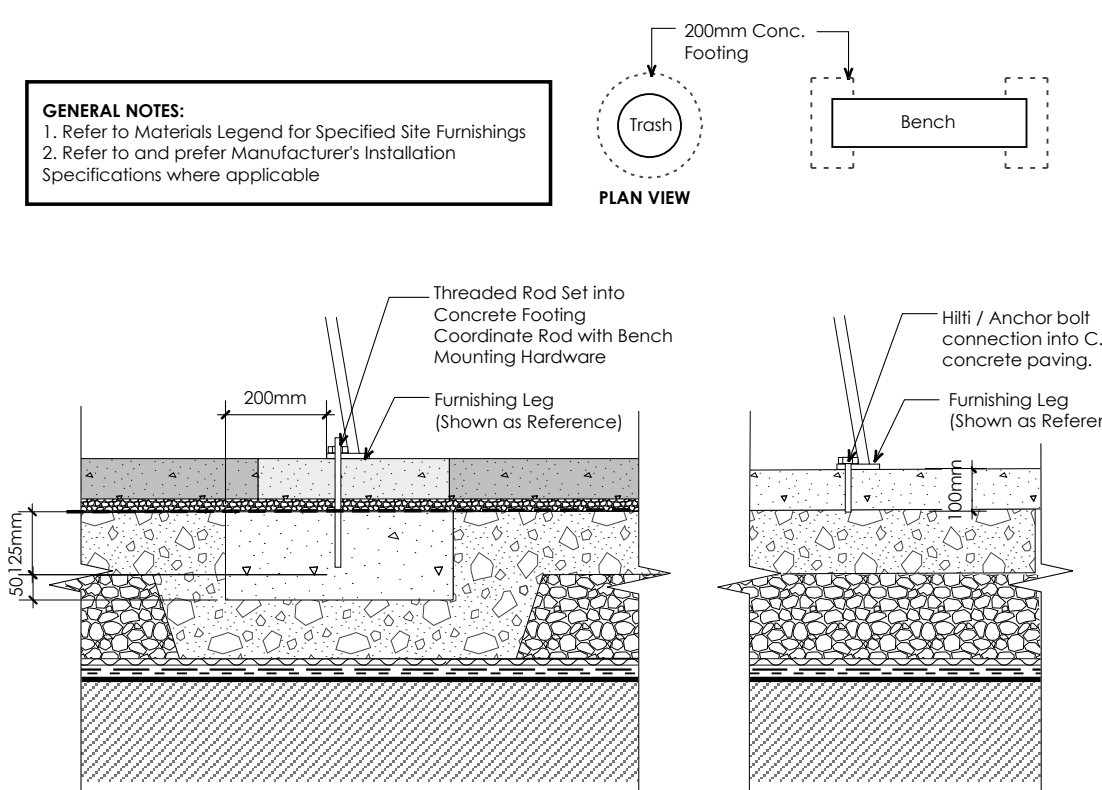
**Ld-09 L1.1 Unit Paver Layout #1 - At Lobby Entry**  
Scale: 1:10



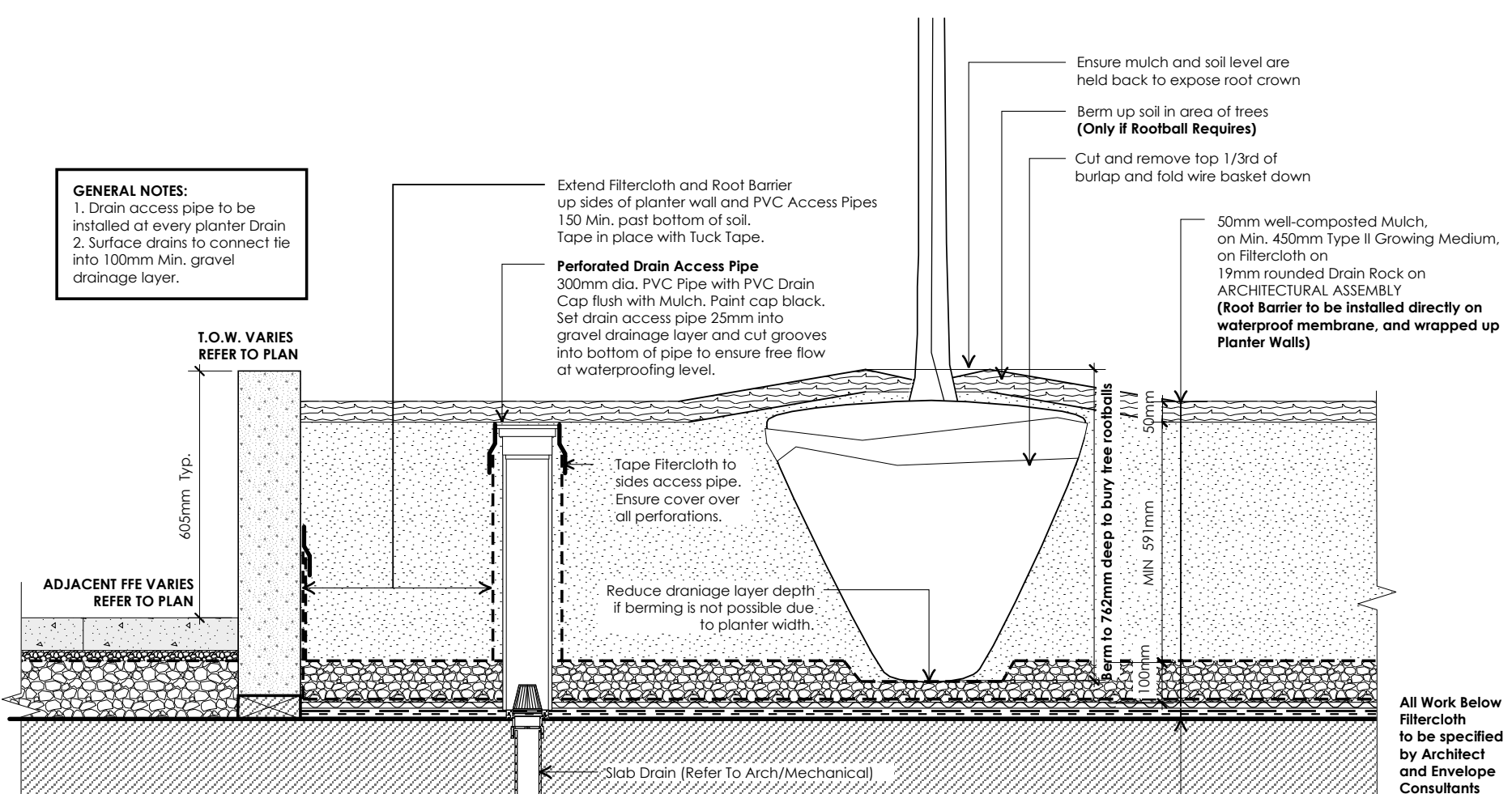
**Ld-11 L1.1 Unit Paver Layout #3 - At Central Promenade**  
Scale: 1:10



**Ld-10 L1.1 Unit Paver Layout #2 - At Shared Amenity and Retail Spill Out**  
Scale: 1:10

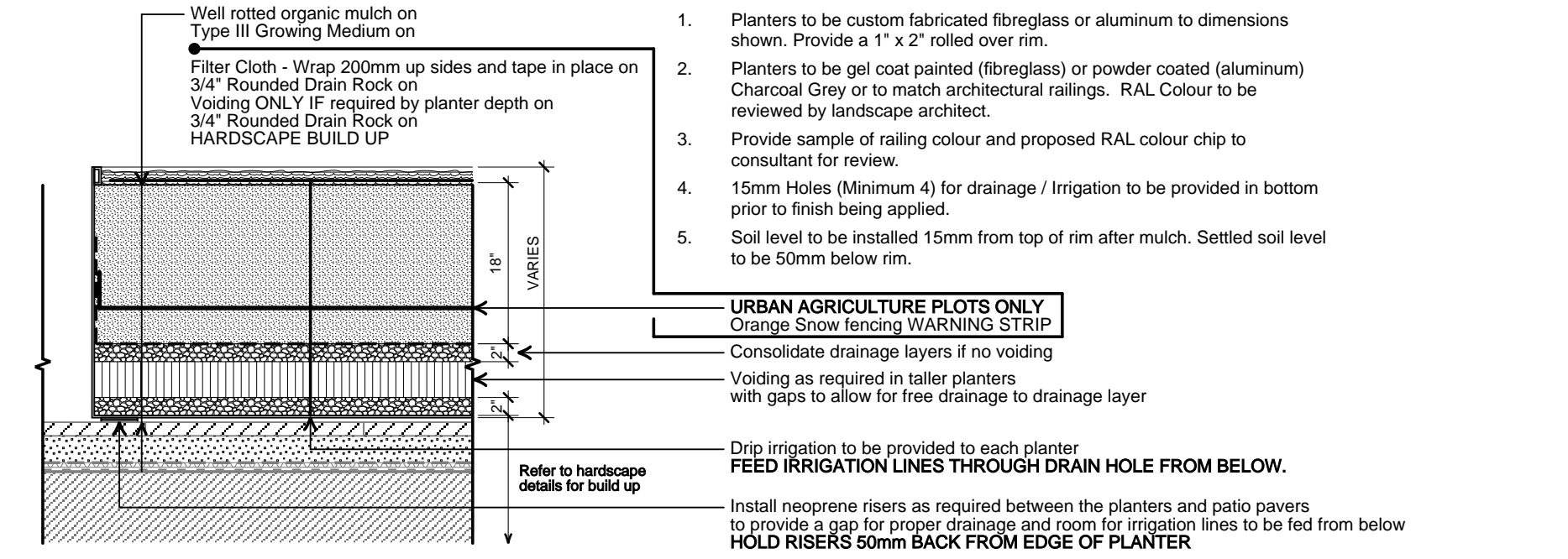


**Ld-12 L1.1 Site Furniture Attachment Detail**  
Scale: 1:10



**Ld-13 L1.1 Typical Tree Planting Detail On-Slab**  
Scale: 1:10

**Ld-13b L1.1 Typical Drain Access Pipe in Planting & Lawn**  
Scale: 1:10



**Ld-14 L1.1 Custom Planters (Size Varies)**  
Scale: 1:10

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06 Jan 30, 2025 Issued for BP Review  
05 Dec 20, 2024 Re-issued for DP  
04 Aug 23, 2024 Issued for BP Draft  
03 Aug 02, 2024 90% BP Draft  
02 July 12, 2024 Re-issued for DP  
01 June 21, 2024 Issued for 60% BP

NO.: DATE: ITEM:

Revisions:

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Project:  
**NEW ERA**  
**MAPLE RIDGE**

**MAPLE RIDGE, BC**

Drawn by: MP

Checked by: DC

Date: April 14, 2025

Scale: As Noted

Drawing Title:

**Landscape Details**

Project No.: **21029**

Sheet No.:

**L-3.1**







NOTE: CARRY OVER LIGHTING PRODUCTS FROM ERA PHASE 1 - TO BE CONFIRMED BY NEMETZ

L-1

MINI LOOK  
BOLLARD

SPECIFICATION SHEET  
Project name:  
Type:

PAGE: 1 OF 2



Ground mounted bollard suitable for walkways and paths, being a valid alternative to a wall mounted illumination.

**Luminaire characteristics:**  
Power input: 13W  
Lumens: 334 lm (delivered for 3000K)  
Luminaire efficacy: 27 lm/W (for 3000K)

**Source:** White LED, LM-80 tested.

**Lumen maintenance:** >70% of initial lumens at 50 000 hours (L70), (LM-T9 tested).

**Optics:** Accent light.

**Material:**  
Body: Die-cast aluminum  
Diffuser: Tempered glass

**Electrical:** Universal high efficiency electronic power supply, rated at 50 000 hours, 120-277V.

**Mounting:** Install with flange accessory or fasten to ground.

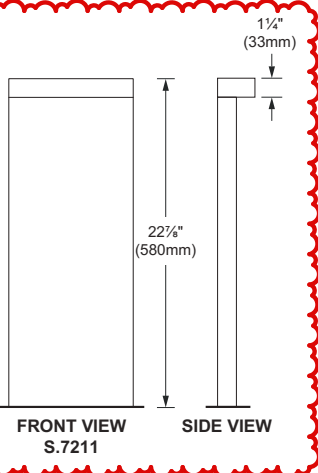
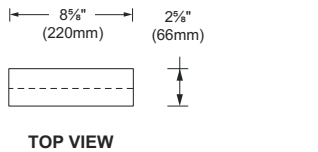
**Finish:** White, Aluminum gray or Burnished bronze.

**Weight:**  
S.7206: 3.3lbs (1.5kg)  
S.7211: 7.94lbs (3.6kg)

**Warranty:** 5 year limited warranty.

**Certification:** cULus listed for wet location.

**Ratings:** IP65, IK08



L-2

SKILL  
SURFACE MOUNTED - FLUSH MOUNTED

Type:  
Project:

SPECIFICATION SHEET  
Page: 1 of 2

Skill is a unique LED luminaire, assuring high lighting performance and total absence of glare. The most modern electronic technology is contained in the thickness of only 3 cm and provides an excellent quality of light while saving energy.

**Luminaire characteristics:**

**Power input:** 6 to 24W

**Lumens:** 130 to 840lm (for 3000K, 90CRI)

**Luminaire efficacy:** 25 to 35lm/W

**Source:** White LED, LM-80 tested.

**Lumen maintenance:** >70% of initial lumens at 50 000 hours (LM-T9 tested).

**Optics:** Accent light.

**Material:**  
Body: Die-cast aluminum  
Diffuser: Toughened glass.

**Mounting:** See mounting options on page 6.

**Electrical:** See remote power supply options on page 8.

**Finish:** White or Aluminum gray painted finish, following a double powder paint in 3 step process: surface treatment containing ceramic nano particles (Bonderite). Epoxy primer paint. Polyester powder paint with high resistance against UV rays and harsh weather conditions.

**Weight:** 0.88 to 2.65lbs (0.4 to 1.2kg)

**Warranty:** 5 year limited warranty.

**Ratings:** IP65, IK08

**Certification:** cULus listed for Wet location

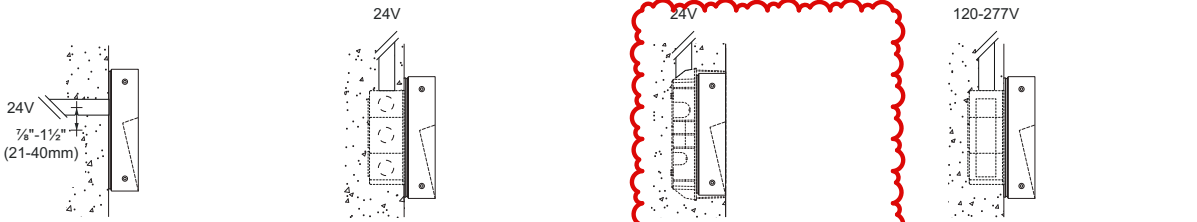
SKILL  
SURFACE MOUNTED - FLUSH MOUNTED

Type:  
Project:

SPECIFICATION SHEET  
Page: 5 of 5

Mounting Options

**Miniskill**  
(S.6250 - S.6270)



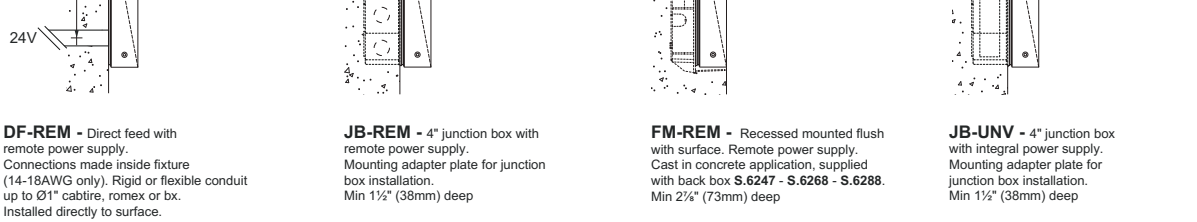
**DF-REM** - Direct feed with remote power supply. Connectors made inside fixture (18AWG only). Right or flexible conduit up to 8 1/2" (216mm) deep.

**JB-REM** - 4" junction box with remote power supply. Mounting adapter plate for junction box installation. Min 1 1/2" (38mm) deep.

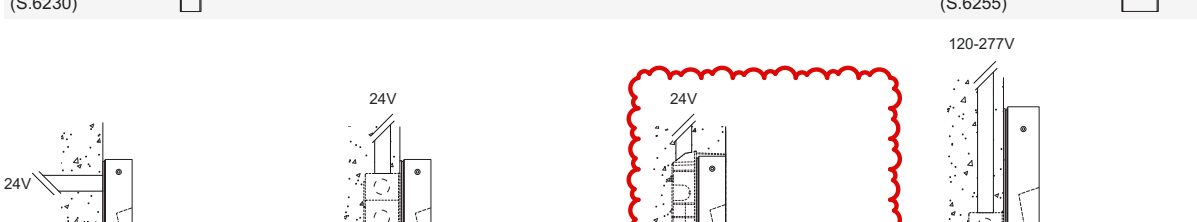
**FM-REM** - Recessed mounted flush with surface. Remote power supply. Cast in concrete application, supplied with back box S.6256 - S.6276. Min 2 1/2" (76mm) deep.

**JB-UNV** - 4" junction box with integral power supply. Mounting adapter plate for junction box installation. Min 1 1/2" (38mm) deep.

**SKILL**  
(S.6260 - S.6280 - S.6240)



**Miniskill Vertical**  
(S.6230)



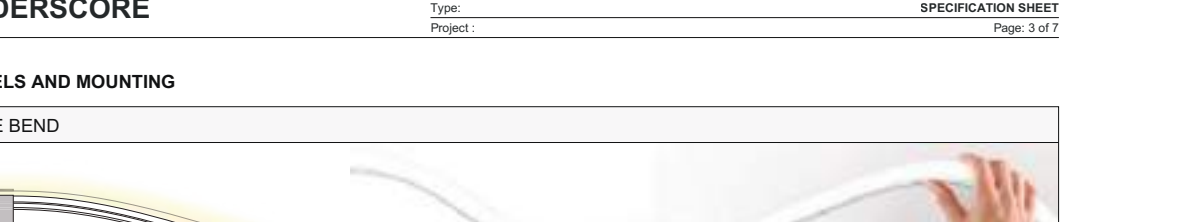
**DF-REM** - Direct feed with remote power supply. Connectors made inside fixture (18AWG only). Right or flexible conduit up to 8 1/2" (216mm) deep.

**JB-REM** - 4" junction box with remote power supply. Mounting adapter plate for junction box installation. Min 1 1/2" (38mm) deep.

**FM-REM** - Recessed mounted flush with surface. Remote power supply. Cast in concrete application, supplied with back box S.6256 - S.6276. Min 2 1/2" (76mm) deep.

**JB-UNV** - 4" junction box with integral power supply. Mounting adapter plate for junction box installation. Min 1 1/2" (38mm) deep.

**Skill Square Large**  
(S.6255)



**FW-REM** - Fixture whip to remote power supply. Installed directly to surface with mounting plate, exposed cable or electrical conduit.

**J2-REM** - 2" junction box with remote power supply. Mounting adapter plate for junction box installation. Min 1 1/2" (38mm) deep.

**FM-REM** - Recessed mounted flush with surface. Remote power supply. Cast in concrete application, supplied with back box S.6256 - S.6276. Min 2 1/2" (76mm) deep.

**JB-UNV** - 4" junction box with integral power supply. Mounting adapter plate for junction box installation. Min 1 1/2" (38mm) deep.

L-4

KEEN  
LED

SPECIFICATION SHEET  
Project name:  
Type:

PAGE: 1 OF 3



Wall mounted projector with a minimal design. Suitable for exterior applications.

**Luminaire characteristics:**  
Power input: 16 W  
Lumens: 950 to 1020 lm (delivered for 3000K)  
Luminaire efficacy: 59 lm/W to 63 lm/W (for 3000K)

**Source:** White LED, LM-80 tested.

**Lumen maintenance:** 70% of initial lumens at 50 000 hours (L70), (LM-T9 tested).

**Optics:** Available in narrow spot, narrow flood or elliptical optics. With rotation around 3 axis.

**Materials:**  
Body: Die-cast aluminum.  
Diffuser: Tempered glass.  
Hardware: Silicone gasket and stainless steel screws.  
Mounting plate: Aluminum.

**Electrical:** Universal high efficiency electronic power supply, rated at 50000 hours, 120-277V.

**Mounting:** Wall or ceiling mounted with 4" junction box or single gang switch box.

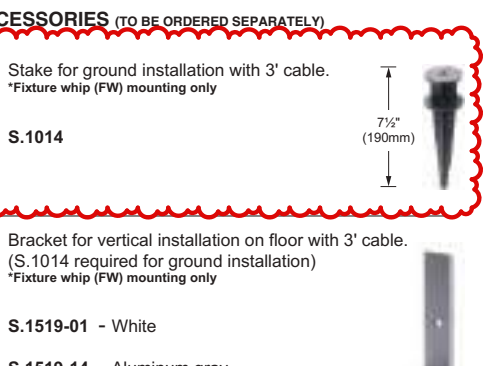
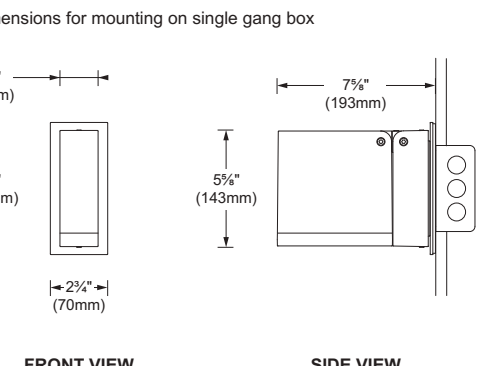
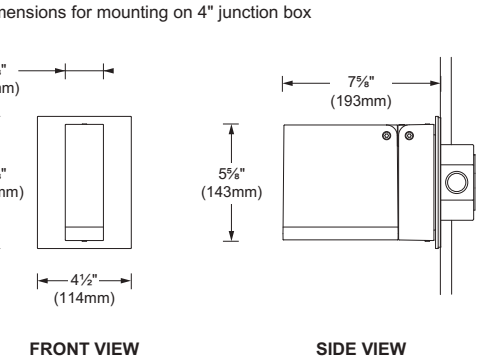
**Finish:** White or Aluminum gray.

**Weight:** 4.4lbs (2kg).

**Warranty:** 5 year limited warranty.

**Certification:** cULus listed for wet location.

**Ratings:** IP65, IK08



L-5

UNDERScore  
InOut

Type:  
Project:

SPECIFICATION SHEET  
Page: 1 of 2



**Luminaire characteristics:**

**Power input:** 2.8 or 3.5W/ft (Remote fixture only)

**Lumens:** 90 or 150lm (for 2500K, 90CRI)

**Luminaire efficacy:** 30 to 55lm/W

**Source:** White LED (LM-80 tested).

**Lumen maintenance:** See page 4 for details.

**Optics:** Underscore InOut can be used to create straight or curved lines on flat surfaces. Darkspot free lighting is guaranteed along the entire strip profile up to the end parts.

**Material:** Coextruded high performance polymer extrusion IP68 factory sealed assembly. Designed for extreme temperatures: -22°F to +113°F (-30°C to +45°C). The high performance polymer has been tested at 170°F (76°C) with glow wire without spitting of smoke. Integral stainless steel split system reducing mechanical stress and increase reliability.

**Mounting:** Universal surface mounted, using mounting accessories (included). Supplied with 3" (80mm) long cable with patented IP68 connection system. See mounting options on page 2 - 3.

**Electrical:** 24V remote electronic driver to be ordered separately (see options on page 7).

**Dimming:** See dimming options on page 7.

**Finish:** White polymer extrusion with milky finish. Extruded anodized aluminum or stainless steel mounting clips.

**Weight:** 0.26ft (0.1kg/ft)

**Warranty:** 5 year limited warranty.

**Ratings:** IP68, IK10

**Certification:** cULus listed for Wet location

UNDERScore  
InOut

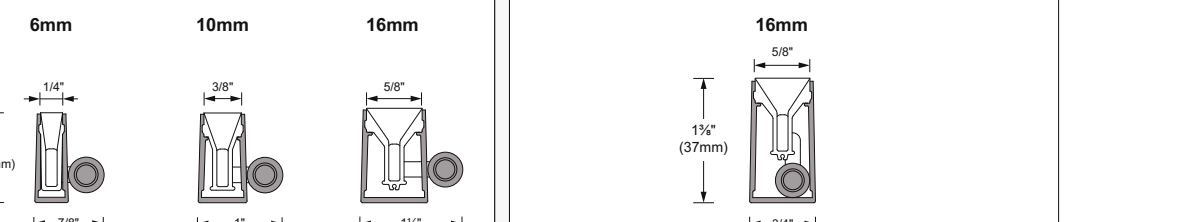
Type:  
Project:

SPECIFICATION SHEET  
Page: 1 of 2



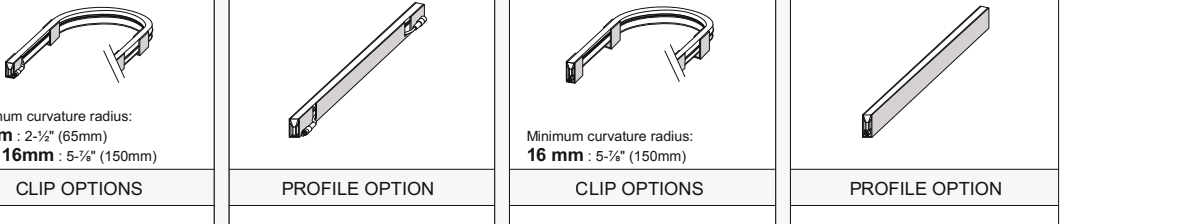
**LOW CLEARANCE MOUNTING - 1" HEIGHT WITH EXPOSED CONNECTORS**

**6mm** **10mm** **16mm**

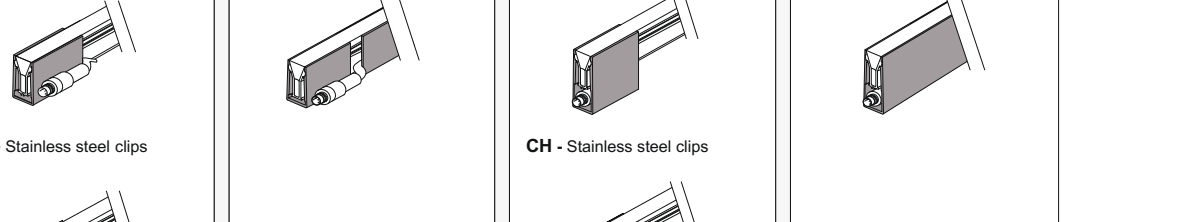


**HIGH PROFILE MOUNTING - 1 1/2" HEIGHT WITH HIDDEN CONNECTORS**

**16mm**



**CURVED INSTALLATION** **STRAIGHT INSTALLATION**



**CLIP OPTIONS** **PROFILE OPTION**

**AL** - Anodized Aluminum clips **LP** - Anodized Aluminum profile

**CL** - Stainless steel clips **CH** - Stainless steel clips

All mounting clip length: 1 1/2" (40mm)

All profile length follows module length. See page 5 for all available lengths.

Ld-01

Lighting Cut Sheets

Scale: NTS

09 Apr 14, 2025 Re-issued for DP  
08 Apr 07, 2025 Issued for Review  
07 Feb 19, 2025 Issued for Review  
06 Jan 30, 2025 Issued for BP  
05 Dec 20, 2024 Re-issued for DP  
04 Aug 23, 2024 Issued for BP Draft  
03 Aug 02, 2024 90% BP Draft  
02 July 12, 2024 Re-Issued for DP  
01 June 21, 2024 Issued for 60% BP

NO.: DATE: ITEM:

Revisions:

Durante Kreuk Ltd.  
102 - 1637 West 5th Avenue  
Vancouver BC V6J 1N5  
t: 604 684 4611  
f: 604 684 0577  
www.dkl.bc.ca

Project:

NEW ERA  
MAPLE RIDGE

MAPLE RIDGE, BC

Drawn by: MP

Checked by: DC

Date: April 14, 2025

Scale: As Noted

Drawing Title:

Landscape Details

Project No.:

21029

Sheet No.:

L-3.3





Land Title Act

**Charge**

General Instrument – Part 1

NEW WESTMINSTER LAND TITLE OFFICE

JUL 06 2021 10:43:47.001

**CA9164171-CA9164172**

## 1. Application

**KOFFMAN KALEF LLP**  
**19TH F, 885 W. GEORGIA ST**  
**VANCOUVER BC**  
**604-891-3619 (55288-1)**

## 2. Description of Land

PID/Plan Number	Legal Description
000-969-222	LOT 26 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 17845
000-969-231	LOT 27 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 17845
030-857-228	LOT 2 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN EPP88082
001-419-684	LOT 3 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 13752
001-886-134	LOT 4 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 14543
001-916-980	PARCEL "B" (EXPLANATORY PLAN 9471) OF LOT 2 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 1112
002-150-051	LOT 18 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 14549
004-031-768	LOT 24 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 15679
004-067-444	LOT 1 BLOCK 2 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 14543
004-681-088	LOT 3 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 14543
005-165-598	LOT 55 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 53750
006-217-796	LOT 50 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 48265
006-357-571	LOT 52 EXCEPT: PART DEDICATED ROAD ON PLAN 81647, DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 49034
008-643-831	LOT 10 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 20094
009-862-510	LOT 1 EXCEPT: THE EAST 47 FEET; DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 13752
009-862-544	LOT 2 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 13752
009-862-552	LOT 4 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 13752
009-966-552	LOT 2 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 14543
009-966-561	LOT 5 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 14543
001-115-341	LOT 9 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN 20094

## 3. Nature of Interest

Type	Number	Additional Information
<b>COVENANT</b>		
<b>PRIORITY AGREEMENT</b>		Granting the above Covenant priority over Mortgage CA8399563 and Assignment of Rents CA8399564

## 4. Terms

Part 2 of this instrument consists of:

**(b) Express Charge Terms Annexed as Part 2**



Land Title Act

**Charge**

General Instrument – Part 1

## 5. Transferor(s)

**487559 B.C. LTD., NO.BC1170338****BANK OF MONTREAL (AS TO PRIORITY)**

## 6. Transferee(s)

**CITY OF MAPLE RIDGE**  
11995 HANEY PLACE  
MAPLE RIDGE BC V2X 6A9

## 7. Additional or Modified Terms

## 8. Execution(s)

This instrument creates, assigns, modifies, enlarges, discharges or governs the priority of the interest(s) described in Item 3 and the Transferor(s) and every other signatory agree to be bound by this instrument, and acknowledge(s) receipt of a true copy of the filed standard charge terms, if any.

Witnessing Officer Signature

Execution Date

Transferor Signature(s)

\_\_\_\_\_  
**PATRICK J. JULIAN**  
**Barrister & Solicitor**  
19TH FLOOR - 885 WEST GEORGIA  
STREET  
VANCOUVER BC V6C 3H4

YYYY-MM-DD

**2021-06-18****487559 B.C. LTD.**

By their Authorized Signatory

\_\_\_\_\_  
**TOM MEIER**

**Officer Certification**

Your signature constitutes a representation that you are a solicitor, notary public or other person authorized by the *Evidence Act*, R.S.B.C. 1996, c.124, to take affidavits for use in British Columbia and certifies the matters set out in Part 5 of the *Land Title Act* as they pertain to the execution of this instrument.

Witnessing Officer Signature

Execution Date

Transferor Signature(s)

\_\_\_\_\_  
**CATHERINE A. SCHMIDT**  
**Commissioner for Taking Affidavits**  
**for British Columbia**  
11995 Haney Place  
Maple Ridge BC V2X 6A9

YYYY-MM-DD

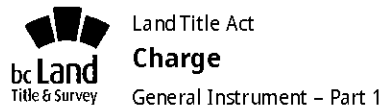
**2021-07-05****CITY OF MAPLE RIDGE**

By their Authorized Signatory

\_\_\_\_\_  
**MICHAEL MORDEN, MAYOR**

Expires: January 31, 2022  
(as to both signatures)





STEPHANIE NICHOLS, CORPORATE  
OFFICER

**Officer Certification**  
Your signature constitutes a representation that you are a solicitor, notary public or other person authorized by the *Evidence Act*, R.S.B.C. 1996, c.124, to take affidavits for use in British Columbia and certifies the matters set out in Part 5 of the *Land Title Act* as they pertain to the execution of this instrument.

Witnessing Officer Signature	Execution Date	Transferor Signature(s)
<div>KENNETH WEI JAE BONG Commissioner for Taking Affidavits for British Columbia 6th Floor - 595 Burrard Street Vancouver BC V7X 1L5</div> <div>My Commission expires February 28, 2023</div>	<div>YYYY-MM-DD</div> <div>2021-06-21</div>	<div>BANK OF MONTREAL By their Authorized Signatory</div> <div>Greg Vriend, Managing Director, Real Estate Finance</div>

**Officer Certification**  
Your signature constitutes a representation that you are a solicitor, notary public or other person authorized by the *Evidence Act*, R.S.B.C. 1996, c.124, to take affidavits for use in British Columbia and certifies the matters set out in Part 5 of the *Land Title Act* as they pertain to the execution of this instrument.

<div><b>Electronic Signature</b> Your electronic signature is a representation that you are a designate authorized to certify this document under section 168.4 of the <i>Land Title Act</i>, RSBC 1996 c.250, that you certify this document under section 168.41(4) of the act, and that an execution copy, or a true copy of that execution copy, is in your possession.</div>	<div>Daniel Solomon Remick 1NQULH</div> <div>Digitally signed by Daniel Solomon Remick 1NQULH Date: 2021-07-05 17:16:22 -07:00</div>
---	--

**TERMS OF INSTRUMENT – PART 2****SECTION 219 COVENANT – NO BUILD AND FUTURE SERVICING (PHASES 2 TO 7)****2017-061-RZ**

THIS AGREEMENT dated for reference the \_\_\_\_\_ day of June, 2021 is

BETWEEN:

**487559 B.C. LTD.**  
22367 Dewdney Trunk Road  
Maple Ridge, British Columbia, V2J 3J4

(the “Covenantor”)

AND:

**CITY OF MAPLE RIDGE**  
11995 Haney Place  
Maple Ridge, British Columbia, V2X 6A9

(the “City”)

WHEREAS:

A. The Covenantor is the registered owner in fee simple of certain lands in the City of Maple Ridge, British Columbia which are legally described as:

1. Parcel Identifier: 000-969-222, Lot 26, District Lot 399, Group 1, New Westminster District, Plan 17845;
2. Parcel Identifier: 000-969-231, Lot 27, District Lot 399, Group 1, New Westminster District, Plan 17845;
3. Parcel Identifier: 030-857-228 LOT 2 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN EPP88082;
4. Parcel Identifier: 001-419-684, Lot 3, District Lot 399, Group 1, New Westminster District, Plan 13752;
5. Parcel Identifier: 001-886-134, Lot 4, District Lot 399, Group 1, New Westminster District, Plan 14543;
6. Parcel Identifier: 001-916-980, Parcel “B” (Explanatory Plan 9471) of Lot 2, District Lot 399, Group 1, New Westminster District, Plan 1112;
7. Parcel Identifier: 002-150-051, Lot 18, District Lot 399, Group 1, New Westminster District, Plan 14549;
8. Parcel Identifier: 004-031-768, Lot 24, District Lot 399, Group 1, New Westminster District, Plan 15679;



9. Parcel Identifier: 004-067-444, Lot 1, Block 2, District Lot 399, Group 1, New Westminster District, Plan 14543;
10. Parcel Identifier: 004-681-088, Lot 3, District Lot 399, Group 1, New Westminster District, Plan 14543;
11. Parcel Identifier: 005-165-598, Lot 55, District Lot 399, Group 1, New Westminster District, Plan 53750;
12. Parcel Identifier: 006-217-796, Lot 50, District Lot 399, Group 1, New Westminster District, Plan 48265;
13. Parcel Identifier: 006-357-571, Lot 52 Except: Part Dedicated Road on Plan 81647, District Lot 399, Group 1, New Westminster District, Plan 49034;
14. Parcel Identifier: 008-643-831, Lot 10, District Lot 399, Group 1, New Westminster District, Plan 20094;
15. Parcel Identifier: 009-862-510, Lot 1 Except: The East 47 Feet; District Lot 399, Group 1, New Westminster District, Plan 13752;
16. Parcel Identifier: 009-862-544, Lot 2, District Lot 399, Group 1, New Westminster District, Plan 13752;
17. Parcel Identifier: 009-862-552, Lot 4, District Lot 399, Group 1, New Westminster District, Plan 13752;
18. Parcel Identifier: 009-966-552, Lot 2, District Lot 399, Group 1, New Westminster District, Plan 14543;
19. Parcel Identifier: 009-966-561, Lot 5, District Lot 399, Group 1, New Westminster District, Plan 14543; and
20. Parcel Identifier 001-115-341, Lot 9 District Lot 399 Group 1 New Westminster District Plan 20094.

(the “Lands”).

- B. The Covenantor wishes to construct a certain number of residential dwelling units, townhouses, street oriented units and some mixed use commercial and rental residential apartment buildings on the Lands in seven phases (each referred to herein as a “Phase”) as generally shown on the sketch plan attached hereto as Schedule “A”, and in accordance with the development concept generally shown in the concept plan attached hereto as Schedule “B”, and for that purpose the Covenantor has applied to the City to rezone the Lands as set out in Maple Ridge Zone Amending Bylaw No. 7336-2017 (the “Zone Amending Bylaw”).
- C. The Covenantor has requested that, given its desire to develop the Lands in Phases, the City defer consideration of a number of development requirements for the Lands that under the City’s usual planning practices would be addressed prior to the adoption of the Zone Amending Bylaw, and the Covenantor has offered to enter into this Covenant pursuant to

Section 219 of the *Land Title Act* to provide the City with the authority to impose conditions for the development of the Lands that are necessary for the orderly development of each Phase. Without limiting the foregoing, the proposed development is expected to generate volumes of vehicle traffic that will result in the need for the widening, realignment, improvement, and in some cases the closure of roads in the vicinity of the Lands, and the Covenantor has agreed that the City may impose requirements for such matters as a condition of the development of the Lands.

- D. Section 219 of the *Land Title Act* provides, inter alia, that a covenant, whether of a negative or positive nature, may be registered as a charge against the title to the land, in favour of the City and that the covenant is enforceable against the Covenantor and the successors in title of the Covenantor.
- E. A covenant under Section 219 of the *Land Title Act* may include provisions in respect of the use of land, the use of a building on or to be erected on lands; that land is to be built on in accordance with the covenant, is not to be built on or that certain buildings are not to be used except in accordance with the provisions of the covenant or; that land is not to be subdivided unless in accordance with the covenant or is not to be subdivided at all.
- F. The Covenantor agrees to the restrictions in the uses of the Lands, the density of building on the Lands and the use and occupancy of all improvements on the Lands on the terms and conditions herein provided for.

NOW THEREFORE THIS AGREEMENT WITNESSETH THAT pursuant to Section 219 of the *Land Title Act*, and in consideration of the premises, the mutual covenants and agreements contained herein and other good and valuable consideration and the sum of One Dollar (\$1.00) now paid by the City to the Covenantor (the receipt and sufficiency whereof is hereby acknowledged), the parties hereto covenant and agree that the Lands shall not be used or built on except in accordance with this Covenant as follows:

- 1. The Covenantor covenants and agrees with the City that:
  - (a) in this Agreement, including its Schedules:
    - (i) “development” includes subdivision of the Lands, the construction, alteration or addition to any buildings or other structures on the Lands, the construction and installation of on-site and off-site services [including public roads] that are required for the development and use of the Lands, the installation of landscaping improvements, and the alteration of the Lands in any way including through the deposit or removal of soil, the removal of trees, or through any excavation or regrading of the Lands, and “develop” means to do any of the foregoing, but “development” and “develop” do not include the demolition of any building existing on the Lands as of the date this Agreement is registered in the Land Title Office;
    - (ii) “development requirements” means the requirements and conditions for the development of the Lands or a portion of the Lands as determined by the City under this Agreement, including but not limited to requirements for on-site and off-site services, road widening and realignment (including closures of roads within or adjoining the Lands), road dedication, intersection and traffic safety improvements including traffic signals, the



provision of public amenities and public art, approval of the form and character of the development, and further covenants under Section 219 of the *Land Title Act* as may be required to address matters such as geotechnical issues and requirements for stormwater management and visitor parking, and also including the specific requirements for each Phase that are identified in Part 2 of Schedule “D”. Without limiting the generality of the foregoing, the development requirements may include the road and traffic safety mitigation measures referred to in Schedule “E” to this Agreement, and such other road and traffic safety mitigation measures as are recommended in a traffic impact assessment submitted pursuant to Schedule “D”, as accepted by the City.

- (b) the Covenantor will not develop the Lands except as permitted under the terms of this Agreement, or as otherwise permitted by the City through its rezoning, development permit, and building permit processes;
- (c) without limiting the generality of section 1(b), the Covenantor will not develop the Lands except in accordance with the ERA Design Guidelines that are attached to and form part of this Agreement as Schedule “F” (the “ERA Design Guidelines”);
- (d) the sketch plan attached hereto as Schedule “A” sets out generally the proposed phasing of the development on the Lands and the concept plan attached hereto as Schedule “B” sets out generally the layout of the proposed development on the Lands;
- (e) there will be no development of any of the Lands within a Phase until the Covenantor has paid to the City the Community Amenity Contribution (the “CAC”) agreed to by the Covenantor for that Phase, in the amount set out in Schedule “C” attached hereto (the “CAC Payment”), being monies necessary to offset demand for community, cultural, and other social and municipal services imposed by the size of the residential, commercial and community development contemplated by the Covenantor;
- (f) the Covenantor hereby grants to the City a perpetual rent charge (the “Rent Charge”) against the Land, ranking prior to all other financial charges and encumbrances, as security for the Covenantor’s obligation to make each CAC Payment under section 1(e) of this Agreement, and the Rent Charge is granted both as a rent charge pursuant to section 219(6)(b) of the *Land Title Act* as an integral part of the statutory covenant created by this Agreement, and as a fee simple rent charge at common law;
- (g) the Covenantor will not commence any development on any of the Lands within a Phase until the Covenantor has submitted to the City all of the applications, plans, specifications, drawings, reports and other information required under Part 1 of Schedule “D” to this Agreement, and has fulfilled all other requirements of Part 1 of Schedule “D” in relation to the application for approval of that Phase, to the City’s satisfaction;
- (h) no development on any part of the Lands within a Phase shall be permitted, and the City shall not be required to issue a building permit for any construction within that Phase, until and unless:

- (i) the City in its discretion determines and approves all development requirements for that Phase, including, without limitation, the approval requirements set out in Schedule “D” attached hereto;
    - (ii) the City issues a Development Permit for the Lands within that Phase;
    - (iii) the Covenantor fulfills all pre-conditions for subdivision of the Lands within or comprising that Phase, including but not limited to the deposit of fees and securities, as imposed by the Approving Officer for the City;
    - (iv) the Covenantor fulfills all additional preconditions for issuance of a building permit as directed by the City following its review and approval of the matters referred to in Schedule “D”, including but not limited to the registration of any additional section 219 covenants that are required to secure one or more development requirements, the registration of one or more statutory rights of way required within that Phase, and the dedication of any parts of the Lands as may be required to re-align or widen a City street in order to implement the recommendations in the traffic impact assessment report and related plans and drawings submitted to and approved by the City for that Phase.
  - (i) the Covenantor shall, at its sole cost, fulfill all development requirements determined and approved by the City in writing for that Phase, following completion of the Phase approval process described in Part 1 of Schedule “D”.
2. The Covenantor hereby covenants and agrees with the City that except as provided in this Agreement, the Covenantor shall not sell or otherwise transfer any of the parcels separately, such that the fee simple title to the parcels will remain registered in the name of a single owner during the Term.
3. As an exception to the restrictions on sale under section 2, the Covenantor may:
- (a) sell or transfer all of the parcels within a Phase to a purchaser or assignee for the purpose of developing the Lands within that Phase, provided that:
    - (i) the registered and beneficial owners of the Lands within that Phase collectively appoint a single person to act as their agent for the purpose of seeking approval from the City for the development of the Lands within that Phase; and
    - (ii) all of the Lands within that Phase are the subject of a single application for development approval under this Agreement;
  - (b) sell or transfer parcels within a Phase upon issuance of one or more occupancy permits for the buildings constructed within that Phase.
4. The City acknowledges and agrees that, notwithstanding the Phase numbering as shown on the plan attached hereto as Schedule “A”, the Covenantor may, in its discretion, proceed with the development of each Phase, or combination of Phases, on the Lands in whatever order it may determine, but otherwise subject to:



- (a) all of the terms and conditions of this Agreement, including without limitation payment of CAC's to the City prior to the development of the Lands relevant to each applicable Phase, as set out in Schedule "C" attached hereto;
  - (b) the right of the City, as a condition of the approval of the development of a Phase, to modify the Phase approval requirements under Schedule "D" by requiring that development requirements for a different Phase be fulfilled earlier than otherwise anticipated, in order to ensure that the development requirements for the Lands are fulfilled in an orderly manner, and that the requirements of the Zone Amending Bylaw are fulfilled.
- 5. The City agrees to execute a partial release of this Agreement from the title to the Lands within a Phase, once the development of the Lands within that Phase has been completed in accordance with this Agreement, releasing and discharging the Covenantor from its obligations under Sections 1(e), (f), (g), (h) and (i) of this Agreement to the extent those obligations apply to the development of that Phase. For certainty, the obligation of the Covenantor under section 1(c), and the ERA Guidelines, shall remain registered on title to the Lands for a period of twenty years following completion of the development of the final Phase to be developed, and shall apply to any alteration or redevelopment of the Lands within that twenty year period.
- 6. The City and the Covenantor each acknowledge and agree that the Director of Planning has the authority to approve minor changes and/or additions to the Phase requirements as set out in Schedule "D", as required to ensure that the development requirements for the Lands are consistent with the Zone Amending Bylaw and the ERA Guidelines.
- 7. IT IS MUTUALLY UNDERSTOOD, agreed and declared by and between the parties hereto that:
  - (a) nothing contained or implied herein shall in any way restrict or abrogate and shall not be deemed to restrict or abrogate, the rights and powers of the City in the exercise of its functions under any public and private statutes, by-laws, orders and regulations, in its absolute discretion, and in accordance with its lawful powers and duties;
  - (b) the discretion provided to the City under this Agreement to determine and approve the development requirements for a Phase is to be interpreted broadly, and in a manner that is consistent with the discretion the City would ordinarily have when considering an application for an amendment to the City's zoning bylaw, and the scope of the development requirements the City may consider necessary for the development of a Phase is not necessarily limited to the matters expressly referred to in this Agreement;
  - (c) the burden of the covenants herein provided for shall run with the Lands and will be personal and binding upon the Covenantor during the Covenantor's seisen of or ownership of any interest in the Lands;
  - (d) notwithstanding anything to the contrary, the Covenantor shall not be liable under any breach of any covenants and agreements contained herein after the Covenantor ceases to have any further interest in the Lands;

- (e) the Covenantor will deliver, after execution hereof, this Agreement to the City in a form acceptable as a Section 219 Covenant and concurrently such instruments of priority as may be necessary to give this Agreement priority over all financial charges and encumbrances which may have been registered against the title to the Lands at the time of submitting this Agreement for registration in the applicable Land Title Office, save and except those specifically approved in writing by the City or in favour of the City;
- (f) the fee simple estate in and to the Lands will not pass or vest in the City under or by virtue of these presents and the Covenantor may fully use and enjoy the Lands except only for the requirements provided for in this Agreement;
- (g) the Covenantor and its successors and assigns shall at all times indemnify and save harmless the City from and against all claims, demands, actions, suits, loss, costs, fines, penalties, charges, damages and expenses including legal fees and litigation expenses whatsoever which the City may incur, suffer or be put to arising out of or in connection with any breach of any covenant or agreement on the part of the Covenantor contained in this Agreement;
- (h) the covenants and agreements on the part of the Covenantor and herein provided for have been made by the Covenantor as contractual obligations as well as having been made pursuant to Section 219 of the *Land Title Act* RSBC 1996, c. 250 and as such will be binding on the Covenantor;
- (i) nothing herein provided for shall be deemed to constitute waivers of any lawful requirements with which the Covenantor would otherwise be obligated to comply with;
- (j) nothing contained or implied in this Agreement shall prejudice or affect the rights and powers of the City in the exercise of its functions under any public or private statutes, bylaws, orders and regulations, all of which may be fully and effectively exercised in relation to the Lands as if the Agreement had not been executed and delivered by the Covenantor;
- (k) no amendment of, addition to, or discharge of this Agreement shall be binding upon the parties hereto unless it is in writing and executed by the parties hereto;
- (l) if any provision provided for in this Agreement is for any reason held to be invalid, illegal, or unenforceable in any respect, such invalidity, illegality, or unenforceability will not affect any other provision of this Agreement which shall be construed as if such invalid, illegal, or unenforceable provisions had never been contained therein and such other provisions shall be enforceable to the fullest extent permitted by law;
- (m) the City, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies, including specific performance, injunction and/or declaratory relief, to enforce its rights under this Agreement;
- (n) the Covenantor shall pay for the preparation and registration, if applicable, of this Agreement together with any concurrent instruments of priority as herein provided for and any amendment, addition or discharge thereof;

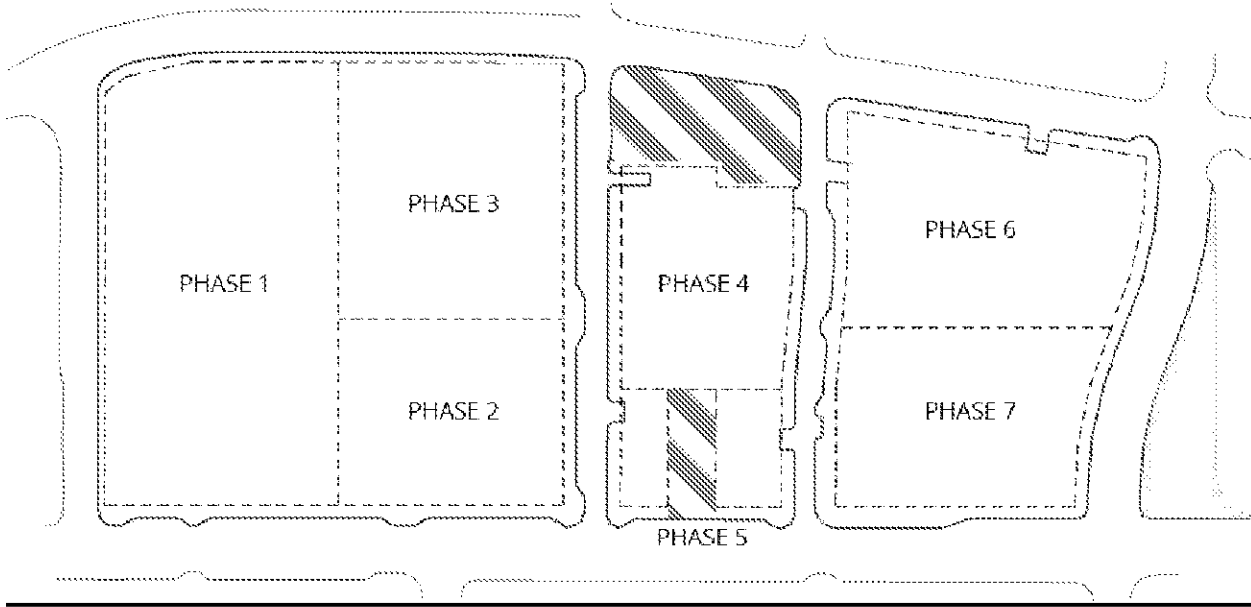


- (o) wherever the singular, masculine or neuter is used herein, the same shall be construed as meaning the plural, feminine or the body corporate or politic according to the context in which it is used;
- (p) the parties hereto shall do and cause to be done all things and execute and cause to be executed all documents which may be necessary to give proper effect to the intention of this Agreement; and
- (q) this Agreement shall enure to the benefit of and be binding upon the Covenantor, the City and their respective successors and assigns.

IN WITNESS WHEREOF the parties hereto have executed this Agreement as of the day, month and year first above written.

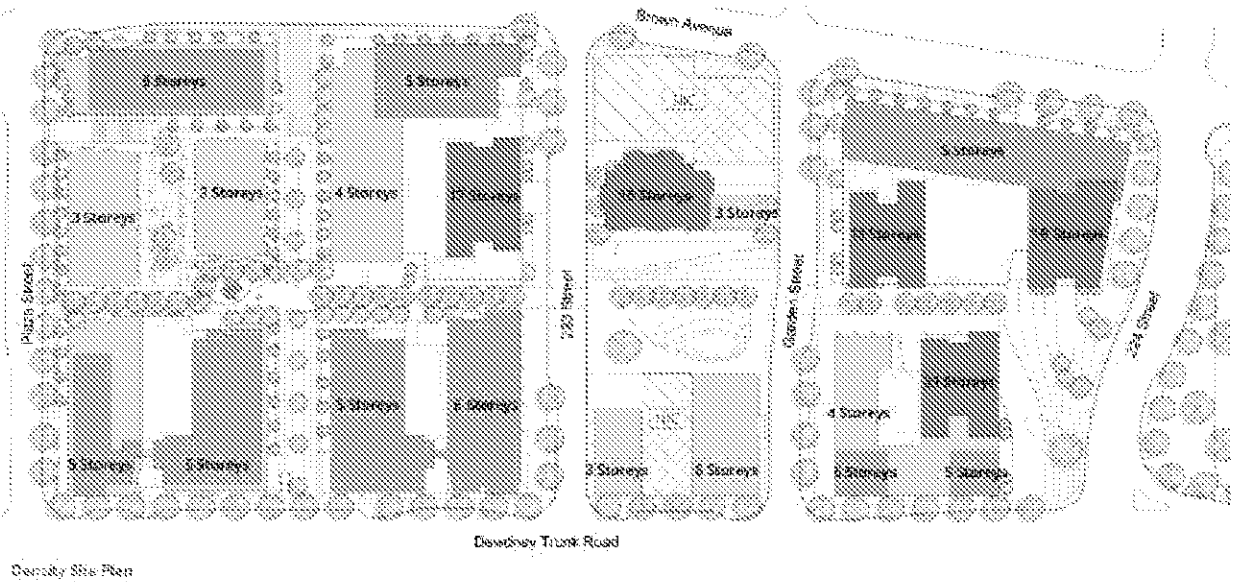
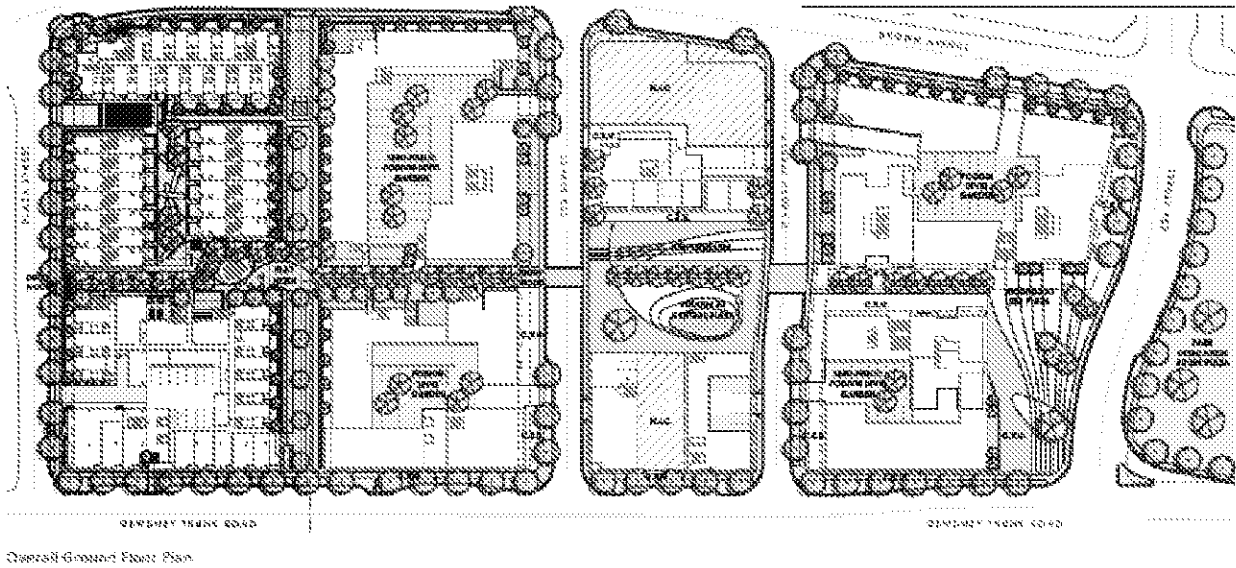
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**SCHEDULE “A”**  
**SKETCH PLAN**





SCHEDULE “B”



SCHEDULE "C"

COMMUNITY AMENITY CONTRIBUTIONS

Phase 1:	NIL
Phase 2:	\$270,349.00
Phase 3:	\$582,537.00
Phase 4:	\$212,417.00
Phase 5:	N/A (commercial building)
Phase 6:	\$749,896.00
Phase 7:	<u>\$489,202.00</u>
TOTAL:	<u>\$2,304,001.00</u>



## SCHEDULE “D”

### PHASE APPROVAL REQUIREMENTS

#### **Part 1 - Approval Requirements for all Phases:**

1. In this Schedule “D”:
  - a) “Adaptable Dwelling” means a housing unit that is constructed with the adaptable housing features described in section 3.8.5 of the British Columbia Building Code;
  - b) “Coordinating Professional” means a professional architect or engineer, licensed to practice in British Columbia, who is retained by the Covenantor to coordinate the design and development of a Phase, and the submission to the City of all applications, plans, information and documents required for the City’s approval of the development of that Phase;
  - c) “Development Permit” means a development permit issued pursuant to section 490 of the *Local Government Act*, SBC 2015 c. 1, as amended or replaced from time to time, authorizing the development of the Lands or any part of the Lands;
  - d) “east-west pedestrian way” means the east-west pedestrian corridor to be provided by the Covenantor through the Lands and that is described in the ERA Guidelines;
  - e) “OCP Guidelines” means the development permit guidelines for development of the Lands under the City of Maple Ridge Official Community Plan Bylaw No. 7060-2014, as amended or replaced from time to time;
  - f) “Zoning Bylaw” means the City of Maple Ridge Zoning Bylaw No. 7600-2019, as amended or replaced from time to time, as it applies to the Lands.
2. The Covenantor must do all of the following before undertaking any development of the Lands within a Phase:
  - a) designate the Coordinating Professional for that Phase, and advise the City in writing of that designation;
  - b) fulfill all of the application submission and review requirements of the ERA Guidelines for that Phase;
  - c) prepare detailed plans for the development of that Phase, demonstrating that the proposed development is consistent with the Zoning Bylaw, the ERA Design Guidelines, and the OCP Guidelines, and submit those plans to the City’s Director of Planning along with a preliminary analysis of compliance of the proposed development with respect to the Zoning Bylaw, the OCP Guidelines,

the ERA Design Guidelines, and with all other applicable City bylaws and policies, prior to scheduling one or more pre-application meetings with City staff;

- d) include with the submission referred to in section 2(c) the following, as required by the City:
  - i. a traffic impact assessment prepared by a professional engineer, that updates the November 5, 2020 traffic impact assessment prepared by Bunt & Associates Engineering Ltd., a copy of which is on file with the City's Engineering Department, and that describes in detail the road widening and realignment (including closures of roads within or adjoining the Lands), road dedication, intersection and traffic safety improvements including traffic signals, and other traffic mitigation measures recommended for that Phase, including but not limited to those measures specifically contemplated under Schedule "E" to this Agreement;
  - ii. a site servicing study prepared by a professional engineer that describes in detail the on-site and off-site services required for that Phase, including but not limited to upgrades or enhancements to City sanitary sewer, stormwater and potable water infrastructure;
  - iii. surveys and plans showing all proposed public access areas, including statutory rights of way for the east-west pedestrian way, and areas to be dedicated as public roads;
  - iv. a market analysis demonstrating whether there is a market-based demand for Adaptable Units within that Phase, and a proposal for the number of Adaptable Units that will be provided;
  - v. details of all amenities to be provided, or that have already been provided, that are required under the Zoning Bylaw as a condition of an increase in density within that Phase; and
  - vi. all other information, plans, specifications and other material required under the ERA Design Guidelines;
- e) schedule and convene a pre-application meeting with City staff to discuss compliance of the submission with the Zoning Bylaw, the OCP Guidelines, and the ERA Design Guidelines, as well as City requirements for:
  - i. on-site and off-site services required for the Phase;
  - ii. road re-alignment, road widening, and road and traffic safety improvements required for the Phase;
  - iii. statutory right of way securing public access and use of the east-west landscaped pedestrian way;
  - iv. statutory rights of way for City infrastructure;
  - v. further covenants under section 219 of the Land Title Act, to address geotechnical issues, and requirements for visitor parking and stormwater management; and



- vi. the Phase-specific requirements described in Part 2 of this Schedule “D”;
- f) submit an application for a Development Permit to the City along with all supporting plans and documents as required by the City under the Maple Ridge Development Procedures Bylaw No. 5879-1999, as amended or replaced from time to time, and payment of all applicable application fees;
- g) following the conclusion of pre-application meetings with City staff, arrange for the preparation and submission to the City of any amendments or revisions to the materials submitted under sections 1(b), (c), and (d), and amendments to the Development Permit application, that are required as a result of the City’s preliminary evaluation of the development proposal;
- h) hold a Development Information Meeting, in accordance with City policy, and make a submission to the City’s Advisory Design Panel, and then amend and revise the submission as necessary to resolve any identified issues and concerns arising from the Development Information Meeting and the comments of the Advisory Design Panel;
- i) submit a subdivision application to create the legal parcel for the applicable Phase, and satisfy all requirements established by the Approving Officer in a Preliminary Review Letter for final approval of the subdivision;
- j) prepare for the approval of the City and the Approving Officer all necessary legal documents required by the City to secure the development requirements for that Phase including statutory rights-of-way for pedestrian access, further section 219 covenants for provision of and construction of roads and City services, a geotechnical covenant, a visitor parking covenant, a storm water management covenant, mutual access easements, and housing agreements, as may be applicable;
- k) enter into such agreements, covenants or rights of way required by the City to secure all requirements for road dedication, road widening, road relocation and traffic safety improvements, as per the traffic impact assessment report and related plans and drawings submitted to and approved by the City for that Phase.

**Part 2 – Phase Specific Requirements:**

**Phase 2**

The Covenantor must provide the following within Phase 2:

- At least four electric vehicle charging stations that are fully equipped and capable of providing a minimum of a Level 2 electric vehicle charging level (as defined by SAE International’s J1772 standard), and that meet the minimum standards

prescribed under the City's Off-Street Parking and Loading Bylaw No. 4350 – 1990, as amended or replaced from time to time.

- Construction in compliance with any BC Energy Step Code requirements that may be adopted by the City of Maple Ridge with sustainable elements such as low-flow fixtures, rainwater management, heat recovery ventilators and low VOC material specifications.
- Sustainability measures.
- Continuation of the east-west landscaped pedestrian way with registration of a section 218 statutory right of way and section 219 covenant and rent charge providing for public access to and use of the pedestrian way, and access to and use of other public amenities to be provided and included as a part of or connected to the pedestrian way, while allowing access to commercial uses within Phase 2, and including an agreement for maintenance by the owner, and authorizing the City to undertake such maintenance and to charge back the owner for its costs in the event the owner defaults. The agreement will be on similar terms and conditions as Statutory Right of Way CA8389661, Section 219 covenant CA8389662, and Rent Charge CA8389663 but subject to any modified terms and conditions as may be required by the design of Phase 2, the uses and amenities within Phase 2, as approved by the City, and the requirements and conditions for public access to and use of the public amenities, as well as access to any commercial uses within Phase 2, all as determined through the development approval process established under this Agreement.

### **Phase 3**

The Covenantor must provide the following within Phase 3:

- At least four electric vehicle charging stations that are fully equipped and capable of providing a minimum of a Level 2 electric vehicle charging level (as defined by SAE International's J1772 standard), and that meet the minimum standards prescribed under the City's Off-Street Parking and Loading Bylaw No. 4350 – 1990, as amended or replaced from time to time.
- Construction in compliance with any BC Energy Step Code requirements that may be adopted by the City of Maple Ridge with sustainable elements such as low-flow fixtures, rainwater management, heat recovery ventilators and low VOC material specifications.
- Sustainability measures.
- Continuation of the east-west landscaped pedestrian way with registration of a section 218 statutory right of way and section 219 covenant and rent charge providing for public access to and use of the pedestrian way, and access to and use of other public amenities to be provided and included as a part of or connected to the pedestrian way, while allowing access to commercial uses within Phase 3, and including an agreement for maintenance by the owner, and authorizing the City to undertake such maintenance and to charge back the owner for its costs in the event the owner defaults. The agreement will be on similar terms and conditions as Statutory Right of Way CA8389661, Section 219 covenant CA8389662, and Rent Charge CA8389663 but subject to any modified terms and conditions as may be



required by the design of Phase 3, the uses and amenities within Phase 3, as approved by the City, and the requirements and conditions for public access to and use of the public amenities, as well as access to any commercial uses within Phase 3, all as determined through the development approval process established under this Agreement.

#### **Phase 4**

The Covenantor must provide the following within Phase 4:

- At least four electric vehicle charging stations that are fully equipped and capable of providing a minimum of a Level 2 electric vehicle charging level (as defined by SAE International's J1772 standard), and that meet the minimum standards prescribed under the City's Off-Street Parking and Loading Bylaw No. 4350 – 1990, as amended or replaced from time to time.
- Seating area and theatre structure at the Central Plaza incorporating potential public art elements.
- A Clubhouse, if not provided in another Phase.
- Sustainability measures.
- Construction in compliance with any BC Energy Step Code that may be adopted by the City of Maple Ridge with sustainable elements such as low-flow fixtures, rainwater management, heat recovery ventilators and low VOC material specifications.
- Continuation of the east-west landscaped pedestrian way with registration of a section 218 statutory right of way and section 219 covenant and rent charge providing for public access to and use of the pedestrian way, and access to and use of other public amenities to be provided and included as a part of or connected to the pedestrian way such as the seating area and theatre structure, while allowing access to commercial uses within Phase 4, and including an agreement for maintenance by the owner, and authorizing the City to undertake such maintenance and to charge back the owner for its costs in the event the owner defaults. The agreement will be on similar terms and conditions as Statutory Right of Way CA8389661, Section 219 covenant CA8389662, and Rent Charge CA8389663 but subject to any modified terms and conditions as may be required by the design of Phase 4, the uses and amenities within Phase 4, as approved by the City, and the requirements and conditions for public access to and use of the public amenities, as well as access to any commercial uses within Phase 4, all as determined through the development approval process established under this Agreement.

#### **Phase 5**

The Covenantor must provide the following within Phase 5:

- At least four electric vehicle charging stations that are fully equipped and capable of providing a minimum of a Level 2 electric vehicle charging level (as defined by SAE International's J1772 standard), and that meet the minimum standards prescribed under the City's Off-Street Parking and Loading Bylaw No. 4350 – 1990, as amended or replaced from time to time.
- Sustainability measures.
- A Clubhouse, if not provided in another Phase.
- Construction in compliance with any BC Energy Step Code requirements that may be adopted by the City of Maple Ridge with sustainable elements such as low-flow fixtures, rainwater management, heat recovery ventilators and low VOC material specifications.
- To the extent that the east-west landscaped pedestrian way crosses over or abuts Phase 5, continuation of the east-west landscaped pedestrian way with registration of a section 218 statutory right of way and section 219 covenant and rent charge providing for public access to and use of the pedestrian way, and access to and use of other public amenities to be provided and included as a part of or connected to the pedestrian way, while allowing access to commercial uses within Phase 5, and including an agreement for maintenance by the owner, and authorizing the City to undertake such maintenance and to charge back the owner for its costs in the event the owner defaults. The agreement will be on similar terms and conditions as Statutory Right of Way CA8389661, Section 219 covenant CA8389662, and Rent Charge CA8389663 but subject to any modified terms and conditions as may be required by the design of Phase 5, the uses and amenities within Phase 5, as approved by the City, and the requirements and conditions for public access to and use of the public amenities, as well as access to any commercial uses within Phase 5, all as determined through the development approval process established under this Agreement..

## **Phase 6**

The Covenantor must provide the following within Phase 6:

- At least four electric vehicle charging stations that are fully equipped and capable of providing a minimum of a Level 2 electric vehicle charging level (as defined by SAE International's J1772 standard), and that meet the minimum standards prescribed under the City's Off-Street Parking and Loading Bylaw No. 4350 – 1990, as amended or replaced from time to time.



- Construction in compliance with any BC Energy Step Code requirements that may be adopted by the City of Maple Ridge with sustainable elements such as low-flow fixtures, rainwater management, heat recovery ventilators and low VOC material specifications.
- Public plaza and other public areas referred to in the ERA Design Guidelines.
- Continuation of the east-west landscaped pedestrian way with registration of a section 218 statutory right of way and section 219 covenant and rent charge providing for public access to and use of the pedestrian way, and access to and use of other public amenities to be provided and included as a part of or connected to the pedestrian way such as the public plaza, while allowing access to commercial uses within Phase 6, and including an agreement for maintenance by the owner, and authorizing the City to undertake such maintenance and to charge back the owner for its costs in the event the owner defaults. The agreement will be on similar terms and conditions as Statutory Right of Way CA8389661, Section 219 covenant CA8389662, and Rent Charge CA8389663 but subject to any modified terms and conditions as may be required by the design of Phase 6, the uses and amenities within Phase 6, as approved by the City, and the requirements and conditions for public access to and use of the public amenities, as well as access to any commercial uses within Phase 6, all as determined through the development approval process established under this Agreement.
- Sustainability measures.

### **Phase 7**

The Covenantor must provide the following within Phase 7:

- Public plaza and associated amenities referred to in the ERA Design Guidelines.
- Public message board.
- Public art piece having a minimum value of \$100,000, such amount to be adjusted by an amount equal to the percentage increase in the Consumer Price Index (All Items – British Columbia) between the year of this agreement and the year of the development application being made, with registration of a section 219 covenant in favour of the City including an agreement for maintenance by the owner, and authorizing the City to undertake such maintenance and to charge back the owner for its costs in the event the owner defaults.
- At least four electric vehicle charging stations that are fully equipped and capable of providing a minimum of a Level 2 electric vehicle charging level (as defined by SAE International's J1772 standard), and that meet the minimum standards prescribed under the City's Off-Street Parking and Loading Bylaw No. 4350 – 1990, as amended or replaced from time to time.

- Construction in compliance with any BC Energy Step Code requirements that may be adopted by the City of Maple Ridge with sustainable elements such as low-flow fixtures, rainwater management, heat recovery ventilators and low VOC material specifications.
- Continuation of the east-west landscaped pedestrian way with registration of a section 218 statutory right of way and section 219 covenant and rent charge providing for public access to and use of the pedestrian way, and access to and use of other public amenities to be provided and included as a part of or connected to the pedestrian way, such as the public plaza, while allowing access to commercial uses within Phase 7, and including an agreement for maintenance by the owner, and authorizing the City to undertake such maintenance and to charge back the owner for its costs in the event the owner defaults. The agreement will be on similar terms and conditions as Statutory Right of Way CA8389661, Section 219 covenant CA8389662, and Rent Charge CA8389663 but subject to any modified terms and conditions as may be required by the design of Phase 7, the uses and amenities within Phase 7, as approved by the City, and the requirements and conditions for public access to and use of the public amenities, as well as access to any commercial uses within Phase 7, all as determined through the development approval process established under this Agreement.
- Sustainability measures.



**SCHEDULE “E”****TRAFFIC MITIGATION MEASURES**

The November 5, 2020 traffic impact assessment prepared by Bunt & Associates Engineering Ltd. (the “TIA”) is an over-arching assessment of anticipated traffic impacts related to the development of the Lands as contemplated under the Zone Amending Bylaw and this Agreement. This Agreement is intended to give the City:

1. the authority to request updates to the TIA when the City’s approval for the development of each Phase is sought by the Covenantor, with the understanding that each update to the TIA may propose alternative or additional requirements to those set out in the TIA, for the City’s consideration;
2. the authority to require additional road dedication or road closures at the development permit or the building permit stage for each Phase, if needed to support the road and traffic safety mitigation measures required by the City for that Phase, and such road closures may included but are not limited to the potential closure of Garden Street from Dewdney Trunk Road to Brown Avenue;
3. the authority to require traffic safety improvements and infrastructure for roads in the vicinity of the Lands, including but not limited to the improvements and infrastructure referred to in the following Table:

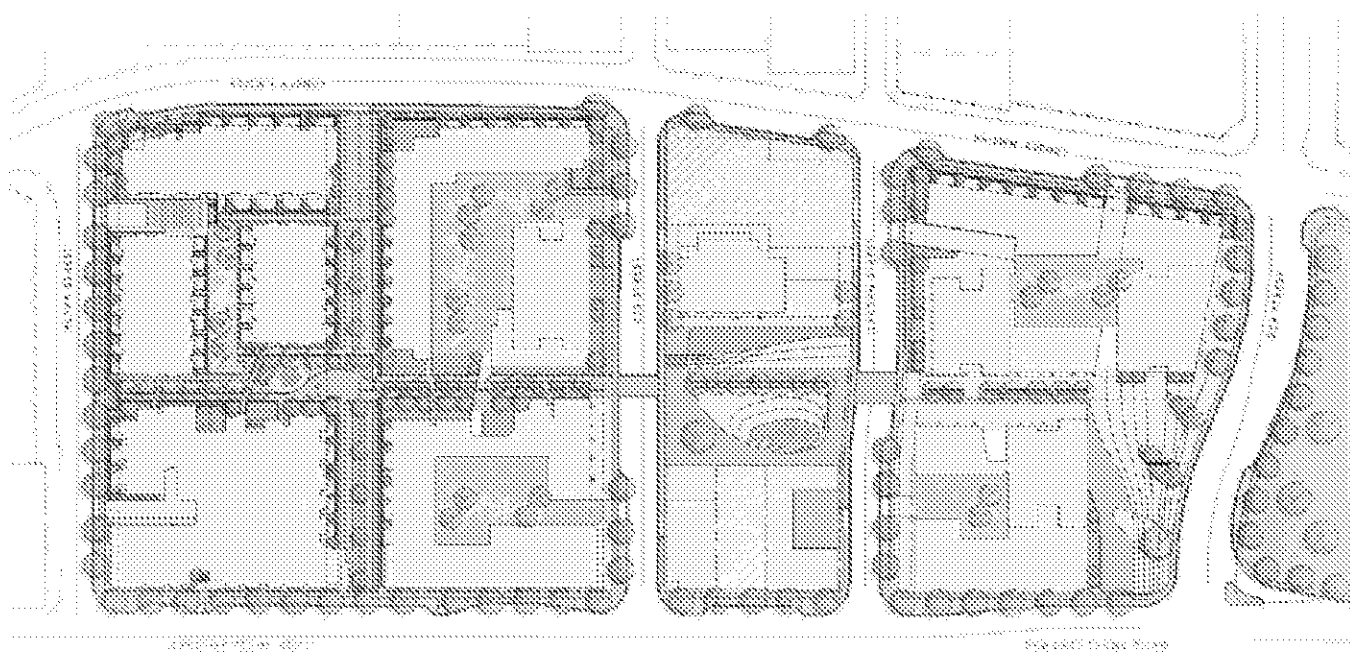
LOCATION	MITIGATION MEASURE	COMPLETION BY PHASE
224 Street and Dewdney Trunk Road	Optimize signal timing and splits.	Phase 1
	Extend the northbound left-turn bay storage length, which will require the removal of some on-street parking.	Phase 7
	Reconfigure all left-turn phases from protected only to protected/permissive. This will require re-aligning the intersection to address the site distance and other issues created by the offset alignment.	Phase 7
222 Street and Dewdney Trunk Road	Optimize signal timing and splits.	Phase 1
	Reconfigure north and south approaches to a left-turn lane and a through and right-turn lane. The extension of turning bays will require the removal of some on-street parking.	Phase 7
	Provide a protected + permitted northbound left signal phase.	Phase 7
222 Street and Brown Avenue	Convert from the existing all-way stop control to a roundabout	Phase 7
223 Street and Dewdney Trunk Road	Restrict 15 m of parking to provide left-turn storage length	As required by construction phase
Garden Street and Dewdney Trunk Road	Restrict 15 m of parking to provide left-turn storage length	As required by construction phase



**SCHEDULE "F"**  
**ERA DESIGN GUIDELINE**



# DESIGN GUIDELINES


**IREDALE**  
 AGENTS & BROKERS



## PROLOGUE

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The best master development plans are ones that allow for deliberate evolution, a living process that acquires layers of greater depth and complexity as the patterns of use are established. This development is designed for now and for the future.

ERA is conceived as a multi-phased project to be developed over a period of time. These guidelines will set up the patterns to allow ERA and the Maple Ridge Town Centre to positively evolve over the next generation into a mature and unified community.



*Figure 1: Aerial View of ERA Development Master Plan*

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## APPENDICES

1. ERA Design Guidelines Checklist for Development Permit Applications Phase 2 - 7
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# 1. HOW TO APPLY THESE DESIGN GUIDELINES

## 1.1 Introduction

The Town Centre Area is a very important part of Maple Ridge and would benefit from a variety of planning and design activities to improve its role as a key community node. To achieve a balanced community, the proposed Master Development Plan (MDP) provides guidelines for the form and character and coordination of common features for the comprehensive development of a 7 phase (2.97 Ha) mixed use development site including commercial, office and multiple residential development increasing the vitality and livability in Maple Ridge Town Centre.

As standards and benchmarks for new developments, these guidelines are informed by best practices in urban design but tailored to the unique conditions of Maple Ridge Town Centre. They will be the basis to guide and shape new buildings and open spaces in conjunction with the OCP Development Permit Area Guidelines to reinforce the objectives of the MDP. At the same time, they provide flexibility within certain parameters to encourage distinction, variety and creative architectural responses.

Where developments do not conform to the guidelines but propose alternative standards, they should be assessed to ensure the intent and spirit of these guidelines are met concerning the Vision, Principles and Intent presented in this document along with the rest of the regulatory applicable documents.

## 1.2 Application and Intent

1.2.1 The Guidelines: These Design Guidelines, hereinafter referred to as the ERA Design Guidelines (ERA DG) shall be used in conjunction with applicable Maple Ridge Bylaws and the Official Community Plan (OCP) Development Permit Area Guidelines, to guide the development of the master plan proposed 7 phase mixed use development project in the Maple Ridge Town Centre as shown in figure 2. They form part of the No Build Restrictive Covenant on Phase 2 through 7. The objective is to assist in achieving the proposed the Master Development Plan (MDP) for this project. The ERA DG take precedent over the Town Centre Development Permit Area Guidelines for the West Precinct. The Policies contained in the Official Community Plan and the Town Centre Area Plan continue to apply.

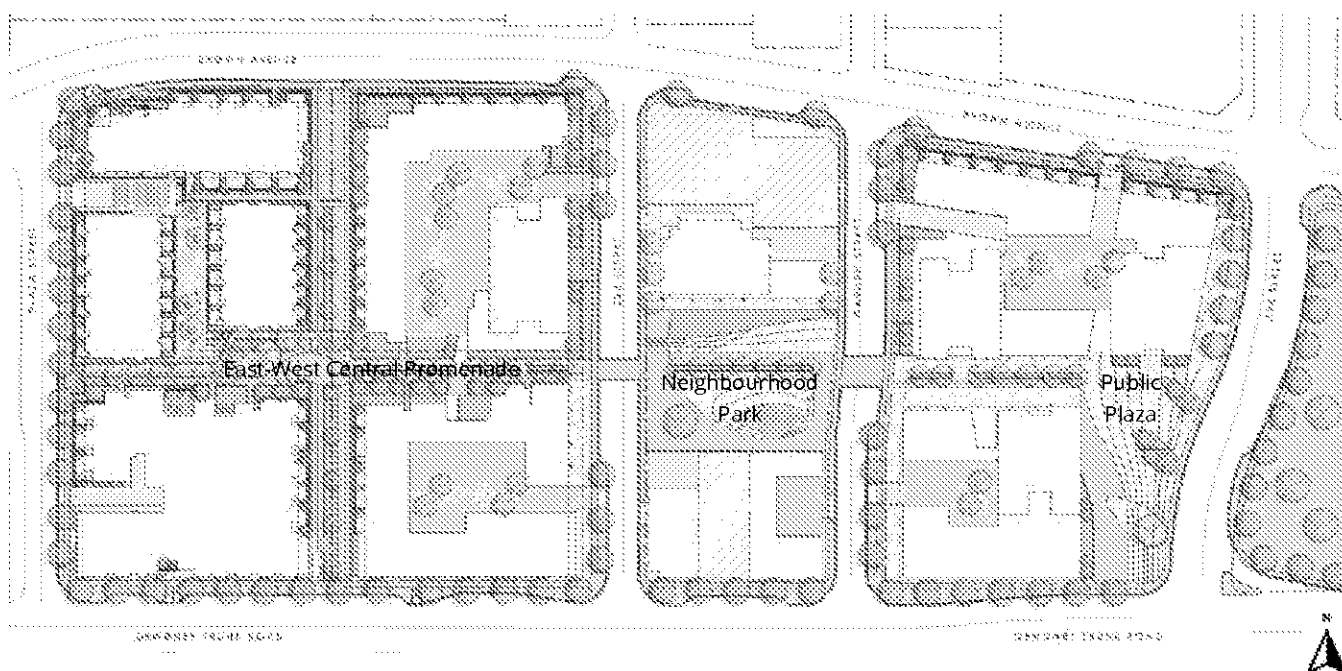


Figure 2: Illustrative ERA Development Master Plan (Building footprints and features shown may vary).

The ERA DG shall be used by the Architects, Landscape Architects and other professionals as the basis for coordinating and achieving a comprehensive development that is seamless across the phases and demonstrating to City staff how each guideline is satisfied.

The purpose is to insure that the key design elements for this project, including: site layout, form and character, public art, stormwater management, sustainability, the pedestrian/public realm, amenities, etc., are coordinated between phases and completed at build out. The ERA DG document will be registered on title in accordance with Council requirements.

1.2.2 Application Process: Unlike other rezoning development applications where the development permits are issued with detailed plans at Final Adoption of the zoning, this development is pre-zoning a seven phase site. Therefore, development permits are to be considered and issued by Council at a future date on a phase by phase basis. Therefore, the ERA DG will take precedence and will be the overarching guidelines. They will be a fundamental tool in interpreting the compliance of future phases with OCP Section 8.11 Town Centre Development Permit (TCDP) Guidelines in terms of form and character Council envisions in the Town Centre Area, within the context of the overall concept and elements to be achieved through this project.

Section 219 Covenant – No Build and Future Servicing sets out requirements for the development permit application process and requirements. These Design Guidelines should be read in conjunction with this Section 219 Covenant.

Future applications for each phase will be subject to the submission requirements and fees in place at the time applications are made. Given the pre-zoned nature of the development site, future applications will require additional supporting reports, plans and details as part of the development permit applications. This will include the following:

1. A Pre-Application Meeting: Before any applications are made, a suitably developed proposal will be submitted and a Pre-Application meeting scheduled with the City. This meeting will include City Department representatives to provide direction to the applicant and their consultants on what plans, information and consultant studies are to be prepared and the exact nature of a development application to be submitted to the Planning Department.

Unless otherwise indicated, the following materials are to be provided one to two months in advance of such a pre-application meeting:

- A. Sufficiently Detailed Architectural and Landscaping Plans: The concept for a phase needs to be developed sufficiently taking into consideration the ERA and OCP Guidelines, including a preliminary analysis of compliance, and the materials in subsequent points.
- B. General Subdivision Information: A full title search with copies of registered documents, preliminary subdivision plans including road dedications, list of anticipated legal documents for the phase, and any related information necessary to provide full context.
- C. Building Code, Fire Code and Other Supporting Materials: All supporting information upon which development of this phase and as has been deferred from the rezoning to the development permit application process, not necessarily finalized, is to be provided.
- D. Transportation Studies: Terms of reference for each phase is to be obtained from the Engineering Department in advance of the pre-application meeting, to better determine the information and analysis required to be included in the pre-application meeting with the City.
- E. Other: Any matters of specifics associated with a given phase that requires consultant studies or other assessments to support the submission for the pre-application meeting.



2. Application Submission and Review: Complete applications will be required to be submitted with the development of each phase, including the items listed below; These items with the full application submission are to have addressed comments and suggestions from the pre-application meeting and include the additional information that may have been identified:

- A. Development Permit Application: The following applies to each application in each phase:
- B. The submission checklist in Schedule D of the Maple Ridge Development Procedures Bylaw No. 5879-1999 for a development permit application will therefore need to be expanded to include additional supporting materials and information for future development permit applications, including those supporting materials and information typically required for rezoning as stated in Schedule C of the same procedural bylaw. A full evaluation against the ERA DG and the applicable Town Centre Development Permit Area Checklists will accompany the submission.
- C. Addressing comments from City Departments and outside agencies, including the School District and the Ministry of Transportation and Infrastructure.
- D. Immediately following the submission of an application, the developer will post one or more Development Information Signs (DIS) with information as required by the City.
- E. The proposal is to be submitted to the Advisory Design Panel for review and comment.
- F. Once the proposed development has addressed all concerns and comments, the developer will undertake notification and will hold a Development Information Meeting (DIM).
- G. Subdivision Application: The submission shall contain the plans, legal documents and consulting reports as may be prescribed by the Approving Officer. It will accompany the Development Permit Application and govern the creation of a parcel to contain a particular phase, and will be subject to requirements and standards in place at the time for road, servicing and other terms and conditions not completed at the rezoning stage. All requirements will be established through a Preliminary Review Letter or similar tool issued by the Approving Officer.
- H. General Subdivision Information: A full title search with copies of registered documents, preliminary subdivision plans including road dedications, list of anticipated legal documents for the phase, and any related information necessary to provide full context.
- I. Sufficiently Detailed Architectural and Landscaping Plans: The concept for a phase needs to be developed sufficiently taking into consideration the ERA and OCP Guidelines, including a preliminary analysis of compliance, and the materials in subsequent points.
- J. Building Code, Fire Code and Other Supporting Materials: These will be provided with the above Applications, as well as other studies, as established through the Pre-application Meeting. Additional requirements or corrections may be required as the projects progress through review toward approval. These will include, but are not be limited to: geotechnical reports, updated traffic or servicing reports, stormwater management strategies specifically integrating Tier A stormwater management into landscaping, market studies, Fraser River Escarpment considerations, code analysis, etc.
- K. Other Applications: These may include development variance permits, tree cutting permits and applications as may be required by other Departments.

Where possible, the above applications and reports would be processed concurrently.

3. Application Coordination: It will be the responsibility of the developer to assign one Coordinating Registered Professional (CRP) to manage and ensure all aspects of each application align and are coordinated. Communications will be through the Principal File Manager for the City and the CRP for the developer.

### 1.3 ERA Development Vision & Key Design Elements

1.3.1 Development Vision: ERA is envisioned as a well-designed pedestrian, biking and transit-oriented mixed-use development in the heart of the Maple Ridge Town Centre. It will reinforce the existing Civic core with a variety of uses such as recreation and civic spaces, a mix of new residential (in form and tenure), commercial and new employment opportunities, a neighbourhood park with a community gathering space, green building design and infrastructure.

#### 1.3.2 Key design elements:

- A. Residential components of varying scale and density, positioned to maximize daylight penetration into the site and to create visual interest from every corridor.
- B. A prominent east-west public pedestrian open space and pedestrian way from Plaza Street to 224 Street, punctuated with activity areas (including child play, central activity plaza with theatre-like seating, and a major commercial plaza) and linkages to semi public and private space paths and mews and the City sidewalk system.
- C. Mews surrounded by both residential and commercial activity to ensure there are plenty of “eyes on the street” at all hours of the day.
- D. Public plaza in the east portion of the site, lined with commercial shops (inner “shopping row”) and containing significant surface treatment, landscaping, pedestrian amenities, placemaking elements and a public art element.
- E. An attractive and vibrant streetscape along Dewdney Trunk Road with building spaces designed to accommodate ground level shops and services, and access to upper level non-residential components where they occur.
- F. Strategically placed commercial spaces to create an intimately scaled shopping corridor combined with residential use above.
- G. Sustainable design solutions to maximize rooftop space for green houses, gardens and power generation.

The overall design objective is to develop attractive, durable buildings appropriate to the City of Maple Ridge and a public realm that complements the natural beauty of the area, reflecting the policies and objectives outlined in the Official Community Plan while supporting the social, recreational and commercial life in the community.

Future developments, whether they are adjacent or within the community itself, will have this language of patterns to ground their projects and to inform their designs.



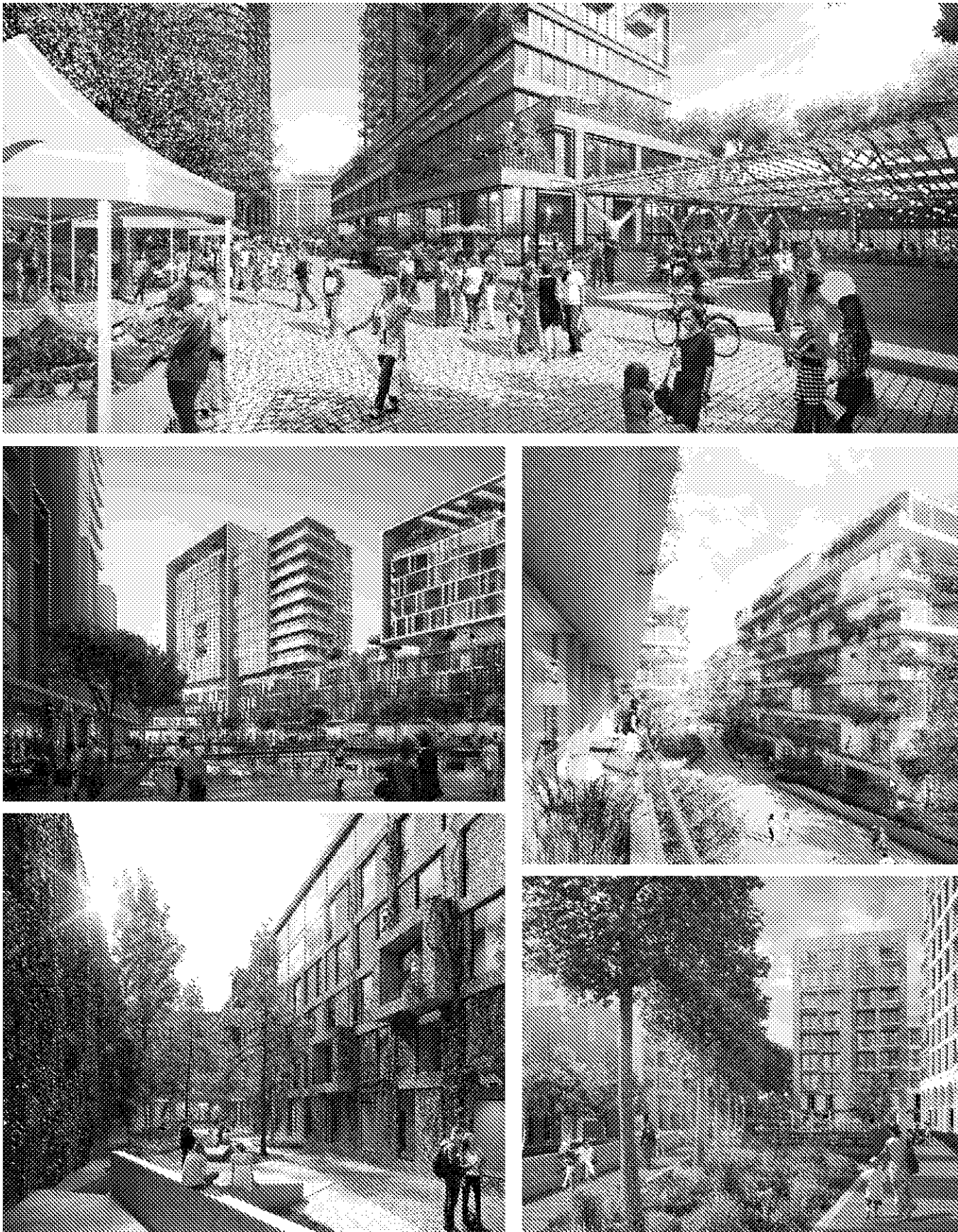


Figure 3: These images demonstrate the possible pattern for a mixed use and vibrant neighborhood complete with public gathering spaces and the important relationship between the buildings and the pedestrian walkways through the site. The buildings should engage the public realm and help define and soften the edges of pedestrian corridors (Representational of character only).

## 2. OVERALL SITE GUIDELINES

### 2.1 Design Principles and Purpose

2.1.1 The objective of this document is to provide a design framework for the built form and public realm that provides consistency in neighborhood character, while also allowing for change in site planning and architectural design through each development phase.

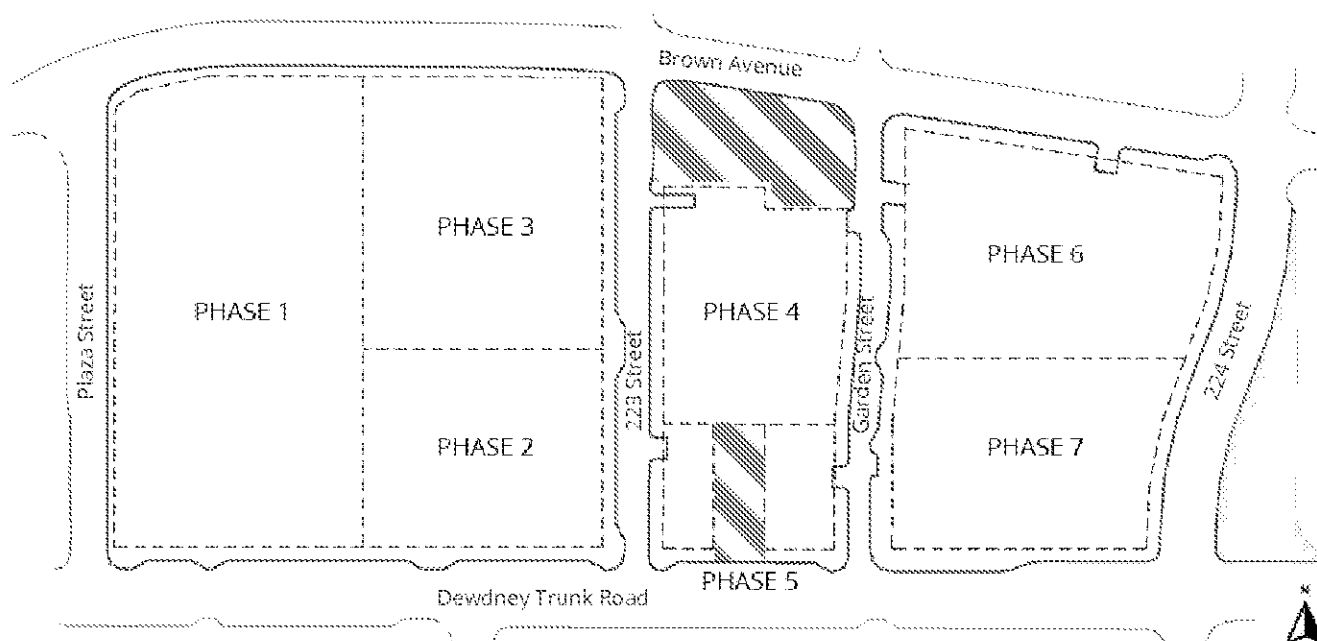


Figure 4: Phasing Plan. The phasing of the development takes into consideration the density for each phase, gradually increasing throughout the phases of the project with the location of the most dense phases (Phase 6 and phase 7) closer to the Civic Core of the city.

### 2.2 Design Considerations by Phase

#### 2.2.1 Phase 1

High exposure on the north, west and south frontages – Brown, Plaza and Dewdney Trunk – calls for a dignified residential character with the south-west corner marking the entrance to the development along Dewdney Trunk Road and in a way also marking the entrance to the downtown core, more specifically the Downtown West Precinct. As described in section 8.11 of the Town Centre Development Permit Area Guidelines, the goal is to provide enhanced architectural concepts that contribute to pleasing compositions in mass, quality and character; and promote vibrancy of the urban realm. This phase is primarily marked by five and six-storey residential buildings with two groupings of three-storey Ground-Level Apartments in-between. The public green path through the entire site begins here, winding its way between the five-storey and ground-level apartment buildings. Commercial retail units will be located at ground level along Dewdney Trunk Road.

##### 2.2.1.1 Key urban design role:

The south-west corner marks the entrance to the development along Dewdney Trunk Road and in a way also marks the entrance to the downtown core.

#### 2.2.2 Phase 2

This five or six-storey building along Dewdney Trunk Road mirrors the scale and massing of the phase 1 building across the greenway north-south running spine. The building will feature ground-oriented patios



looking out across the spine and across 223rd Street towards the park in phase 4 (See phase 4 for more detailed description of the park). Commercial retail units will be located at ground level along Dewdney Trunk Road.

#### 2.2.2.1 Key urban design role:

This building will form the western edge of the neighbourhood park.

#### 2.2.3 Phase 3

The first tower will be introduced with this phase, accompanied by lower scale four, five or six-storey buildings forming a central courtyard space with the development's amenity space facing out over the central greenway spine to the south.

The tower will range in height from 15 to 17 storeys and it will have the standard 3-meter allowance above for roof articulation and ground-level apartments or an enhanced amenity space for the overall development. The tower should be oriented north-south to help minimize the shadows across the development.

#### 2.2.3.1 Key urban design role:

The tower will help form the north-west corner boundary to the neighbourhood park at the heart of the overall development.

#### 2.2.4 Phase 4

This phase is comprised of a single 10-storey tower and an open green space - neighbourhood park. The neighbourhood park will be the central focus of the development and as such should provide for a variety of different uses and leisure activities. There should be a balance between open lawn space and hardscape surfaces for public gatherings. Some permanent landscape seating should be integrated into the overall design.

#### 2.2.4.1 Key urban design role:

This building will form the northern edge of the neighbourhood park.

#### 2.2.5 Phase 5

Designed to be the only purely commercial phase of the overall site, this phase is comprised of two independent buildings. A two-storey commercial building located at the north-east corner of 223 Street and Dewdney Trunk and a four to six-storey commercial building on the north-west corner of Garden Street and Dewdney Trunk.

The buildings will have high exposure along Dewdney Trunk but will also be expected to properly orient themselves towards the neighbourhood park to the north so as not to turn their backs on it. Special attention will need to be given to engage the pedestrian movement in the east-west direction across the neighbourhood park. These two buildings should be considered as having three front orientations.

#### 2.2.5.1 Key urban design role:

These buildings will form the southern edge of the neighborhood park and because they are both purely commercial in nature, they should actively engage with it. The inclusion of café's or similar type uses will help animate the streets along 223rd, Garden and Dewdney Trunk Road and the neighborhood park to the north.

## 2.2.6 Phase 6

Flanked by two towers of 11 storeys on the west and 19 storeys on the east, this development has the highest density. Between the two towers is a five-six storey residential building sitting on a common base lined with boutique sized commercial units facing 224 Street and the inner “shopping row” that runs from the central green space to 224 Street.

### 2.2.6.1 Key urban design role:

This development will form the north-eastern corner of the central outdoor space and also mark the entrance to the boutique shopping row between phase six and seven.

## 2.2.7 Phase 7

Comprised of the tallest building in the overall development at 21 storeys, this phase also forms the backdrop to the new public plaza located at the north-west corner of 224 street and Dewdney Trunk Road. The base of the buildings will be comprised entirely of commercial retail units to help animate the streets and plaza. Above this will be another four or five storeys of residential development.

### 2.2.7.1 Key urban design role:

This development will form the eastern edge of the central outdoor space and also have significant exposure to the new public plaza at the north-west corner of 224 street and Dewdney Trunk Road. Special care should be taken to properly address and engage the plaza at ground level with the appropriate scale and articulation of the façade, including the types of CRU's that occupy this space.

Landscaping should play a critical role in the development of this plaza and help define this important outdoor room. Special attention should also be given to the treatment of the paving and ground treatment to ensure this space is appreciated at street level and from above when looking down on it.

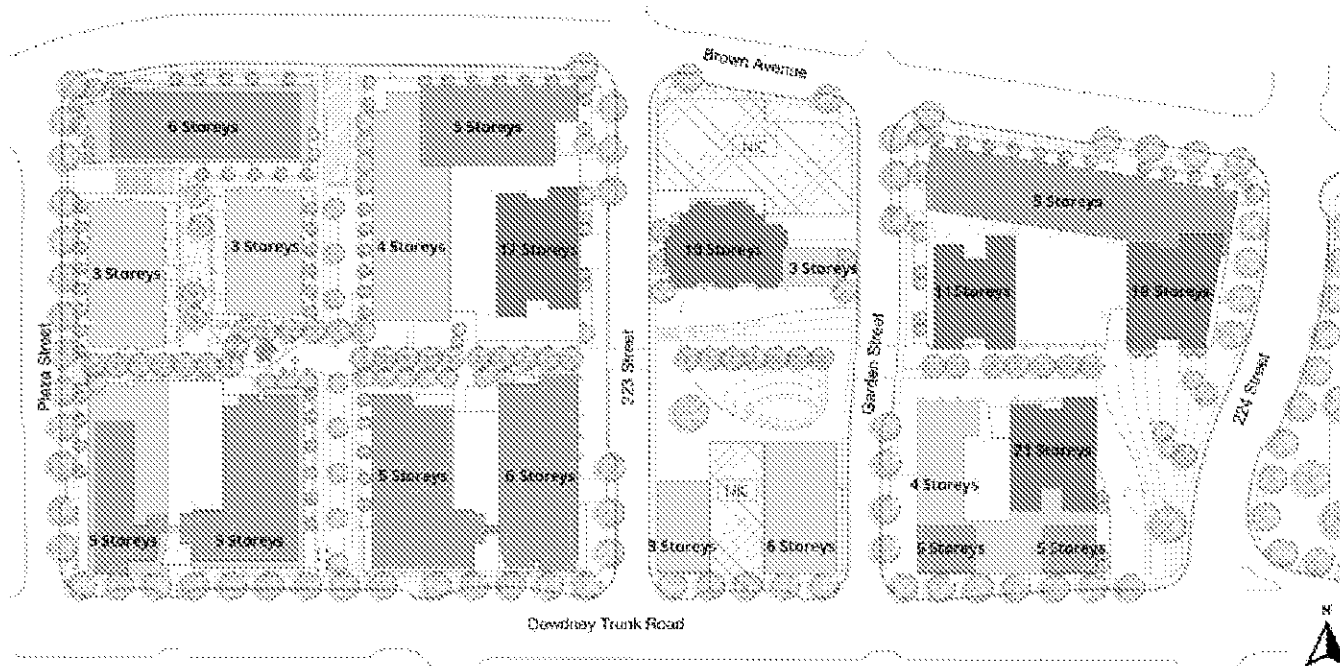


Figure 5: The development gradually increases in density as it expands from west to the east. The densest part of the development is closer to the civic core and culminates in a public plaza (Building footprints may vary).

Parcels not part of this comprehensive development must be taken into account. Efforts will be made to allow for a friendly interface with these properties. In the instance where these parcels become part of the development (or if they become an independent development), they must be integrated well into the overall development and adhered to the ERA DG.



## 2.3 Key Urban Design Principles

- A. **Pedestrian friendliness**  
Streetscapes and public walkways are to be articulated using appropriate scale, character and texture. Street trees, front doors (from ground-level apartments and retail storefronts), different scaled plantings and a variety of paving treatments are to be used to help define these paths and enhance the pedestrian experience. Security is also to be considered through the use of lighting and lower planting along path's edges.
- B. **Public accessibility and permeability**  
Provide clearly defined barrier-free access in both North-South and East-West directions through the site. Landscape elements, both soft and hard are to define and enhance these paths.
- C. **Livability**  
The overall layout and design of individual buildings and dwellings has been designed to create a variety of different living experiences and to provide a high standard of privacy, usable open space, and recreational amenities. This is achieved through different massing, height and configuration of buildings throughout the various phases of the overall development.
- D. **Neighbourliness**  
The form and layout of the site plan was driven by a desire for open space and connectivity. A variety of open and public spaces will facilitate community gathering.
- E. **Diversity**  
Diverse housing types including a range of small and large units, condos and ground-level apartments, and low-rise and high-rise buildings will appeal to a large cross-section of the community. Appropriate architectural expression should be given to each as described in other sections of this document.
- F. **Sustainability**  
Sustainable design and development principles and practices will be employed such as, water efficient landscape design and rain and storm water management.
- G. **Placemaking**  
Public spaces acting as nodes making a positive and memorable contribution to Maple Ridge Town Centre. These spaces should be designed to facilitate a wide variety of civic activities and provide visual interest for the residential buildings surrounding them throughout the use of hardscape, landscape and plazas.



Figure 6: The images above demonstrate a variety of appropriate building typologies for the different scales of residential uses in the development (Representational of character only).

## 2.4 Pedestrian Pathways and Access Points

These guidelines recommend the emphasis should be given to usable public open space, directly accessible from within a unit. It is strongly encouraged to design common spaces that help maximize the resident's open space experience within the development.

Pedestrian site connectivity and permeability are two of the fundamental objectives of the Policy Statement for ERA, and subsequently play a prominent role in the public open space system for the site.

Access points at the middle of the block as entries are configured to allow strong physical connections and visual corridors to the interior of the site, see figure 7.

In the commercial retail areas, this translates into wider, multi-functional linear public spaces. In the residential areas, the internal mews are reflective of more intimate spaces, while maintaining strong public access to all parts of the site.

There will be a SROW in place for all phases. This has already been put in place for phase 1. Refer to Section 219 Covenant - No Build and Future Servicing for further details.

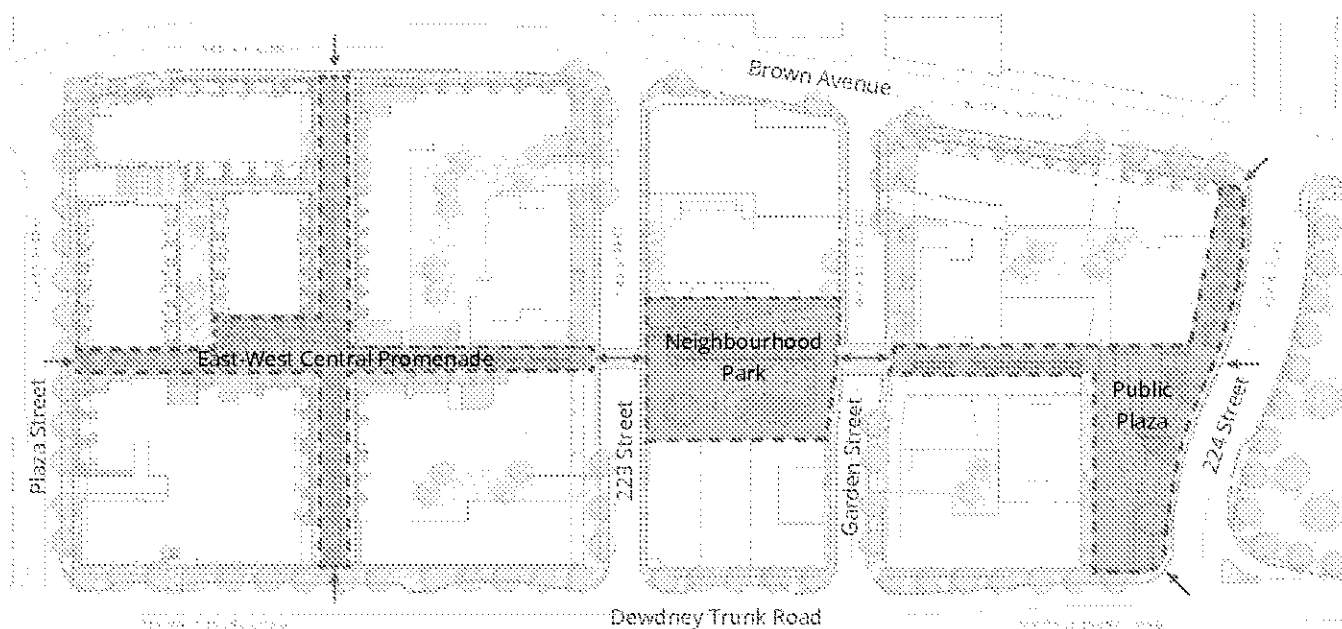


Figure 7: Open space Area and Access Points. Access to public spaces within the development is provided throughout from East-West and North-South (Buildings footprint and features may vary).

## 2.5 Landscape Design

### 2.5.1 General considerations

The landscaping should address the form and character of the development through visual form cues from the architecture, while aiming to soften the scale of the architecture in both the primary public pedestrian circulation routes and nodes, as well as the more intimate private amenity spaces.

Taking into consideration the diverse needs of all the stakeholders, and taking cues from the TC Area Plan policies and DP Area guidelines to develop a strong concept for both human activities and the longevity and vibrancy of the integrated natural element. In implementing a holistic approach of "sustainable intensification", these guidelines should provide a range of breathable and enjoyable spaces for those living, working, and visiting this new City Core.

### 2.5.2 Key Principles

The following minimum general guidelines below should be incorporated to the greatest extent possible:

- A. The redevelopment of the ERA site will feature a mobility network that prioritizes pedestrian circulation and comfort above other transportation modes. Sidewalks and pedestrian street crossings will be continuous, universally accessible, barrier free and clearly designated; Special attention should be paid to crossings at roadways along the East-West Central Promenade; where the pedestrian should be prioritized and protected using an elevated crosswalk (aka. "speed table") with appropriate markings for vehicular traffic & tactile warnings accessible design.
- B. Accessibility Standards and Best Management Practices (BMP) should be applied generously wherever possible in all aspects of landscape design.
- C. In considering the soil conditions of the geotechnical report, the landscape design should maximize soil volumes on-site to increase stormwater detention, prior to infiltration to ground. Additional measures of detention & retention may be required on-site to support the development of each phase. Refer to Rain and Stormwater Best Management Practices Section 2.6.4; using appropriate devices to reduce the impacts of runoff over proposed impervious areas.
- D. Landscape design should take into consideration solar exposure, and appropriately utilize and orient deciduous shade trees to create spaces that are sustainable and useable year-round, while maximizing sunlight wherever possible.
- E. Landscape design should take into consideration the human scale when determining a hierarchy of circulation paths, heights of walls and other elements that delineate programmatic spaces, screening and layering of planting elements, and arrangement of furnishings.
- F. Landscape Design elements should complement the architecture to maintain a cohesive legibility within various 'Character Zones'.
- G. A hierarchy of wayfinding elements (including signage) should be carefully considered and incorporated to allow ease of navigation particularly for those visiting the site, but also as elements that add to the identification and differentiation of various 'Character Zones'. The elements of effective wayfinding being (1) landmarks or other immediate identifiable elements to infer current location and help people to orient themselves, (2) clear routes to destinations, (3) a hierarchy of well defined paths, nodes, and edges of programmed space.
- H. Public art and/or landscape features – particularly that integrate either water or lighting – should be considered at prominent locations to reinforce the importance of the Central Promenade and navigability of the development.
- I. Lighting design should be an integral part of landscape design. While the architecture may provide ambient lighting, the landscape should aim to – at a minimum – provide path lighting along accessible routes. Area lighting may be used as the density increases and the nodes become larger and more commercial/public. Additionally planting may be accented with spotlighting or various other effects to animate spaces that will be used beyond dusk.
- J. Landscape design at the streetscapes should create a continuous and cohesive public realm. Private areas along streetscapes should be vertically separated wherever possible to provide privacy in the instance that the space is actively used, as well as eyes on the street. In these instances, the landscape should still contribute to the visual enhancement of the public realm through use of interesting materials, and ideally, planting material separation.



- K. Walls and fences should be integrated as appropriate where stronger demarcation of private space is to occur. The material used should reflect the Character of the architecture as appropriate to that zone. Consideration of Crime Prevention Through Environmental Design (CPTED) should be forefront when determining heights, with ideal wall heights being 0.61m, with wall-mounted step-lighting at 0.36-0.46m height and a maximum of every 5.0 m on centre unless otherwise noted.
- L. Appropriate soil volumes and drainage layers must be used to ensure the vibrancy of plant material into maturity. A minimum of 0.46m soil depth must be provided to all plants (noting this refers to most shrubs, perennials, and groundcovers without rhizomatous root structures; lawns & extensive green-roof sedums excepted with typical depths of 0.30m), with ideal depth being 0.61-0.76m. All trees require a minimum depth of 0.76m on-slab and an ideal depth of 0.91-1.2m where applicable off-slab; soil may be locally bermed-up to meet this condition. All trees require a minimum soil volume of 10 cbm.; this requirement may be relaxed with discretion in raised planted beds on-slab, where smaller trees may be planted to provide solar shading or ornamental trees planted for aesthetics (noting that these trees may not be counted by the City as "replacement trees" as their mature height & canopy will be limited).



Figure 8: The images above show pedestrian circulation & nodes of various hierarchies, with planting used to soften the edges & appropriately buffer public uses from private uses.

### 2.5.3 Landscaping and Screening

In Phases 2 – 7:

1. All developed portions of the lot not covered by buildings, structures or paved areas shall be landscaped including the retention of mature trees where feasible. This landscaping shall be maintained.
2. Along the developed sides of the lot which abut a highway, a continuous landscaping strip of not less than 1.5 metres [5 ft.] in width shall be provided within the lot.
3. The boulevard areas of highways abutting a lot shall be seeded or sodded with grass on the side of the highway abutting the lot, except at driveways, and walkways.

### 2.6 Landscape Sustainability Initiatives

There are several sustainability initiatives incorporated in the proposed landscape and overall site design.

#### 2.6.1 Green Mobility

Given the site location within the downtown core of Maple Ridge the development is well suited for walking, cycling and transit use. These mobility options are expected to attract residents who do not depend on the private motor vehicle.

1. The project rates high on a walkability scale. It is within a 5 minute walk of the civic core, and a 10 minute walk of all of the typical shopping destinations.
2. The site has excellent accessibility to Transit that connects Maple Ridge to adjacent communities.
3. The site offers opportunities for people to live and work within the downtown core and live car free.
4. Resident and visitor bike parking is to be provided on each phase.
5. Bike racks for visitors at street level must be located as per City's by-law and placed in such a way that bikes will not encroach on City's right-of-way when parked.

#### 2.6.2 Sustainable Food Systems

1. Urban agriculture not only contributes to food security but it has a number of other benefits including access to green space, social sustainability and healthy living. For this development we are proposing urban agriculture garden plots and edible planting integrated into the ornamental planting beds.
2. Shared garden plots can best be provided as part of consolidated common outdoor amenity space. Where this is not generally provided, opportunities for gardening could be considered in private outdoor spaces: patios, balconies and roof decks. The incorporation of garden plots should enhance the overall design of that common outdoor amenity and should be considered as one of the variety of programmed uses of those spaces.
3. Urban Agriculture will be provided through the inclusion of rooftop urban agriculture plots for residents. This amenity will include areas dedicated to tool storage, a potting bench and a hose bib(s). These plots help improve the resiliency of local food production, facilitate neighbourly social interaction and contribute to healthy living.

4. Edible Landscaping will be integrated into the townhouse and rooftop garden areas. Where practical, landscape selection will favor plants that also provide food; including fruit bearing bushes that will serve as both ornamental and functional assets.
5. Edible landscaping that may drop fruits and berries, should be conveniently located in high pedestrian traffic areas, fully accessible to residents, and not over public property to ensure that food is harvested without delay and to avoid attracting rodents and pests.

### 2.6.3 Access to Nature and Open Space

The form and layout of the site plan was driven by a desire for open space, both public and private, and connectivity. The pedestrian walkway not only is a circulation route through the site, but it is the primary connector to all of the open spaces in the development including: semi-private residential courtyards, public children's play and pet-friendly areas, the central park and the public plaza along 224 Street.

These open spaces contribute to the overall greening of the site and access to nature/green space.

### 2.6.4 Rain and Stormwater Best Management Practices

1. Comprehensive stormwater management plans should be prepared to address both the site and the surrounding area. These should consider existing soil conditions, geotechnical issues, and limitations created by underground parking structures. All Storm Water Management Plans need to be designed to align with current City policies and practices particularly with respect to the 3 tier criteria for Stormwater Management as outlined in the City's Design Criteria Manual (DCM).
2. With consideration of the geotechnical report of the proposed development, existing stormwater infiltration is highly limited on-site due to a high-clay content soil taxonomy, as well as a high-water table. Additionally, the displacement of existing soils in favour of impervious development, further reduces stormwater infiltration. As such, various measures should be aimed at meeting the Stormwater Management Requirements (Section D2 of the DCM). With respect to the ERA development, the following practices will help mitigate small & extended stormwater events.
  - A. Slowing stormwater runoff by maximizing the amount of water detained in new soil volumes on-slab. By increasing the depth of topsoil with a range of 450-762mm as permitted by the design, a majority of the rainwater in typical rain events will be absorbed in the landscape without permeation to-ground/escarpment.
  - B. For rain events with extended intensity or duration, additional systems of detention & retention are required to meet City's drainage requirements. Green Infrastructure & Low Impact Development (LID) solutions should be preferred to engineered detention tanks placed on-site.
  - C. Where stormwater cannot be immediately infiltrated into the landscape or collected for retention, this run-off (from on-site to off-site) should be directed to landscaped boulevards before reaching and further inundating the municipal system. Where streetscape sodded or planted boulevards do not exist, this runoff will be calculated as part of the allowable runoff analysis by Design Engineer to be submitted to the City.
  - D. Where possible, concrete buildings should support & maximize the area of extensive or, preferably, intensive green-roof infrastructure.
  - E. Where possible, the developer may opt to re-use captured/retained rainfall within the building systems, or within the landscape irrigation.
  - F. The Stormwater Management Plan will need to consider the Fraser Escarpment Policies where applicable.
3. Other specific strategies may include green roofs, bio-retention areas, rain gardens and other infiltration features. Permeable paving materials should be utilized where possible to reduce run-off and total load on the stormwater system.



4. Installation of oil/water and soil/grit separators in the storm drain system within parking areas is required.
5. Where possible, rainwater should be directed into small-scale storage and treatment areas located in greenways and other open spaces. Specific strategies – where allowed in reference to geotechnical reports – may include vegetated swales with bioremediation, permeable paving, rain barrels, and underground cisterns to collect rainwater from roofs.
6. From an aesthetic point of view, a distinctive minimal design should be adopted for area drains, catch basins and manhole covers, and used throughout ERA development. This may be an existing set of products or may include unique designs created for this development.

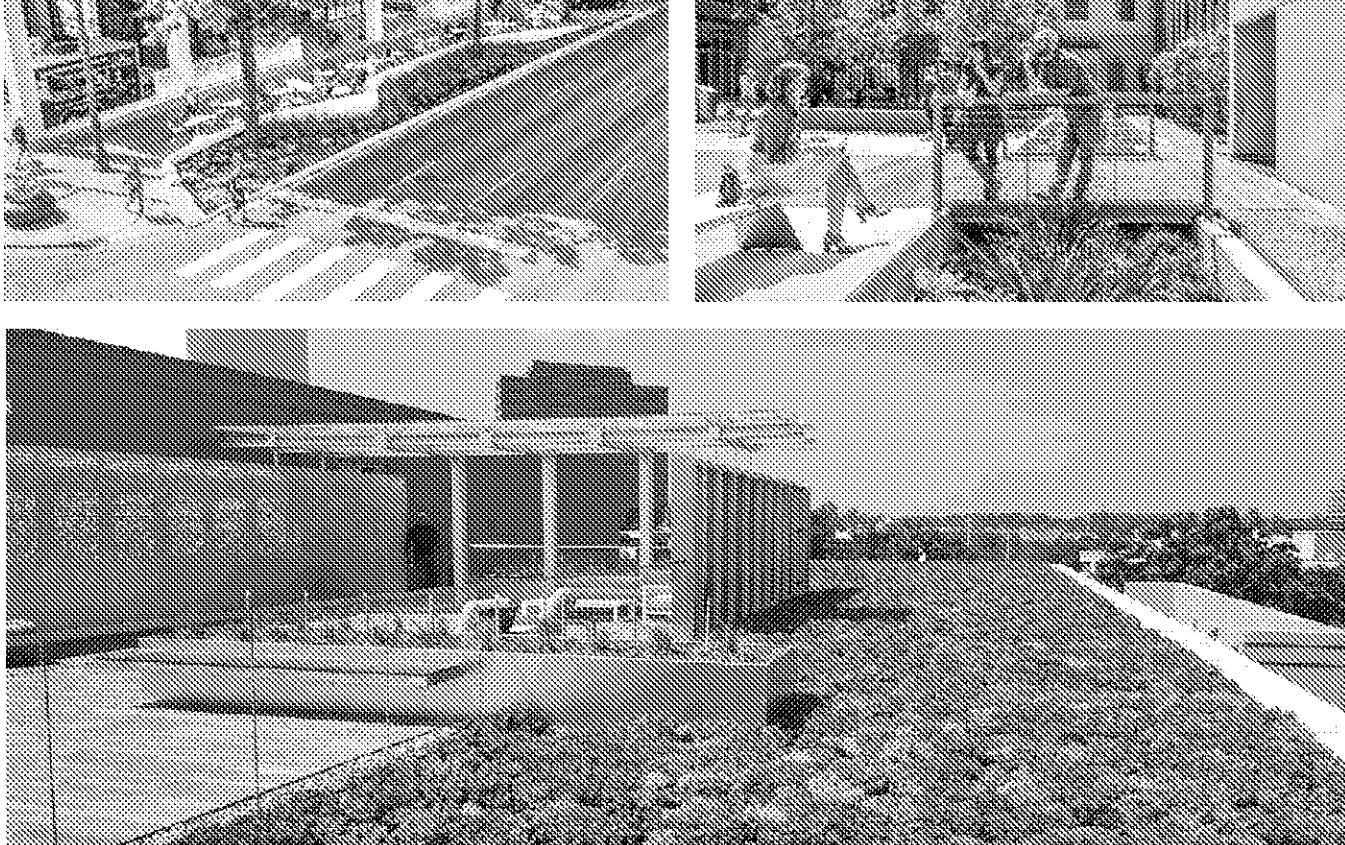


Figure 9: illustrative images above reflect some moderate measures of "green infrastructure" that can support the slowing of stormwater to traditionally engineered detention systems (Representational of character only).

#### 2.6.5 Bird Friendly Design

The landscape design proposes strategies to promote native birds in the urban environment. The proposed landscape plantings incorporate canopy/habitat stratification for a variety of bird species. Plants should be utilized that will attract birds and insects with both native and non-native/non-invasive plant species. Bird houses are also proposed in large shade trees.

#### 2.6.6 Water Efficiency in the Landscape

There are two ways water efficiency is targeted in the landscape.

- A. The landscape planting design should utilize planting that, A) Demonstrates low-water use or drought tolerance; B) Is native or adaptive to local climate and soils to make the lower-maintenance & naturally more drought tolerant; C) Application of mulch topping, to retain any moisture in the soil and even out soil temperature and insulate roots from heat-stress. These planting measures will further reduce water demands for the project.

- B. Irrigation system may be implemented to aid in calculations to attain CoMR Design Criteria "Tier A". It is not required if native/drought-tolerant planting is used throughout, but is strongly recommended for longevity of the landscape design. This irrigation system should be high-efficiency (with smart controls, staggered scheduling, rain-sensors, etc.) and use drip irrigation where possible for added efficiency of delivery to planting.

1. Where possible, the developer may opt to re-use captured/retained rainfall within the building systems, or within the landscape irrigation. Stormwater re-use in irrigation is highly encouraged as a sustainable practice & BMP for stormwater responsibility.

## 2.7 Green Lane/Mews

2.7.1 For the design of open common spaces, especially mews and/or courts, the following should always be considered:

- A. Orientation - Orient to sunlight and provide good physical and visual access to the sidewalk.
- B. Dimensions - Design as adequate for seating, planting etc., but not so large as to appear barren and uninviting.
- C. Seating - Provide comfortable height and depth, and appropriate arrangement.
- D. Pavement - Use non-glare, non-slip, and safe surface materials.
- E. Trees and Planting - Consider provision for shade and sun. Use to create space and define human scale. Provide protection from wind.
- F. Barrier Free - Provide accessible areas for handicapped.
- G. Amenities - Use pedestrian scaled lighting and other features.
- H. Enclosure - Use landscaping or structure to provide a sense of enclosure, and
- I. Shelter - Use screens and canopies to shelter spaces from wind and rain.

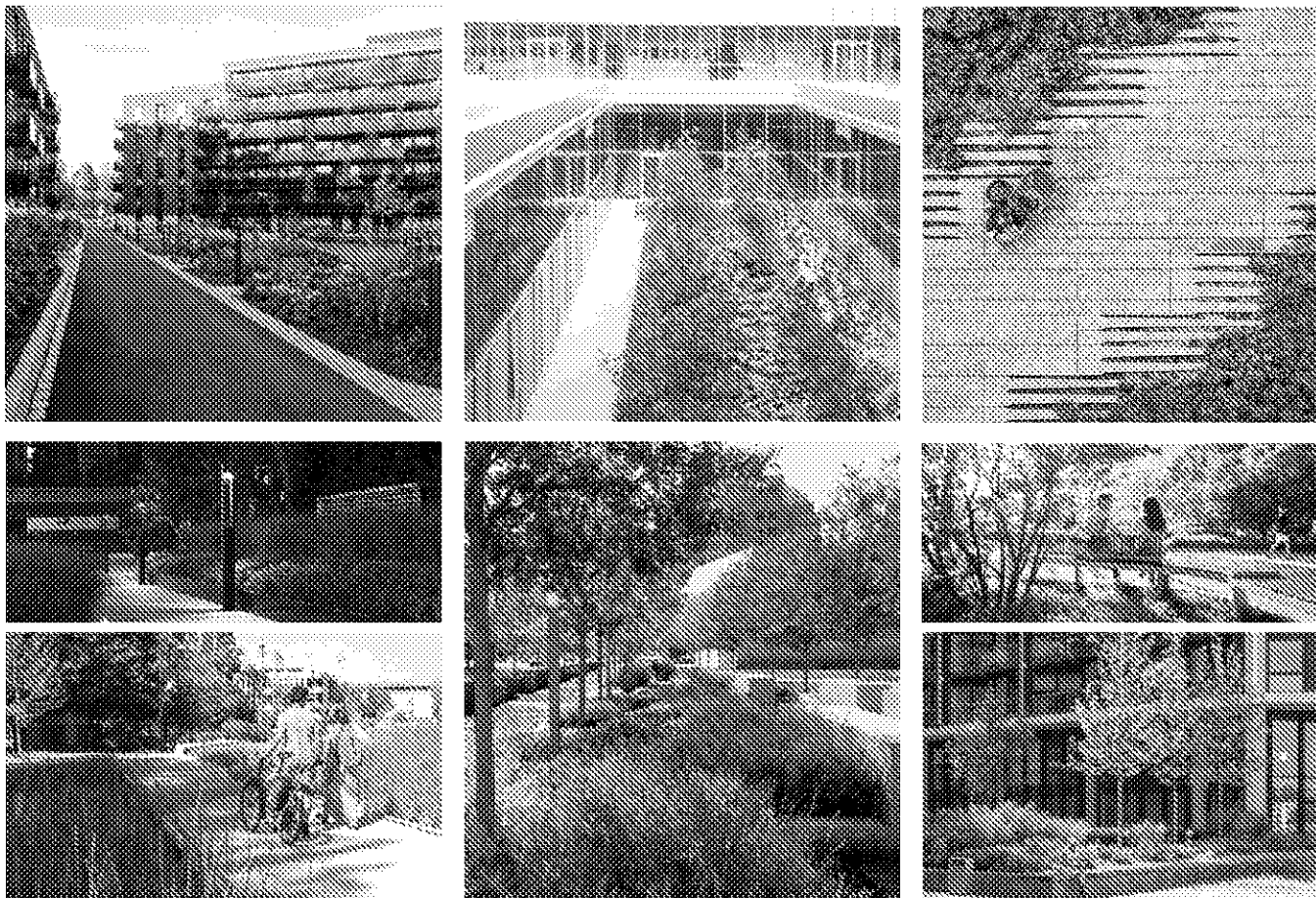


Figure 10: This series of illustrative images shows many dimensions & materials of the larger pedestrian corridors. Ensure that these shared spaces feel inviting to linger in, with seating, lush planting, and clear sightlines (Representational of character only).

## 2.8 Public Plaza

As the City of Maple Ridge continues to grow, there is an increasing need and demand to revitalize existing parks and open spaces as well as to create new parks and publicly-accessible open spaces.

The new public plaza is one of the main proposed open spaces within ERA. It is envisioned as a part of a pedestrian view corridor that runs across the entire development. The plaza occupies a major location at the intersection of Dewdney Trunk Road and 224 Street and its proximity to the civic core makes it ideal to accommodate future civic events, create a sense of place and become a setting for a variety of uses such as commercial, cultural and public art.

The plaza should visually integrate into the rest of its context and provide a meaningful space in the City's Town Centre. The edges of the square must offer room for restaurants, cafes and retail spaces to spill out into the open space, allowing people to enjoy the views and enhance the experience of the residents and visitors.

Its main goal is to become a major urban gathering point that can be closed to vehicular traffic at specific times for celebrations, exhibitions, festivals, vendors and markets.

Through consideration of this context, the public plaza should fit seamlessly within the City's larger public open space network.

### 2.8.1 Key design elements

- A. Must be a major portion of the block, with at least two sides open to the street.
- B. Located and oriented to maximize sunlight access throughout the day and provide uses that take advantage of the sunny location (e.g. cafés and patios).
- C. The edges of plazas should be lined with active uses at-grade, including building entrances, to animate and support the open space. Spill-out spaces, such as patios should be encouraged.
- D. At least one edge is open to the public sidewalk. The remaining edges are comprised of building facades (either one building or multiple buildings), potentially with mid-block pedestrian connections.
- E. Must be located at the same grade level as the public sidewalk. Where changes in grade are an important element of the overall design and programming, clear and direct access from the public sidewalk must be accommodated, and ensure universal accessibility.
- F. Should provide continuous weather protection in the form of canopies or arcades at the perimeter of the space.
- G. Plazas may contain a primary building entrance.
- H. Small scale elements should be used to create a human scale, and to define smaller sub-areas within the plaza for ample seating and gathering in the sun and shade.
- I. Creative and dynamic solutions for ample areas of seating should be provided.
- J. Provide pedestrian scale lighting at appropriate locations.



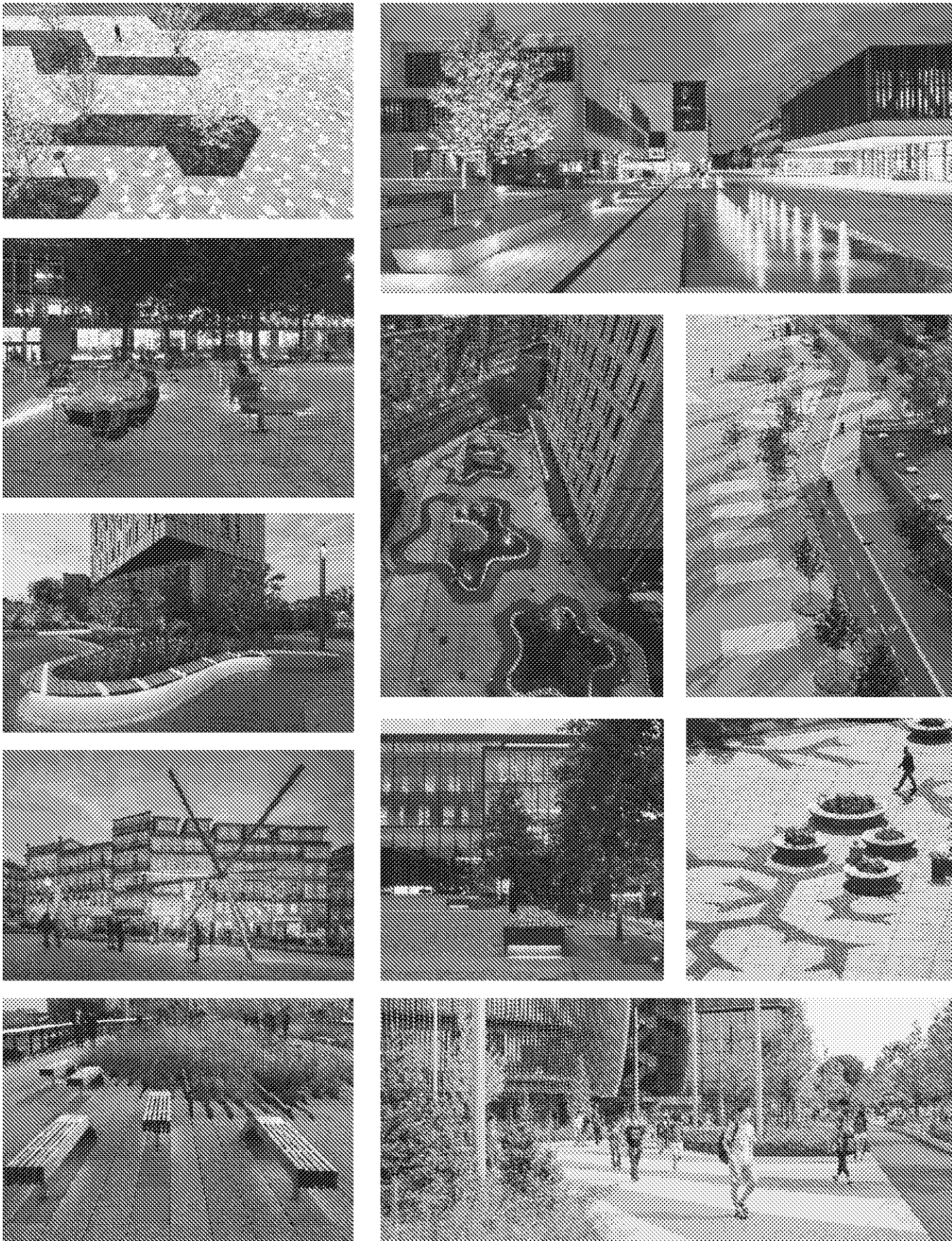


Figure 11: The images above show various approaches to large open spaces, adjacent to the site's higher density buildings. The openness of these spaces prevent a sense of enclosure, and facilitate larger gatherings (Representational of character only).

## 2.9 Neighbourhood Park

The neighbourhood park offers public open space closely associated with surrounding high-density residential developments. It will provide a communal area for a range of recreational uses including unstructured play, relaxation, and social interaction, totaling an area of 1,910 m<sup>2</sup> and includes a should include a dedicated storage space of around 25 m<sup>2</sup>.

The park is highly accessible for pedestrians being located central to the ERA development, and central to the major East-West Promenade that engages all proposed buildings. Local streets also draw pedestrians in from Dewdney Trunk Road and adjacent existing neighbourhoods.

### 2.9.1 Preliminary Neighbourhood Park Guidelines

The park design has yet to be determined but consideration should be made for large flexible seating areas, a play area for small kids, and other uses which support community programming, such as:

1. Large topographical manipulations to create a theatre and amphitheatre that should include a water feature (and possibly public art lighting features) to draw in visitors and drown out nearby noise pollution. Activating this central space will help ensure people are spending time or walking through during the daytime and evening.
2. The edges of the square should allow room for restaurants and cafes at the Office/Future Commerical Units to spill out into the open space.
3. A significant portion of the plaza/park should maintain open paved area to offer the opportunity for a variety of programmed activities such as vendors, performers, exhibitions, outdoor recreation, etc.

### 2.9.2 Preliminary Play Programming

The ERA Development will provide ample opportunity for a variety of playscapes in each phase of development. The primary goal being play spaces that not only engage toddlers, but children of all ages and physical and cognitive ability.

1. The primary element of 'challenge' refers to the physical and cognitive encounters that a play space provides. The difference between "hazard" and "challenge" must be understood when creating play settings. All due consideration should be taken to adhere to the Safety Guidelines provided by the CSA Group to ensure that the graduated challenges that provide a level of difficulty for all ages, does not create any safety hazards.
2. All play spaces should be softened with planting material that further enhances the space through sensory play & even with the integration of edible plantings.
3. Kids love a challenge, and the following are examples of equipment for testing various abilities at various levels of risk:
  - » Balance Beams
  - » Boulders
  - » Ropes Bridges
  - » Climbing Structures
  - » Platforms
  - » Slides
  - » Sound Makers
  - » Spinners
  - » Splash Pads
  - » Spring Riders
  - » Steppers
  - » Towers and Playhouses



Figure 12: Play should be maximized as much as possible within the defensible enclaves & rooftop-amenities of the lower-density building. As the development reaches higher densities, the plazas should incorporate elements to promote play (Representational of character only).



## 2.10 Semi-Private & Private Residential

Strategically design and locate landscaping to create division between public, semi-private, and private realms. This is achieved through varying heights of plant material, low retaining walls, and fences.

### 2.10.1 Semi- Private Amenity Gardens

1. Residential patios at podium level will receive additional patio space with raised planters on slab, with hedging and dense planting to screen from semi-private amenity space.
2. Amenity space will include a variety of programming: e.g. open turf space for sport play, relaxing trellised decks for passive activity, children's playgrounds, agricultural plots, etc.
3. Green roofs on podiums and towers.

### 2.10.2 Private Patios

1. Use layers of landscape moving from shorter plants to taller plants as a transition from the public realms to the private realms.
2. Use landscaping, such as hedging and taller plants, to buffer views into homes and provide privacy for yards and entries.
3. Use low fences, maximum height of 1.0m (3 ft), to define extents of private yards and provide secure areas for residents, particularly along the mews and greenways.



Figure 13: These images show the layering effect that provides a natural, soft buffer between semi-public & private patios. This can be supplemented further by fencing between public & semi-public spaces. (Representational of character only).

### 3. STREET DESIGN GUIDELINES

#### 3.1 Overall Site Circulation Guidelines

To the extent possible, landscaping along street frontages should coordinate with adjacent street properties to provide a consistent visual corridor.

##### 3.1.1 Cycle Lanes and Pedestrian Paths

These guidelines ensure the provision of adequate facilities for pedestrians, bicycles and transit to promote and facilitate alternative modes of transportation and improve circulation within and around ERA development.

3.1.1.1 The key principles to be applied throughout the development are:

- A. Provide sidewalks within residential projects, connecting from the street or driveway to unit entrances.
- B. Locate buildings to be contiguous wherever possible, and make accommodations for pedestrian circulation between adjacent businesses and sites.
- C. Where new on-street parking is located, provide pedestrian walkways that connect directly to the development.
- D. Provide pedestrian walkways with landscape amenities from within public areas to shop entries, and to other pedestrian oriented uses and destinations.
- E. All public areas must be fully accessible. Ramps, stairs, sidewalks, site furniture, crosswalks, and paving materials must all be designed to allow everyone to enjoy the community and outdoor commercial amenities.

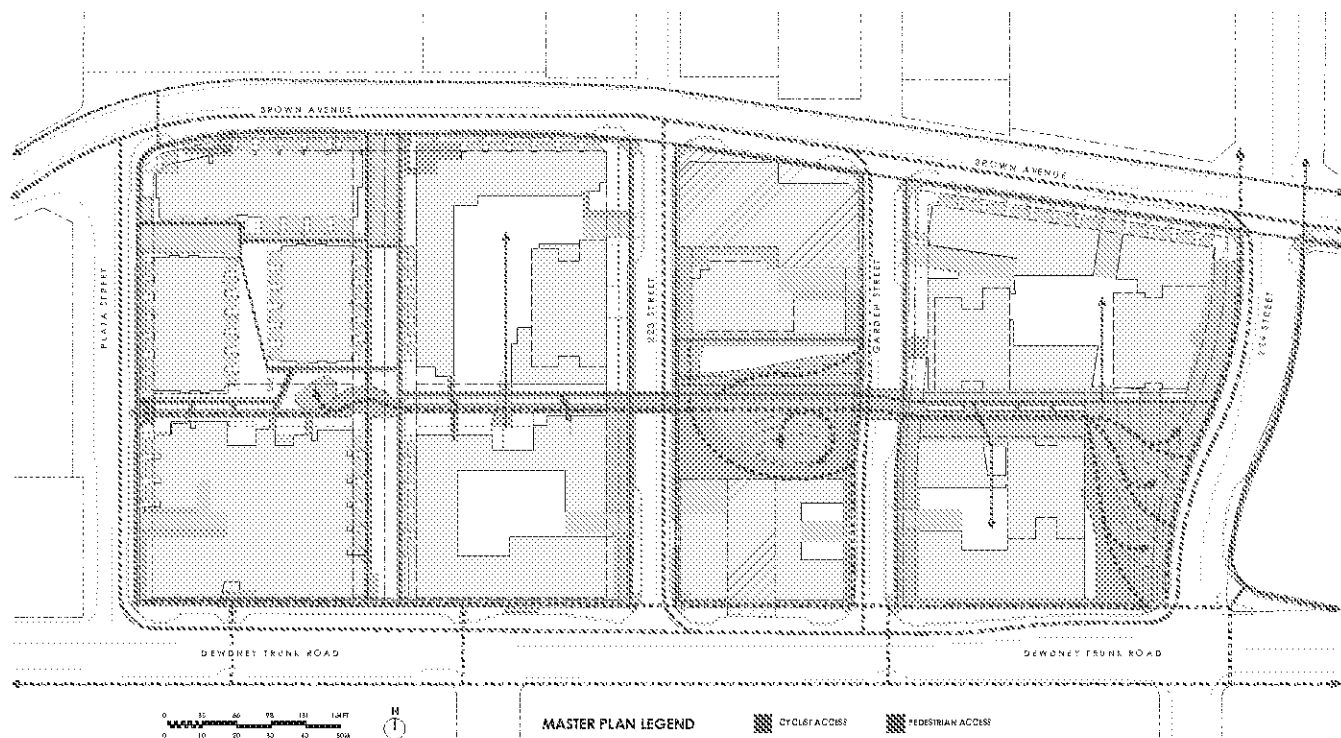


Figure 14: Cycling and Pedestrian Access Plan. Pedestrian walkways of all sizes should seek to create a visual hierarchy through the paving materials & patterns (Building footprints and features shown may vary).

### 3.1.2 Internal Pedestrian Walkways

Public/off-site right-of-ways and sidewalks should conform to CoMR Design Criteria standards & coordinated design through Servicing Agreements. Internal/on-site pedestrian connections destinations and public spaces should be designed to provide clear orientation and convenient access to residences, offices, commercial destinations and public spaces. Materials and design detailing should reinforce the hierarchy of routes, utilizing variations in both texture and colour.

1. Major routes serving larger numbers of pedestrians should be a minimum of 3.5m wide, and should be constructed with high quality paving materials such as concrete and/or stone pavers. These paths should meet BC Building Code requirements for wheelchair accessibility, either as part of the main route or on an alternate alignment.
2. Secondary routes and those leading to private residences should be a minimum of 1.5m wide, and should be constructed with materials such as concrete and/or stone pavers. Providing safe and comfortable access by individuals with limited mobility.
3. Paving at pedestrian paths should be designed to utilize concrete, stone and/or concrete pavers combined with textured finishes for ramps. The front edge of stair treads should be textured, and/or constructed with a material that visually signals the change in level. In cases where pedestrian and bike paths are adjacent, a separation may be created with roughly textured borders between the two zones.
4. Lighting may be incorporated into or adjacent to stairs and ramps wherever possible.

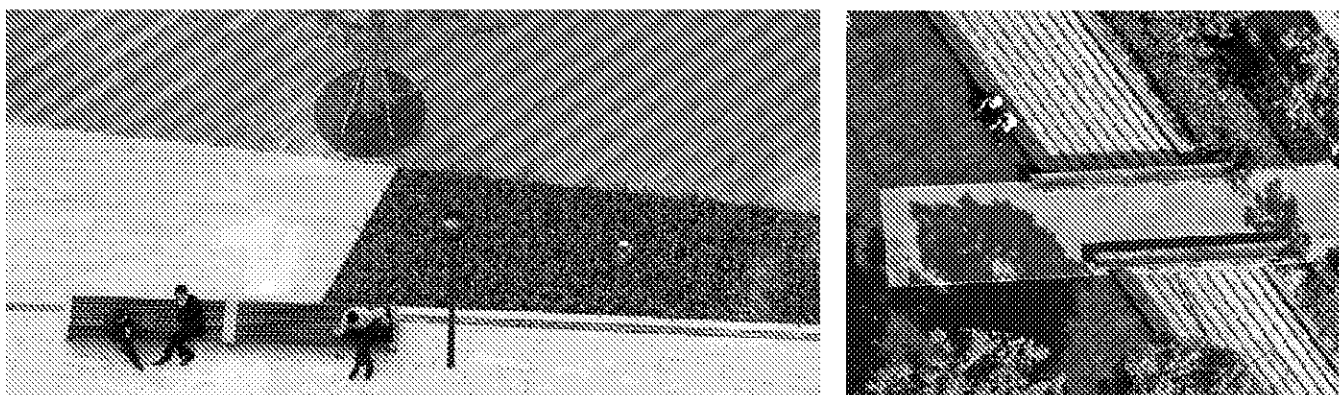


Figure 15: Pedestrian walkways of all sizes should seek to create a visual hierarchy through the paving materials & patterns (Representational of character only).

### 3.1.3 Accessibility

All public areas must be fully accessible. Ramps, stairs, sidewalks, site furniture, crosswalks, and paving materials must all satisfy the applicable and most up-to-date accessibility requirements and be designed to allow everyone to enjoy the community and outdoor commercial amenities.

### 3.1.4 Pedestrian Crossings

Streets should provide a pedestrian-scaled environment that calms traffic and allows for a variety of modes of travel. The scale of streets within the development should allow for vehicular access while creating a liveable public open space. Sidewalks should vary in width to create corner bump outs at street corners and mid-block crosswalks only where permitted by the City, with features that increase the safety and liveability of the street.

- Pedestrian Priority
- Special Intersections
- Accessibility
- Lighting



### 3.1.5 Internal Street Furniture

The concept for the site furnishing is to be modern and minimal so as to not visually clutter the open space, or detract from or clash with the architecture. Rather a family of site furnishing - including benches, social seating, bike racks, waste receptacles, bollards and lighting elements – as complementary to the building design in each 'Character Zone' and consistent with City pedestrian amenity and furniture design and standards.

For any amenities/furnishings on the abutting streets, the downtown standard site furnishing from the CoMR are required to be followed. These standards might change from time to time and they need to be in line with the standards required at the time of the application. The current standards are as follows:

- A. Tree grates – Dobney Foundry, Ivy Pattern, bare metal finish
- B. Benches – Victory Stanley RB-28 9 with arm rest in middle
- C. Garbage – Victory Standley SD-35
  - » side door opening – City to provide lock
  - » black recyclable cage/ basket option

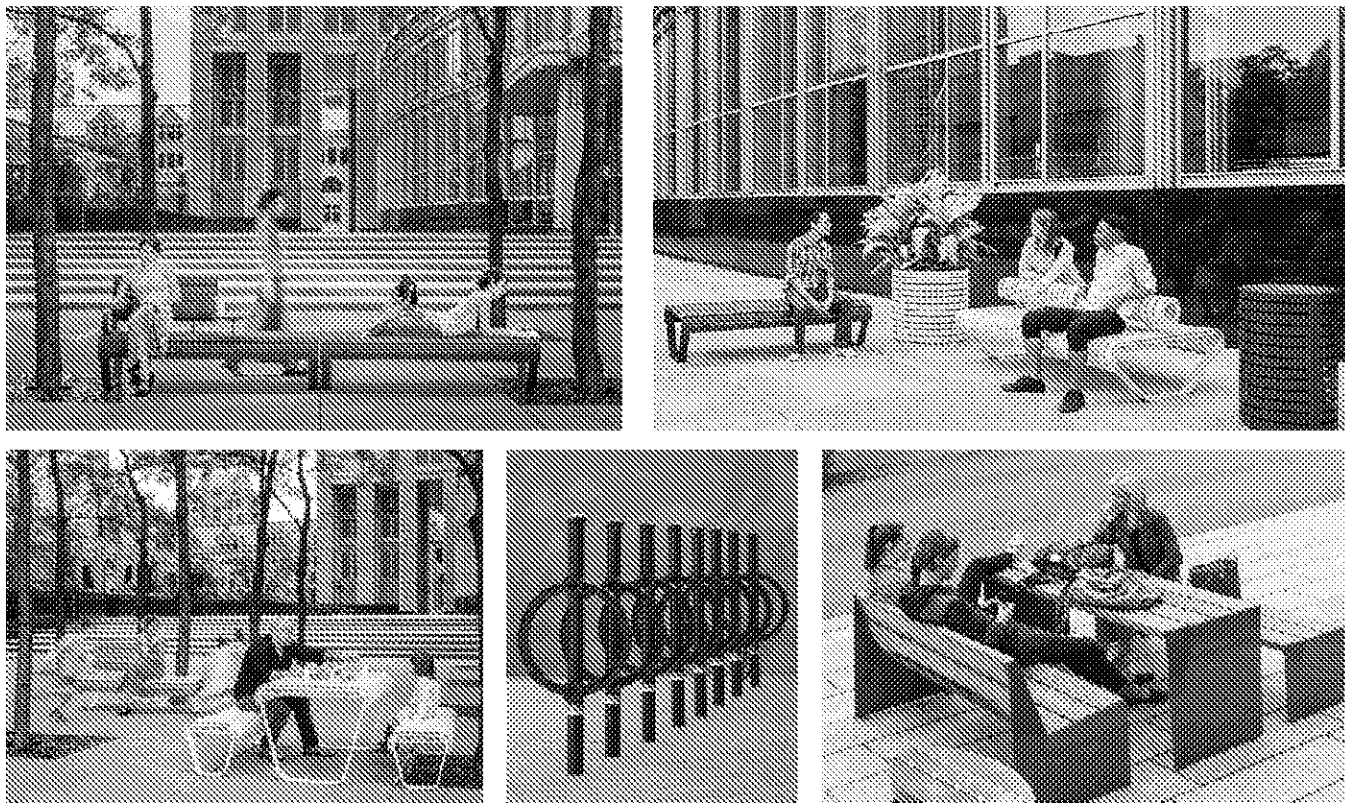


Figure 16: The images above show a variety of contemporary site furnishings that can take on the materiality & colouring of the surrounding building architecture, to create a cohesive palette (Representational of character only). Offsite furnishing should match existing CoMR standards.

### 3.1.6 Transit Shelters

If the provision of a transit shelter is required, provide transit shelter that enhances the streetscape and that offers adequate seating and shade.

The Developer should consider Translink/Coast Mountain Bus Company's employer and findings of the applicable local Traffic Study. Translink/Coast Mountain Bus Company should be consulted at an early stage of the development planning process in order to ensure that adequate service is provided and sufficient provision is made for transit routes, stops and shelters, primarily along Dewdney Trunk Road.

3.1.7 Public Art

Public art not only animates a space, it provides wayfinding opportunities and designates a meeting place, landmark, or memory to increase the awareness of place. Commercial areas, especially at the corner of major downtown intersections, are ideal places to provide public art. Public art can also be weaved into the fabric of the east-west pedestrian spine, street frontages and the Plaza to embody specific community themes and commemorative elements.

Art can add humour and, if done at an appropriate scale (and is durable, colourful, and constructed with care) it can add to the marketability of a commercial development. The use of water, ponds, and fountains will also aid in the buffering of traffic noise from 224 Street by adding an acceptable white noise to the space.

In the final phase, an appropriate selection process with the City will be established to select an artist and the major art work to be installed by the developer in the Plaza.

Appropriate lighting of the artwork shall be considered to add another dimension to the experience.

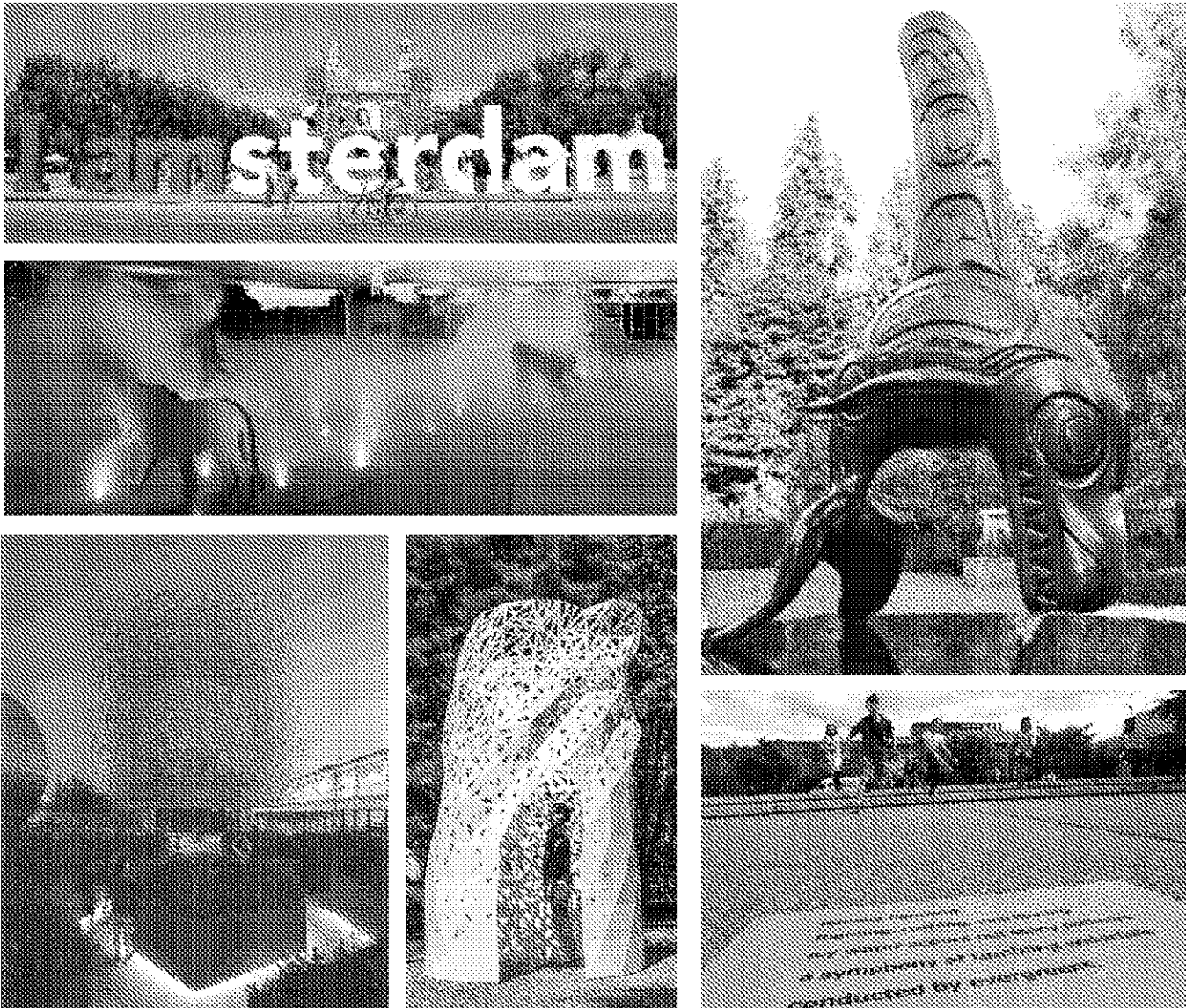


Figure 17: Public art within this development should follow the appropriate selection process. The thematic examples above show large, iconic, or interactive works indicative of what may be implemented (Representational of character only).

3.1.8 Internal Signage

As noted in section 2.5.2 Landscape Design - Key Principles G; the three elements of effective wayfinding should be followed. It is essential to establish hierarchy of signs that follow a signature theme throughout ERA Development to ensure that the design of signage for each landmark, node, and path, references this theme.

Signs should either physically relate directly to the features they represent or be grouped to create a comprehensive wayfinding system.



Figure 18: These images show signage in a variety of sizes & materials, to improve wayfinding through the site. A signage consultant may be involved to create a cohesive hierarchy throughout the development (Representational of character only).



3.1.9 Street Lighting

3.1.9.1 Lighting for Comfort

While street lighting will ultimately be dictated by Civil Engineering requirements, the choice and location of site furnishings and lighting should emphasize safety, comfort and quality, and should reflect a consistent contemporary character for ERA as noted in 'Street Furniture'. Select and locate lighting fixtures to meet CPTED design principles. City streetlights should be 4000K.

See <http://www.designcentreforcpted.org> for more information.

3.1.9.2 Lighting Distribution

CPTED principles invite more lighting wherever possible, however, impacts of lighting on adjacent areas to the immediate programmatic space, should be considered. Illuminating Engineering Society of North America (IESNA) Practice Manual notes that site lighting should not shine onto neighbouring property. Furthermore, exterior lighting fixtures that use luminaries with 1000 lumens or more should be shielded, and those with over 3500 lumens should meet the Full Cutoff Classification as described by the IESNA.



Figure 19: This series of illustrative images shows a variety of lighting applications. From area & pedestrian lighting, to accent & feature lighting. Lighting increases sense of safety and should be a primary focus of the urban design. (Representational of character only).

### 3.1.9.3 Reduction Of Light Dispersal

Efforts should be undertaken to reduce light pollution such as limiting excessive use of decorative lighting on the exterior of buildings, ensure light fixtures are oriented downwards and are designed to light wall or ground surfaces, reduce glare effects by generally avoiding the direct exposure of light sources (indirect instead of direct lighting), etc.

### 3.1.10 Tree and Vegetation

Trees and shrubs should provide a green structure and context throughout the development, with the intensification of buildings heading east, mimicked in the density and size of tree and shrub species chosen. Consideration of solar exposure, prevailing winds and wind tunnels, and other microclimate elements in relation to the specific program of each open space, should be considered in determining an appropriate plant palette.

#### 3.1.10.1 Key design principles

- A. While unobstructed lines of sight must be maintained at intersections, crosswalks and entrances to individual buildings, screening elements for unsightly service areas and utilities should be provided.
- B. Tree placement should be determined in relation to other landscape features to define public, semi-private and private space, to direct and enhance views, and to soften street edges.
- C. Landscape material should use arrangements of spaces to create outdoor spaces that explicitly address the feeling of safety.
- D. Along internal roads, install street trees 8-10m on centre with minor variations to suit entry driveways and site utilities. Trees are to be a minimum of 7cm caliper. Trees along public streets are determined by City of Maple Ridge.

#### 3.1.10.2 Suggested tree species

The following list are suggested species of trees to be used in the ERA development:

1. *Acer circinatum* (Vine Maple)
2. *Acer glabrum* var. *douglasii* (Douglas Maple)
3. *Acer rubrum* (Red Maple)
4. *Cornus nuttallii* (Pacific Dogwood)
5. *Fagus crenata* (Japanese Beech)
6. *Fraxinus angustifolia* 'Raywood' (Raywood Ash)
7. *Gleditsia triacanthos* (Honey Locust)
8. *Liquidambar styraciflua* (American Sweetgum)
9. *Magnolia* spp. (Magnolia)
10. *Picea omorika* (Serbian Spruce)
11. *Pseudotsuga menziesii* (Douglas Fir)
12. *Quercus palustris* (Pin Oak)
13. *Robinia pseudocacia* (Black Locust)
14. *Thuja plicata* (Western Red Cedar)



Figure 20: Vegetation is applied in many ways throughout the site; and should be maximized as much as possible for sustainable practice, aesthetic appeal, and restorative function (Representational of character only).



### 3.2 Specific Street Guidelines

The following streetscape guidelines address each street interface and respond to specific existing or proposed uses on neighbouring properties.

#### 3.2.1 Dewdney Trunk Road

Generous pedestrian walkways at the Commercial Retail Units (CRU) are to be designed to allow CRU to spill-out towards streets, creating friction/interaction with pedestrians. Architectural cover to provide rain protection and engagement along the storefront facades.

1. Public/off-site right-of-ways and sidewalks should conform to City's Design Criteria standards and coordinated design through Servicing Agreements.
2. Typical: A 1.2m wide exposed aggregate concrete boulevard strip will contain:
  - A. Street trees in decorative metal tree grates ("Dobney Foundry, Ivy-48CAT" pattern, bare metal finish), in a continuous soil cell trench as required for sufficient volume (10 cbm/tree);
  - B. Site furnishings, including benches ("Victory Stanley RB-28", with centre arm rest, Black), garbage receptacle ("Victory Stanley SD-35", side door opening c/w lock provided by City, Black, and option for recyclable cage/basket) or as directed by the City.
  - C. Street & pedestrian lighting standards.
  - D. A 2.0m wide broom finished commuter sidewalk will be adjacent to the property line. Street parallel parking
3. Additional to the off-site streetscape design, Plaza Street will have casual surveillance from residential patios that are vertically separated from the street for privacy.

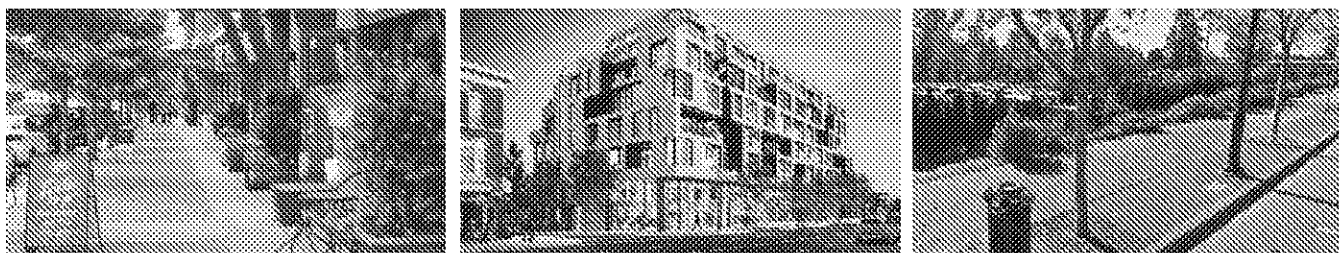


Figure 21: The images above show an overall approach to streetscape design; with a mix of commercial and street-facing residential units. (Representational of character only).

#### 3.2.2 Brown Avenue

Casual surveillance from Residential patios vertically separated from street for privacy.

1. Street trees will be part of a continuous lawn boulevard
2. On street parking
3. A multi-use path for cyclists and pedestrians
4. Boulevards either side of Avenue

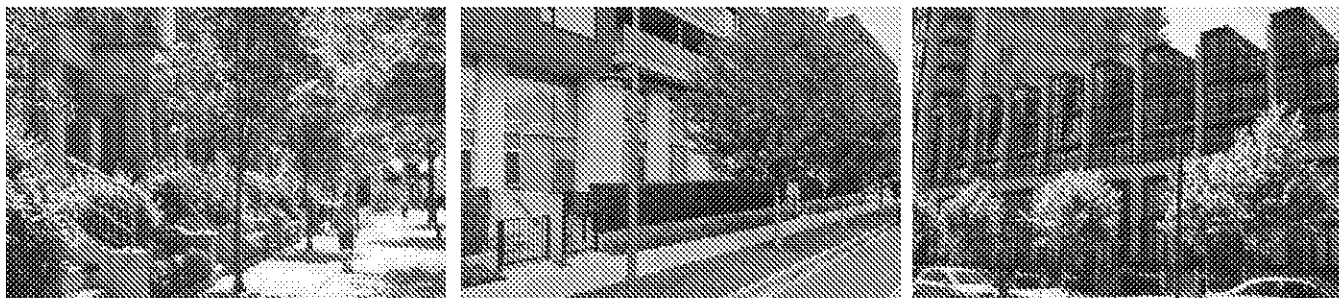


Figure 22: The images above show an overall approach to urban design; with an emphasis of quieter, ground-oriented residential units. (Representational of character only).

### 3.2.3 Mews / Pedestrian Way

Transition from raised residential private patios, to at-grade barrier free public space.

1. 2.4m (8') wide pedestrian path
2. Grass shoulders expand path surface to 4m wide to accommodate fire truck access
3. Residential patios either side of path are raised to provide 'eyes on the street'
4. Live work units (more urban in nature) adjacent Dewdney Trunk Road

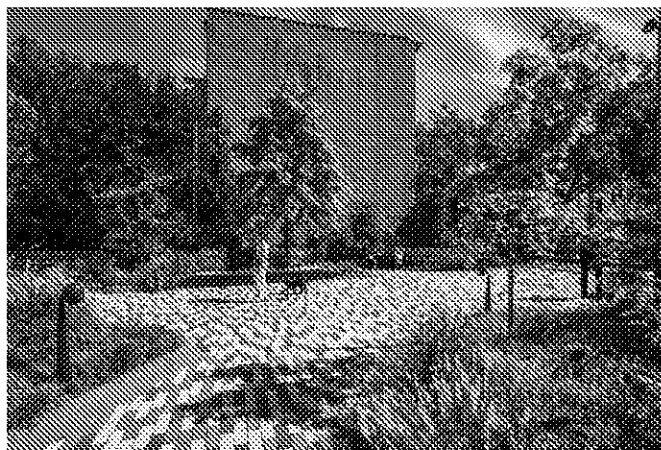


Figure 23: The images above show an overall approach to urban design; with an emphasis on wider, but quiet, shared public corridors. (Representational of character only).

### 3.2.4 223 Street / Garden Street

Collector thoroughfares for pedestrians. Pedestrian-oriented with measures to reduce vehicular speed. The future of Garden street will be subject to review by the City and may be eliminated.

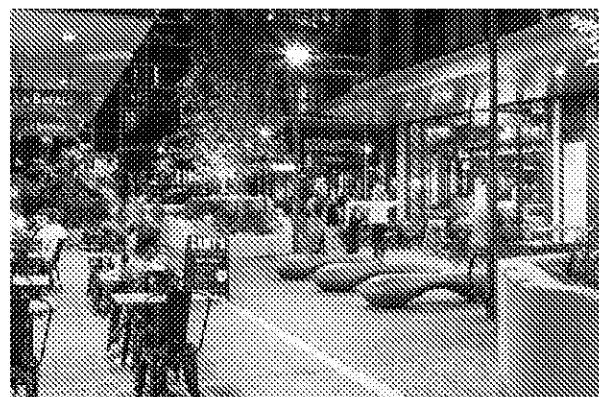


Figure 24: The images above show an overall approach to a more live-work urban form; with smaller commercial-residential units partially separated from the public realm. (Representational of character only).

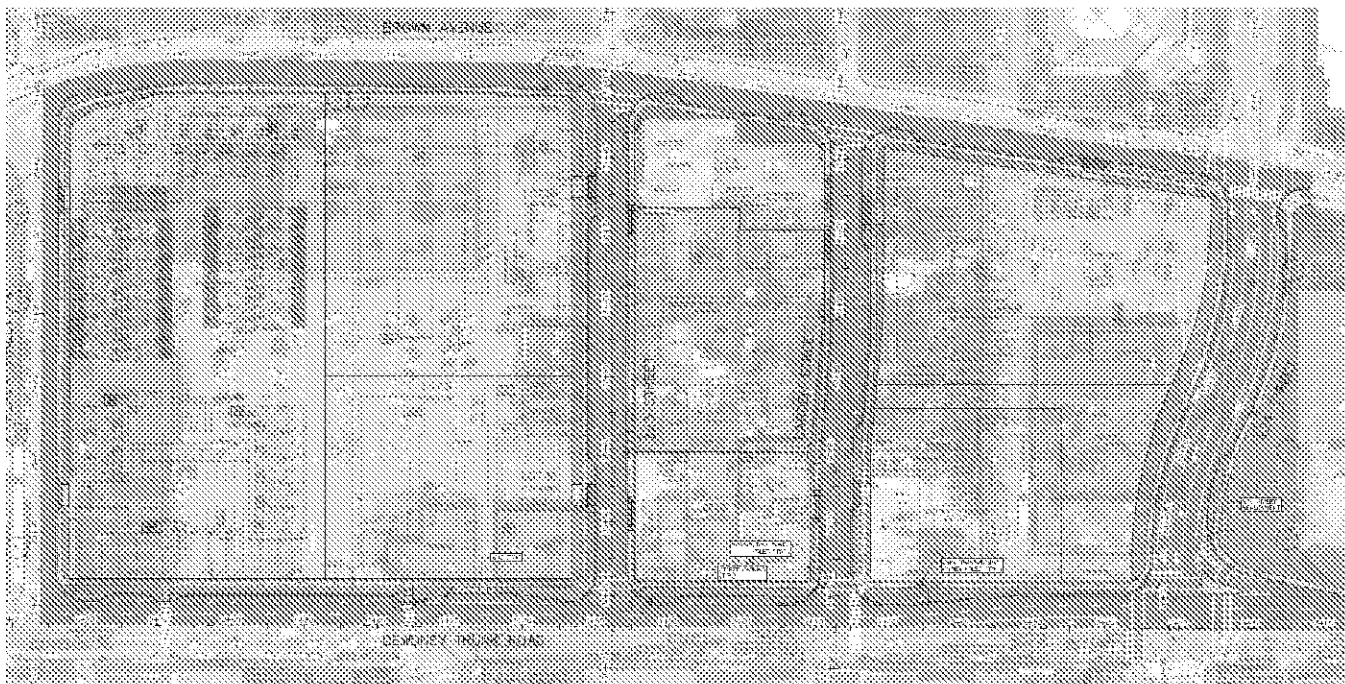
### 3.2.5 224 Street and New Public Corner Plaza

The realignment of 224th Street was driven by the desire to improve a problematic intersection at Dewdney Trunk Road and improve the effectiveness of the vehicular circulation.

It was also proposed to create a more substantial public plaza in keeping with the OCP and designating this area as a new urban gathering point for Maple Ridge.

Pedestrian first orientation to be the entry and extension to the public plaza.

1. 224 Street is realigned thus creating an open space on the east side
2. Built form is pulled back from the street to create an open space that spans the street
3. Paving treatment may extend across the street (contingent on additional study and coordination with the City) and could be closed for festivals. Plaza and pedestrian path to the east could accommodate a farmer's market and/or potential location of public art.



*Figure 25: The intersection of 224 Street and Dewdney Trunk Road promises to become a major urban gathering point that can be closed to traffic at specific times of year for celebrations and civic events.*



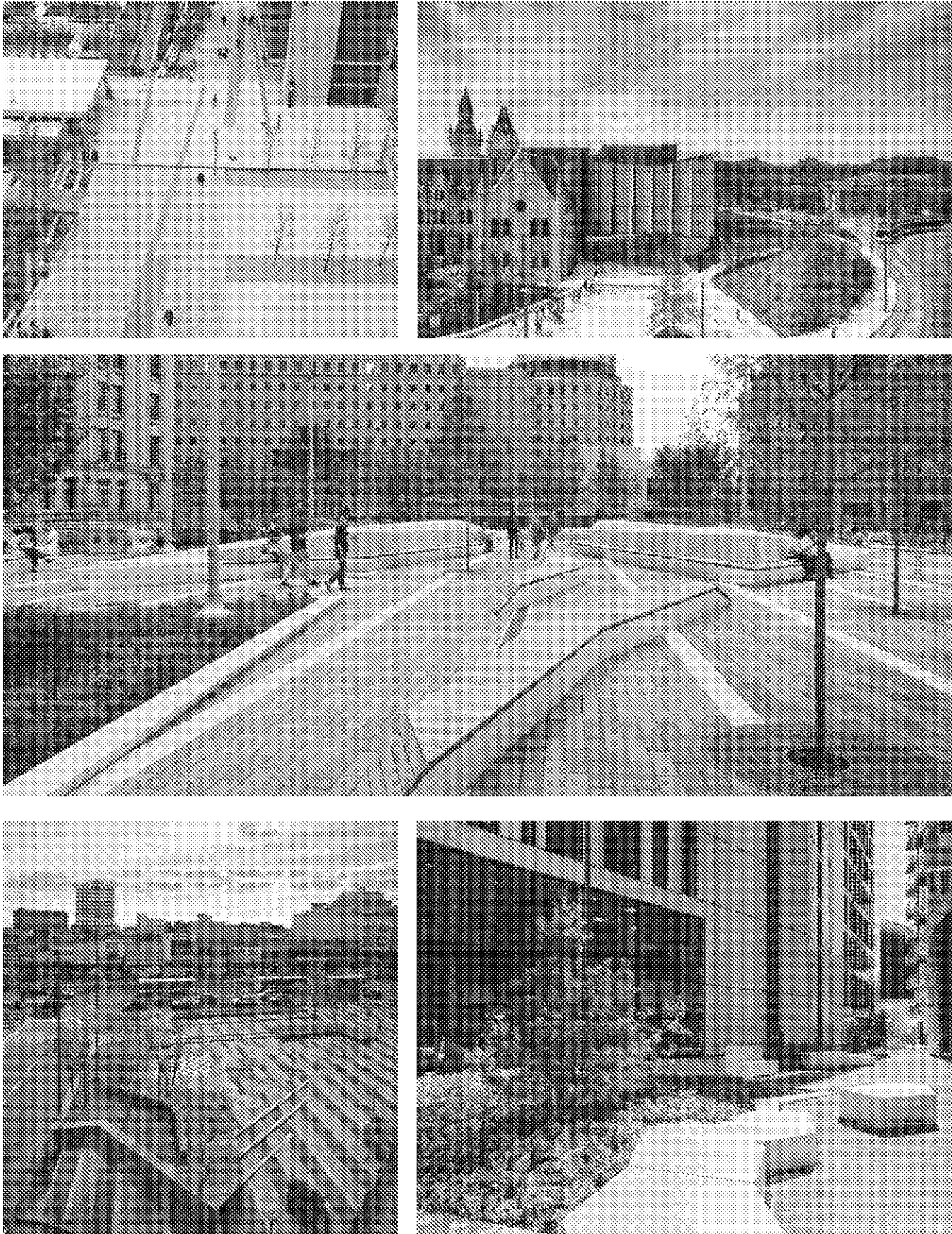


Figure 26: The images above show an overall approach to a more urban form; with large open public plaza for congregation & events (Representational of character only).

### 3.2.6 East-West Promenade

#### 3.2.6.1 At Residential

1. 2.4m (8') wide
2. Heavily planted
3. Finer-grained in residential scale & character
4. Places to sit and gather
5. Incorporation of rain water infiltration

#### 3.2.6.2 At Neighbourhood

1. Path widens at public park/plaza
2. Incorporation of extensive tree canopy
3. Plaza space to south (adj. Commercial/Office) is multi-use  
park space to north is a neighbourhood & community destination
4. Park has berm to create interest & play opportunity on a flat site
5. Incorporation of rain-water trench & water feature in plaza

#### 3.2.6.3 At 224 Street

1. Path widens to 3m (10')
2. Single row of large trees rain-water trench takes on a more urban character more hardscape  
& urban in nature adjacent to commercial

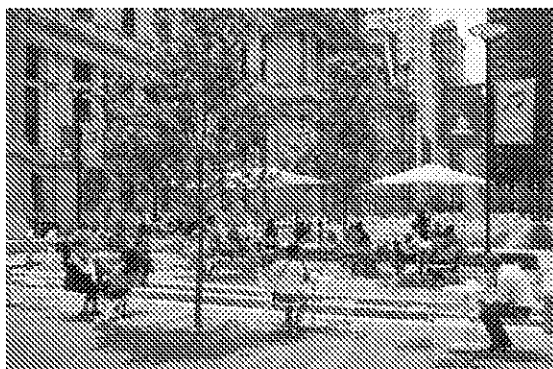
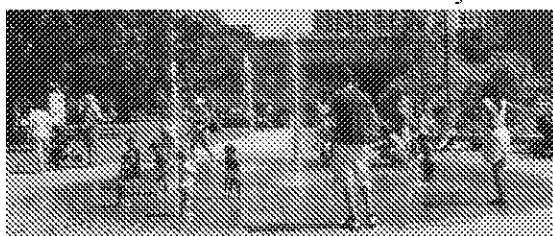


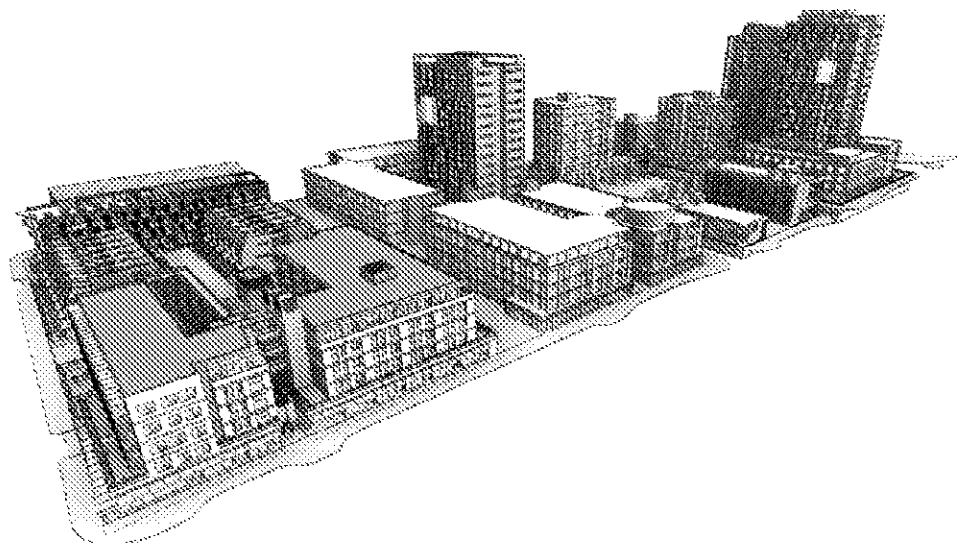
Figure 27: This series of images shows the potential programming of the nodes along the east-west central promenade (Representational of character only).

## 4. BUILDING DESIGN GUIDELINES

### 4.1 Massing and Building Form

As the existing context has not yet incorporated the principles of the OCP, this master plan strives to create an appropriate response through the seven different phases. To mark the progression through the site, the massing, density and height of the buildings increases gradually as they get closer to the intersection of Dewdney Trunk Road and 224 street, the most urban part of the site and home to a new civic core of Maple Ridge.

- A. Throughout the development, medium-sized buildings are typically stepped back above the first 3-5 storeys of the building creating a lower podium that reduces the overall mass of the buildings at street level. This design solution defines the difference of uses within the building, such as residential units over retail and office spaces.
- B. Street walls (building faces directly adjacent to a street) should be between three and six storeys in height. If less than three storeys they are usually not well defined, and over six storeys they can be oppressive to most people. In most instances, street walls should step back above the third to fifth floor.
- C. The point towers should strive to achieve a height to width ratio of no less than 2:1 to 3:1 for the 21-storey building. When not possible, the massing of the building should encourage visual verticality through key architectural moves.
- D. The podium should be at least two but not more than five storeys high. This morphology permits small-scale retail storefronts on the commercial streets. This duality of commercial and residential creates a strong street presence, with the tower providing residential density that enlivens the neighbourhood and makes the local commercial activity economically viable.
- E. This form of building can also accommodate two storeys of ground-level apartments above the podium level without creating an overwhelming street front as long as the upper units are setback from the cornice line.
- F. A clear difference between the entrances to the towers and the residential units should be maintained throughout the project, keeping the street fronts for retail and residential units.



*Figure 28: The varied heights give the development character and create visual interest. The residential towers are strategically placed on the most eastern side of the site to reduce the shade over the community park and to provide a visual dynamic for the development (Representational of character only).*



## 4.2 Height

The master plan incorporates a variety of building heights, with the tallest being – around 21 storeys – located in the southeast of the site, in close proximity to the proposed new public plaza. Tower buildings decrease in height to the west creating an overall skyline of decreasing height transition across the site.

Mid-rise buildings range in height from 4 to 6 stories along Dewdney Trunk Road and Brown Avenue.

The heights have been intentionally varied to create a dynamic profile across the development. The towers are staggered to provide good site lines for the units and ensure that maximum sunlight penetrates deep into the site.



*Figure 29: Upper floors on 5-storey buildings should be stepped back to assist in reducing the massing. Corners at intersections may deviate from this pattern to help establish a stronger street presence in these locations.*

Taller buildings are offset from one another to limit views from one building to the other. The staggering arrangement, along with the north-south orientation of these buildings also help mitigate shadows into the site.



*Figure 30: Master Plan Shadow Study. Taller buildings are strategically placed to minimize shadow impact throughout the site (Building footprints may vary).*

### 4.3 Setbacks

Several setbacks to roads are increased in order to accommodate green pedestrian passage and to accommodate green infrastructure and/or to provide sufficient space to allow larger trees to grow in the years to come.

- A. The typical residential setback is between 3 to 6 meters (10-12 ft), which is sufficient to provide an outdoor patio or stoop that can accommodate outdoor living and/or seating.
- B. Other commercial or retail areas have a minimum 0.6m (24 inch) setback along Dewdney Trunk Road which will increase the overall width of the sidewalk zone, while allowing for outdoor seating, signage or other requirements, while not impacting the path of travel on public sidewalks.
- C. A setback between each phase of approximately 7.5 to 18.3m (25-60 ft) shall be maintained, to accommodate sufficient breathing room between buildings and with significant landscaping or other landscape features.
- D. Lastly, an approximate 19.2m (63 ft) setback is provided on the east side of Phase 7 to ensure sufficient area to create a civic gathering space that will extend across the street, integrating the development into the rest of its context.

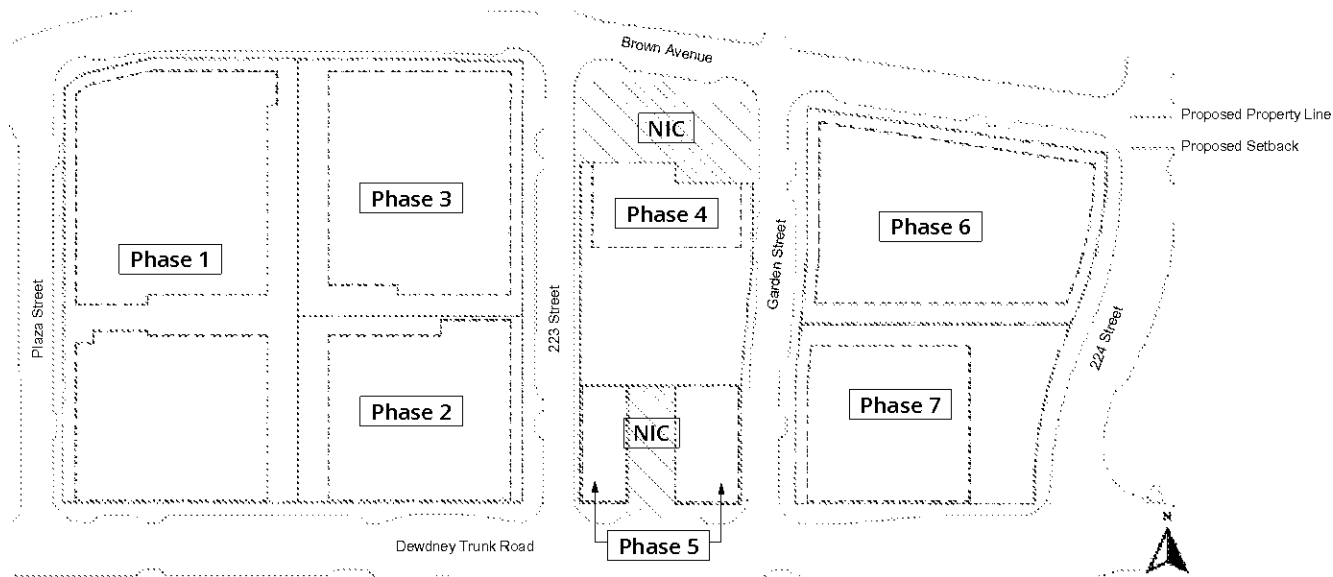


Figure 31: Master Plan Setbacks. The established setbacks on each phase outline the overall space required to create vibrant links between public and private areas.

### 4.4 Garbage / Recycling Staging Areas

#### 4.4.1 Internal Storage

The solid waste and recycling storage in a building must be accessible to all occupants of the development, and be clean, well lit, and conveniently located for users to get access to, including those with restricted mobility. The location of such a room or rooms is preferred to be within the underground portion of a building, such as an underground parking level, permitting convenient staging on refuse collect day.

#### 4.4.2 Staging Areas

Especially for multi-unit residential buildings, if containers are stored underground, they will likely need to be relocated above ground for servicing on collection day (by building staff or a container pullout service provider). Thus, a temporary storage area should be incorporated in the ground level portion of

the building and site plans provided for collection vehicles to manoeuvre and to collect materials without impeding street, parking entrances and sidewalk circulation (ensuring no height clearance issues).

#### **4.4.3 Other Design Considerations:**

- A. Ensure that waste collection vehicles have ample room to maneuver at the site planning stage to ensure that these functions do not spill over into either the public right-of-way or public spaces.
- B. Both internal and staging areas are to comply fully with the City Waste Collection policies.
- C. Any portion of the staging area at grade must be within private property sufficiently setback from streets and pedestrian areas.
- D. Loading areas, garbage containers and recycling containers shall be screened from any adjacent residential lot or from overlook from upper storeys, to a height of at least 2.5 metres [8 ft.] by buildings, a landscaping screen, a solid decorative fence, or a combination thereof.

#### **4.5 Building Sustainability Strategies**

As these guidelines are further developed in City policy, these will be reflected in every phase. In the phases where it is mandated, these will apply and may take the form of:

- 1. Increased envelope thermal performance (both walls and fenestration),
- 2. Increased energy efficiency of mechanical systems such as the inclusion of HRV's,
- 3. Inclusion of more passive strategies such as window shading on the applicable orientations,
- 4. Green roofs
- 5. Use of low or classified Zero-VOC construction materials and finishes,
- 6. Re-Use, Recycle or Use Rapidly Renewable materials
- 7. Choose Third-Party Certified Products, etc.

#### **4.6 Parking Considerations For All Phases**

**4.6.1** All vehicle parking is to be provided in a below grade or above ground parking structure.

**4.6.2** For above ground parking structures, the design of these parking structures should blend in with architectural elements of the main commercial or residential building, including the use of architectural or decorative grille or other similar elements for wall openings. Steel mesh is not recommended.

**4.6.3** Parking provided is to be separated between commercial and office use parking spaces (i.e. non-residential parking) and residential parking spaces (i.e. parking for residents and parking for their visitors).

**4.6.4** There will be a security gate between the non-residential and residential parking. The security system will be designed to allow access to visitor's parking spaces by visitors, but retaining security to the portion of the parking spaces for residents should be recommended where the overall building configuration makes it possible.

**4.6.5** Residential parking shall separate parking for visitors and parking for residents.

**4.6.6** A gate into the parking area from the street level is optional as determined by circulation and security considerations.

**4.6.7** Pedestrian access, including those with restricted mobility, will be designed to provide safe and convenient access between the separate parking areas and the portion of the building the parking is intended to serve. Access from residential parking areas will take into consideration families with children and individuals using mobility assistance equipment.



4.7 Building Typologies

4.7.1 Multi-family Residential

4.7.1.1 Key urban design role

These buildings are the face of the residential neighbourhoods. They present opportunities for highly-varied architectural responses through differences in form, scale, materiality and character, contributing to a rich pedestrian environment and scale.

4.7.1.2 Form and character

1. Ground-level apartments: Designed to blend into the whole building facade, the two storey units at grade will contribute to a richer, more human scale at the pedestrian level. With front doors 0.6m (2 ft) above the street level, these two-storey units generally follow the principles of a townhouse design. Raised entry areas provide comfortable semi-private space for a garden and patio.
2. Massing: A strategy of setting floors back at the upper levels will help to soften the building massing and increase access to daylight. Setbacks are especially encouraged in mid-block courtyards for daylighting and privacy as well as providing generous deck space for residents. On all buildings, where possible, interesting roof shapes are encouraged to enrich the overall texture and visual amenity of the development. These may include curving forms, deep overhangs, sloped planes and the like, appropriate to an understated modernist expression. Stair enclosures serving roof decks can also add to this diversity of form.
3. Green roofs: It is anticipated that some green roofs will be provided on most of the concrete buildings. This principle would further enhance the character of the building significantly. Green roofs should be designed as visual amenity as well as an attractive environment for outdoor common space. In wood framed buildings, roof vegetation should be incorporated in self contained planters.
4. Transparency and legibility of indoor public spaces: Common interior spaces such as entry lobbies, stairs, exercise rooms, lounges and the like should be afforded as much transparency to the exterior as possible, encouraging a visual connection between these spaces and the exterior community.



Figure 32: These images convey five different approaches to similar sized buildings that still relate to one another through their simple geometries and common materiality (Representational of character only).

## 4.7.2 Mixed Use Buildings

### 4.7.2.1 Key urban design role

On commercial streets, including Dewdney Trunk Road and 224 St, the multi-family form is combined with retail and service commercial uses at the ground floor. This provides a continuum of mixed-use frontages that attracts pedestrian activity and creates a vibrant public realm.

### 4.7.2.2 Form and character

1. Design expression: The potential for mixed-use buildings to enrich the Town Centre is high and should be incorporated wherever possible in the architectural design. In responding to the urban design roles mentioned above, the design should:
  - A. Provide a comfortable scale for a walkable shopping environment; commercial tenancies (i.e. storefronts) are to be divisible into increments of between 5.5 and 7.5 meters.
  - B. The emphasis should be towards boutique style commercial tenants and not the typical large anchor tenant (The frontage would be greater, providing more exposure). Creative storefront merchandising will also be necessary to provide views into these spaces, as well as views to the street from within the commercial units.
  - C. The residential portions of the mixed-use buildings will generally follow the design principles for multi-family buildings as previously described.
2. Massing:
  - A. Generally 4 to 6 storeys in height. The primary aim in the design of mixed-use buildings is to create a streetscape composed of a series of different but complementary building frontages ranging in width from 7.5 to 20m.
  - B. An overheight ground floor of approximately 3.6 m provides good flexibility for a variety of retail uses as well as streetfront space.
  - C. Setbacks at the uppermost residential storeys of mixed use buildings will be encouraged to create a comfortable street level scale and increased privacy for residents. Setbacks should be used to help differentiate between the commercial and residential parts of a building. Each use should be given a clear expression through a change of materiality, a change of plane or a combination of both.



Figure 33: These images demonstrate various approaches to delineating between the ground floor commercial and upper floor residential uses. (Representational of character only).

### 4.7.3 Townhouses and Ground Level Apartments (T&GLA's)

#### 4.7.3.1 Key urban design role:

The T&GLA's play a valuable role in the neighbourhood character of ERA, contributing a smaller-scale form. Although stand-alone T&GLA's is not the prevalent typology in the development, most residential buildings include this type of unit concept at grade to extend this intimate, pedestrian-friendly character throughout the public realm.

#### 4.7.3.2 Form and character:

1. **Massing:** T&GLA's forms and articulation should clearly distinguish individual units both in plan and elevation. This can be achieved in a variety of ways including sloped roof, recesses, vertical 'framing', etc. Consistent with the concept of a small-scale form, setting back the frontage above the second level is encouraged to further reinforce the legibility of each unit. Architectural elements such as individual roofs and entry canopies should be considered. Simple roof shapes can provide further architectural interest.
2. **Access to outdoors:** Semi-private spaces should be designed to further distinguish individual units while providing a rich landscaped buffer between units and the public realm. At upper levels, balconies, terraces and roof decks are strongly encouraged to provide a more private outdoor experience as well as providing increased articulation.
3. **Simple, clean expression:** T&GLA's designs should reflect the current modern trends around the world - using solid planes to contrast with generous areas of glazing and clearly defined outdoor spaces.



Figure 34: These images demonstrate the successful incorporation of T&GLA's in larger scale buildings (Representational of character only).



#### 4.7.4 Tower

##### 4.7.4.1 Key urban design role

The tower typology is paramount in the overall development fabric. Location and height are strategically utilized to:

1. Provide a three dimensional composition that 'frames' the mixed use core of the Central Neighbourhood, giving it legibility from Dewdney Trunk Road; and
2. Assist the pedestrian with neighbourhood orientation — for example, the tower at the intersection of Dewdney Trunk Road and 224 street will clearly mark the location of the proposed Public Plaza.

##### 4.7.4.2 Form and character

1. General expression: Anticipating the coming changes to the BC Building Code in addressing thermal performance, ERA looks to introduce an aesthetic for towers that recalls the simple, strong and clean expression of elements associated with modern European design; with the goal of achieving balanced window to wall ratios in alignment with current thermal performance requirements, towers should strive to achieve a composition of simple forms and massing.
2. Massing: Tower floor plate sizes are based on an average gross floor area of 650 m<sup>2</sup> (7,000 sq ft).
3. Articulation and orientation: It is intended that towers be generally oriented with the long axis running north-south to optimize access to daylight and minimize the impact on upland views.
4. Consideration should be given to how the towers are terminated at the upper levels. Tops of towers should be articulated to ensure continuity of the design throughout the buildings.



Figure 35: The images above showcase a variety of tower designs that utilize punched openings and a balance between glazing and solid walls (Representational of character only).

## 4.7.5 Commercial/Offices

### 4.7.5.1 Key urban design role

The two commercial buildings in Phase 5 along Dewdney Trunk Road play lead roles as gateways marking entry to the new green space located to the north of the site. In response to their high profile locations, these buildings are intended to provide for uses that will help stimulate the vitality of the pedestrian realm and invigorate the neighbouring green space.

### 4.7.5.2 Form and character

#### 1. Design expression:

- B. In the context of the contemporary architecture proposed throughout ERA, the commercial buildings provide an opportunity for unique and innovative design. A dramatic expression responding to their prominent placement and their specific use is expected to set them apart and enhance the architectural diversity of the development. At the same time, these buildings should complement the scale, giving careful attention to articulated streetwalls that enhance the pedestrian experience.
- C. Following the principles of good sustainable design, these buildings will also strive to deliver an enhanced workplace for occupants. Access to daylight, glazed stair enclosures to encourage their use, access to outdoor space, operable windows and envelope systems designed for optimal energy conservation are anticipated initiatives that will impact the exterior building design.
- D. Addressing the above goals, designers are challenged to create a new model for commercial buildings in the ERA development.

#### 2. Massing:

Significant ground floor height gives the commercial buildings a scale appropriate to their gateway roles and to their frontages on Dewdney Trunk Road. Designs should seek to create visually engaging frontages at this level, ensuring these significant masses are broken down into increments to create a more comfortable scale for pedestrians. Active spaces should be located at the perimeter, as opposed to closed individual offices. Public lobbies, located along Dewdney Trunk Road, Garden Street and 223rd Street should be highly transparent and clearly visible for pedestrians.

#### 3. A livable workplace:

- A. Occupant comfort is a primary aim for these buildings. As one of the simplest but most effective ways of creating livable study and office environments, good daylighting should be an integral part of the building design. This starts with work stations located within 9m of exterior glazing but can be optimized by bouncing light deeper into the space using light shelves and tempering glare with the use of exterior sunshades. Centre atrium can also increase recess to daylight.
- B. Control of one's workspace environment is another aspect of user comfort and can influence the external expression. Options for occupants include operable windows, mechanical systems zoned for smaller areas where they can respond more quickly to individual demands and access to outdoor space.
- C. Balconies, terraces and roof decks are all opportunities for a breath of fresh air, a coffee break or an ad hoc meeting. Their contribution to the exterior architecture can also be very positive, helping to articulate the mass in a variety of ways as well as bringing life to the street.



Figure 36: These images demonstrate street-friendly commercial buildings that enhance the vitality of the neighbourhood (Representational of character only).

## 4.8 General Residential Building Design Guidelines

### 4.8.1 Building materials

- A. Building elevations emphasizing one or two natural building materials, in addition to glazing, are strongly encouraged
- B. Accent materials providing colour, interest and contrast to the overall building design are encouraged, e.g. ceramic tile or glazed brick and coloured glass and spandrel panels are recommended for their durability and wide range of colour choices
- C. Well crafted, durable materials that support sustainability and Town Centre themes are expected throughout
- D. Natural building materials including wood, stone and brick should be included in the expression on lower floors and along the streetwall. Exposed concrete is acceptable to use on the commercial areas but in a smaller amount
- E. Functional screens, shading devices and other passive solar design elements that complement the architecture are recommended
- F. The use of red brick is discouraged
- G. The level of design quality required for streetscape elements should be maintained
- H. If heavy timber and engineered wood elements are used especially along the base of the building and at entrances they should be protected from weathering using best building practices and appropriate finishes that preserve the natural colour and texture
- I. Colours should be chosen to complement the palette of natural stone and wood: tones of grey are considered most appropriate
- J. Soffit materials should be consistent with the building's overall durability and quality
- K. Wood, fibre cement or metal soffits are preferred where feasible
- L. Clear vision glass is preferred over tinted products
- M. Material transitions should avoid a "wallpaper" look



Figure 37: Examples of different material expressions (Representational of character only).



4.8.2 Building colours

The use of colour contributes greatly to the sense of a building's cohesiveness within its neighbourhood. Generally, successful colour schemes are ones that use two or three harmonious colours: a predominant body colour, a second colour for large architectural elements (such as doors), and a third for decoration. The colour of the roof material is an important component of the building's overall colour scheme. The following points should be always considered:

- A. The primary colour palette within the Town Centre should be more subdued than highly expressive and trendy.
- B. The use of brighter, complementary accent colours to offset low winter light levels and increase visual interest.
- C. Colour combinations should strive to be subtle and as timeless as possible.



Figure 38: These images demonstrate the proper use of colour to achieve accents in the facade of various buildings and elements (Representational of character only).

#### 4.9 Commercial Retail Units (CRU)

These guidelines ensure that the commercial retail units (CRU) located at the ground floor of the building create an active pedestrian realm, that is an engaging and well-populated environment with a variety of uses and activities.

##### 4.9.1 CRU design principles.

The following principles should be implemented in all phases of the development where CRU's are included:

- A. Provide a minimum of 0.6m setback from the property line for the storefront facades.
- B. Locate active commercial uses on the ground floor adjacent to the sidewalk, including retail, restaurants, and personal service uses. Ground floor street frontage space is to be predominantly for active, pedestrian-oriented uses.
- C. Provide continuous storefront windows, open air store frontages, and frequent, highly visible entrances for ground floor commercial uses adjacent to the street and sidewalk.

##### 4.9.2 Materials and proportions

- A. Glazing is encouraged in all retail units and should be predominant in all the building facades.
- B. High quality and durable trim materials, such as anodized or painted aluminum, are permitted and encouraged for windows and door trims.
- C. Continuous store windows and frequent, highly visible display storefronts along the street are a paramount feature as they provide visual interest and promote walkability.
- D. Open air store frontage type is acceptable along the internal corridors of the development. It brings the retail activity to the street, engaging pedestrians who walk by.
- E. Retail glazing is to be clean and not include frosted glass.

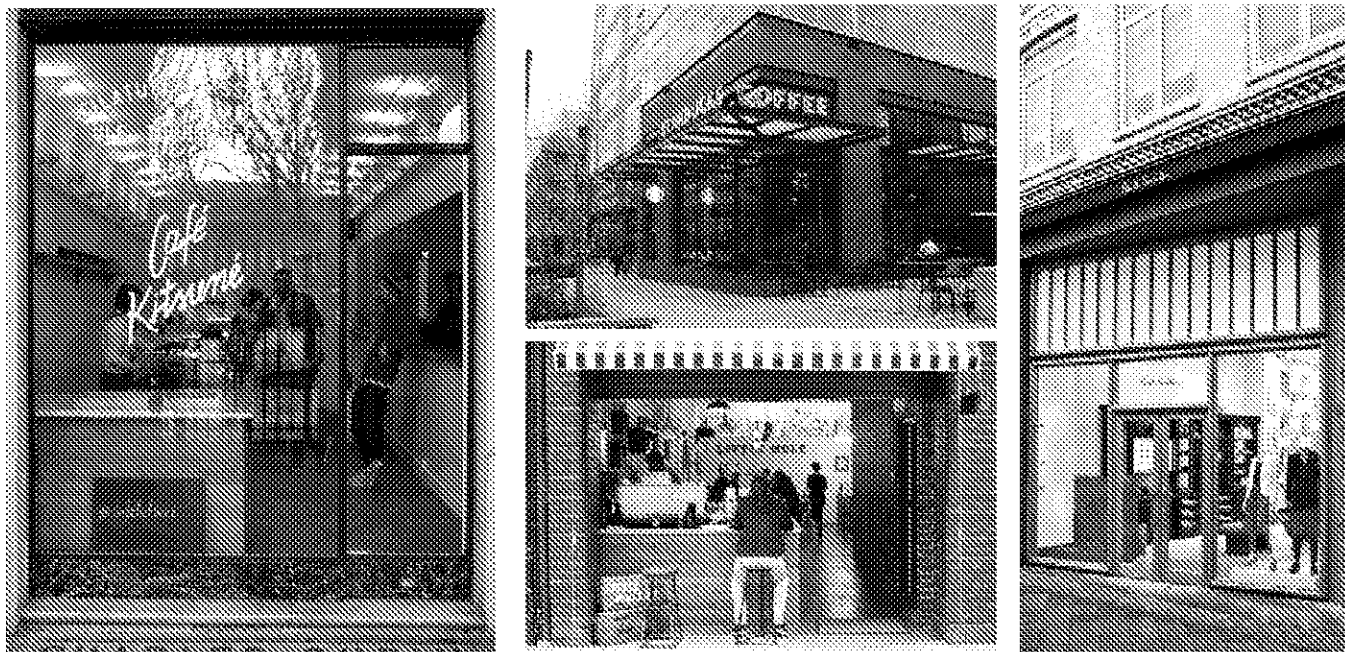


Figure 39: The images above show a variety of glazed entrances as examples of what can be implemented in the various types of CRU's spaces throughout the development (Representational of character only).

#### 4.10 Entrances

The design of an entrance has a significant influence on both the appearance and functionality of the building. Entrances provide a focal point for residents and visitors and serve to welcome people into the building.

An entrance should be clearly visible and accessible at all times. It should promote and create a positive impression for all building users and make them feel welcome. If an entrance is hard to find or if it is difficult to access due to heavy doors or narrow door width, it creates a poor first impression and may make some people feel less welcome or even excluded.

The position of an entrance may be highlighted with architectural features such as a canopy or a door recess. Emphasis should be placed on recessing accesses to avoid encroachment into City's right-of-ways with awnings or canopies. A change in surface texture of the pavement or forecourt may help to signal the location of an entrance, particularly for people with visual difficulties.

##### 4.10.1 Key Design Elements

All building entrances within the development would take into consideration the following:

- A. Locate all customer entries and entryways to be directly visible from the public sidewalk, and accessible from public and private walkways. Corner buildings are encouraged to have corner entries but it is not mandatory.
- B. Provide a pedestrian walkway from the public sidewalk to the entry frontage of buildings set back from the street edge.
- C. Design public street facing residential facades with individual entries, such as steps, porches, and paths from living units to the street to help break down the scale of multi-unit buildings.
- D. Audio clues, such as a small fountain or rustling plants, and olfactory features such as fragrant plants can also assist.
- E. Artificial lighting can highlight the entrance to a building and make it more obvious at night for everyone.

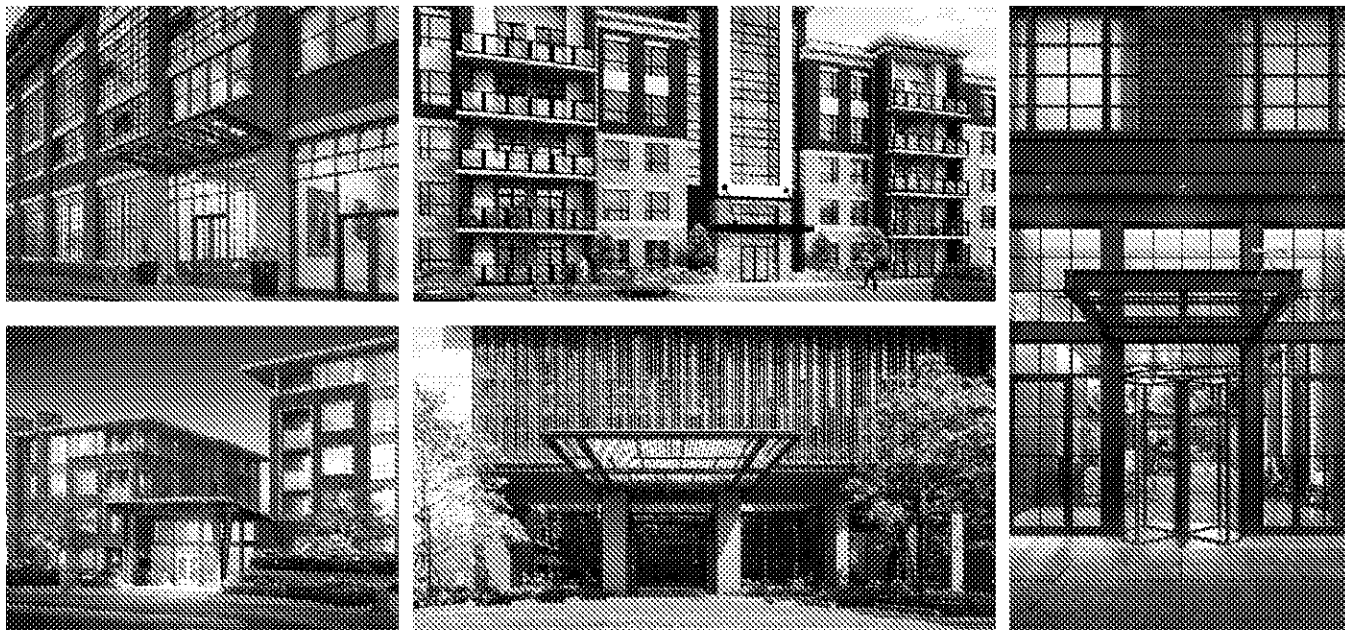


Figure 40: These images demonstrate the proper use of canopies to help mark the main entrance of the building (Representational of character only).



#### 4.11 Awnings and Canopies

- A. Glazed canopies with laminated clear glass and/or diffused glass must be used above all CRU's along Dewdney Trunk Road.
- B. Canopies should extend from the face of the building to provide sufficient pedestrian weather protection.
- C. Any canopy that might encroach into the City's right-of-ways must be designed to be removable and are subject to City approval.

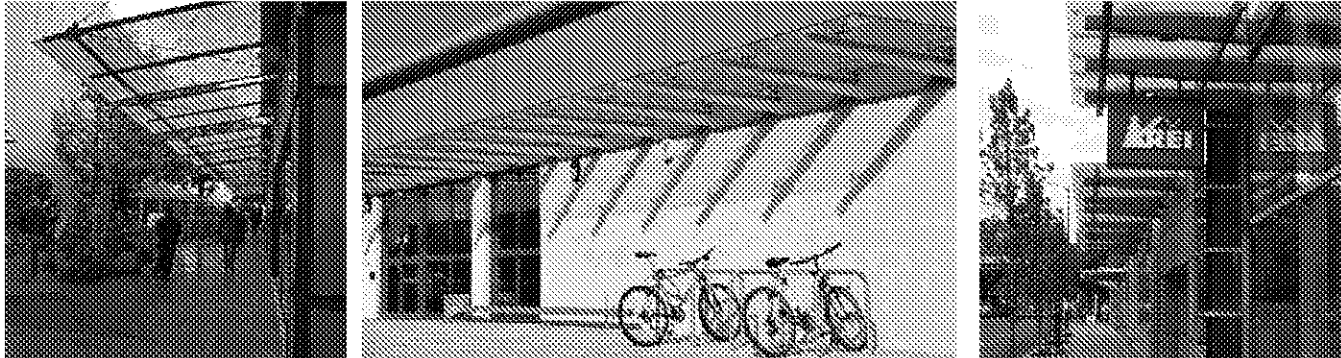


Figure 41: Canopies must provide shelter to pedestrians, visitors and retail customers without obstructing daylight to the CRU's (Representational of character only).

#### 4.12 Signage and Lighting

- A. Entry points must be clearly identified through architecture, signage and landscape design.
- B. Signage to parking must be clearly visible.
- C. Tailored merchant signs and lighting will serve to personalize each storefront. Signage is encouraged to be hung from underside of glazed canopy.
- D. Simple surface mounted fixtures should be considered to provide the required light levels at entries. Large backlit signs are not encouraged.
- E. Provide decorative elements to add personality and individuality to different establishments.
- F. Use 'warm light' sources (LED preferred) for superior visual acuity and enhanced perception of light. No higher than 3100 K.

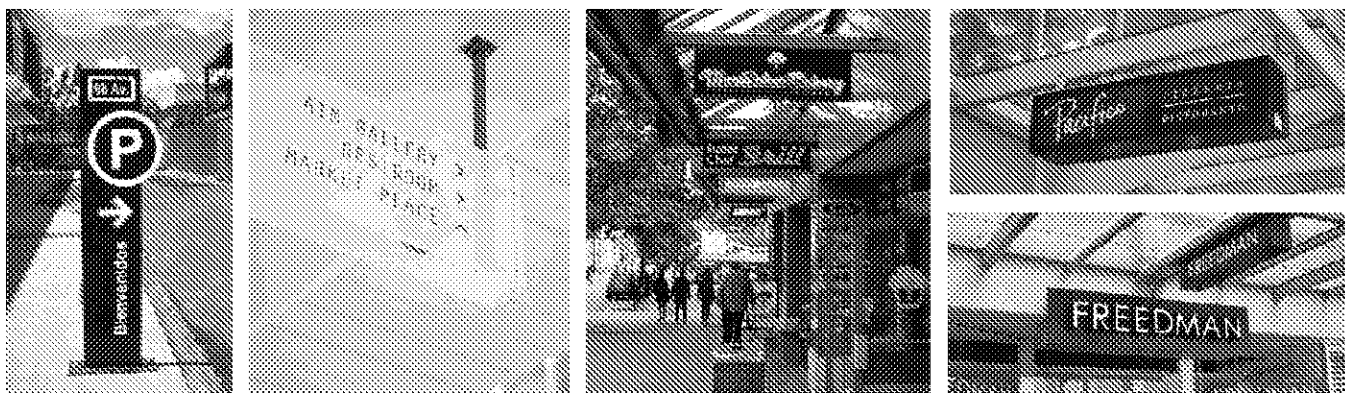


Figure 42: Signage within the development should be clear and easy to identify. (Representational of character only). Any other signage should meet City standards.

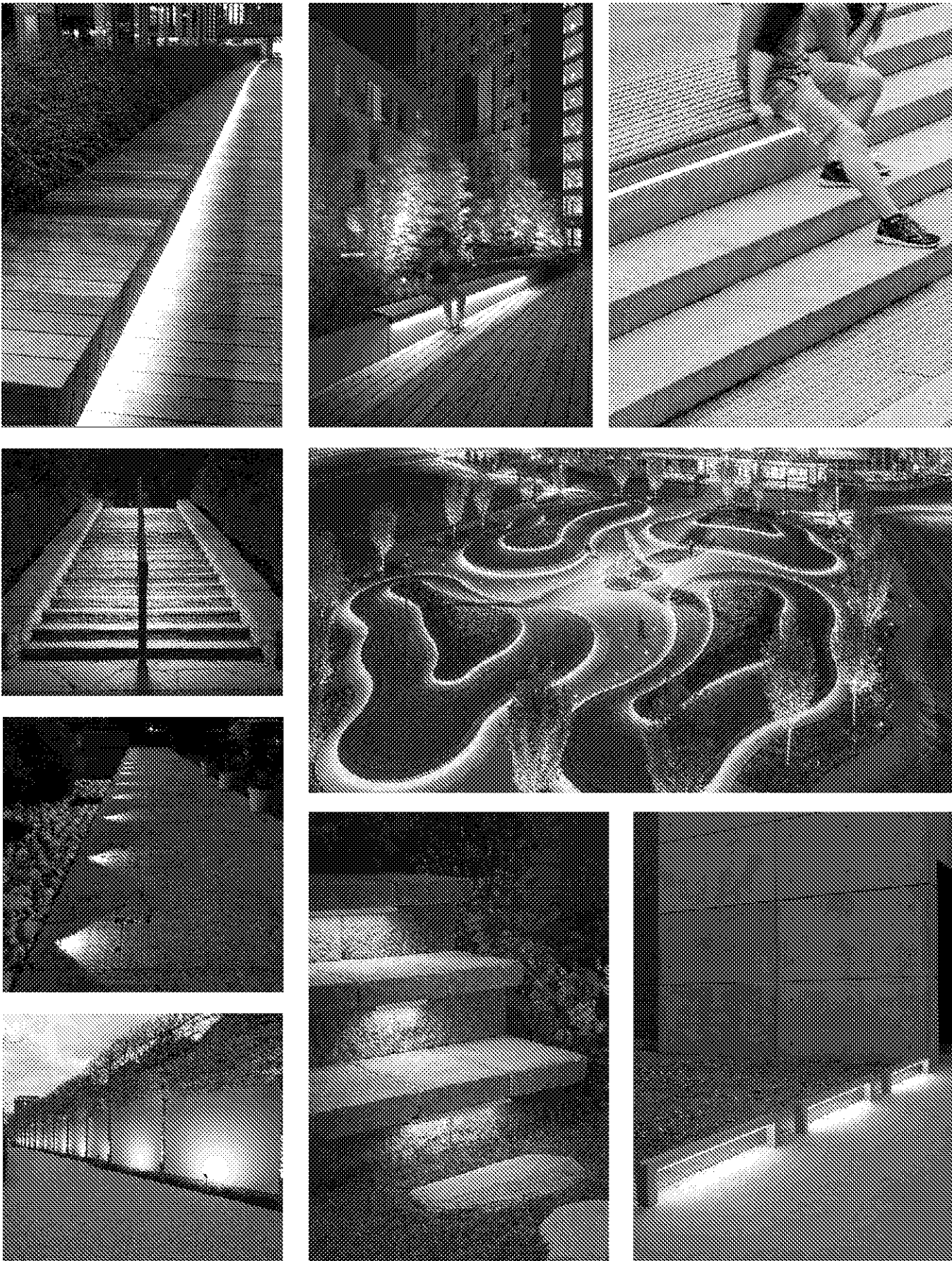


Figure 43: Examples of lighting fixtures used to identify entry points and to accentuate exterior paths (Representational of character only).

#### 4.13 Digital Information Board

Located in Phase 7 of the development, the digital information board is meant to become a versatile digital tapestry capable of conveying multiple graphic mediums. It should also be conceived in such a way that acts as a public art element.

The form and structure has intentionally not been defined at this time but should compliment the eventual architectural expression of adjacent buildings and the public plaza along 224 Street.



Figure 44: Illustrative images of information boards examples (Representational of character only).



APPENDICES

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ERA Design Guidelines Checklist for Development Permit Applications Phase 2 - 7

In accordance with the conditions set by City Council permitting the ERA development site to be pre-zoned, these design guidelines have been established for all phases of the ERA site to ensure the comprehensive developed is achieved on a phase by phase basis.

These guidelines govern the design requirements for each phase to be firstly assessed against the form and character guidelines approved by Council and summarised below; and secondly pursuant with Section 8.11 of the Official Community Plan. The checklist for these guidelines are to be appended to this checklist to demonstrate consistency is achieved.

This checklist is to be prepared by the architect of record for the project to demonstrate the proposed design was developed in accordance with the ERA guidelines. Please assess and describe the compliance of the proposed design of the project with respect to the ERA Key Guideline Concepts and with the ERA Form and Character Guidelines.

Description of the both the ERA and the OCP Key Guideline Concepts should be suitable and sufficiently descriptive to quote to ADP and to Council by for File Managers to quote in Development Permit Application Reports to Council. For the ERA and the OCP Form and Character Guidelines, clearly describe how the proposed design complies with each of the listed guidelines, or describes why a guideline is not complied with or why it is inapplicable.

Attached to this checklist are the following:

- General Town Centre Guidelines Checklist
- West Precinct Guidelines Checklist

This set of checklists are to accompany Development Permit Applications for each phase and submissions to the ADP.

1.3.2 Key design elements	Describe how this project and the design complies
A. Residential components of varying scale and density, positioned to maximize daylight penetration into the site and to create visual interest from every corridor.	
B. A prominent east-west public pedestrian open space and pedestrian way from Plaza Street to 224 Street, punctuated with activity areas (including child play, central activity plaza with theatre-like seating, and a major commercial plaza) and linkages to semi public and private space paths and mews and the City sidewalk system.	
C. Mews surrounded by both residential and commercial activity to ensure there are plenty of "eyes on the street" at all hours of the day.	
D. Public plaza in the east portion of the site, lined with commercial shops (inner "shopping row") and containing significant surface treatment, landscaping, pedestrian amenities, placemaking elements and a public art element.	

1.3.2 Key design elements	Describe how this project and the design complies
E. An attractive and vibrant streetscape along Dewdney Trunk Road with building spaces designed to accommodate ground level shops and services, and access to upper level non-residential components where they occur.	
F. Strategically placed commercial spaces to create an intimately scaled shopping corridor combined with residential use above.	
G. Sustainable design solutions to maximize rooftop space for green houses, gardens and power generation.	
2.3 Key Urban Design Principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable
A. Pedestrian friendliness Streetscapes and public walkways are to be articulated using appropriate scale, character and texture. Street trees, front doors (from ground-level apartments and retail storefronts), different scaled plantings and a variety of paving treatments are to be used to help define these paths and enhance the pedestrian experience. Security is also to be considered through the use of lighting and lower planting along path's edges.	
B. Public accessibility and permeability Provide clearly defined barrier-free access in both North-South and East-West directions through the site. Landscape elements, both soft and hard are to define and enhance these paths.	
C. Livability The overall layout and design of individual buildings and dwellings has been designed to create a variety of different living experiences and to provide a high standard of privacy, usable open space, and recreational amenities. This is achieved through different massing, height and configuration of buildings throughout the various phases of the overall development.	
D. Neighbourliness The form and layout of the site plan was driven by a desire for open space and connectivity. A variety of open and public spaces will facilitate community gathering.	



2.3 Key Urban Design Principles	Describe how this project and the design complies
<p><b>E. Diversity</b> Diverse housing types including a range of small and large units, condos and ground-level apartments, and low-rise and high-rise buildings will appeal to a large cross-section of the community. Appropriate architectural expression should be given to each as described in other sections of this document.</p>	
<p><b>F. Sustainability</b> Sustainable design and development principles and practices will be employed such as, water efficient landscape design and rain and storm water management.</p>	
<p><b>G. Placemaking</b> Public spaces acting as nodes making a positive and memorable contribution to Maple Ridge Town Centre. These spaces should be designed to facilitate a wide variety of civic activities and provide visual interest for the residential buildings surrounding them throughout the use of hardscape, landscape and plazas.</p>	
2.5.2 Key Principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable
<p><b>A.</b> The redevelopment of the ERA site will feature a mobility network that prioritizes pedestrian circulation and comfort above other transportation modes. Sidewalks and pedestrian street crossings will be continuous, universally accessible, barrier free and clearly designated; Special attention should be paid to crossings at roadways along the East-West Central Promenade; where the pedestrian should be prioritized and protected using an elevated crosswalk (aka. "speed table") with appropriate markings for vehicular traffic &amp; tactile warnings accessible design.</p>	
<p><b>B.</b> Accessibility Standards and Best Management Practices (BMP) should be applied generously wherever possible in all aspects of landscape design.</p>	
<p><b>C.</b> In considering the soil conditions of the geotechnical report, the landscape design should maximize soil volumes on-site to increase stormwater detention, prior to infiltration to ground. Additional measures of detention &amp; retention may be required on-site to support the development of each phase. Refer to Rain and Stormwater Best Management Practices Section 2.6.4; using appropriate devices to reduce the impacts of runoff over proposed impervious areas.</p>	

2.5.2 Key Principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
D. Landscape design should take into consideration solar exposure, and appropriately utilize and orient deciduous shade trees to create spaces that are sustainable and useable year-round, while maximizing sunlight wherever possible.	
E. Landscape design should take into consideration the human scale when determining a hierarchy of circulation paths, heights of walls and other elements that delineate programmatic spaces, screening and layering of planting elements, and arrangement of furnishings.	
F. Landscape Design elements should complement the architecture to maintain a cohesive legibility within various 'Character Zones'.	
G. A hierarchy of wayfinding elements (including signage) should be carefully considered and incorporated to allow ease of navigation particularly for those visiting the site, but also as elements that add to the identification and differentiation of various 'Character Zones'. The elements of effective wayfinding being (1) landmarks or other immediate identifiable elements to infer current location and help people to orient themselves, (2) clear routes to destinations, (3) a hierarchy of well defined paths, nodes, and edges of programmed space.	
H. Public art and/or landscape features -- particularly that integrate either water or lighting -- should be considered at prominent locations to reinforce the importance of the Central Promenade and navigability of the development.	
I. Lighting design should be an integral part of landscape design. While the architecture may provide ambient lighting, the landscape should aim to -- at a minimum -- provide path lighting along accessible routes. Area lighting may be used as the density increases and the nodes become larger and more commercial/public. Additionally planting may be accented with spotlighting or various other effects to animate spaces that will be used beyond dusk.	
J. Landscape design at the streetscapes should create a continuous and cohesive public realm. Private areas along streetscapes should be vertically separated wherever possible to provide privacy in the instance that the space is actively used, as well as eyes on the street. In these instances, the landscape should still contribute to the visual enhancement of the public realm through use of interesting materials, and ideally, planting material separation.	

2.5.2 Key Principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
K. Walls and fences should be integrated as appropriate where stronger demarcation of private space is to occur. The material used should reflect the Character of the architecture as appropriate to that zone. Consideration of Crime Prevention Through Environmental Design (CPTED) should be forefront when determining heights, with ideal wall heights being 0.61m, with wall-mounted step-lighting at 0.36-0.46m height and a maximum of every 5.0 m on centre unless otherwise noted.	
L. Appropriate soil volumes and drainage layers must be used to ensure the vibrancy of plant material into maturity. A minimum of 0.46m soil depth must be provided to all plants (noting this refers to most shrubs, perennials, and groundcovers without rhizomatous root structures; lawns & extensive green-roof sedums excepted with typical depths of 0.30m), with ideal depth being 0.61-0.76m. All trees require a minimum depth of 0.76m on-slab and an ideal depth of 0.91-1.2m where applicable off-slab; soil may be locally bermed-up to meet this condition. All trees require a minimum soil volume of 10 cbm.; this requirement may be relaxed with discretion in raised planted beds on-slab, where smaller trees may be planted to provide solar shading or ornamental trees planted for aesthetics (noting that these trees may not be counted by the City as "replacement trees" as their mature height & canopy will be limited).	
2.8.1 Key design elements	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. Must be a major portion of the block, with at least two sides open to the street.	
B. Located and oriented to maximize sunlight access throughout the day and provide uses that take advantage of the sunny location (e.g. cafés and patios).	
C. The edges of plazas should be lined with active uses at-grade, including building entrances, to animate and support the open space. Spill-out spaces, such as patios should be encouraged.	
D. At least one edge is open to the public sidewalk. The remaining edges are comprised of building facades (either one building or multiple buildings), potentially with mid-block pedestrian connections.	



2.9.1 Key design elements	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
E. Must be located at the same grade level as the public sidewalk. Where changes in grade are an important element of the overall design and programming, clear and direct access from the public sidewalk must be accommodated, and ensure universal accessibility.	
F. Should provide continuous weather protection in the form of canopies or arcades at the perimeter of the space.	
G. Plazas may contain a primary building entrance.	
H. Small scale elements should be used to create a human scale, and to define smaller sub-areas within the plaza for ample seating and gathering in the sun and shade.	
I. Creative and dynamic solutions for ample areas of seating should be provided.	
J. Provide pedestrian scale lighting at appropriate locations.	
2.9.1 Preliminary Neighbourhood Park Guidelines	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
1. Large topographical manipulations to create a theatre and amphitheatre that should include a water feature (and possibly public art lighting features) to draw in visitors and drown out nearby noise pollution. Activating this central space will help ensure people are spending time or walking through during the daytime and evening.	
2. The edges of the square should allow room for restaurants and cafes at the Office/Future Commerical Units to spill out into the open space.	
3. A significant portion of the plaza/park should maintain open paved area to offer the opportunity for a variety of programmed activities such as vendors, performers, exhibitions, outdoor recreation, etc.	
2.10.1 Semi-Private Amenity Gardens	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
1. Residential patios at podium level will receive additional patio space with raised planters on slab, with hedging and dense planting to screen from semi-private amenity space.	
2. Amenity space will include a variety of programming; e.g. open turf space for sport play, relaxing trellised decks for passive activity, children's playgrounds, agricultural plots, etc.	
3. Green roofs on podiums and towers.	

2.10.2 Private Patios	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
1. Use layers of landscape moving from shorter plants to taller plants as a transition from the public realms to the private realms.	
2. Use landscaping, such as hedging and taller plants, to buffer views into homes and provide privacy for yards and entries.	
3. Use low fences, maximum height of 1.0m (3 ft), to define extents of private yards and provide secure areas for residents, particularly along the mews and greenways.	
3.1.1 Cycle Lanes and Pedestrian Paths	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. Provide sidewalks within residential projects, connecting from the street or driveway to unit entrances.	
B. Locate buildings to be contiguous wherever possible, and make accommodations for pedestrian circulation between adjacent businesses and sites.	
C. Where new on-street parking is located, provide pedestrian walkways that connect directly to the development.	
D. Provide pedestrian walkways with landscape amenities from within public areas to shop entries, and to other pedestrian oriented uses and destinations.	
E. All public areas must be fully accessible. Ramps, stairs, sidewalks, site furniture, crosswalks, and paving materials must all be designed to allow everyone to enjoy the community and outdoor commercial amenities.	
3.1.2 Internal Pedestrian Walkways	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
1. Major routes serving larger numbers of pedestrians should be a minimum of 3.5m wide, and should be constructed with high quality paving materials such as concrete and/or stone pavers. These paths should meet BC Building Code requirements for wheelchair accessibility, either as part of the main route or on an alternate alignment.	
2. Secondary routes and those leading to private residences should be a minimum of 1.5m wide, and should be constructed with materials such as concrete and/or stone pavers. Providing safe and comfortable access by individuals with limited mobility.	

3.1.2 Internal Pedestrian Walkways	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
3. Paving at pedestrian paths should be designed to utilize concrete, stone and/or concrete pavers combined with textured finishes for ramps. The front edge of stair treads should be textured, and/or constructed with a material that visually signals the change in level. In cases where pedestrian and bike paths are adjacent, a separation may be created with roughly textured borders between the two zones.	
4. Lighting may be incorporated into or adjacent to stairs and ramps wherever possible.	
3.1.10.1 Key design principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. While unobstructed lines of sight must be maintained at intersections, crosswalks and entrances to individual buildings, screening elements for unsightly service areas and utilities should be provided.	
B. Tree placement should be determined in relation to other landscape features to define public, semi-private and private space, to direct and enhance views, and to soften street edges.	
C. Landscape material should use arrangements of spaces to create outdoor spaces that explicitly address the feeling of safety.	
D. Along internal roads, install street trees 8-10m on centre with minor variations to suit entry driveways and site utilities. Trees are to be a minimum of 7cm caliper. Trees along public streets are determined by City of Maple Ridge.	
3.2.1 Dewdney Trunk Road	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
1. Public/off-site right-of-ways and sidewalks should conform to City's Design Criteria standards and coordinated design through Servicing Agreements.  2. Typical: A 1.2m wide exposed aggregate concrete boulevard strip will contain:  A. Street trees in decorative metal tree grates ("Dobney Foundry, Ivy-48CAT" pattern, bare metal finish), in a continuous soil cell trench as required for sufficient volume (10 cbm/tree);	



3.2.1 Dewdney Trunk Road	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>B. Site furnishings, including benches ("Victory Stanley RB-28", with centre arm rest, Black), garbage receptacle ("Victory Stanley SD-35", side door opening c/w lock provided by City, Black, and option for recyclable cage/basket) or as directed by the City.</p> <p>C. Street &amp; pedestrian lighting standards.</p> <p>D. A 2.0m wide broom finished commuter sidewalk will be adjacent to the property line. Street parallel parking</p> <p>3. Additional to the off-site streetscape design, Plaza Street will have casual surveillance from residential patios that are vertically separated from the street for privacy.</p>	
3.2.2 Brown Avenue	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Street trees will be part of a continuous lawn boulevard</p> <p>2. On street parking</p> <p>3. A multi-use path for cyclists and pedestrians</p> <p>4. Boulevards either side of Avenue</p>	
3.2.3 Mews / Pedestrian Way	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. 2.4m (8') wide pedestrian path</p> <p>2. Grass shoulders expand path surface to 4m wide to accommodate fire truck access</p> <p>3. Residential patios either side of path are raised to provide 'eyes on the street'</p> <p>4. Live work units (more urban in nature) adjacent Dewdney Trunk Road</p>	
3.2.4 223 Street / Garden Street	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>Collector thoroughfares for pedestrians.</p> <p>Pedestrian-oriented with measures to reduce vehicular speed.</p>	

3.2.5 224 Street and New Public Corner Plaza	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. 224 Street is realigned thus creating an open space on the east side</p> <p>2. Built form is pulled back from the street to create an open space that spans the street</p> <p>3. Paving treatment may extend across the street (contingent on additional study and coordination with the City) and could be closed for festivals. Plaza and pedestrian path to the east could accommodate a farmer's market and/or potential location of public art.</p>	
3.2.6 East-West Promenade	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>At Residential</p> <p>1. 2.4m (8') wide</p> <p>2. Heavily planted</p> <p>3. Finer-grained in residential scale &amp; character</p> <p>4. Places to sit and gather</p> <p>5. Incorporation of rain water infiltration</p>	
<p>At Neighbourhood</p> <p>1. Path widens at public park/plaza</p> <p>2. Incorporation of extensive tree canopy</p> <p>3. Plaza space to south (adj. Commercial/Office) is multi-use park space to north is a neighbourhood &amp; community destination</p> <p>4. Park has berm to create interest &amp; play opportunity on a flat site</p> <p>5. Incorporation of rain-water trench &amp; water feature in plaza</p>	
<p>At 224 Street</p> <p>1. Path widens to 3m (10')</p> <p>2. Single row of large trees rain-water trench takes on a more urban character more hardscape &amp; urban in nature adjacent to commercial</p>	

4.1 Massing and Building Form	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. Throughout the development, medium-sized buildings are typically stepped back above the first 3-5 storeys of the building creating a lower podium that reduces the overall mass of the buildings at street level. This design solution defines the difference of uses within the building, such as residential units over retail and office spaces.	
B. Street walls (building faces directly adjacent to a street) should be between three and six storeys in height. If less than three storeys they are usually not well defined, and over six storeys they can be oppressive to most people. In most instances, street walls should step back above the third to fifth floor.	
C. The point towers should strive to achieve a height to width ratio of no less than 2:1 to 3:1 for the 21-storey building. When not possible, the massing of the building should encourage visual verticality through key architectural moves.	
D. The podium should be at least two but not more than five storeys high. This morphology permits small-scale retail storefronts on the commercial streets. This duality of commercial and residential creates a strong street presence, with the tower providing residential density that enlivens the neighbourhood and makes the local commercial activity economically viable.	
E. This form of building can also accommodate two storeys of ground-level apartments above the podium level without creating an overwhelming street front as long as the upper units are setback from the cornice line.	
F. A clear difference between the entrances to the towers and the residential units should be maintained throughout the project, keeping the street fronts for retail and residential units.	
4.3 Setbacks	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. The typical residential setback is between 3 to 6 meters (10-12 ft), which is sufficient to provide an outdoor patio or stoop that can accommodate outdoor living and/or seating.	
B. Other commercial or retail areas have a minimum 0.6m (24 inch) setback along Dewdney Trunk Road which will increase the overall width of the sidewalk zone, while allowing for outdoor seating, signage or other requirements, while not impacting the path of travel on public sidewalks.	



4.3 Setbacks	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
C. A setback between each phase of approximately 7.5 to 18.3m (25-60 ft) shall be maintained, to accommodate sufficient breathing room between buildings and with significant landscaping or other landscape features.	
D. Lastly, an approximate 19.2m (63 ft) setback is provided on the east side of Phase 7 to ensure sufficient area to create a civic gathering space that will extend across the street, integrating the development into the rest of its context.	
4.4 Garbage / Recycling Staging Areas	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>4.4.1 Internal storage</p> <p>The solid waste and recycling storage in a building must be accessible to all occupants of the development, and be clean, well lit, and conveniently located for users to get access to, including those with restricted mobility. The location of such a room or rooms is preferred to be within the underground portion of a building, such as an underground parking level, permitting convenient staging on refuse collect day.</p>	
<p>4.4.2 Staging Areas</p> <p>Especially for multi-unit residential buildings, if containers are stored underground, they will likely need to be relocated above ground for servicing on collection day (by building staff or a container pullout service provider). Thus, a temporary storage area should be incorporated in the ground level portion of the building and site plans provided for collection vehicles to manoeuvre and to collect materials without impeding street, parking entrances and sidewalk circulation (ensuring no height clearance issues).</p>	
<p>4.4.3 Other Design Considerations:</p> <p>A. Ensure that waste collection vehicles have ample room to maneuver at the site planning stage to ensure that these functions do not spill over into either the public right-of-way or public spaces.</p> <p>B. Both internal and staging areas are to comply fully with the City Waste Collection policies.</p> <p>C. Any portion of the staging area at grade must be within private property sufficiently setback from streets and pedestrian areas.</p> <p>D. Loading areas, garbage containers and recycling containers shall be screened from any adjacent residential lot or from overlook from upper storeys, to a height of at least 2.5 metres [8 ft.] by buildings, a landscaping screen, a solid decorative fence, or a combination thereof.</p>	

4.5 Building Sustainability Strategies	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Increased envelope thermal performance (both walls and fenestration),</p> <p>2. Increased energy efficiency of mechanical systems such as the inclusion of HRV's,</p> <p>3. inclusion of more passive strategies such as window shading on the applicable orientations,</p> <p>4. Green roofs</p> <p>5. use of low or classified Zero-VOC construction materials and finishes,</p> <p>6. Re-Use, Recycle or Use Rapidly Renewable materials</p> <p>7. Choose Third-Party Certified Products, etc.</p>	
4.6 Parking Considerations For All Phases	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
4.6.1 All vehicle parking is to be provided in a below grade or above ground parking structure.	
4.6.2 For above ground parking structures, the design of these parking structures should blend in with architectural elements of the main commercial or residential building, including the use of architectural or decorative grille or other similar elements for wall openings. Steel mesh is not recommended.	
4.6.3 Parking provided is to be separated between commercial and office use parking spaces (i.e. nonresidential parking) and residential parking spaces (i.e. parking for residents and parking for their visitors).	
4.6.4 There will be a security gate between the non-residential and residential parking. The security system will be designed to allow access to visitor's parking spaces by visitors, but retaining security to the portion of the parking spaces for residents should be recommended where the overall building configuration makes it possible.	
4.6.5 Residential parking shall separate parking for visitors and parking for residents.	
4.6.6 A gate into the parking area from the street level is optional as determined by circulation and security considerations.	

4.6 Parking Considerations For All Phases	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
4.6.7 Pedestrian access, including those with restricted mobility, will be designed to provide safe and convenient access between the separate parking areas and the portion of the building the parking is intended to serve. Access from residential parking areas will take into consideration families with children and individuals using mobility assistance equipment.	
4.7.1.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Ground-level apartments: Designed to blend into the whole building facade, the two storey units at grade will contribute to a richer, more human scale at the pedestrian level. With front doors 0.6m (2 ft) above the street level, these two-storey units generally follow the principles of a townhouse design. Raised entry areas provide comfortable semi-private space for a garden and patio.</p> <p>2. Massing: A strategy of setting floors back at the upper levels will help to soften the building massing and increase access to daylight. Setbacks are especially encouraged in mid-block courtyards for daylighting and privacy as well as providing generous deck space for residents. On all buildings, where possible, interesting roof shapes are encouraged to enrich the overall texture and visual amenity of the development. These may include curving forms, deep overhangs, sloped planes and the like, appropriate to an understated modernist expression. Stair enclosures serving roof decks can also add to this diversity of form.</p> <p>3. Green roofs: It is anticipated that some green roofs will be provided on most of the concrete buildings. This principle would further enhance the character of the building significantly. Green roofs should be designed as visual amenity as well as an attractive environment for outdoor common space. In wood framed buildings, roof vegetation should be incorporated in self contained planters.</p> <p>4. Transparency and legibility of indoor public spaces: Common interior spaces such as entry lobbies, stairs, exercise rooms, lounges and the like should be afforded as much transparency to the exterior as possible, encouraging a visual connection between these spaces and the exterior community.</p>	



4.7.2.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Design expression: The potential for mixed-use buildings to enrich the Town Centre is high and should be incorporated wherever possible in the architectural design. In responding to the urban design roles mentioned above, the design should:</p> <p>A. Provide a comfortable scale for a walkable shopping environment; commercial tenancies (i.e. storefronts) are to be divisible into increments of between 5.5 and 7.5 meters.</p> <p>B. The emphasis should be towards boutique style commercial tenants and not the typical large anchor tenant (The frontage would be greater, providing more exposure). Creative storefront merchandising will also be necessary to provide views into these spaces, as well as views to the street from within the commercial units.</p> <p>C. The residential portions of the mixed-use buildings will generally follow the design principles for multi-family buildings as previously described.</p> <p>2. Massing:</p> <p>A. Generally 4 to 6 storeys in height. The primary aim in the design of mixed-use buildings is to create a streetscape composed of a series of different but complementary building frontages ranging in width from 7.5 to 20m.</p> <p>B. An overheight ground floor of approximately 3.6 m provides good flexibility for a variety of retail uses as well as streetfront space.</p> <p>C. Setbacks at the uppermost residential storeys of mixed use buildings will be encouraged to create a comfortable street level scale and increased privacy for residents. Setbacks should be used to help differentiate between the commercial and residential parts of a building. Each use should be given a clear expression through a change of materiality, a change of plane or a combination of both.</p>	

4.7.3.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Massing: T&amp;GLA's forms and articulation should clearly distinguish individual units both in plan and elevation. This can be achieved in a variety of ways including sloped roof, recesses, vertical 'framing', etc. Consistent with the concept of a small-scale form, setting back the frontage above the second level is encouraged to further reinforce the legibility of each unit. Architectural elements such as individual roofs and entry canopies should be considered. Simple roof shapes can provide further architectural interest.</p> <p>2. Access to outdoors: Semi-private spaces should be designed to further distinguish individual units while providing a rich landscaped buffer between units and the public realm. At upper levels, balconies, terraces and roof decks are strongly encouraged to provide a more private outdoor experience as well as providing increased articulation.</p> <p>3. Simple, clean expression: T&amp;GLA's designs should reflect the current modern trends around the world - using solid planes to contrast with generous areas of glazing and clearly defined outdoor spaces.</p>	
4.7.4.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. General expression: Anticipating the coming changes to the BC Building Code in addressing thermal performance, ERA looks to introduce an aesthetic for towers that recalls the simple, strong and clean expression of elements associated with modern European design; with the goal of achieving balanced window to wall ratios in alignment with current thermal performance requirements, towers should strive to achieve a composition of simple forms and massing.</p> <p>2. Massing: Tower floor plate sizes are based on an average gross floor area of 650 m<sup>2</sup> (7,000 sq ft).</p> <p>3. Articulation and orientation: It is intended that towers be generally oriented with the long axis running north-south to optimize access to daylight and minimize the impact on upland views.</p> <p>4. Consideration should be given to how the towers are terminated at the upper levels. Tops of towers should be articulated to ensure continuity of the design throughout the buildings.</p>	

4.7.5.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Design expression:</p> <p>A. In the context of the contemporary architecture proposed throughout ERA, the commercial buildings provide an opportunity for unique and innovative design. A dramatic expression responding to their prominent placement and their specific use is expected to set them apart and enhance the architectural diversity of the development. At the same time, these buildings should complement the scale, giving careful attention to articulated streetwalls that enhance the pedestrian experience.</p> <p>B. Following the principles of good sustainable design, these buildings will also strive to deliver an enhanced workplace for occupants. Access to daylight, glazed stair enclosures to encourage their use, access to outdoor space, operable windows and envelope systems designed for optimal energy conservation are anticipated initiatives that will impact the exterior building design.</p> <p>C. Addressing the above goals, designers are challenged to create a new model for commercial buildings in the ERA development.</p> <p>2. Massing:</p> <p>Significant ground floor height gives the commercial buildings a scale appropriate to their gateway roles and to their frontages on Dewdney Trunk Road. Designs should seek to create visually engaging frontages at this level, ensuring these significant masses are broken down into increments to create a more comfortable scale for pedestrians. Active spaces should be located at the perimeter, as opposed to closed individual offices. Public lobbies, located along Dewdney Trunk Road, Garden Street and 223rd Street should be highly transparent and clearly visible for pedestrians.</p>	
<p>3. A livable workplace:</p> <p>A. Occupant comfort is a primary aim for these buildings. As one of the simplest but most effective ways of creating livable study and office environments, good daylighting should be an integral part of the building design. This starts with work stations located within 9m of exterior glazing but can be optimized by bouncing light deeper into the space using light shelves and tempering glare with the use of exterior sunshades. Centre atrium can also increase recess to daylight.</p>	



4.7.5.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>B. Control of one's workspace environment is another aspect of user comfort and can influence the external expression. Options for occupants include operable windows, mechanical systems zoned for smaller areas where they can respond more quickly to individual demands and access to outdoor space.</p> <p>C. Balconies, terraces and roof decks are all opportunities for a breath of fresh air, a coffee break or an ad hoc meeting. Their contribution to the exterior architecture can also be very positive, helping to articulate the mass in a variety of ways as well as bringing life to the street.</p>	
4.9 Commercial Retail Units (CRU)	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>4.9.1 CRU design principles.</p> <p>The following principles should be implemented in all phases of the development where CRU's are included:</p> <p>A. Provide a minimum of 0.6m setback from the property line for the storefront facades.</p> <p>B. Locate active commercial uses on the ground floor adjacent to the sidewalk, including retail, restaurants, and personal service uses. Ground floor street frontage space is to be predominantly for active, pedestrian-oriented uses.</p> <p>C. Provide continuous storefront windows, open air store frontages, and frequent, highly visible entrances for ground floor commercial uses adjacent to the street and sidewalk.</p>	
<p>4.9.2 Materials and proportions</p> <p>A. Glazing is encouraged in all retail units and should be predominant in all the building facades.</p> <p>B. High quality and durable trim materials, such as anodized or painted aluminum, are permitted and encouraged for windows and door trims.</p> <p>C. Continuous store windows and frequent, highly visible display storefronts along the street are a paramount feature as they provide visual interest and promote walkability.</p> <p>D. Open air store frontage type is acceptable along the internal corridors of the development. It brings the retail activity to the street, engaging pedestrians who walk by.</p>	

4.9 Commercial Retail Units (CRU)	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
E. Retail glazing is to be clean and not include frosted glass.	
4.10.1 Key Design Elements	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>All building entrances within the development would take into consideration the following:</p> <p>A. Locate all customer entries and entryways to be directly visible from the public sidewalk, and accessible from public and private walkways. Corner buildings are encouraged to have corner entries but it is not mandatory.</p> <p>B. Provide a pedestrian walkway from the public sidewalk to the entry frontage of buildings set back from the street edge.</p> <p>C. Design public street facing residential facades with individual entries, such as steps, porches, and paths from living units to the street to help break down the scale of multi-unit buildings.</p> <p>D. Audio clues, such as a small fountain or rustling plants, and olfactory features such as fragrant plants can also assist.</p> <p>E. Artificial lighting can highlight the entrance to a building and make it more obvious at night for everyone.</p>	
4.11 Awnings and Canopies	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>A. Glazed canopies with laminated clear glass and/or diffused glass must be used above all CRU's along Dewdney Trunk Road.</p> <p>B. Canopies should extend from the face of the building to provide sufficient pedestrian weather protection.</p> <p>C. Any canopy that might encroach into the City's right-of-ways must be designed to be removable and are subject to City approval.</p>	

4.12 Signage and Lighting	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>A. Entry points must be clearly identified through architecture, signage and landscape design.</p> <p>B. Signage to parking must be clearly visible.</p> <p>C. Tailored merchant signs and lighting will serve to personalize each storefront. Signage is encouraged to be hung from underside of glazed canopy.</p> <p>D. Simple surface mounted fixtures should be considered to provide the required light levels at entries. Large backlit signs are not encouraged.</p> <p>E. Provide decorative elements to add personality and individuality to different establishments.</p> <p>F. Use 'warm light' sources (LED preferred) for superior visual acuity and enhanced perception of light. No higher than 3100 K.</p>	
4.13 Digital Information Board	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>Located in Phase 7 of the development, the digital information board is meant to become a versatile digital tapestry capable of conveying multiple graphic mediums. It should also be conceived in such a way that acts as a public art element. The form and structure has intentionally not been defined at this time but should compliment the eventual architectural expression of adjacent buildings and the public plaza along 224 Street.</p>	

Project information

To be completed by the Architect on record for this project:

File number: \_\_\_\_\_

Date prepared:\_\_\_\_\_

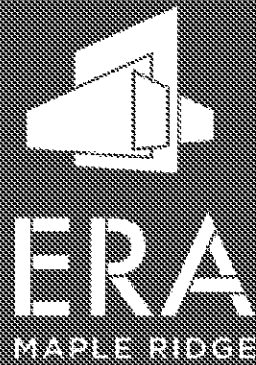
Architect:\_\_\_\_\_

Print Name

\_\_\_\_\_

Signature





### **PRIORITY AGREEMENT**

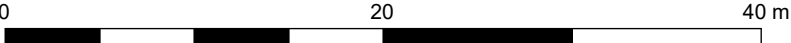
**BANK OF MONTREAL** (the “Chargeholder”) is the holder of a Mortgage and an Assignment of Rents encumbering the Lands which the Mortgage and Assignment of Rents are registered in the New Westminster Land Title Office under number(s) CA8399563 and CA8399564 respectively (collectively, the “Bank Charge(s)").

The Chargeholder, being the holder of the Bank Charge(s), by signing the Form C General Instrument attached hereto as Part I, in consideration of the payment of Ten Dollars (\$10.00) and other good and valuable consideration (the receipt and sufficiency of which is hereby acknowledged and agreed to by the Chargeholder) hereby consents to the granting of this Section 219 Covenant and hereby covenants that this Section 219 Covenant shall bind the Bank Charge(s) in the Lands and shall rank in priority upon the Lands over the Bank Charge(s) as if the Section 219 Covenant had been registered prior to the Bank Charge(s) and prior to the advance of any monies pursuant to the Bank Charge(s). The grant of priority is irrevocable, unqualified and without reservation or limitation.

SUBDIVISION PLAN OF  
LOT 2 PLAN EPP88082  
LOTS 9 AND 10 PLAN 20094  
LOT 50 PLAN 48265  
LOT 55 PLAN 53750  
ALL OF DISTRICT LOT 399 GROUP 1  
NEW WESTMINSTER DISTRICT

BCGS 92G.027

SCALE 1 : 400



ALL DISTANCES ARE IN METRES AND DECIMALS THEREOF

THE INTENDED PLOT SIZE OF THIS PLAN IS 864 mm IN WIDTH BY  
560 mm IN HEIGHT (D-SIZE) WHEN PLOTTED AT A SCALE OF 1:400

INTEGRATED SURVEY AREA No. 36 (CITY OF MAPLE RIDGE)  
NAD83 (CSRS) 4.0.0.BC.1.MVRD

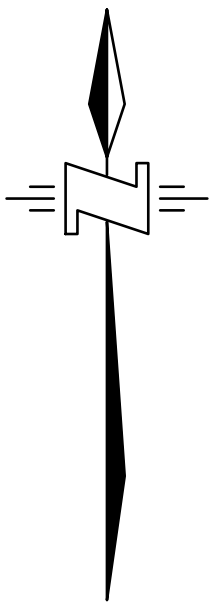
GRID BEARINGS ARE DERIVED FROM OBSERVATIONS BETWEEN GEODETIC  
CONTROL MONUMENTS 85H0739 AND 85H0729 AND ARE REFERRED TO THE  
CENTRAL MERIDIAN OF UTM ZONE 10 (123° WEST LONGITUDE)

THE UTM COORDINATES AND ESTIMATED ABSOLUTE ACCURACY ACHIEVED  
ARE DERIVED FROM THE MASCOT PUBLISHED COORDINATES AND STANDARD  
DEVIATIONS FOR GEODETIC CONTROL MONUMENTS 85H0739 AND 85H0729.

THIS PLAN SHOWS HORIZONTAL GROUND-LEVEL DISTANCES, UNLESS  
OTHERWISE SPECIFIED. TO COMPUTE GRID DISTANCES, MULTIPLY  
GROUND-LEVEL DISTANCES BY THE AVERAGE COMBINED FACTOR OF  
0.99960725. THE AVERAGE COMBINED FACTOR HAS BEEN DETERMINED BASED  
ON CONTROL MONUMENTS 85H0739 AND 85H0729

LEGEND:

SYMBOLS	DESCRIPTION
FOUND	PLACED
	CONTROL MONUMENT
	STANDARD IRON POST
	LEAD PLUG
	DENOTES HECTARE(S)
	DENOTES SQUARE METRE(S)
	DENOTES NOTHING FOUND
	DENOTES SET SHORT IRON POST IN ASPHALT



DATUM.....NAD83 (CSRS) 4.0.0.BC.1.MVRD  
UTM ZONE..... 10  
UTM NORTHING..... 5451829.592  
UTM EASTING..... 528617.172  
ABSOLUTE ACCURACY..... 0.01

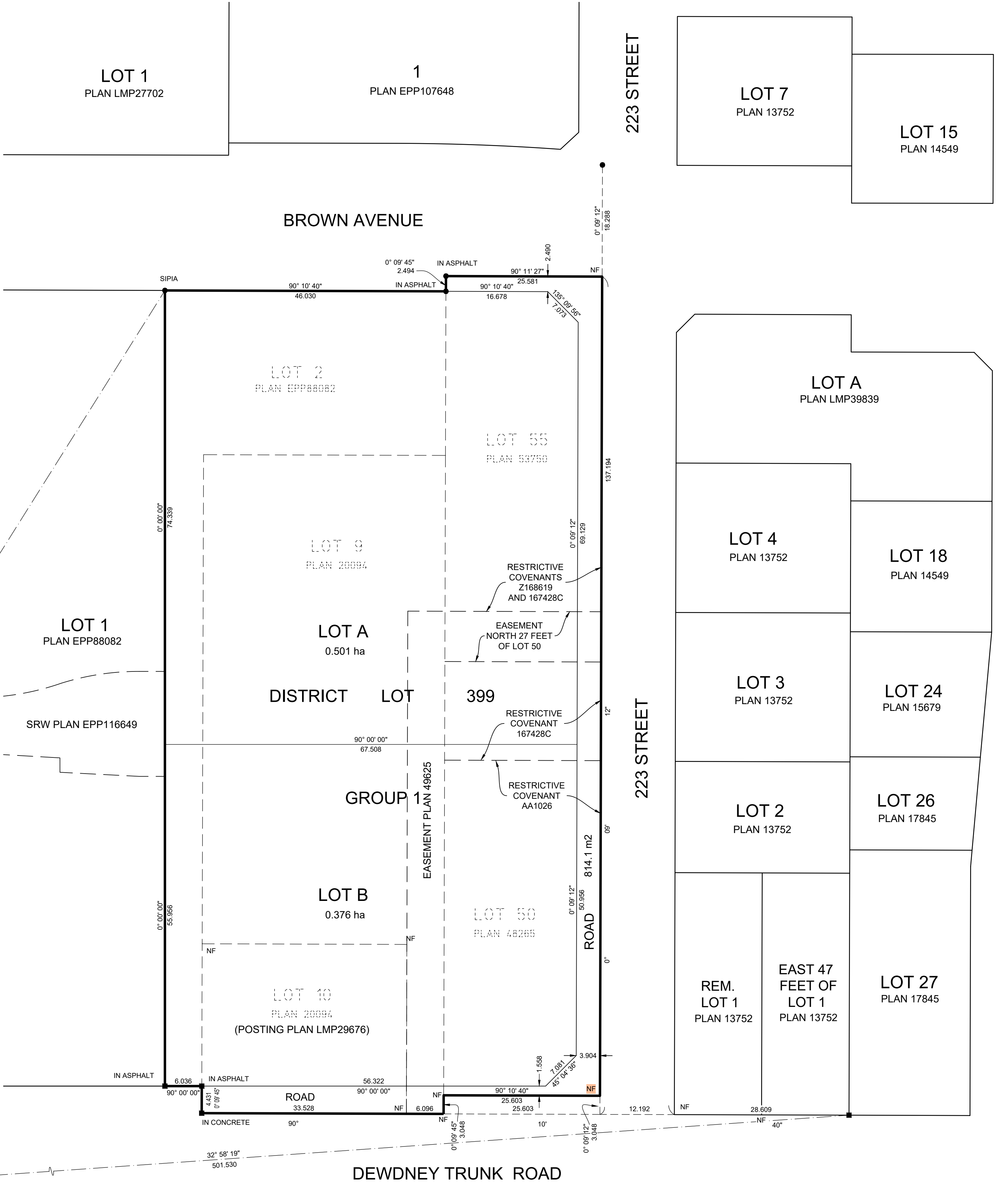
DATUM.....NAD83 (CSRS) 4.0.0.BC.1.MVRD  
UTM ZONE..... 10  
UTM NORTHING..... 5451830.509  
UTM EASTING..... 528610.585  
ABSOLUTE ACCURACY..... 0.01

THIS PLAN LIES WITHIN THE JURISDICTION OF THE APPROVING  
OFFICER FOR THE CITY OF MAPLE RIDGE

THE FIELD SURVEY REPRESENTED BY THIS PLAN WAS COMPLETED  
ON THE \_\_\_\_ DAY OF \_\_\_\_ 2024  
JI JUN SUN, BCLS #1063

THIS PLAN LIES WITHIN THE METRO VANCOUVER REGIONAL DISTRICT - CITY OF MAPLE RIDGE

PLAN EPP\_\_\_\_\_



PRELIMINARY



**CITY OF MAPLE RIDGE**

**BYLAW NO. 8034-2025**

A Bylaw to amend the text of Maple Ridge Zoning Bylaw No. 7600-2019, as amended.

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**WHEREAS**, it is deemed expedient to amend the Maple Ridge Zoning Bylaw No. 7600-2019 as amended:

**NOW THEREFORE**, Council of the City of Maple Ridge, enacts as follows:

1. This Bylaw may be cited as “Maple Ridge Zone Amending Bylaw 8034-2025”.
1. Maple Ridge Zoning Bylaw No. 7600-2019, as amended, is hereby amended as follows:
  - a. Part 10 Comprehensive Development Zones, Section 1035 CD-1-17 Zone is amended as follows:
    - i. 1035.5(2)(b) is amended by changing the allowable density as follows:

A “bonus density”, up to a maximum of 1.55 times the Lot Area, may be obtained by providing the following.
    - ii. 1035.5(2)(b)(ii) is amended by changing the allowable density as follows:

An amount equal to 0.3 times the Lot Area for each Storey above the second, not to exceed 1.2 times.
    - iii. 1035.5(2)(d) is amended by changing the allowable density as follows:

The maximum Density in this phase, including any “bonus density” and “amenity bonus density,” is 2.85 times the Lot Area.
    - iv. 1035.10(4) is amended by deleting “to 7” as follows:

Phases 2, for Apartment Use
    - v. 1035.10(4)(a)(i) is amended as follows:

a minimum of seven (7) electric Vehicle charging stations shall be provided.
    - vi. 1035.10 is amended by adding section (5) as follows:

Phases 3 to 7, for Apartment Use:

a. Notwithstanding the Maple Ridge Off-Street Parking and Loading Bylaw No. 4350 – 1990:

(i) a minimum of four (4) electric Vehicle charging stations shall be provided.

vii. 1035.11(8)(a)(ii) to be amended as follows:

Minimum seven (7) electric Vehicle charging stations;

2. Maple Ridge Zoning Bylaw No. 7600-2019 as amended is hereby amended accordingly.

**READ** a first time the       day of       , 20

**READ** a second time the       day of       , 20

**READ** a third time the       day of       , 20

**ADOPTED** the       day of       , 20

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**PRESIDING MEMBER**

---

**CORPORATE OFFICER**

	GROSS SITE AREAS:		ROAD DEDICATION AREAS:		NET SITE AREAS (used to calculate FSR):	
	SQ. FT.	SQ. M.	SQ. FT.	SQ. M.	SQ. FT.	SQ. M.
Phase 2 - South	45,308.80	4,209.33	4,827.60	448.49	40,481.20	3,760.84

SETBACKS	PHASE BOUNDARY	223 STREET	DEWDNEY TRUNK ROAD	NORTH SOUTH MEWS
Required	N/A	N/A	N/A	N/A
Proposed	14' - 5 1/4'	Residential: between 10'-2" Commercial: 6'-0"	6'-0"	29'-6"
	4.40m	Residential: between 3.10m Commercial: 1.83m	1.83m	8.99m

SUMMARY OF BUILDING PROPOSED (SF)

BLDG	LEVELS	NUMBER OF RES. UNITS	TOTAL GFA (SF)	COMMERCIAL GFA (SF)	RESIDENTIAL GFA (SF)	TOTAL EXCLUSION (SF)	TOTAL INDOOR AMENITY EXCLUSION (SF)	TOTAL NET AREA (SF)	RESIDENTIAL FSR	COMMERCIAL FSR	TOTAL FSR BY PHASE	MAX FSR ALLOWABLE (PER REZONING)	ALLOWABLE NET GFA (SF)
PH 2 - LOW RISE	6	120	119,683	9,627	110,056	18,782	5,224	95,677	2.13	0.24	2.36	2.65	107,275.18
			11,119	894	10,224	1,745	485	8,889				9,966	

L1 BUILDING FOOTPRINT	SQ. FT.	SQ. M.
	26,231	2,437

SITE COVERAGE 66%

TOTAL NET PH2 UNUSED AREA +/-	SQ. FT.	SQ. M.
	95,677	8,889
	-11,596.18	-1,077

PHASE 2 - PARKING (Reference bylaw - City of Maple Ridge Bylaw No.4350-1990 )													
LEVEL			L1	P1	P2					PROVIDED		TOTAL REQ'D	+/-
Residential			0	66	66					132		117	15
Visitor			0	6	0					6			
Commercial			10	0	0					10		10	
TOTAL										148		133	
Small Stall Count (10% of total)			1	7	7					15		13	
Accessible Stall Count			1	1	3					5		4	
EV Stall Count			0	7	0					7	*For residential use	7	

Note : Resident Rental Parking Ratio is proposed at 0.8 stalls/unit (bachelor); 0.9 stalls/unit (1 bedroom); 0.1 stalls/unit (additional bedroom)  
Note : Visitor Parking Ratio is 0.1 stall / unit - where on-street supply is available (Reference bylaw No. 4350-1190 - Section 10.2).  
Note : Commercial Parking Ratio is 1 stall /100m² GFA for small Retail units (Section 10.3).  
Note : On Street parking number is per civil drawings and calculations. The number is not included in total provided stalls number.  
Note : In all CD Zones, each parking space provided for Residential use, excluding Visitor parking spaces, shall be provided with roughed-in Level 2 infrastructure (Schedule F - 7663-2020).  
Note : A minimum of seven (7) electric vehicle charging stations will be provided for residential apartment use.

Amenity (not included for FSR)		SQ. FT.	SQ. M.
Indoor Amenities		5,224	485
Outdoor Amenities (L1 Courtyard & Rooftop Amenity)		6,299	585

Common Open Areas (not included for FSR)		SQ. FT.	SQ. M.
L1 Common Open Area		11,761	1,093
Private Balconies & Patios		12,800	1,189
Rooftop Outdoor Amenity		3,466	322

Adaptable Units  
Note : There are no adaptable units considered in the current total unit numbers.

PHASE 2 - Bike Storage	Short Term Provided Bike Storage		Long Term Provided Bike Storage	
	RESI	COMM	RESI	COMM
P2	0	0	0	0
P1	36	2	30	2
L1	0	0	0	0
Total	36	2	30	2

\*Note : There are no Bylaw requirements for personal storage lockers

Maple Ridge Long-Term Bicycle Parking requirement = 1 space per 4 units  
Maple Ridge Short-Term Bicycle Parking requirement = 6 spaces per 20 units  
Commercial Long-Term Bicycle Parking requirement = 1 space per 750m²  
Commercial Short-Term Bicycle Parking requirement = 6 spaces for every 1500m²

\*Charging receptacles will be provided for 10% of long-term bike lockers

REQUIRED BIKE STORAGE	PROVIDED BIKE STORAGE
30	30
36	36
2	2
6	6

DENSITY CALCULATIONS

PHASE 2 LOW RISE	Units		120		Studio		0 units		0%	
	Total GFA Area (SF)	Total GFA Area (SM)	110,056	10,224	1 Bed	25 units	21%	1 Bed + den	30 units	25%
					2 Bed	40 units	33%	2 bed + den	5 units	4%
					3 bed	20 units	17%			
	Unit Size Range ( SF)				459-574	575-660	738-780	870	943-977	
Level	GROSS FLOOR AREA (SF)	GROSS FLOOR AREA (SM)	UNITS	STUDIO	1BR	1BR+DEN	2BR	2BR+DEN	3BR	
L1 Commercial	9,627	894	0	0	0	0	0	0	3,672	341
L1 Res.	8,896	826	0	0	0	0	0	0	3,022	281
2	20,232	1,880	24	0	5	6	8	1	4	3,022
3	20,232	1,880	24	0	5	6	8	1	4	3,022
4	20,232	1,880	24	0	5	6	8	1	4	3,022
5	20,232	1,880	24	0	5	6	8	1	4	3,022
6	20,232	1,880	24	0	5	6	8	1	4	3,022
Roof										
Totals	110,056	10,224	120	0	25	30	40	5	20	18,782
			100%	0%	21%	25%	33%	4%	17%	

PHASE 2 - AREA BREAKDOWN

Area Use	Name	Area	Area (m2)
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AMENITY			
AMENITY	AMENITY	5224 ft²	485 m²
		5224 ft²	485 m²

COMMON			
COMMON	COMMON AREA	12546 ft²	1166 m²
COMMON	RESIDENTIAL & COMMERCIAL SHARED SERVICE CORRIDOR	825 ft²	77 m²
COMMON	STAIR A	1060 ft²	99 m²
COMMON	STAIR B	1121 ft²	104 m²
COMMON	STAIR C	170 ft²	16 m²
COMMON	STAIR D	1228 ft²	114 m²
		16951 ft²	1575 m²

COMMON LOBBY			
COMMON LOBBY	FILING/RECORDS	53 ft²	5 m²
COMMON LOBBY	JANITOR	47 ft²	4 m²
COMMON LOBBY	MAIL & PARCEL	221 ft²	21 m²
COMMON LOBBY	RENTAL ADMIN	202 ft²	19 m²
COMMON LOBBY	RES. ENTRY LOBBY	954 ft²	89 m²
		1477 ft²	137 m²

OUTDOOR AMENITY			
OUTDOOR AMENITY	COMMUNAL GARDEN OUTDOOR AMENITY	3466 ft²	322 m²
OUTDOOR AMENITY	OUTDOOR AMENITY	2833 ft²	263 m²
		6299 ft²	585 m²

RENTAL			
RENTAL	1 BED	13357 ft²	1241 m²
RENTAL	1 BED + DEN	17897 ft²	1663 m²
RENTAL	2 BED	30972 ft²	2877 m²
RENTAL	2 BED + DEN	4370 ft²	406 m²
RENTAL	3 BED	19452 ft²	1807 m²
		86048 ft²	7994 m²

RETAIL			
RETAIL	CRU #1	1721 ft²	160 m²
RETAIL	CRU #2	716 ft²	66 m²
RETAIL	CRU #3	806 ft²	75 m²
RETAIL	CRU #4	566 ft²	53 m²
RETAIL	CRU #5	2568 ft²	239 m²
RETAIL	CRU #6	1228 ft²	114 m²
RETAIL	CRU #7	1196 ft²	111 m²
		8801 ft²	818 m²

RETAIL CORRIDOR			
RETAIL CORRIDOR	RESIDENTIAL AND COMMERCIAL SHARED SERVICE CORRIDOR	826 ft²	77 m²
		826 ft²	77 m²

SERVICES			
SERVICES	COMMERCIAL REFUSE	457 ft²	42 m²
SERVICES	RESIDENTIAL REFUSE	550 ft²	51 m²
SERVICES	SERVICES	321 ft²	30 m²
		1329 ft²	123 m²
Grand total		126954 ft²	11794 m²



## ERA Design Guidelines Checklist for Development Permit Applications Phase 2 - 7

In accordance with the conditions set by City Council permitting the ERA development site to be pre-zoned, these design guidelines have been established for all phases of the ERA site to ensure the comprehensive developed is achieved on a phase by phase basis.

These guidelines govern the design requirements for each phase to be firstly assessed against the form and character guidelines approved by Council and summarised below; and secondly pursuant with Section 8.11 of the Official Community Plan. The checklist for these guidelines are to be appended to this checklist to demonstrate consistency is achieved.

This checklist is to be prepared by the architect of record for the project to demonstrate the proposed design was developed in accordance with the ERA guidelines. Please assess and describe the compliance of the proposed design of the project with respect to the ERA Key Guideline Concepts and with the ERA Form and Character Guidelines.

Description of the both the ERA and the OCP Key Guideline Concepts should be suitable and sufficiently descriptive to quote to ADP and to Council by for File Managers to quote in Development Permit Application Reports to Council. For the ERA and the OCP Form and Character Guidelines, clearly describe how the proposed design complies with each of the listed guidelines, or describes why a guideline is not complied with or why it is inapplicable.

Attached to this checklist are the following:

- General Town Centre Guidelines Checklist
- West Precinct Guidelines Checklist

This set of checklists are to accompany Development Permit Applications for each phase and submissions to the ADP.

1.3.2 Key design elements	Describe how this project and the design complies
A. Residential components of varying scale and density, positioned to maximize daylight penetration into the site and to create visual interest from every corridor.	This project features diverse unit sizes with views from all 4 sides of the building, including mountains on the North and Town Centre views from the South.
B. A prominent east-west public pedestrian open space and pedestrian way from Plaza Street to 224 Street, punctuated with activity areas (including child play, central activity plaza with theatre-like seating, and a major commercial plaza) and linkages to semi public and private space paths and mews and the City sidewalk system.	A prominent east/west plaza is included along the North side of Phase 2 with amenities such as courtyards and a dog run which will be added to Phase 3.
C. Mews surrounded by both residential and commercial activity to ensure there are plenty of "eyes on the street" at all hours of the day.	Residential units are located along all streets, mews, and pathways above L2.
D. Public plaza in the east portion of the site, lined with commercial shops (inner "shopping row") and containing significant surface treatment, landscaping, pedestrian amenities, placemaking elements and a public art element.	Commercial units are located along Dewdney Trunk Road, while landscaping and amenities are included along the E/W and N/S pathways.

1.3.2 Key design elements	Describe how this project and the design complies
E. An attractive and vibrant streetscape along Dewdney Trunk Road with building spaces designed to accommodate ground level shops and services, and access to upper level non-residential components where they occur.	The building's ground floor is designed to accommodate retail and commercial spaces, with large storefront windows facing DTR. This creates an active and engaging streetscape, while also providing convenient services for residents and the surrounding community.
F. Strategically placed commercial spaces to create an intimately scaled shopping corridor combined with residential use above.	The placement of commercial units along the ground floor creates an intimate shopping corridor along DTR, with residential units located above. This mixed-use approach contributes to a vibrant and pedestrian-friendly environment
G. Sustainable design solutions to maximize rooftop space for green houses, gardens and power generation.	The building incorporates a green roof on the lower portion of the structure to manage stormwater runoff and reduce the urban heat island effect. The rooftop amenity community gardens further contribute this.
2.3 Key Urban Design Principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. Pedestrian friendliness Streetscapes and public walkways are to be articulated using appropriate scale, character and texture. Street trees, front doors (from ground-level apartments and retail storefronts), different scaled plantings and a variety of paving treatments are to be used to help define these paths and enhance the pedestrian experience. Security is also to be considered through the use of lighting and lower planting along path's edges.	The project incorporates a variety of paving treatments, street trees, lights, and landscaping to enhance the pedestrian experience along DTR and the public walkways within the site.
B. Public accessibility and permeability Provide clearly defined barrier-free access in both North-South and East-West directions through the site. Landscape elements, both soft and hard are to define and enhance these paths.	Phase 2 site provides barrier-free access in both the North-South and East-West directions, with pedestrian walkways and landscape elements clearly defining these routes. This ensures that the site is accessible and permeable to all members of the community.
C. Livability The overall layout and design of individual buildings and dwellings has been designed to create a variety of different living experiences and to provide a high standard of privacy, usable open space, and recreational amenities. This is achieved through different massing, height and configuration of buildings throughout the various phases of the overall development.	The varying sizes and configurations of the units create a diverse range of living experiences, while the provision of private balconies, communal gardens, and a rooftop terrace offer ample usable open space and recreational amenities for residents.
D. Neighbourliness The form and layout of the site plan was driven by a desire for open space and connectivity. A variety of open and public spaces will facilitate community gathering.	Phase 2 building has a U-shaped layout, which creates a central courtyard that serves as a focal point for community gathering and provides a variety of open spaces for residents to enjoy. This design prioritizes connectivity and fosters a sense of community within the development.

2.3 Key Urban Design Principles	Describe how this project and the design complies
<p><b>E. Diversity</b> Diverse housing types including a range of small and large units, condos and ground-level apartments, and low-rise and high-rise buildings will appeal to a large cross-section of the community. Appropriate architectural expression should be given to each as described in other sections of this document.</p>	<p>The project offers a diverse range of housing types, single-bedroom and family suites to appeal to a wide range of residents. This variety is reflected in the building's architecture, which features a mix of mid-century modern materials and expressions.</p>
<p><b>F. Sustainability</b> Sustainable design and development principles and practices will be employed such as, water efficient landscape design and rain and storm water management.</p>	<p>The project demonstrates a commitment to sustainable design by incorporating a green roof and providing space for communal gardening. Additionally, the overall landscaping design emphasizes features for effective rain and stormwater management.</p>
<p><b>G. Placemaking</b> Public spaces acting as nodes making a positive and memorable contribution to Maple Ridge Town Centre. These spaces should be designed to facilitate a wide variety of civic activities and provide visual interest for the residential buildings surrounding them throughout the use of hardscape, landscape and plazas.</p>	<p>The central courtyard formed by the building's U-shaped layout acts as a community gathering space, while the pedestrian walkways and plazas provide opportunities for a variety of civic activities and visual interest for surrounding residents.</p>
2.5.2 Key Principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p><b>A. The redevelopment of the ERA site will feature a mobility network that prioritizes pedestrian circulation and comfort above other transportation modes. Sidewalks and pedestrian street crossings will be continuous, universally accessible, barrier free and clearly designated; Special attention should be paid to crossings at roadways along the East-West Central Promenade; where the pedestrian should be prioritized and protected using an elevated crosswalk (aka. "speed table") with appropriate markings for vehicular traffic &amp; tactile warnings accessible design.</b></p>	<p>Phase 2 prioritizes pedestrian comfort with continuous, universally accessible walkways and crossings. Special attention is given to the East-West Central Promenade crossing, where an elevated crosswalk with appropriate markings and tactile warnings prioritizes pedestrian safety.</p>
<p><b>B. Accessibility Standards and Best Management Practices (BMP) should be applied generously wherever possible in all aspects of landscape design.</b></p>	<p>This project will follow BCBC 2024 accessibility requirements and BCBC 2018 Adaptable unit requirements. Public pathways are designed to be accessible by the public.</p>
<p><b>C. In considering the soil conditions of the geotechnical report, the landscape design should maximize soil volumes on-site to increase stormwater detention, prior to infiltration to ground. Additional measures of detention &amp; retention may be required on-site to support the development of each phase. Refer to Rain and Stormwater Best Management Practices Section 2.6.4; using appropriate devices to reduce the impacts of runoff over proposed impervious areas.</b></p>	<p>The landscape design incorporates rain gardens and other stormwater management features to maximize on-site soil volumes and increase stormwater detention, minimizing the impact of runoff on the surrounding environment.</p>



2.5.2 Key Principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
D. Landscape design should take into consideration solar exposure, and appropriately utilize and orient deciduous shade trees to create spaces that are sustainable and useable year-round, while maximizing sunlight wherever possible.	The landscape design utilizes diverse tree canopies to provide seasonal shading and maximize solar exposure in the colder months. This approach creates comfortable and sustainable outdoor spaces that can be enjoyed year-round.
E. Landscape design should take into consideration the human scale when determining a hierarchy of circulation paths, heights of walls and other elements that delineate programmatic spaces, screening and layering of planting elements, and arrangement of furnishings.	The landscape design considers the human scale by incorporating elements such as tiered planting, seating areas, and a variety of paving materials to create a comfortable and inviting pedestrian experience.
F. Landscape Design elements should complement the architecture to maintain a cohesive legibility within various 'Character Zones'.	The landscape design uses a cohesive plant palette and materials that complement the architectural style of the building, ensuring a consistent aesthetic throughout the development.
G. A hierarchy of wayfinding elements (including signage) should be carefully considered and incorporated to allow ease of navigation particularly for those visiting the site, but also as elements that add to the identification and differentiation of various 'Character Zones'. The elements of effective wayfinding being (1) landmarks or other immediate identifiable elements to infer current location and help people to orient themselves, (2) clear routes to destinations, (3) a hierarchy of well defined paths, nodes, and edges of programmed space.	The project incorporates a variety of wayfinding elements, including signage, landmarks, and a hierarchy of paths, to help people navigate the site and differentiate between the various character zones.
H. Public art and/or landscape features – particularly that integrate either water or lighting – should be considered at prominent locations to reinforce the importance of the Central Promenade and navigability of the development.	While this rental project does not include public art, the building's architecture itself creates visual interest through its varying heights, U-shaped layout, and material choices. The use of large windows, balconies, and architectural details further enhances the visual appeal of the development and contributes to the overall aesthetic of the Town Centre.
I. Lighting design should be an integral part of landscape design. While the architecture may provide ambient lighting, the landscape should aim to – at a minimum – provide path lighting along accessible routes. Area lighting may be used as the density increases and the nodes become larger and more commercial/public. Additionally planting may be accented with spotlighting or various other effects to animate spaces that will be used beyond dusk.	Phase 2 incorporates a comprehensive lighting design that includes path lighting along accessible routes and area lighting in denser, more public spaces. This layered approach ensures safety and enhances the usability of the outdoor spaces after dusk.
J. Landscape design at the streetscapes should create a continuous and cohesive public realm. Private areas along streetscapes should be vertically separated wherever possible to provide privacy in the instance that the space is actively used, as well as eyes on the street. In these instances, the landscape should still contribute to the visual enhancement of the public realm through use of interesting materials, and ideally, planting material separation.	The landscape design along the streetscape prioritizes a continuous and cohesive public realm by using vertical separation between public and private areas using planting and hardscape, ensuring both privacy and visual interest through the use of diverse planting materials.

2.5.2 Key Principles	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
K. Walls and fences should be integrated as appropriate where stronger demarcation of private space is to occur. The material used should reflect the Character of the architecture as appropriate to that zone. Consideration of Crime Prevention Through Environmental Design (CPTED) should be forefront when determining heights, with ideal wall heights being 0.61m, with wall-mounted step-lighting at 0.36-0.46m height and a maximum of every 5.0 m on centre unless otherwise noted.	The project incorporates a fence to separate the loading area from the central courtyard, ensuring privacy and security for residents while maintaining the aesthetic quality of the shared outdoor space.
L. Appropriate soil volumes and drainage layers must be used to ensure the vibrancy of plant material into maturity. A minimum of 0.46m soil depth must be provided to all plants (noting this refers to most shrubs, perennials, and groundcovers without rhizomatous root structures; lawns & extensive green-roof sedums excepted with typical depths of 0.30m), with ideal depth being 0.61-0.76m. All trees require a minimum depth of 0.76m on-slab and an ideal depth of 0.91-1.2m where applicable off-slab; soil may be locally bermed-up to meet this condition. All trees require a minimum soil volume of 10 cbm.; this requirement may be relaxed with discretion in raised planted beds on-slab, where smaller trees may be planted to provide solar shading or ornamental trees planted for aesthetics (noting that these trees may not be counted by the City as "replacement trees" as their mature height & canopy will be limited).	Phase 2 strategically utilizes the stepped slab design of the parkade roof to increase soil volumes for landscaping in certain areas. This approach allows for deeper planting beds and healthier plant growth, contributing to the overall quality and sustainability of the landscape design.
2.8.1 Key design elements	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. Must be a major portion of the block, with at least two sides open to the street.	The Phase 2 building occupies a major portion of the block and has two sides open to the street, fulfilling the requirement for a prominent presence within the development.
B. Located and oriented to maximize sunlight access throughout the day and provide uses that take advantage of the sunny location (e.g. cafés and patios).	The south building's position and orientation maximize sunlight access throughout the day. The inclusion of commercial spaces with patios along the ground floor takes advantage of this sunny location and encourages outdoor activities.
C. The edges of plazas should be lined with active uses at-grade, including building entrances, to animate and support the open space. Spill-out spaces, such as patios should be encouraged.	The central courtyard is lined with active uses at ground level, including building entrances and commercial spaces with spill-out patios. This design promotes activity and interaction within the courtyard, creating a vibrant and welcoming space.
D. At least one edge is open to the public sidewalk. The remaining edges are comprised of building facades (either one building or multiple buildings), potentially with mid-block pedestrian connections.	Phase 2 provides a generous setback for a public sidewalk while maintaining a flush building façade with alternating materials for all mid-block pedestrian connections.

2.8.1 Key design elements	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
E. Must be located at the same grade level as the public sidewalk. Where changes in grade are an important element of the overall design and programming, clear and direct access from the public sidewalk must be accommodated, and ensure universal accessibility.	Phase 2 entries are located at the same grade as the public sidewalk for direct access.
F. Should provide continuous weather protection in the form of canopies or arcades at the perimeter of the space.	The commercial area along DTR has a continuous canopy that provides weather protection for pedestrians, enhancing the comfort and usability of the space.
G. Plazas may contain a primary building entrance.	The design incorporates a setback residential entry on the east side of the courtyard, providing a more private and intimate entrance for residents.
H. Small scale elements should be used to create a human scale, and to define smaller sub-areas within the plaza for ample seating and gathering in the sun and shade.	The central courtyard incorporates small-scale elements such as tiered planting, seating areas, and a variety of paving materials to create a human scale and define intimate sub-areas for gathering and relaxation.
I. Creative and dynamic solutions for ample areas of seating should be provided.	The central courtyard extends to the East-West pedestrian pathway, where additional seating is provided.
J. Provide pedestrian scale lighting at appropriate locations.	Pedestrian-scaled lighting is strategically placed throughout the courtyard and along pathways to ensure safety and visibility during the evening hours.
<del>2.9.1 Preliminary Neighbourhood Park Guidelines</del>	<del>Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.</del>
<del>1. Large topographical manipulations to create a theatre and amphitheatre that should include a water feature (and possibly public art lighting features) to draw in visitors and drown out nearby noise pollution. Activating this central space will help ensure people are spending time or walking through during the daytime and evening.</del>	
<del>2. The edges of the square should allow room for restaurants and cafes at the Office/Future Commerical Units to spill out into the open space.</del>	
<del>3. A significant portion of the plaza/park should maintain open paved area to offer the opportunity for a variety of programmed activities such as vendors, performers, exhibitions, outdoor recreation, etc.</del>	
2.10.1 Semi- Private Amenity Gardens	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
1. Residential patios at podium level will receive additional patio space with raised planters on slab, with hedging and dense planting to screen from semi-private amenity space.	Residential patios at the podium level are designed with raised planters and dense planting to provide privacy and screening from the semi-private amenity spaces, creating a more intimate and comfortable outdoor experience for residents.
2. Amenity space will include a variety of programming: e.g. open turf space for sport play, relaxing trellised decks for passive activity, children's playgrounds, agricultural plots, etc.	Key amenity programs include a, a communal garden, outdoor kitchen and dining areas, and strategically placed outdoor gym equipment.
3. Green roofs on podiums and towers.	The project incorporates green roofs on both the podium and tower levels.



<b>2.10.2 Private Patios</b>	<b>Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.</b>
1. Use layers of landscape moving from shorter plants to taller plants as a transition from the public realms to the private realms.	The landscape design uses a layered planting approach to create a transition from public to private realms, with shorter plants along the sidewalks and taller plants near the building's edge.
2. Use landscaping, such as hedging and taller plants, to buffer views into homes and provide privacy for yards and entries.	On Level 2 South, the landscape design uses a combination of hedging and taller plants to create a visual and physical buffer, screening views into residential units from the public realm.
3. Use low fences, maximum height of 1.0m (3 ft), to define extents of private yards and provide secure areas for residents, particularly along the mews and greenways.	No fences are required since residential units are located on Level 2; however, privacy screens will be incorporated between each suite.
<b>3.1.1 Cycle Lanes and Pedestrian Paths</b>	<b>Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.</b>
A. Provide sidewalks within residential projects, connecting from the street or driveway to unit entrances.	ERA Ph2 includes sidewalks that connect street frontages and driveways to residential unit entrances, away from DTR, ensuring safe and convenient pedestrian access throughout the development.
B. Locate buildings to be contiguous wherever possible, and make accommodations for pedestrian circulation between adjacent businesses and sites.	The project emphasizes pedestrian connectivity through a network of clearly defined pathways, including the East-West and North-South routes, which link various parts of the development and provide access to key amenities and public spaces from all phases.
C. Where new on-street parking is located, provide pedestrian walkways that connect directly to the development.	Phase 2 prioritizes pedestrian accessibility by incorporating sidewalks that connect on-street parking areas directly to the development along 223rd and DTR.
D. Provide pedestrian walkways with landscape amenities from within public areas to shop entries, and to other pedestrian oriented uses and destinations.	This site incorporates pedestrian walkways with landscaping to enhance walkability and connect South CRUs, Northern Amenities, and other pedestrian-friendly destinations. This design creates a welcoming and accessible environment for residents and visitors.
E. All public areas must be fully accessible. Ramps, stairs, sidewalks, site furniture, crosswalks, and paving materials must all be designed to allow everyone to enjoy the community and outdoor commercial amenities.	The project prioritizes universal accessibility by ensuring all public areas, including sidewalks and site furniture, are designed to be usable by everyone. The overall landscape is relatively flat, with grade changes of less than 5%, further enhancing accessibility and ease of movement for all individuals.
<b>3.1.2 Internal Pedestrian Walkways</b>	<b>Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.</b>
1. Major routes serving larger numbers of pedestrians should be a minimum of 3.5m wide, and should be constructed with high quality paving materials such as concrete and/or stone pavers. These paths should meet BC Building Code requirements for wheelchair accessibility, either as part of the main route or on an alternate alignment.	Major pedestrian routes in the project are designed to be a minimum of 3.5 meters wide and constructed with high-quality paving materials like concrete or stone pavers. These routes comply with BC Building Code accessibility requirements.
2. Secondary routes and those leading to private residences should be a minimum of 1.5m wide, and should be constructed with materials such as concrete and/or stone pavers. Providing safe and comfortable access by individuals with limited mobility.	Secondary pedestrian routes in the project, including the North and East pathways, are designed to be a minimum of 1.5 meters wide and constructed with materials such as concrete or stone pavers, ensuring safe and comfortable access for individuals with limited mobility.

<b>3.1.2 Internal Pedestrian Walkways</b>	<b>Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.</b>
3. Paving at pedestrian paths should be designed to utilize concrete, stone and/or concrete pavers combined with textured finishes for ramps. The front edge of stair treads should be textured, and/or constructed with a material that visually signals the change in level. In cases where pedestrian and bike paths are adjacent, a separation may be created with roughly textured borders between the two zones.	The project will avoid the need of ramps or stairs along the pathways and will use concrete paving materials and textures to ensure accessibility.
4. Lighting may be incorporated into or adjacent to stairs and ramps wherever possible.	The project incorporates path lighting within the landscape design to enhance safety and visibility during the evening hours.
<b>3.1.10.1 Key design principles</b>	<b>Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.</b>
A. While unobstructed lines of sight must be maintained at intersections, crosswalks and entrances to individual buildings, screening elements for unsightly service areas and utilities should be provided.	The project uses street lighting to maintain sightlines while screening service areas such as loading areas.
B. Tree placement should be determined in relation to other landscape features to define public, semi-private and private space, to direct and enhance views, and to soften street edges.	Tree placement defines spaces and directs views while softening the streetscape.
C. Landscape material should use arrangements of spaces to create outdoor spaces that explicitly address the feeling of safety.	The project's landscape design creates a sense of safety through the use of open sightlines, strategic planting and lighting arrangements, and a clear separation between public and private areas.
D. Along internal roads, install street trees 8-10m on centre with minor variations to suit entry driveways and site utilities. Trees are to be a minimum of 7cm caliper. Trees along public streets are determined by City of Maple Ridge.	The project follows the City of Maple Ridge's guidelines for street tree placement along public streets, ensuring proper spacing and tree caliper for a cohesive streetscape.
<b>3.2.1 Dewdney Trunk Road</b>	<b>Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.</b>
1. Public/off-site right-of-ways and sidewalks should conform to City's Design Criteria standards and coordinated design through Servicing Agreements.  2. Typical: A 1.2m wide exposed aggregate concrete boulevard strip will contain:  A. Street trees in decorative metal tree grates ("Dobney Foundry, Ivy-48CAT" pattern, bare metal finish), in a continuous soil cell trench as required for sufficient volume (10 cbm/tree);	On the south West side of DTR, along the ROW, the project incorporates a 1.2m wide exposed aggregate concrete boulevard with street trees planted in decorative metal grates set in a continuous soil cell trench to meet City standards and provide a cohesive streetscape.

3.2.1 Dewdney Trunk Road	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>B. Site furnishings, including benches ("Victory Stanley RB-28", with centre arm rest, Black), garbage receptacle ("Victory Stanley SD-35", side door opening c/w lock provided by City, Black, and option for recyclable cage/basket) or as directed by the City.</p> <p>C. Street &amp; pedestrian lighting standards.</p> <p>D. A 2.0m wide broom finished commuter sidewalk will be adjacent to the property line. Street parallel parking</p> <p>3. Additional to the off-site streetscape design, Plaza Street will have casual surveillance from residential patios that are vertically separated from the street for privacy.</p>	<p>The project will meet the requirement for additional streetscape design along Dewdney by incorporating casual surveillance from residential patios, which are vertically separated from the street to ensure privacy for residents.</p>
3.2.2 Brown Avenue	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Street trees will be part of a continuous lawn boulevard</p> <p>2. On street parking</p> <p>3. A multi-use path for cyclists and pedestrians</p> <p>4. Boulevards either side of Avenue</p>	
3.2.3 Mews / Pedestrian Way	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. 2.4m (8') wide pedestrian path</p> <p>2. Grass shoulders expand path surface to 4m wide to accommodate fire truck access</p> <p>3. Residential patios either side of path are raised to provide 'eyes on the street'</p> <p>4. Live work units (more urban in nature) adjacent Dewdney Trunk Road</p>	<p>The North-South mews/pathway is designed as a 2.4m wide pedestrian path with grass shoulders, while raised residential patios on either side provide "eyes on the street" for added security.</p>
3.2.4 223 Street / Garden Street	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>Collector thoroughfares for pedestrians.</p> <p>Pedestrian-oriented with measures to reduce vehicular speed.</p>	<p>The design of 223rd Street prioritizes pedestrian movement and incorporates traffic management measures to reduce vehicle speeds, creating a safer and more pedestrian-friendly environment.</p>



3.2.5 224 Street and New Public Corner Plaza	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. 224 Street is realigned thus creating an open space on the east side</p> <p>2. Built form is pulled back from the street to create an open space that spans the street</p> <p>3. Paving treatment may extend across the street (contingent on additional study and coordination with the City) and could be closed for festivals. Plaza and pedestrian path to the east could accommodate a farmer's market and/or potential location of public art.</p>	
3.2.6 East-West Promenade	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>At Residential</p> <p>1. 2.4m (8') wide</p> <p>2. Heavily planted</p> <p>3. Finer-grained in residential scale &amp; character</p> <p>4. Places to sit and gather</p> <p>5. Incorporation of rain water infiltration</p>	<p>The East/West Promenade area is designed as a 2.4m wide path with lush plantings, seating areas, and stormwater management via drainage system, creating a pedestrian-friendly and environmentally conscious space. This will be further enhanced during the development of Phase 3.</p>
<p>At Neighbourhood</p> <p>1. Path widens at public park/plaza</p> <p>2. Incorporation of extensive tree canopy</p> <p>3. Plaza space to south (adj. Commercial/Office) is multi-use park space to north is a neighbourhood &amp; community destination</p> <p>4. Park has berm to create interest &amp; play opportunity on a flat site</p> <p>5. Incorporation of rain-water trench &amp; water feature in plaza</p>	<p>The East/West Promenade extends through the site with tree canopies, connecting to a central courtyard with a variety of amenity programs, including a seating area, outdoor BBQ, and outdoor exercise and dining areas.</p>
<p>At 224 Street</p> <p>1. Path widens to 3m (10')</p> <p>2. Single row of large trees rain-water trench takes on a more urban character more hardscape &amp; urban in nature adjacent to commercial</p>	

4.1 Massing and Building Form	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. Throughout the development, medium-sized buildings are typically stepped back above the first 3-5 storeys of the building creating a lower podium that reduces the overall mass of the buildings at street level. This design solution defines the difference of uses within the building, such as residential units over retail and office spaces.	Phase 2 building steps back on L2, above the retail spaces on DTR.
B. Street walls (building faces directly adjacent to a street) should be between three and six storeys in height. If less than three storeys they are usually not well defined, and over six storeys they can be oppressive to most people. In most instances, street walls should step back above the third to fifth floor.	The building's street walls along DTR and 223rd Street are six stories in height, stepping back above the second floor to create a varied and modulated streetscape.
<del>C. The point towers should strive to achieve a height to width ratio of no less than 2:1 to 3:1 for the 21-storey building. When not possible, the massing of the building should encourage visual verticality through key architectural moves.</del>	
D. The podium should be at least two but not more than five storeys high. This morphology permits small-scale retail storefronts on the commercial streets. This duality of commercial and residential creates a strong street presence, with the tower providing residential density that enlivens the neighbourhood and makes the local commercial activity economically viable.	Podium appears 2 storeys tall (double height Commercial retail)
E. This form of building can also accommodate two storeys of ground-level apartments above the podium level without creating an overwhelming street front as long as the upper units are setback from the cornice line.	Upper units above the podium are set back 3m.
F. A clear difference between the entrances to the towers and the residential units should be maintained throughout the project, keeping the street fronts for retail and residential units.	Retail entries are fronting DTR while the residential entry is set back on 223 with different cladding materials, creating a clear difference between them.
4.3 Setbacks	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
A. The typical residential setback is between 3 to 6 meters (10-12 ft), which is sufficient to provide an outdoor patio or stoop that can accommodate outdoor living and/or seating.	Residential setback is 3m.
B. Other commercial or retail areas have a minimum 0.6m (24 inch) setback along Dewdney Trunk Road which will increase the overall width of the sidewalk zone, while allowing for outdoor seating, signage or other requirements, while not impacting the path of travel on public sidewalks.	Commercial setback is 6' (1.83m)

4.3 Setbacks	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
C. A setback between each phase of approximately 7.5 to 18.3m (25-60 ft) shall be maintained, to accommodate sufficient breathing room between buildings and with significant landscaping or other landscape features.	On phase 2, a 4.3m setback is provided from Phase 3. The remaining additional setback of 6.1m will be provided during Phase 3.
<del>D. Lastly, an approximate 19.2m (63 ft) setback is provided on the east side of Phase 7 to ensure sufficient area to create a civic gathering space that will extend across the street, integrating the development into the rest of its context.</del>	
4.4 Garbage / Recycling Staging Areas	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
4.4.1 Internal storage The solid waste and recycling storage in a building must be accessible to all occupants of the development, and be clean, well lit, and conveniently located for users to get access to, including those with restricted mobility. The location of such a room or rooms is preferred to be within the underground portion of a building, such as an underground parking level, permitting convenient staging on refuse collect day.	The project's solid waste and recycling storage is located within the commercial parking area, providing convenient access for all residents and commercial tenants, including those with restricted mobility, and facilitating efficient waste collection within the private property.
4.4.2 Staging Areas Especially for multi-unit residential buildings, if containers are stored underground, they will likely need to be relocated above ground for servicing on collection day (by building staff or a container pullout service provider). Thus, a temporary storage area should be incorporated in the ground level portion of the building and site plans provided for collection vehicles to manoeuvre and to collect materials without impeding street, parking entrances and sidewalk circulation (ensuring no height clearance issues).	Staging area is located North of the loading area on the ground level.
4.4.3 Other Design Considerations: A. Ensure that waste collection vehicles have ample room to maneuver at the site planning stage to ensure that these functions do not spill over into either the public right-of-way or public spaces. B. Both internal and staging areas are to comply fully with the City Waste Collection policies. C. Any portion of the staging area at grade must be within private property sufficiently setback from streets and pedestrian areas. D. Loading areas, garbage containers and recycling containers shall be screened from any adjacent residential lot or from overlook from upper storeys, to a height of at least 2.5 metres [8 ft.] by buildings, a landscaping screen, a solid decorative fence, or a combination thereof.	Phase 2 provides ample room to maneuver within the loading area for waste collection, as reviewed with the traffic consultant. All staging and collection activity happens on private property, screened from the public realm.



4.5 Building Sustainability Strategies	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. increased envelope thermal performance (both walls and fenestration),</p> <p>2. increased energy efficiency of mechanical systems such as the inclusion of HRV's,</p> <p>3. inclusion of more passive strategies such as window shading on the applicable orientations,</p> <p>4. Green roofs</p> <p>5. use of low or classified Zero-VOC construction materials and finishes,</p> <p>6. Re-Use, Recycle or Use Rapidly Renewable materials</p> <p>7. Choose Third-Party Certified Products, etc.</p>	Energy Step Code 3 compliance is achieved in this project by addressing key performance areas: glazing, insulation R-values, solar shading, and mechanical HVAC systems.
4.6 Parking Considerations For All Phases	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
4.6.1 All vehicle parking is to be provided in a below grade or above ground parking structure.	All residential parking is below grade
4.6.2 For above ground parking structures, the design of these parking structures should blend in with architectural elements of the main commercial or residential building, including the use of architectural or decorative grille or other similar elements for wall openings. Steel mesh is not recommended.	Commercial parking is screened by architectural elements such as stone cladding and simulated wood fencing.
4.6.3 Parking provided is to be separated between commercial and office use parking spaces (i.e. nonresidential parking) and residential parking spaces (i.e. parking for residents and parking for their visitors).	Residential parking is separated from commercial
4.6.4 There will be a security gate between the non-residential and residential parking. The security system will be designed to allow access to visitor's parking spaces by visitors, but retaining security to the portion of the parking spaces for residents should be recommended where the overall building configuration makes it possible.	A security gate is provided for residents, with entry-phone access for visitors.
4.6.5 Residential parking shall separate parking for visitors and parking for residents.	Visitor parking is located at the front of the below-grade parkade.
4.6.6 A gate into the parking area from the street level is optional as determined by circulation and security considerations.	A gate is provided for after-hours security.

4.6 Parking Considerations For All Phases	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
4.6.7 Pedestrian access, including those with restricted mobility, will be designed to provide safe and convenient access between the separate parking areas and the portion of the building the parking is intended to serve. Access from residential parking areas will take into consideration families with children and individuals using mobility assistance equipment.	Phase 2 provides accessibility compliant access points for all entries to CRUs, residential areas, and parkade lobbies.
4.7.1.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Ground-level apartments: Designed to blend into the whole building facade, the two storey units at grade will contribute to a richer, more human scale at the pedestrian level. With front doors 0.6m (2 ft) above the street level, these two-storey units generally follow the principles of a townhouse design. Raised entry areas provide comfortable semi-private space for a garden and patio.</p> <p>2. Massing: A strategy of setting floors back at the upper levels will help to soften the building massing and increase access to daylight. Setbacks are especially encouraged in mid-block courtyards for daylighting and privacy as well as providing generous deck space for residents. On all buildings, where possible, interesting roof shapes are encouraged to enrich the overall texture and visual amenity of the development. These may include curving forms, deep overhangs, sloped planes and the like, appropriate to an understated modernist expression. Stair enclosures serving roof decks can also add to this diversity of form.</p> <p>3. Green roofs: It is anticipated that some green roofs will be provided on most of the concrete buildings. This principle would further enhance the character of the building significantly. Green roofs should be designed as visual amenity as well as an attractive environment for outdoor common space. In wood framed buildings, roof vegetation should be incorporated in self contained planters.</p> <p>4. Transparency and legibility of indoor public spaces: Common interior spaces such as entry lobbies, stairs, exercise rooms, lounges and the like should be afforded as much transparency to the exterior as possible, encouraging a visual connection between these spaces and the exterior community.</p>	<p>Massing: The building demonstrates a clear strategy of setting back levels 2-6 softening the massing and creating generous deck spaces, as seen in the stepped design. The varied material palette and articulated facade further enrich the building's texture and visual interest.</p> <p>Green roofs: Level 2, being concrete, will include a green roof system.</p> <p>Transparency and legibility of indoor public space: The ground floor features large expanses of glazing, indicating a focus on transparency and visual connection between interior spaces and the exterior community. Common areas like lobbies, CRUs, and amenities will benefit from ample natural light and visual accessibility.</p>

4.7.2.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Design expression: The potential for mixed-use buildings to enrich the Town Centre is high and should be incorporated wherever possible in the architectural design. In responding to the urban design roles mentioned above, the design should:</p> <p>A. Provide a comfortable scale for a walkable shopping environment; commercial tenancies (i.e. storefronts) are to be divisible into increments of between 5.5 and 7.5 meters.</p> <p>B. The emphasis should be towards boutique style commercial tenants and not the typical large anchor tenant (The frontage would be greater, providing more exposure). Creative storefront merchandising will also be necessary to provide views into these spaces, as well as views to the street from within the commercial units.</p> <p>C. The residential portions of the mixed-use buildings will generally follow the design principles for multi-family buildings as previously described.</p> <p>2. Massing:</p> <p>A. Generally 4 to 6 storeys in height. The primary aim in the design of mixed-use buildings is to create a streetscape composed of a series of different but complementary building frontages ranging in width from 7.5 to 20m.</p> <p>B. An overheight ground floor of approximately 3.6 m provides good flexibility for a variety of retail uses as well as streetfront space.</p> <p>C. Setbacks at the uppermost residential storeys of mixed use buildings will be encouraged to create a comfortable street level scale and increased privacy for residents. Setbacks should be used to help differentiate between the commercial and residential parts of a building. Each use should be given a clear expression through a change of materiality, a change of plane or a combination of both.</p>	<p>Phase 2 provides a comfortable scale for a walkable shopping environment, with commercial tenancies divisible into increments of 5.5 to 7.5 meters. CRU sizes are limited to 1000 SF for “boutique” style commercial tenants, while maintaining headroom of 5.33m. The residential portions of the mixed-use 6 storey building follows the design principles for multi-family buildings as previously described, with a focus on upper-level setbacks, a variety of unit types, and a clear distinction from the commercial components through material and plane changes.</p>



4.7.3.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Massing: T&amp;GLA's forms and articulation should clearly distinguish individual units both in plan and elevation. This can be achieved in a variety of ways including sloped roof, recesses, vertical 'framing', etc. Consistent with the concept of a small-scale form, setting back the frontage above the second level is encouraged to further reinforce the legibility of each unit. Architectural elements such as individual roofs and entry canopies should be considered. Simple roof shapes can provide further architectural interest.</p> <p>2. Access to outdoors: Semi-private spaces should be designed to further distinguish individual units while providing a rich landscaped buffer between units and the public realm. At upper levels, balconies, terraces and roof decks are strongly encouraged to provide a more private outdoor experience as well as providing increased articulation.</p> <p>3. Simple, clean expression: T&amp;GLA's designs should reflect the current modern trends around the world - using solid planes to contrast with generous areas of glazing and clearly defined outdoor spaces.</p>	<p>The building's mid-century design forms and articulation clearly distinguish individual units in plan and elevation, using elements like flat roof overhangs, recesses, and vertical framing with horizontal expressions. Glazed canopies provide weather protection and shade along DTR. The materiality of the commercial level is clearly separated by the characteristics of the residential levels.</p> <p>The building includes balconies, terraces, and a rooftop amenity terrace at upper levels to provide a more private outdoor experience and increased articulation. The rooftop amenity terrace is a significant outdoor space that provides residents with opportunities for recreation, relaxation, and communal gardening.</p>
4.7.4.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. General expression: Anticipating the coming changes to the BC Building Code in addressing thermal performance, ERA looks to introduce an aesthetic for towers that recalls the simple, strong and clean expression of elements associated with modern European design; with the goal of achieving balanced window to wall ratios in alignment with current thermal performance requirements, towers should strive to achieve a composition of simple forms and massing.</p> <p>2. Massing: Tower floor plate sizes are based on an average gross floor area of 650 m<sup>2</sup> (7,000 sq ft).</p> <p>3. Articulation and orientation: It is intended that towers be generally oriented with the long axis running north-south to optimize access to daylight and minimize the impact on upland views.</p> <p>4. Consideration should be given to how the towers are terminated at the upper levels. Tops of towers should be articulated to ensure continuity of the design throughout the buildings.</p>	<p>Phase 2 is a low-rise 6 storey building and will focus on thermal performance to meet step code 3 requirements.</p>

4.7.5.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>1. Design expression:</p> <p>A. In the context of the contemporary architecture proposed throughout ERA, the commercial buildings provide an opportunity for unique and innovative design. A dramatic expression responding to their prominent placement and their specific use is expected to set them apart and enhance the architectural diversity of the development. At the same time, these buildings should complement the scale, giving careful attention to articulated streetwalls that enhance the pedestrian experience.</p> <p>B. Following the principles of good sustainable design, these buildings will also strive to deliver an enhanced workplace for occupants. Access to daylight, glazed stair enclosures to encourage their use, access to outdoor space, operable windows and envelope systems designed for optimal energy conservation are anticipated initiatives that will impact the exterior building design.</p> <p>C. Addressing the above goals, designers are challenged to create a new model for commercial buildings in the ERA development.</p> <p>2. Massing:</p> <p>Significant ground floor height gives the commercial buildings a scale appropriate to their gateway roles and to their frontages on Dewdney Trunk Road. Designs should seek to create visually engaging frontages at this level, ensuring these significant masses are broken down into increments to create a more comfortable scale for pedestrians. Active spaces should be located at the perimeter, as opposed to closed individual offices. Public lobbies, located along Dewdney Trunk Road, Garden Street and 223rd Street should be highly transparent and clearly visible for pedestrians.</p>	<p>The design of the Phase 2 is described as mid-century modern, with a focus on a nuanced expression that responds to their prominent placement and specific use. The articulated streetwalls, achieved through variations in materials, setbacks, and glazing, contribute to a more engaging pedestrian experience.</p> <p>Phase 2 is committed to good sustainable design, including access to daylight, glazed stair enclosures, access to outdoor space, operable windows, and an energy-efficient envelope system, combined with efficient HVAC.</p> <p>The South glazed façade along DTR is broken up into segments using stone cladding to separate the individual commercial retail units.</p>
<p>3. A livable workplace:</p> <p>A. Occupant comfort is a primary aim for these buildings. As one of the simplest but most effective ways of creating livable study and office environments, good daylighting should be an integral part of the building design. This starts with work stations located within 9m of exterior glazing but can be optimized by bouncing light deeper into the space using light shelves and tempering glare with the use of exterior sunshades. Centre atrium can also increase recess to daylight.</p>	<p>The proposed indoor amenities include study areas with access to daylight. Note that this project will not contain office spaces.</p>

4.7.5.2 Form and character	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>B. Control of one's workspace environment is another aspect of user comfort and can influence the external expression. Options for occupants include operable windows, mechanical systems zoned for smaller areas where they can respond more quickly to individual demands and access to outdoor space.</p> <p>C. Balconies, terraces and roof decks are all opportunities for a breath of fresh air, a coffee break or an ad hoc meeting. Their contribution to the exterior architecture can also be very positive, helping to articulate the mass in a variety of ways as well as bringing life to the street.</p>	<p>Phase 2 highlights the importance of occupant control over their workspace environment by providing operable windows, mechanically zoned areas for individual temperature control, and access to outdoor space.</p> <p>This is further enhanced by providing residents with access to indoor amenity study areas mentioned above.</p>
4.9 Commercial Retail Units (CRU)	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>4.9.1 CRU design principles. The following principles should be implemented in all phases of the development where CRU's are included:</p> <p>A. Provide a minimum of 0.6m setback from the property line for the storefront facades.</p> <p>B. Locate active commercial uses on the ground floor adjacent to the sidewalk, including retail, restaurants, and personal service uses. Ground floor street frontage space is to be predominantly for active, pedestrian-oriented uses.</p> <p>C. Provide continuous storefront windows, open air store frontages, and frequent, highly visible entrances for ground floor commercial uses adjacent to the street and sidewalk.</p>	<p>Phase 2 prioritizes pedestrian-oriented design with active commercial spaces on the ground floor, set back 0.6m providing a comfortable scale for a walkable shopping environment.</p> <p>The building's form and articulation create a unique and visually engaging presence while respecting the pedestrian experience and incorporating elements like as access to daylight, glazed curtain walls with visible entrances, canopies, and outdoor spaces for commercial use.</p>
<p>4.9.2 Materials and proportions</p> <p>A. Glazing is encouraged in all retail units and should be predominant in all the building facades.</p> <p>B. High quality and durable trim materials, such as anodized or painted aluminum, are permitted and encouraged for windows and door trims.</p> <p>C. Continuous store windows and frequent, highly visible display storefronts along the street are a paramount feature as they provide visual interest and promote walkability.</p> <p>D. Open air store frontage type is acceptable along the internal corridors of the development. It brings the retail activity to the street, engaging pedestrians who walk by.</p>	<p>A. The project encourages the use of glazing in all retail units and building facades, with a focus on high-quality, durable trim materials like cultured stone cladding, and aluminum for windows and door trims.</p> <p>B. Continuous store windows and frequent, highly visible display storefronts are emphasized to provide visual interest and promote walkability.</p> <p>C. Open air store frontages are provided along the CRU entries to bring retail activity to the street and engage pedestrians.</p>



4.9 Commercial Retail Units (CRU)	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
E. Retail glazing is to be clean and not include frosted glass.	Retail glazing is clear glass.
4.10.1 Key Design Elements	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>All building entrances within the development would take into consideration the following:</p> <p>A. Locate all customer entries and entryways to be directly visible from the public sidewalk, and accessible from public and private walkways. Corner buildings are encouraged to have corner entries but it is not mandatory.</p> <p>B. Provide a pedestrian walkway from the public sidewalk to the entry frontage of buildings set back from the street edge.</p> <p>C. Design public street facing residential facades with individual entries, such as steps, porches, and paths from living units to the street to help break down the scale of multi-unit buildings.</p> <p>D. Audio clues, such as a small fountain or rustling plants, and olfactory features such as fragrant plants can also assist.</p> <p>E. Artificial lighting can highlight the entrance to a building and make it more obvious at night for everyone.</p>	<p>A. Phase 2 ensures that all customer entries and entryways are directly visible from the public sidewalk and accessible from public and private walkways. Corner entries are provided for corner CRUs, while residential entries are separated using setbacks and differentiating materials.</p> <p>B. The project provides pedestrian walkways from the public sidewalk to the entry.</p> <p>C. Residential units are on L2-6, stepped back from the commercial setback. Planting along the L2 perimeter helps separate the living units from the busy DTR street.</p> <p>D. The project's diverse use of planting will provide audio clues.</p> <p>E. Landscape lighting will help illuminate building entries and pathways at night.</p>
4.11 Awnings and Canopies	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>A. Glazed canopies with laminated clear glass and/or diffused glass must be used above all CRU's along Dewdney Trunk Road.</p> <p>B. Canopies should extend from the face of the building to provide sufficient pedestrian weather protection.</p> <p>C. Any canopy that might encroach into the City's right-of-ways must be designed to be removable and are subject to City approval.</p>	<p>A. Glazed canopies are provided along DTR above all CRUs.</p> <p>B. Canopies extend to their maximum depth up to the property line.</p> <p>C. See point B</p>

4.12 Signage and Lighting	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>A. Entry points must be clearly identified through architecture, signage and landscape design.</p> <p>B. Signage to parking must be clearly visible.</p> <p>C. Tailored merchant signs and lighting will serve to personalize each storefront. Signage is encouraged to be hung from underside of glazed canopy.</p> <p>D. Simple surface mounted fixtures should be considered to provide the required light levels at entries. Large backlit signs are not encouraged.</p> <p>E. Provide decorative elements to add personality and individuality to different establishments.</p> <p>F. Use 'warm light' sources (LED preferred) for superior visual acuity and enhanced perception of light. No higher than 3100 K.</p>	<p>A. Entry points will be identified through landscaped pathways, facade materials, and lighting, and signage.</p> <p>B. Parking and loading signage will be clearly visible.</p> <p>C. Signage will be positioned at the underside of the glazed CRU canopy.</p> <p>D. Surface mount light fixtures will be provided between each Retail unit.</p> <p>E. Retail units will be clearly separated by the use of stone cladding.</p> <p>F. Our electrical consultant has been notified.</p>
4.13 Digital Information Board	Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable.
<p>Located in Phase 7 of the development, the digital information board is meant to become a versatile digital tapestry capable of conveying multiple graphic mediums. It should also be conceived in such a way that acts as a public art element. The form and structure has intentionally not been defined at this time but should compliment the eventual architectural expression of adjacent buildings and the public plaza along 224 Street.</p>	<p>To be included in Phase 7.</p>

### Project information

To be completed by the Architect on record for this project:

File number: 2021-586-DP

Date prepared: 2025-02-24

Architect: RICHARD BERNSTEIN  
Print Name



\_\_\_\_\_  
Signature



# Town Centre Development Permit Area Guidelines Downtown West Precinct

Pursuant with Section 8.11 of the Official Community Plan, development in the Town Centre will be assessed against the following form and character and green building design guidelines. The guidelines apply to Ground-Oriented Multi Family; Low-Rise Apartment; Medium & High-Rise Apartment; Flexible Mixed-Use; Town Centre Commercial; Port Haney Multi-Family, Commercial & Waterfront; and Port Haney Heritage Adaptive Use. See the guidelines in Section 8.11 of the Official Community Plan for a detailed list and descriptions of development permit area guidelines.

These guidelines are intended to aid in the review of development permits in the Town Centre and are to be completed by the architect of record for the project. The project will be reviewed for consistency with the guidelines by the Planning Department and the Advisory Design Panel.

In the checklist, you are only required to address the guidelines with the icon that relates to your project:

**TCC** = Town Centre Commercial (commercial developments only)

**MU** = Mixed-Use (ground-oriented developments, with commercial on the ground level and either offices or residential above)

**MFR** = Multi-Family Residential (ground-oriented developments and low-rise, medium-rise, and high-rise apartments)

Precinct Key Guidelines and Green Building Design Guidelines must also be completed for projects within the Town Centre.

## **Development and Design Objectives**

The following summarizes the development and design objectives for each section on the following checklist.

### **A. Building Setbacks, Form, Mass and Height**

- To promote a cohesive building style and strong pedestrian oriented urban realm in Maple Ridge Town Centre by ensuring new buildings, renovations and/or additions have consistent architectural and urban design setbacks, form, mass, and height.
- To help define the street and sidewalk areas as active public spaces.

### **B. Building Façades, Materials and Colour**

- To ensure additions, renovations and/or new infill projects in the Town Centre have a coherent architectural design concept where windows, doors, siding material and other façade elements create a pleasing composition compatible with surrounding buildings, commercial and neighbourhood character.
- To enhance the architectural and massing concepts of a building as well as the quality, character and vibrancy of the urban environment of the Town Centre through the use of harmonious, quality materials and colours.
- To screen rooftop and ground mounted mechanical equipment and trash storage from public view and thereby ensure commercial and mixed-used buildings maintain an attractive appearance to the street.

### **C. Building Site Considerations**

- To ensure public outdoor spaces are designed so that they improve use and activities, incorporate universal access, reduce vandalism, increase safety and provide more attractive, functional outdoor spaces in the Town Centre.
- To provide street trees and landscape elements that reinforce the 'urban' character and vibrancy of the Town Centre, enrich the pedestrian friendly character of streets in the district, and integrate this important commercial and higher density residential area with the character and quality of the surrounding residential neighbourhood.
- To ensure parking lots are designed to be accessible, but do not intrude upon the surrounding residential area, nor the urban, pedestrian-oriented quality of the Town Centre.
- To facilitate off-street parking and car storage at the rear of commercial and mixed-use buildings to maintain street inter-connectivity, traditional use of the lane as a service street, and to provide a secondary vehicular and pedestrian thoroughway in the Town Centre.
- To ensure service loading and mechanical equipment is designed to protect the surrounding businesses and residential areas from unsightly, noisy and noxious environments.





# Town Centre Development Permit Area Guidelines Checklist

		Meet Guidelines:	Yes X	No	Not Applicable
<b>A. Building Setbacks, Form, Mass, and Height</b>					
<b>A.1 Building Mass and Form</b>					
A1.1	Maintain the mass and scale of buildings	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A1.2	Enhance the block with corner commercial buildings	TCC MU	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A1.3	Accent corner buildings	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A1.4	Use pedestrian-scale design elements	TCC MU	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A1.5	Feature pedestrian amenities	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A1.6	Design large buildings into smaller modules	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A1.7	Accommodate street-fronting units	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A1.9	Ensure appropriate roof pitch	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A1.10	Use design elements to reduce roof mass and scale	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>A.2 Building Heights</b>					
A2.1	Vary building heights	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A2.2	Maintain alignment of architectural features	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A2.3	Integrate taller buildings	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A2.4	Step back taller buildings	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A2.5	Match building heights at the end of blocks	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A2.6	Manage phased development	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A2.7	Protect views	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>A.3 Building Setbacks</b>					
A3.1	Place buildings to reinforce sidewalk activity	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A3.2	Situate building entrances for visibility	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A3.3	Provide adequate throughways and lighting	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A3.4	Provide clear sight lines from building foyers and lobbies to allow visual surveillance	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A3.5	Separate residential entrances from commercial entrances	MU	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A3.6	Respect existing buildings	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A3.7	Distinguish entrances with arrival areas and courtyards	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A3.8	Locate ramps and entrances in areas that are highly visible	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Explain how the objectives for Building Setbacks, Form, Mass and Height are met. If you have selected “no” or “not applicable”, explain why the guideline does not apply to your project or why it cannot be met.</p> <p><b>Building setback</b>            Building setbacks are considered for the project as a way of creating open spaces and incorporating pedestrian amenities such as soft landscaped areas, outdoor patios, street furniture, canopies and alignment of massing. Also for creating animated street walls, interesting rhythms and frontages. Refer to plans and elevations.            Setback around the project and along different perimeter roads : On 223rd street it is 10 feet (3.05m), on Dewdney Trunk Road it is 6 feet ( 1.83 m). The setback along north-south pedestrian mews is 29'-6" (9m). This includes space required for the fire lane and stormwater detention tanks at P1 level. The setback along the east-west connection is 14'-5" (4.40m).</p> <p><b>Building form</b>            Phase 2 massing is as per ERA design guidelines and provides a mix of a residential building, with outdoor amenity space. The building edges help shape and form the pedestrian and public realm and divides private, semi public and public spaces. A Legible entrance to the residential building, retail or the amenity to animate and support the public realm are located on all the surrounding streets. Phase 2 is a 6 storey residential building on L2, which is set backed from the level 1 commercial. The amenities is located on the North West wing as part of the building form.</p> <p><b>Height</b>            Phase 2 building height will remains classified as a low-building at 6 storeys, minimizing shadow impact and preserving views.</p>					



# Town Centre Development Permit Area Guidelines Checklist

Meet Guidelines:		Yes X	No	Not Applicable
<b>B. Building Façades, Materials and Colour</b>				
<b>B.1 Building Façade</b>				
B1.1	Address both sides of the block with corner commercial buildings	TCC MU	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.2	Orient main entrances to face the sidewalk	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.3	Locate windows, doors, and entry features at the street level	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.4	Use a mix of common facade patterns and elements	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.5	Reflect original façades and building scale	TCC MU MFR	<input type="checkbox"/>	<input checked="" type="checkbox"/>
B1.6	Respect original architectural elements	TCC MU MFR	<input type="checkbox"/>	<input checked="" type="checkbox"/>
B1.7	Respect old and new design	TCC MU MFR	<input type="checkbox"/>	<input checked="" type="checkbox"/>
B1.8	Maintain the horizontal rhythm of the street wall	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.9	Provide a visual division between the street level and upper floors	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.10	Include continuous canopies, awnings or overhangs	TCC MU	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.11	Ensure appropriate placement and materials for awnings or canopies	TCC MU	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.13	Use windows to provide 'eyes on the street'	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.14	Enhance the public realm	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B1.15	Ensure signage reflects building scale, character, and materials	TCC MU	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>B.3 Building Materials</b>				
B3.1	Enhance the public realm with high quality materials and detailing	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B3.2	Use materials consistently	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B3.3	Avoid the use of inappropriate materials	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B3.6	Use a mix of quality materials	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>B.4 Building Colours</b>				
B4.1	Select appropriate colours	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B4.2	Highlight architectural details, awnings, and entrances	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B4.3	Ensure a cohesive, consistent colour palette	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>B.5 Screening and Storage</b>				
B5.1	Locate and enclose trash, composting, and recycling to keep out of site of general public	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B5.2	Screen mechanical equipment	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B5.3	Avoid conflict with neighbouring properties	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
B5.4	Locate building ventilation systems to minimize noise and exhaust	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Explain how the objectives for Building Façades, Materials and Colour are met. If you have selected "no" or "not applicable", explain why the guideline does not apply to your project or why it cannot be met.</p> <p>Architectural expression and design elements at the building base such as entrances, canopies, building materials, green walls, base building materials etc, are in a pattern, scale and proportion that relates to neighboring buildings and engages pedestrians. High quality, durable materials with appropriate textures achieve visual interest and longevity of the façade.</p> <p>With the colour palette selected, a harmonious relationship with the neighboring buildings is enhanced not only through materials and colours but also through height transition and built form design.</p>				



# Town Centre Development Permit Area Guidelines Checklist

		Meet Guidelines:	Yes X	No	Not Applicable
<b>C. Building Site Considerations</b>					
<b>C.1 Public Outdoor Space and Hardscapes</b>					
C1.1	Provide public outdoor space	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.2	Ensure public outdoor space is highly visible	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.3	Provide connections between buildings, sidewalks, and outdoor open spaces	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.4	Ensure universal access for all public spaces	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.6	Provide hardscape elements to enhance the street environment	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.7	Design hardscape elements as part of the building	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.8	Integrate pedestrian amenities with walls and/or landscaped areas	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.9	Provide public art	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.10	Ensure new elements complement existing	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.12	Provide smooth routes	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C1.13	Ensure barrier-free access	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>C.2 Parking and Parking Lots</b>					
C2.1	Provide required parking underground, where feasible	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C2.2	Screen large surface parking lots while maintaining surveillance	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C2.3	Maximize pedestrian safety within parking lots	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C2.4	Provide visible signage	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C2.5	Consider developing underground parking garages	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C2.7	Locate parking lot equipment away from the public street	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>C.3 Lanes, Service and Loading Areas</b>					
C3.1	Use lanes for service, parking access and loading	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C3.2	Utilize lanes as secondary vehicular and pedestrian thoroughways	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C3.3	Strengthen visual access of the lane	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C3.5	Consider lanes as a community amenity	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C3.7	Locate loading and service areas away from the street front	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C3.8	Separate loading from parking and pedestrian paths (It's screened)	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C3.9	Screen loading areas	TCC MU	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>C.4 Street Trees and Landscape</b>					
C4.2	Use the right species	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C4.8	Maintain sight lines	TCC MU MFR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Explain how the objectives for Building Site Considerations are met. If you have selected "no" or "not applicable", explain why the guideline does not apply to your project or why it cannot be met.</p> <p>C3.1 to C3.5 - the site doesn't have a lane access. However main parking, waste collection, and loading accesses to the site is through 223rd street and the loading is screened from public realm by the building elements in both phases. C3.1 to C3.5 applies to 223rd street access. All waste collection activity will happen inside private property.</p> <p>All pedestrian elements within both the public &amp; private areas carry through the elements from Phase 1 to this current development. The scales shift slightly as we increase in density on this site. As such the character and scale of the planting design and tree canopies adjust to maintain the same feelings of enclosure and/or openness as experienced travelling into the site from the west. The paving treatments are consistent between the developments; with full accessibility to all areas of the development (using poured concrete and concrete pavers) Contrasting colours are used in the material to clearly suggest subtle changes in programming. With the E-W Central Promenade continuing and widening on this site, elements of built-in seating (for rest and passive engagement with adjacent programs) and lighting are added to ensure this corridor is safe, comfortable and usable year-round after dusk. All streetscapes are anticipated (through civil &amp; municipal coordination) to continue the same approaches on Dewdney Trunk Road and 223 St., with appropriately sized street tree canopies that reduce the scale of the buildings and buffer the speed of the traffic on the roads.</p> <p>Along the E-W Central Promenade on the west side, we chose to mirror the existing node at Phase 1 development (ie. Kids Playground) with a substantial dog-run and a central lawn for lounging that will service the residents of this neighbourhood. This positioning will solidify a clear central node between Phase 1 and 2. Much more seating is provided through this section of the central promenade and at the amenity area to allow residents and visitors to "stay in" rather than just "move-through" this lush linear park. This makes it much more of a memorable place within the Maple Ridge Town Centre.</p> <p>As stated previously, the desire for universal access to people of all abilities is paramount to the success of this public realm (including the E-W Central Promenade). Wherever possible, soft spaces are positioned for wayfinding to clearly delineate paths of travel to central amenities, while also working on reducing the scale of these large open spaces to comfortable human-scaled experiences.</p>					

Residential and visitors' parking are located in two levels of underground parking. Commercial parking and loading are located on L1. Loading areas are screened from 223rd street by building elements and is within the property line.

Maple Ridge Town Centre

Development Permit Area Guidelines Checklist





# Town Centre Development Permit Area Guidelines

## Downtown West Precinct Key Guidelines

### KEY GUIDELINE CONCEPTS

#### 1. Provide a gateway to the Town Centre

- a. Does proposed development maintain architectural quality and character of associated new development?

• Consistent: Yes ☒ No ☐ N/A ☐ Yes, it does. By :

- ERA Ph2 impact on the local community and environment by the proposed use and scale

Explain: - How well the development creates a sense of place and has distinct architecture and landscape

- Well-connected public realm corridors and spaces: East-West & North-South pedestrian mews and the amenities

- How the design is accessible and welcoming

#### 2. Create a pedestrian-oriented, boutique-style shopping district

- b. Does the building's form and mass support a strong pedestrian-oriented urban realm and help define the street and sidewalk areas as active public spaces? Taller buildings should be stepped back podium style.

• Consistent: Yes ☒ No ☐

Explain: Refer to the response on page 2

#### 3. Enhance the quality, character and vibrancy of the Town Centre

- c. Does proposed development promote the quality, character, and vibrancy of the urban environment?

• Consistent: Yes ☒ No ☐

Explain: Yes, it does. By how the use of varied and related colours, material textures, planting, light and integrated storm water management enliven the town center environment. Educational signage on the east-west mews provides the educational opportunity for the community to understand how the integrated stormwater management principles for the entire site and how rain water harvesting for irrigation from the green roofs work.

- d. Are colours consistent and materials of sustainable quality?

• Consistent: Yes ☒ No ☐

Explain: Yes, they have longer life span (more than 50 years) and lower maintenance requirements and are durable i.e Fiber Cement panels. Same applies to window/wall materials in the tower.

#### 4. Reference traditional architectural styles

- e. Does the development incorporate traditional architectural styles that include materials such as brick and wood?

• Consistent: Yes ☒ No ☐

Explain: The Clubhouse uses Glulam posts as the main facade features. Soffit at both low-rises are simulated wood soffit to bring West Coast architecture warmth and character together with other materials.

- f. Do buildings have characteristics that reflect good urban design principles, for example consistency in setbacks, form, mass and height throughout the precinct?

• Consistent: Yes ☒ No ☐

Explain: Refer to the response on page 2

#### 5. Capitalize on important views

- g. Does proposed new development capitalize on mountain and/or river views?

• Consistent: Yes ☒ No ☐ Not Applicable ☐

Explain: With its longest elevation on the north-south axis the building exposes the maximum number of units to either morning or afternoon sunlight. This arrangement still allows excellent views to the northwest and northeast of the surrounding mountains and on the south and southeast side to beautiful views of Mt. Baker. Courtyard units have views to internal courtyards while the outer units have views of the surrounding streets.



# Town Centre Development Permit Area Guidelines

## Downtown West Precinct Key Guidelines

- h. Have the important views of existing buildings been considered in relation to the proposed development?

• Consistent: Yes ☒ No ☐ Not Applicable ☐

Explain: Existing buildings are :  
 - West : ERA Phase 1 with the shared view of north-south pedestrian mews  
 - South : Retail CRUs on Dewdney Trunk Road to view ERA Phase 2 retail frontage and 5 levels of residential above  
 - North : School board site and proposed multi family development have view of Phase 2 and 3

### 6. Provide public outdoor space

- i. Are public spaces designed to accommodate a range of activities, incorporate universal access, reduce vandalism, and increase safety?

• Consistent: Yes ☒ No ☐

Explain: Yes it does  
 - east-west pedestrian connection :  
 - South : Retail on Dewdney Trunk Road to view ERA Phase 2 retail frontage and 5 levels of residential above  
 - North : School board site and proposed multi family development have view of Phase 2 and 3

### 7. Provide climate appropriate landscaping and green features

- j. Are landscape elements designed to enrich the pedestrian environment, moderate the internal building climate, manage stormwater on site, and reference the architectural quality of the building(s)?

• Consistent: Yes ☒ No ☐

Explain: Yes, the landscape planting design is designed with seasonal expression both aesthetically, providing a year-round gardens of visual interest, and functionally, considering solar gain shading/penetration through tree canopies. The soil volume on site is a significant mitigation of stormwater events, meeting Tier A requirements to manage all runoff on site through infiltration & detention. The architecture of the building is not specifically references, but the planting design responds to the massing to create a more pedestrian-scaled environment in the public realm.

### 8. Maintain street interconnectivity

- k. Does proposed development maintain street interconnectivity and the use of the lane as a service street and secondary vehicular and pedestrian thoroughway?

• Consistent: Yes ☒ No ☐ Not Applicable ☐

Explain: Yes, north-south mews (which separates Phase 1 & Phase 2&3) and east-west mews (which separates Phase 2 & Phase 3 in) are pedestrian thoroughways. East-west pedestrian spine connects all seven Phases of ERA development.

- l. Is required parking provided underground?

• Consistent: Yes ☒ No ☐

Explain: Yes, there are two levels of underground parking which are all residential and visitors.



# Town Centre Development Permit Area Guidelines

## Green Building Techniques

### A. Building Setbacks, Form, Mass, and Height

A1.8 Design flexible ground-floor unit spaces in commercial and mixed-use buildings.

- Consistent: Yes ☒ No ☐

Explain: Commercial space is extended along the entire Phase 2 Dewdney Trunk frontage with multiple access points to provide maximum flexibility for future planning. Tenancies and ground floor layout will be decided by the client and the market demands.

A1.11 Accommodate roof gardens, trellises, and green features.

- Consistent: Yes ☒ No ☐

Explain: Yes, low impact development (LID) green features are integrated throughout. Outdoor covered spaces/trellis is provided at the Clubhouse, a central location where gathering may occur year round and over P1 ramp. No specific opportunities for urban gardening, although many spaces are able to be re-appropriated by strata for this program if so desired.

A2.8 Site buildings to capitalize on daylight and solar opportunities.

Explain: Yes, the U-shape design of the building takes advantage of sunlight throughout all times of day. The relatively low height also reduces the shadow impact to the surrounding properties. Glass canopies along DTR will provide rain coverage without blocking natural light.

A2.9 Protect solar access to surrounding buildings and minimize wind tunnel effects.

- Consistent: Yes ☒ No ☐

Explain: Solar gain in amenity areas has been duly considered. Deciduous trees have been preferred for their ability to shade in summer, but lose leaves to maximize sunlight in winter. However, some coniferous trees have been used in key areas for variety, for year-round screening between specific adjacencies that warrant more privacy, and to reduce the perceived scale of the building by pedestrians at key locations.

### B. Building Façades, Materials, and Colour

B1.12 Use exterior shading devices to block summer sun.

- Consistent: Yes ☒ No ☐

Explain: Shading is optimized by using overhangs for particularly the south, east and west elevations. The North courtyard will benefit the most by shading the summer sun.

B2.1 Design outdoor lighting to minimize light pollution.

- Consistent: Yes ☒ No ☐

Explain: Shading is optimized by using overhangs for particularly the south, east and west elevations. Landscape lighting will provide outdoor lighting for visibility.

B2.2 Encourage energy efficient lighting.

- Consistent: Yes ☒ No ☐

Explain: Yes. An efficient and effective use of lighting is considered by the project electrical engineer which is an important factor in energy management in a building and presents the design of energy efficient lighting systems.

B3.4 Select environmentally responsible building materials.

- Consistent: Yes ☒ No ☐

Explain: Materials will be carefully selected based on their environmental characteristics and life cycle. i.e fiber cement production relies on recycled materials and consumes fewer resources. Timber and wood is considered the most sustainable building material. Phase 2 is a wood frame building.

B3.5 Minimize the use of unsustainable building materials.

- Consistent: Yes ☒ No ☐

Explain: Unsustainable material are from resources that can not be replenished such as plastic - which won't apply to ERA Ph2







# Town Centre Development Permit Area Guidelines

## Green Building Techniques

- B3.7 Consider life-cycle cost.
- Consistent: Yes ☒ No ☐

Explain: Life cycle cost analysis (LCCA) is a process of evaluating the economic performance of a building. Although it's outside of the scope of architect's work, the ownership group will consider the cost of maintenance, operations, utility cost, etc against their investment.

### C. Building Site Considerations

- C1.5 Locate outdoor plazas to capture the sun.
- Consistent: Yes ☒ No ☐

Explain: Phase 2 at the west section of east-west promenade widens to accommodate a central lawn and public open space in transitioning to Phase 1 existing playground node. The location and configuration is ideal for sun exposure. In addition the amenity entrance and frontage is located here with ample seating. Phase 2 commercial corners are ideal sunny places for spill-out spaces such as patios

- C1.11 Use materials that are functional, durable and include recycled or salvaged content.
- Consistent: Yes ☒ No ☐

Explain: Materials will be carefully selected based on their environmental characteristics and life cycle. i.e fiber cement production relies on recycled materials and consumes fewer resources. Timber and wood is considered the most sustainable building material. Phase 2 is a wood frame building

- C1.14 Encourage use of infiltration techniques.
- Consistent: Yes ☐ No ☐

Explain:

- C2.6 Locate adequate priority parking in visible areas convenient to entrances.
- Consistent: Yes ☒ No ☐

Explain: Disability parking spaces are close to the accessible building core entrances at the underground parkade.

- C2.8 Use permeable pavement and infiltration devices on appropriate sites.
- Consistent: Yes ☒ No ☐

Explain: Yes, permeable paving is continued along the fire access lane for terra-firma infiltration. All other paving on-slab is directional drainage to soil volumes for infiltration & on-site detention.

- C2.9 Provide shade trees and landscaping.
- Consistent: Yes ☒ No ☐

Explain: Yes, provided, refer to Plant List on L2.1

- C2.10 Provide secure and sheltered bicycle storage facilities for short-term uses.
- Consistent: Yes ☒ No ☐

Explain: Secure short term visitor bike parkings (class B) are located at-grade throughout the site, specifically at Lobby entries, the Central Promenade, and along 223 St offsite blvd. and placed at the furnishing and planting zone. Refer to L1.1 and L1.2

- C2.11 Provide long-term bicycle parking.
- Consistent: Yes ☒ No ☐

Explain: Yes, provided, refer to A102 parking plans and A103 level 1





# Town Centre Development Permit Area Guidelines

## Green Building Techniques

- C2.12 Provide end-of-trip facilities.
- Consistent: Yes ☐ No ☒

Explain: This is not applicable to ERA Ph2 with residential and commercial use.

- C3.4 Minimize impervious paving of the lane.
- Consistent: Yes ☒ No ☐

Explain: No impervious paving is placed in the fire-lane (north - south pedestrian connection).

- C3.6 Respect existing grades.
- Consistent: Yes ☒ No ☐

Explain: Existing grades are respected & coordinated with Civil.

- C4.1 Plant street trees.
- Consistent: Yes ☒ No ☐

Explain: Street trees are indicated for coordination through Offsite Servicing Agreement with CoMR, to follow design begun in Phase 1. Refer to arborist report and tree management plan for retained trees and number of replacement trees

- C4.3 Minimize use of high maintenance plants.
- Consistent: Yes ☒ No ☐

Explain: Plants selected are adaptive or native to this biome. Drought-tolerance is an intended sustainability feature of the planting design. Refer to L2.1 & L2.2.

- C4.4 Maximize the use of native and climate appropriate species.
- Consistent: Yes ☒ No ☐

Explain: Plants selected are adaptive or native to this biome. Drought-tolerance is an intended sustainability feature of the planting design. Refer to L2.1 & L2.2.

- C4.5 Consider the inclusion of community gardens.
- Consistent: Yes ☒ No ☐

Explain: Community gardens have been included on the rooftop level.

- C4.6 Design and place landscape to facilitate year round moderation of the internal building climate.
- Consistent: Yes ☒ No ☐

Explain: Yes, the landscape planting design is designed with seasonal expression both aesthetically, providing a year-round gardens of visual interest, and functionally, considering solar gain shading/penetration through tree canopies. The planting design responds to the massing to create a more pedestrian-scaled environment in the public realm ( applies to ground level unit only)

- C4.7 Minimize erosion potential.
- Consistent: Yes ☒ No ☐

Explain: All soil volumes are retained within concrete planters are flush with grade, or elevated and are sloped at less than 3:1. Erosion is not a concern.





# Town Centre Development Permit Area Guidelines

## Green Building Techniques

C4.9 Provide adequate landscape maintenance.

- Consistent: Yes ☒ No ☐

Explain: Plants selected are largely low maintenance, and irrigation should be smart-controlled. A maintenance contract will be absorbed by strata.

C4.10 Consider incorporating landscape plantings for green features.

- Consistent: Yes ☒ No ☐

Explain: Noted, & incorporated.

C4.11 Incorporate low impact stormwater features.

- Consistent: Yes ☒ No ☐

Explain: Noted & incorporated detention tanks.

C4.12 Consider rainwater collection for re-use.

- Consistent: Yes ☒ No ☐

Explain: Rainwater is being collected in retention tank for irrigation re-use at the clubhouse and for it's irrigation use.

C4.13 Use natural plantings and green space to support habitat.

- Consistent: Yes ☒ No ☐

Explain: Plantings are largely pollinator friendly, and provide habitat for many bird species through a variety of shrubs, grasses, and trees.

C4.14 Retain existing mature trees.

- Consistent: Yes ☒ No ☐

Explain: Refer to arborist report, few trees retained along Dewdney Trunk Road



2025-02-21







April 21, 2025

**File Number:** 2024-228-RZ – ERA Phase 2

**Project Address:** 12011 – 223<sup>rd</sup> Street, Maple Ridge, BC

**Application:** Development Permit Resubmission

**Attention:** Maryam Lotfi, Planner 2, City of Maple Ridge

**Subject:** 12011 – 223<sup>rd</sup> Street, Maple Ridge - Response to ADP comments

Dear Maryam,

In response to the Advisory Design Panel comments received on March 14, 2025, for the above-noted project, we have revised the architectural and landscape design to address the key concerns. The updated drawings reflect improvements to circulation, massing, shading, and integration with the surrounding context. Notable revisions include adjustments to the residential and commercial access, stair vestibules, and rooftop shading, as well as refinements to the loading area screening and courtyard lighting strategy. Landscape revisions incorporate a more diverse tree palette, adjustments to planter layouts, and compliance with accessibility and city offset requirements.

Please be advised that our design team has provided significant revisions to five of the seven architectural items that were raised by Design Panel. In the two items that were carefully considered but remain as designed we have provided further explanations regarding why we feel we have properly addressed the concerns with our initial design and do not feel it is feasible to revise further. Please see the comments and responses to each item in red below. With respect to the landscape architectural items that were raised by Panel, revisions have been provided which fully address all nine items raised.

We trust that these updates effectively respond to the City's feedback and will help advance the project to the next phase of approvals. Please find the attached updated architectural and landscape drawings for your review.

## RESPONSES TO ARCHITECTURAL COMMENTS

1. Residential lobby looks a bit tight and does not have proper access to the loading area. Consider adding access to loading area that can also act as second access to the amenity space.

**CDA: We have revised the commercial corridor to serve as a service corridor for both commercial and residential tenants. Residents will be provided with fob access to the amenities and to the lobby from the loading area, while commercial tenants will have access to the refuse room. The co-working area has been removed to open up the lobby and increase its overall size for the residents. We will work with the project's interior designer to replace the enclosed co-working with a more open seating space. Refer to sheet A104 for revised area.**

2. Recommend adding vestibule to the stairs in the parkade coming up directly to the residential levels.

**CDA: Vestibule has been included within the stair core on P2. The vestibule is W: 3'-9" and L: 8'-2". This has been reviewed and approved by our code consultant. Refer to drawing A103 for the updated plan and provided dimensions.**

3. Recommend that the mass relief on the topper floor be similar to the building mass in Phase 1

**CDA: This project provides a mass relief on Level 2, stepping the residential back from the commercial frontage. A further mass relief is provided by terminating the proud aluminum frames on Level 5 which achieves another setback on Level 6. If we were to reduce the mass further by altering the roof line, this would create conditions that significantly increase the risk of water penetrating the building envelope, which is not a preferred outcome.**

4. Recommend variations in overhangs to be similar to either the original DP or other developments in the area.

**CDA: Variations in overhangs is achieved through the articulation of the white frames along the East, West, and South of the building. This is further enhanced by providing varying roof overhangs along the East and West sides. A variation of sloped and flat overhangs at the south elevation have been provided to articulate a more playful rhythm along Dewdney Trunk Road. Refer to A301 and updated renders on A502 and A503.**

5. Consider providing access from the staircase adjacent to the elevators in the main lobby directly to the main lobby.

**CDA: Access has been provided by including a door to the North of the stair core on L1. Refer to updated sheet A104.**

6. The North-facing courtyard will be very shaded throughout the year. Consider stepping back the top floor to bring more light into the courtyard

**CDA: North light, while not direct, is excellent for providing a soft, diffused light, which is often preferred for outdoor amenity spaces. The reflected light from the light-coloured façade of the U-shaped building will create a brighter courtyard space than the shadow studies would suggest, and our landscape design will focus on plant selections that thrive in these conditions, creating a pleasant and usable space. When the courtyard is used most, the shade created by the surrounding massing will offer welcome relief during hot Maple Ridge summers. It is important to note that residents will also have access to a rooftop amenity terrace that receives ample sunlight throughout the day, providing an alternative for those seeking more direct sun exposure. After careful consideration, we have determined that a top-floor setback would provide marginal benefits and would compromise the architectural design, unit layouts, structural efficiency and feasibility for marginal benefits.**

7. Consider adding a trellis canopy and greenery over the loading area and garbage bin station to be less visible to surrounding buildings.

**CDA: While a full trellis canopy over the loading area presents operational challenges due to waste collection bin maneuvering requirements, we propose an alternative solution to mitigate visibility from the surrounding buildings. This solution comprises a raised 20-foot-tall, landscaped screen, supported by hollow steel columns, strategically positioned between the outdoor amenity space and the loading area to reduce visibility from the units across. Refer to sheet A104 and section 1/A402 for updated drawings.**

## RESPONSES TO LANDSCAPE COMMENTS

1. Consider adding one more tree species to the project as there is currently large concentration of Japanese Maple.

**DKL: We have substituted another multi-stem tree species to replace some of the Japanese maple and contribute to a more diverse tree palette.**

2. Recommend paring down the number of trees per planter. Industry standard is 10 cubic metres/tree.

**DKL: We removed two trees where soil volume per tree was the lowest. The individual and shared soil volumes are appropriate for the trees selections used in the design.**



3. Review the plant palette. Ensure species will thrive in designed in location.

**DKL: The plant palette and plans have been reviewed to better accommodate the available sunlight. Rooftop planting has been replaced with species that are more suited for full sun conditions. Courtyard planting has been adjusted to include only plants that will tolerate shaded conditions.**

4. On East elevation, trees will conflict with balconies above.

**DKL: The species chosen for the planter below the balcony are smaller growing species. The tree locations have been adjusted slightly to better suit the space.**

5. Ensure rooftop design plans align between two disciplines.

**DKL: The rooftop level has been coordinated with the latest architectural background.**

**CDA: Architectural drawings have been updated to match Landscape. Refer to roof plans on A102 and A107.**

6. Consider adding an element of shade to the rooftop (trellis, pergola, etc.)

**DKL: We have added a trellis to the rooftop level to provide an option of shaded seating and shading for the core.**

**CDA: Architectural drawings have been updated to match Landscape. Refer to roof plans on A102 and A107.**

7. Double check location of trees at street level to ensure that they do not conflict with City of Maple Ridge offset guidelines.

**DKL: Street level trees have been reviewed and adjusted as necessary to meet offset requirements from sanitary and hydrants. The Northern most street tree along 223 street was added in response to previous city comments and conflicts with meeting the required offset from the proposed lamp standard. One lamp standard on Dewdney is within the minimum offset of an existing tree and will be reviewed with the overall Offsite design. We are working with civil to address the noted conflicts as part of the offsite works.**

8. Improve angles of planters along north promenade to compliment phase 3 design intent more strongly.

**DKL: Planters along North promenade have been reviewed and a minor adjustment was made to better align with the angles in phase 3.**

9. Landscape Amenities are required to be accessible to all residents therefore the BC Building Code 2024 requirement of a 1600mm wide accessible path must be met.

**DKL: The design is generally compliant with the 2024 requirements and any pinch points can be easily modified to accommodate a 1600mm clear width.**

**CDA: Dimension has been provided to clearly indicate 1.6m clearance at the entrance of the outdoor amenity. Refer to L-1.1 and A104.**

Sincerely,



2025-04-21

---

Richard Bernstein, Architect AIBC, AIA  
Principal  
Chris Dikeakos Architects Inc.

## **Development Information Meeting Summary – April 8, 2025**

**Application: 2024-228-RZ**

**Location: 12011 223rd Street, Maple Ridge**

The applicant team held a Development Information Meeting (DIM) on April 8, 2025, from 5:00 PM to 7:00 PM at the Maple Ridge Library. The meeting was attended by approximately 22 individuals with most participants departing by 5:45 PM. Three representatives were present on behalf of the applicant: Sarah Glazier and Paul Williams of Williams Management, and Philip Polotsky of Chris Dikeakos Architects.

Of the attendees, 21 were immediate neighbours, specifically from ERA Phase 1 at 22255 Dewdney Trunk Road. One attendee was from outside the immediate area.

### **Key Comments and Concerns Raised:**

#### **1. Strata Separation and Parkade Connectivity:**

Residents of ERA Phase 1 expressed concern about potential physical or financial connections between their strata and the proposed development. Specifically, they sought reassurance that there would be no shared access to the underground parkade and no impact on their strata fees. The applicant team confirmed that ERA Phase 2 will be a completely separate development, with no parkade connection and no shared costs or strata obligations.

#### **2. Parking and Traffic Congestion:**

Both immediate neighbours and the attendee from outside the area raised concerns regarding traffic impacts and parking availability. In response, the applicant team noted that the project includes 132 parking stalls for 120 residential units, along with designated commercial and visitor parking. The team also referenced the Transportation Impact Assessment (TIA), confirming that a traffic engineer has assessed potential impacts and identifying mitigation strategies.

### **Additional Notes:**

- The majority of attendees did not sign the attendance sheet; a copy of the sign-in sheet is attached.
- No feedback forms were submitted, although copies were available at the event. An example feedback form is attached for reference.
- No emails or written comments have been received to date regarding this application.



### Notice of Development Information Meeting

You are invited to attend a Development Information Meeting where representatives from Chris Dikeakos Architects will present details on Development Permit Application 2024-228-RZ.

**DATE:** April 8, 2025

**TIME:** 5:00pm - 7:00pm

**LOCATION AND ADDRESS:**  
Fraser Room at the Maple Ridge Library  
130-22470 Dewdney Trunk Road, Maple Ridge

The purpose of the application is to develop 12011 223rd Street into a mixed-use 6-storey building with 120 rental units and commercial use at grade on the south portion of the lot (ERA PHASE 2). The intent of the meeting is to seek input from the area residents on the proposed amendments and address any questions which may arise.

Please join us. Your thoughts are important to us. If you are unable attend the meeting and would like information regarding the proposal, please contact 604-291-2660 and/ or info@dikeakos.com or the City of Maple Ridge Planning Department, at 604-467-7341.





Parker gets some help from a volunteer in picking out a suit. (Neil Corbett/The News)



Wilfe Leung with fellow Plea Community Services event organizers Paige Dong (left) and Kristine Aquino. (Neil Corbett/The News)



## ANNUAL WATERMAIN CLEANING

The City of Maple Ridge will be continuing our annual watermain flushing/cleaning program

**Beginning March 24, 2025 for approximately (4) weeks.**

### ROUTINE FLUSHING AREAS

**From 223 St. to 232 St. between the Fraser River boundary and 141 Ave.**

This maintenance work will improve water quality; however, during this cleaning process some residents could experience water pressure drops and milky or dirty water.


Please take the following steps to correct the problem:

- MILKY WATER:** Open tap slightly to bleed air from the water lines.
- DIRTY WATER:** Turn an outside tap on until the water becomes clear.

This temporary interruption in service will be as brief as possible. Your understanding and cooperation is appreciated.

**If you have any questions or concerns please call Public Works at 604-463-9581.**

MapleRidge.ca/Waterworks



11995 Haney Place, Maple Ridge BC V2X 6A9

mapleridge.ca  
@yourmapleridge

Maple Ridge

# Puttin' on the Glitz gets grads ready to celebrate

**Neil Corbett**

School's out for spring break, but the spacious rotunda at Thomas Haney Secondary was bustling Tuesday, with teens in suits and prom dresses looking over racks of formal clothes, trying on dress shoes, and sorting through ties, jewelry, belts, and accessories.

Volunteers helped them get the right "fit," then applied makeup, styled their hair, and took formal grad photos with cap and gown.

Every girl The News spoke with loved her new dress they'd picked out that morning at Puttin' on the Glitz.

The annual event is organized by Plea Community Services, in partnership with the School District #42, and the Kiwanis service club that came on board this year. They ensure every high school graduate has formal attire for their big day.

The event provided an impressive selection for the teens, and everything looked like new – some still in their packages.

"I didn't think there would be this many dresses," said Destiny, who chose a long, red gown she fell in love with, and was still working on picking out shoes and accessories.

Dawn Beavon was a volunteer seamstress working on Destiny's new dress, and it was her first year working with the charity group.

"The last time I was involved, I was getting my dress," said Beavon, recalling that about 20 years ago local kids had to drive to Vancouver, because this service wasn't offered here yet.

Beavon was impressed with her hometown for Puttin' on the Glitz.

"It's phenomenal, I love it – the number of dresses of every colour," she enthused.

There were also a lot of finishing touches, and at an accessory table, Faith was pulling on some elegant long gloves, that a volunteer helper told her looked great with her dress.

One of the event organizers with Plea, Wilf Leung, had been at the first local Puttin' on the Glitz, 15 years ago.

"It started as a small event with 20 students, and this year we've had more than 50 register," he said, noting there were hundreds of dresses donated, and racks of men's suits and clothing available.

About 25 volunteers were there for the day, helping the teens find the look they wanted.

"The smiles make it all worthwhile," Leung said. "And there's the parents and the support people smiling and crying." Everything is free of charge.

They are welcome to keep it afterwards, or can donate it back to the cause.

"It's a great community event where everyone comes together," said Leung. "And every kid deserves a landmark moment like graduation."

**More online**

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# Cathedrals of Fire exhibit on display in church

## Arts in brief

Local artist W.L. Seaton will present the show Cathedrals of Fire this week at St. George Church in Maple Ridge, and was set to remain open to the public daily through Saturday, March 29, from 11 a.m. to 3 p.m., with the artist in attendance. Winston Seaton, 84, said the show is about historic cathedrals that have caught fire, including St. Paul's in London, and Notre Dame in Paris. St. George is at 23500 Dewdney Trunk Rd.

**Art Gallery looking for artists**  
Pitt Meadows Art Gallery is inviting artists to submit an expression of interest for a fall exhibition. The gallery welcomes all mediums including visual arts, sculpture, photography and more. All the artwork in this exhibit will showcase the beauty of Grabenhorst Garden, which is set to open in the summer of 2025. The gallery is seeking artists who are interested in this unique opportunity, which is an inside



Raagaverse is playing The ACT in Maple Ridge this weekend. (Special to The News)

peek at the five-acre garden before it's open to the public, to capture images, create and submit pieces of artwork for the exhibition that will run from Sept. 6 to Oct.19. Deadline for an expression of interest is midnight on Monday, March 31 via email at: [pmag@pittmeadows.ca](mailto:pmag@pittmeadows.ca).

**Indo-Jazz fusion band**  
The Listening Lounge at The ACT will host Juno-nominated Raagaverse, with its celebrated fusion of classical Indian music with jazz, on Friday, March 28.

The Vancouver-based quartet has received attention and appreciation from Canada's music scene since they formed in 2022. Raagaverse is spearheaded by Shruti Ramani (vocals) with the award-winning Jodi Proznick (bass), Noah Franche-Nolan (piano), and Nicholas Bracewell (drums). Raagaverse's music melds ancient Hindustani melodies with dynamic jazz harmonies.

**Classic rock tribute band**  
The music of iconic band Creedence Clearwater Revival (CCR)

is coming to The ACT on Saturday, March 29, with the tribute band Completely Creedence. Completely Creedence Band is a group of talented musicians who share a deep passion for the music of CCR, and love to bring the legendary sounds to life and entertain audiences with their energetic performances. The Band features Jamie Robertson on vocals and rhythm guitar, Murray Atkinson on guitar and backing vocals, Lance Lapointe on bass guitar and backing vocals, and Grant Baird on drums and backing vocals.



Completely Creedence is coming to The ACT on Saturday. (Special to The News)



Cathedrals of Fire is at St. George's church this week. (Special to The News)  
The two-hour show starts at 7:30 p.m. Ticket and info. at [theactmapleridge.org](http://theactmapleridge.org).

## ANNUAL WATERMAIN CLEANING

The City of Maple Ridge will be continuing our annual watermain flushing/cleaning program

**Beginning March 24, 2025 for approximately (4) weeks.**

### ROUTINE FLUSHING AREAS

**From 223 St. to 232 St. between the Fraser River boundary and 141 Ave.**

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**If you have any questions or concerns please call Public Works at 604-463-9581.**

[MapleRidge.ca/Waterworks](http://MapleRidge.ca/Waterworks)

11995 Haney Place,  
Maple Ridge BC V2X 6A9

[mapleridge.ca](http://mapleridge.ca)  
[@yourmapleridge](mailto:@yourmapleridge)

### Notice of Development Information Meeting

You are invited to attend a Development Information Meeting where representatives from Chris Dikeakos Architects will present details on Development Permit Application 2024-228-RZ.

**DATE:** April 8, 2025

**TIME:** 5:00pm - 7:00pm

**LOCATION AND ADDRESS:**  
Fraser Room at the Maple Ridge Library  
130-22470 Dewdney Trunk Road, Maple Ridge



The purpose of the application is to develop 12011 223rd Street into a mixed-use 6-storey building with 120 rental units and commercial use at grade on the south portion of the lot (ERA PHASE 2). The intent of the meeting is to seek input from the area residents on the proposed amendments and address any questions which may arise.

Please join us. Your thoughts are important to us. If you are unable attend the meeting and would like information regarding the proposal, please contact 604-291-2660 and/or [info@dikeakos.com](mailto:info@dikeakos.com) or the City of Maple Ridge Planning Department, at 604-467-7341.

**THANK YOU FOR RECYCLING THIS NEWSPAPER.**

## Carrier of the Week



**Sebastian A.**  
receives 1 medium pizza for continuous great service to our readers.

**LOCATION**  
22441 Dewdney Trunk Rd, Maple Ridge  
**604-310-0001**  
[www.panago.com](http://www.panago.com)

## Development Information Meeting

March 13, 2025  
2024-228-RZ

### Development Information Meeting for Proposed Development at 12011 223<sup>rd</sup> Street

Dear Neighbour,

You are invited to attend a Development Information Meeting to review and discuss the proposed development at 12011 223<sup>rd</sup> Street, Maple Ridge. The intent of this public meeting is to seek input from the area residents on the proposed amendments and address any questions which may arise.

The proposal includes the following components (include all applicable):

- ☐ Form and Character Development Permit
- ☐ 2 lots and 120 rental units
- ☐ Lot A – 0.501ha
- ☐ Lot B – 0.376ha



The meeting will be held on:

DATE: April 8, 2025

TIME: from 5:00pm to 7:00pm

LOCATION AND ADDRESS:

Fraser Room at the Maple Ridge Library  
130-22470 Dewdney Trunk Road, Maple Ridge

Details regarding the proposed development will be presented at the Development Information Meeting. The proponent and project consultants will be available throughout the Meeting to answer any questions and get your input on the proposal prior to proceeding to Council for further consideration. Please also note that this is **NOT** a Public Hearing notice.

Please join us. Your thoughts are important to us. If you are unable to attend this meeting, or require additional information, please do not hesitate to contact the undersigned at 604-291-2660 and [info@dikeyakos.com](mailto:info@dikeyakos.com) or the City of Maple Ridge Planning Department 604-467-7341.

Sincerely,

Richard Bernstein, Architect AIBC  
Applicant on behalf of Johnston Meier



## Notice of Development Information Meeting

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# DEVELOPMENT APPLICATION

12011 223rd Street  
2024-228-RZ

## PROPOSED REZONING

To enable the subdivision of the lot into two lots to develop a mixed-use 6-storey building with 20 rental units and commercial use at grade on the south portion of the lot (ERA PHASE 2). The project has an approximate total gross floor area of 119,683 square feet and a 2.4 FSR.

## LOCATION



## PROPOSED BUILDING



## FOR MORE INFO

### APPLICANT:

CDA Architects  
604-291-2660  
[info@dikeakos.com](mailto:info@dikeakos.com)

### PLANNING DEPARTMENT:

604-467-7341  
[planning@mapleridge.ca](mailto:planning@mapleridge.ca)  
<https://l.ead.me/LandDevMap>

COUNCIL AND PUBLIC HEARING DATES  
COMING SOON

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COMING SOON

BE PART OF  
THE PROCESS





# DEVELOPMENT APPLICATION

12011 223rd Street  
2024-228-RZ

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To enable the subdivision of the lot into two lots to develop a mixed-use 6-storey building with 120 rental units and commercial use at grade on the south portion of the lot (ERA PHASE 2). The project has an approximate total gross floor area of 119,683 square feet and a 2.4 FSR.

## LOCATION



## PROPOSED BUILDING



## FOR MORE INFO

**APPLICANT:**  
CDA Architects  
604-291-2660  
info@cdakeas.com

**PLANNING DEPARTMENT:**  
604-467-7341  
planning@mapleridge.ca  
<https://lead.me/LandDevMap>

BE PART OF  
THE PROCESS

COUNCIL AND PUBLIC HEARING DATES  
COMING SOON

COUNCIL AND PUBLIC HEARING DATES  
COMING SOON





## 2020-309-DP, 12323 222 Street, Multi-Family Development Permit

**Recommendation:**

**THAT the issuance of Development Permit 2020-309-DP for 12323 222 Street be approved.**

**Report Purpose and Summary Statement:**

Development Permit application 2020-309-DP has been received for the subject property, located at 12323 222 Street, formerly 12297 222 Street, 22175 123 Avenue, and 22185 123 Avenue, to allow for the future development of an eight-storey rental apartment building, comprising the following:

- 106 rental dwelling units, including 54 affordable rental units and 52 market rental units, intended to be utilized by senior citizens, people with a disability or diminished ability, war veterans, and sponsored immigrants;
- Group childcare centre on the second floor;
- Four studio guest units to serve as temporary accommodations for visitors of tenants; and
- Ancillary office use and other amenities for the primary housing use.

The subject site is the newly consolidated lot comprising the three properties located at 12297 222 Street, 22175 123 Avenue, and 22185 123 Avenue (Attachment D).

**Previous Council Action:**

Official Community Plan and Zoning Bylaws

First and Second Reading – December 14, 2021

Public Hearing – January 18, 2022

Third Reading – January 25, 2022

Amended First and Second Reading – June 25, 2024

Second Public Hearing – July 16, 2024

Third Reading – July 30, 2024

Adopted – September 10, 2024

Housing Agreement Bylaw:

First, Second and Third Reading – July 9, 2024

Adopted – September 10, 2024

**Environmental/Climate Impact:**

A Restrictive Covenant for protecting environmentally sensitive areas has been registered on the title of the subject properties. A combined Natural Features Development Permit and Watercourse Protection Development Permit will be issued subsequently.

**To:** Mayor and Council

**File number:** 2020-309-DP

## 2020-309-DP, 12323 222 Street, Multi-Family Development Permit

### BACKGROUND:

Applicant:	JM Architecture (Joseph M Minton)		
Legal Description:	Lot 1 District Lot 396 Group 1 New Westminster District Plan EPP124350		
OCP Designation:			
	Existing:	<i>RES (Urban Residential)</i>	
	Proposed:	<i>RES (Urban Residential)</i>	
Within Urban Area Boundary:	Yes		
Area Plan:	N/A		
OCP Major Corridor:	No		
Zoning:			
	Existing:	CD-6-21	
	Proposed:	CD-6-21	
Surrounding Uses:			
	North:	Use:	Single-Detached Residential
		Zone:	RS-1 (Single Detached Residential)
		Designation:	<i>Urban Residential and Conservation</i>
	South:	Use:	Single- Detached Residential
		Zone:	RS-1 (Single Detached Residential)
		Designation:	<i>Single Family Residential</i>
	East:	Use:	Residential Apartments
		Zone:	RM-2 (Medium Density Apartment Residential)
		Designation:	<i>Low-Rise Apartment</i>
	West:	Use:	Single- Detached Residential
		Zone:	RS-1 (Single Detached Residential)
		Designation:	<i>Urban Residential and Conservation</i>
Existing Use of Property:	Vacant		
Proposed Use of Property:	Apartments		
Site Area:	0.544 ha		

Proposed Vehicular Access:	222 Street
Servicing Requirement:	Urban Standard
Flood Plain:	No
Fraser Sewer Area:	No

## **BACKGROUND:**

### **Project Description:**

The subject property is located at the intersection of 123 Avenue and 222 Street, within the designated Urban Area Boundary. The northern and western boundaries of the property are bordered by single-detached residential dwellings. To the south, 123 Avenue runs alongside the site, with additional single-detached residential properties situated across the street. A development application is currently under review for properties located to the south, proposing the future construction of a six-storey multi-unit residential building. To the west, the site is adjacent to the Town Centre Area and fronts onto 222 Street, where two, four-storey multi-unit residential buildings are located directly across the street (Attachments A and B).

The subject site was rezoned to a Comprehensive Development zone (CD-6-21) through a separate rezoning process (2019-255-RZ) to accommodate the proposed development. The zone amending bylaw (Bylaw No. 7812-2021) was adopted during the Regular Council meeting on September 10, 2024. At the same time, an amendment to the Official Community Plan (OCP) was adopted (by Bylaw No. 7803-2021) to support the project. This amendment included adjustments to the Urban Residential and Conservation OCP boundaries, as well as modifications to the OCP text to facilitate the development of the proposed eight-storey residential building.

The developable area on the site is limited by the ravines on the east and west sides. A combined Watercourse Protection Development Permit (WPDP) and Natural Features Development Permit (NFDP) are required for this site due to the steep ravine banks located on the west and east sides of the subject site. A Fortis BC Gas right of way runs along the southern edge of the site, which limits the potential for vehicular access from that side. Following restrictions governed by Fortis BC Gas, the access to the building is off 222 Street with a vehicle ramp at the western side of the lot. The principal entry is proposed through a covered entry from 222 Street as well (Attachment E).

The proposed eight-storey apartment building will contain 54 affordable rental units intended to be utilized by senior citizens, people with a disability or diminished ability, war veterans and sponsored immigrants, and 52 market rental units secured through a housing agreement. Housing Agreement Bylaw No. 8003-2024 was adopted by Council at the Regular Council meeting on September 10, 2024.

Each affordable unit is to be rented out at a rate that is less than 80% of the median market rent, as determined by a market rent appraisal of similar units within Maple Ridge. The remaining 52 units will be market rental units. All of the proposed rental residential units will remain as rental tenure in perpetuity and meet the accessibility design standard defined by the Canadian



Mortgage and Housing Corporation. The proposal will have the following unit-mix: 62% one-bedroom units; 32% two-bedroom units; and 6% three-bedroom units.

Also, this proposal includes a group childcare facility with approximately 328 m<sup>2</sup> of indoor area located on the second floor of the building, with a separate access lobby from the ground level to be designed in compliance with Fraser Health Authority requirements. The subject application also includes an ancillary office space located on the ground level of the building, four studio units, and other amenities for the primary use of the building.

## **PLANNING ANALYSIS:**

### **Official Community Plan:**

The subject property is located within the Urban Area Boundary west of the Town Centre Area Plan. The Official Community Plan (OCP) designates the subject site as *Urban Residential*. The *Urban Residential* designation consists of two residential categories - Neighbourhood Residential and Major Corridor Residential. Currently, the subject property does not face a Major Road Corridor, therefore the proposed development is subject to the Neighbourhood Residential Infill policies. Accordingly, an OCP text amendment to Schedule A, Chapter 3, Section 3-19 was adopted (by *Bylaw No. 7803-2021*) to allow a maximum height of eight storeys for the proposed senior and supportive housing. The proposed development is consistent with Policies 3-31, 3-32, 4-22, 4-25, and 4-36 of the OCP as summarized below:

- *Policy 3 – 31:* Maple Ridge supports the provision of rental accommodation and encourages the construction of rental units that vary in size and number of bedrooms.
- *Policy 3 – 32:* Maple Ridge supports the provision of affordable, rental and special needs housing throughout the City.

The above policies support the provision of affordable rental accommodation for vulnerable groups in the community with varying dwelling unit sizes and numbers of bedrooms.

- *Policy 4 – 22:* Maple Ridge encourages the development of accessible quality childcare facilities throughout the community and will encourage the provision of space for childcare in all assisted rental housing projects developed under senior government programs.

The above policy supports the inclusion of the group childcare centre into this market and non-market development.

- *Policy 4 – 23:* Maple Ridge encourages the development of seniors' care facilities throughout the community that are conveniently located near transit and other services.

Seniors have been proposed as one of the target groups in the market rental and affordable rental components of the development. The development is conveniently located near senior-supportive services and transit routes on Dewdney Trunk Road.

- *Policy 4 – 36:* Maple Ridge supports the establishment of child-care facilities in *Residential, Institutional, Commercial and Industrial* land use designations, subject to compliance with City bylaws and regulations.

This policy supports the inclusion of the proposed neighbourhood childcare centre in the development, as the subject site is located within the *Residential* land use designation of the OCP.

### **Zoning Bylaw:**

The proposed development is in alignment with the site's existing zone, CD-6-21.

The subject site's Comprehensive Development zoning (CD-6-21) includes site-specific parking requirements for a total of 120 stalls, of which 26 are located at grade. The proposed development meets the required number of parking stalls number and allocation requirements.

### **Development Permits:**

#### Multi-Family Residential Development Permit

Pursuant to Section 8.7 of the OCP, a Multi-Family Development Permit is required to ensure the current proposal enhances existing neighbourhoods with compatible housing styles that meet diverse needs and, minimizes potential conflicts with neighbouring land uses.

The following is an assessment of the development proposal's compliance with the applicable Key Guideline Concepts:

1. "New development into established areas should respect private spaces, and incorporate local neighbourhood elements in building form, height, architectural features and massing."

*Staff Comments: The proposed apartment development is located within a transitional area, bridging the higher-density Town Centre to the east and a predominantly low-density residential neighbourhood to the west. To the east, the surrounding context includes relatively new four-storey apartment buildings. A six-storey multi-unit development application (2021-320-DP) is currently under consideration for the properties located to the south. To the north, within the subject site, lies an environmentally sensitive area, beyond which are single detached residential dwellings (Attachment E).*

2. "Transitional development should be used to bridge areas of low and high densities, through means such as stepped building heights, or low-rise ground-oriented housing located to the periphery of higher density developments."

*Staff Comments: The proposed development is intended to provide a mix of affordable and market rental housing, secured in perpetuity. The developable portion of the site is constrained by environmentally sensitive areas to the east and west, as well as by FortisBC Gas right of way restrictions along the southern boundary. To ensure financial viability within these constraints, the project incorporates a taller building form. The design includes*

*a step-back on the eighth floor, oriented toward 123 Street, to reduce visual impact from the street and enhance compatibility with the surrounding context (Attachment E).*

3. "Large-scale developments should be clustered and given architectural separation to foster a sense of community and improve visual attractiveness."

*Staff Comments: The visual appeal of the building is achieved through an architectural design that incorporates masonry and solar cladding elements to allow the surrounding natural landscape and existing trees to remain the dominant visual features of the site (Attachment E).*

4. "Pedestrian circulation should be encouraged with attractive streetscapes attained through landscaping, architectural details, appropriate lighting and by directing parking underground where possible or away from public view through screened parking structures or surface parking located to the rear of the property."

*Staff Comments: The proposed development enhances the streetscape by upgrading the frontage along 123 Avenue and 222 Street to meet collector road standards. It also includes the installation of new street lighting and street trees. The parking structure is located underground and concealed at ground level, minimizing its visual impact (Attachments E and F).*

### **Environmental Implications:**

Given the presence of watercourses and steep slopes, the project is subject to a Watercourse Protection Development Permit (WPDP) and a Natural Features Development Permit (NFDP) for protection of the natural environment and steep slopes.

Applications for a WPDP and NFDP have been received. Staff have reviewed the guidelines established for the WPDP area and NFDP area and have determined that they have been satisfied by the applicant. The applicant has provided the following reports in support of the applications:

- Watercourse Protection and Natural Features Development Permit Area Guidelines Checklist;
- Environmental Assessment Report prepared by CRS Consultants Ltd. Dated November 27, 2023;
- Geotechnical Report prepared by Able Geotechnical dated March 8, 2025;
- Water Sustainability Act Approval, with File Number 2011344, dated March 6, 2025;
- Design Drawings, Figure 2-1 Sheet 01 of 02, Proposed Instream Work Details, Meadows Walk Development, 12297 222 Street, Maple Ridge, BC, prepared by CSR Geotechnical Environmental Water Resources, approved by Mamoud Bashi P.Eng., dated February 25, 2025;
- Design Drawings, Figure 2-1 Sheet 02 of 02, Proposed Instream Work Details, Meadows Walk Development, 12297 222 Street, Maple Ridge, BC, prepared by CSR Geotechnical



Environmental Water Resources, approved by Mamoud Bashi P.Eng., dated February 25, 2025;

- Enhancement Replanting Plan and Cost Estimate prepared by CSR Consultants dated July 29, 2024; and
- Enhancement and Maintenance Agreement dated October 23, 2024.

WPDP and NFDP reviews and approvals are completed by staff. A combined WPDP and an NFDP report has been prepared and is subject to issuance by the Director of Planning and Building.

### **Advisory Design Panel:**

The subject development proposal was presented to the Advisory Design Panel (ADP) at its meeting on May 15, 2024. A revised submission was subsequently reviewed by the ADP on June 19, 2024. The ADP's resolution and comments, along with the applicant's corresponding responses, are provided in Attachment G.

A revised submission was reviewed by the ADP for the third time at its meeting on September 18, 2024. At that meeting, the application was supported as presented, and recommended to proceed to Council (Attachment G).

### **Development Information Meeting:**

In accordance with Council Policy 6.20, at the OCP amendment and zoning amendment application stage, a Development Information Meeting (DIM) was hosted by the applicant at the City of Maple Ridge Public Library on June 13, 2024. It was attended by 11 people (Attachment H).

### **Landscaping Security:**

In accordance with Council Policy 6.28, a refundable security equivalent to 100% of the estimated landscape cost has been provided to ensure satisfactory provision of landscaping in accordance with the terms and conditions of the Development Permit. Based on an estimated landscape cost, the security is \$107,599.21.

## CONCLUSION:

The development proposal for an eight-storey rental apartment building is supported by staff. It is recommended that Development Permit 2020-309-DP for 12323 222 Street be approved (Attachment I).

"Maryam Lotfi"

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Prepared by: Maryam Lotfi, Planner 2

### Attachments:

- (A) Location Map
- (B) Aerial Photo
- (C) Zoning and OCP Map
- (D) Site Survey Plan
- (E) Architectural Plans
- (F) Landscape Plans
- (G) Advisory Design Panel's Resolution/Comments and Applicant's Responses
- (H) Development Information Meeting Summary
- (I) Draft Development Permit

## Report Approval Details

Document Title:	2020-309-DP, 12323 222 Street, Development Permit Report .docx
Attachments:	<ul style="list-style-type: none"><li>- Attachment A - Location Map.pdf</li><li>- Attachment B - Aerial Photo.pdf</li><li>- Attachment C - Zoning and OCP Map.pdf</li><li>- Attachment D - Site Survey Plan.pdf</li><li>- Attachment E - Architectural Plans.pdf</li><li>- Attachment F - Landscape Plans.pdf</li><li>- Attachment G - Advisory Design Panel's Resolution - Comments and Applicant's Responses.pdf</li><li>- Attachment H - Development Information Meeting Summary.pdf</li><li>- Attachment I - Draft Development Permit.pdf</li></ul>
Final Approval Date:	May 14, 2025

This report and all of its attachments were approved and signed as outlined below:

David Purcell-Chung, Manager of Development and Environmental Services

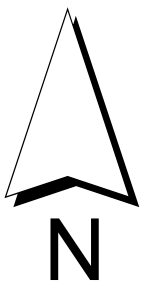
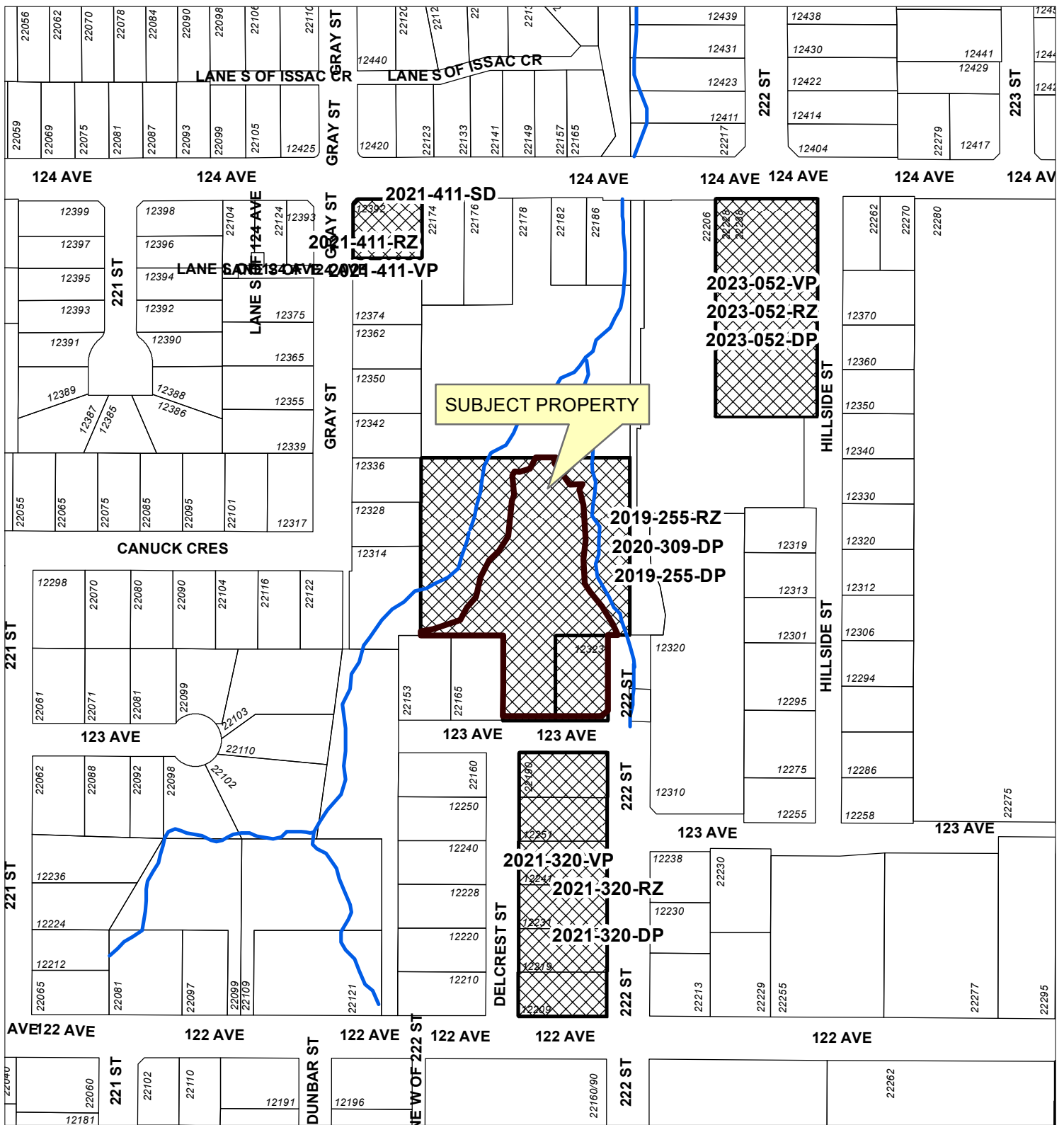
Hasib Nadvi, Deputy Director of Planning and Building

James Stiver, Director of Planning and Building

Carolyn Mushata, Director of Legislative Services and Corporate Officer

Scott Hartman, Chief Administrative Officer





Scale: 1:2,500

## Legend

- Stream
- XXXXXX Active Applications (RZ/SD/DP/VP)

## 12323 222 STREET LOCATION MAP

PLANNING DEPARTMENT



# Maple Ridge

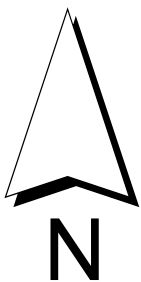
FILE: 2020-309-DP  
DATE: Apr 4, 2025

BY: DM





Aerial Imagery from the Spring of 2023



Scale: 1:2,500



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## 12323 222 STREET AERIAL PHOTO

PLANNING DEPARTMENT



# Maple Ridge

FILE: 2020-309-DP  
DATE: Apr 4, 2025

BY: DM







## NEW WESTMINSTER LAND TITLE OFFICE

Oct-23-2024 13:20:56.006

EPP124350

SURVEY PLAN CERTIFICATION  
PROVINCE OF BRITISH COLUMBIA

0809

PAGE 1 OF 2 PAGES

Your electronic signature is a representation that you are a British Columbia land surveyor and a subscriber under section 168.6 of the *Land Title Act*, RSBC 1996 c.250. By electronically signing this document, you are also electronically signing the attached plan under section 168.3 of the act.

Lakhjot Grewal  
64QJG9Digitally signed by  
Lakhjot Grewal 64QJG9  
Date: 2024.07.17  
09:59:30 -07'00'

1. BC LAND SURVEYOR: (Name, address, phone number)

Lakhjot S. Grewal

Unit 204, 15299 68th Avenue

Surrey

BC V3S 2C1

☐ Surveyor General Certification [For Surveyor General Use Only]

2. PLAN IDENTIFICATION:

Control Number: **172-123-5570**Plan Number: **EPP124350**This original plan number assignment was done under Commission #: **809**LTO Document Reference: **CB1668909**

3. CERTIFICATION:

☒ Form 9☐ Explanatory Plan☐ Form 9A

I am a British Columbia land surveyor and certify that I was present at and personally superintended this survey and that the survey and plan are correct.

The field survey was completed on: 2023 April 07 (YYYY/Month/DD)

The checklist was filed under ECR#:

The plan was completed and checked on: 2024 July 08 (YYYY/Month/DD)

**283440**☒ None ☐ Strata Form S☒ None☐ Strata Form U1☐ Strata Form U1/U2Arterial Highway ☐Remainder Parcel (Airspace) ☐4. ALTERATION: ☐

SUBDIVISION PLAN PLAN OF PARCEL "A" (REFERENCE PLAN 12909)  
LOT 5 EXCEPT: FIRSTLY: PARCEL "B" (REFERENCE PLAN 13374)  
SECONDLY: PARCEL "D" (EXPLANATORY PLAN 13727) PLAN 830;  
LOTS 9 AND 10 BOTH OF PLAN 15728;  
ALL OF DISTRICT LOT 396 GROUP 1  
NEW WESTMINSTER DISTRICT

PLAN EPP124350

PURSUANT TO SECTION 67 OF THE LAND TITLE ACT  
BCGS 92G.027  
THIS PLAN LIES WITHIN INTEGRATED SURVEY AREA NO. 36,  
CITY OF MAPLE RIDGE, NAD83(CSRS) 4.0.0.BC.1.MVRD

0 10 50  
SCALE 1 : 750 DISTANCES ARE IN METRES

THE INTENDED PLOT SIZE OF THIS PLAN  
IS 432mm IN WIDTH BY 560mm IN HEIGHT ( C SIZE )  
WHEN PLOTTED AT A SCALE OF 1:750

GRID BEARINGS ARE DERIVED FROM GEODETIC CONTROL  
MONUMENTS 84H9970 AND 85H0733, AND ARE REFERRED  
TO THE CENTRAL MERIDIAN OF UTM ZONE 10.

THE UTM COORDINATES AND ESTIMATED ABSOLUTE ACCURACY  
ACHIEVED HAVE BEEN DERIVED FROM THE MASCOT PUBLISHED  
COORDINATES AND STANDARD DEVIATIONS FOR GEODETIC  
CONTROL MONUMENTS 84H9970 AND 85H0733.

THIS PLAN SHOWS HORIZONTAL GROUND-LEVEL DISTANCES  
UNLESS OTHERWISE SPECIFIED. TO COMPUTE GRID DISTANCES,  
MULTIPLY GROUND-LEVEL DISTANCES BY THE AVERAGE  
COMBINED FACTOR OF 0.9996086. THE AVERAGE COMBINED  
FACTOR HAS BEEN DETERMINED BASED ON CONTROL  
MONUMENTS 84H9970 AND 85H0733.

LEGEND

- FOUND PLACED
- ○ DENOTES IRON POST
  - ⦿ DENOTES CONTROL MONUMENT
  - PP DENOTES POSTING PLAN
  - NTS DENOTES NOT TO SCALE
  - NF DENOTES EVIDENCE NOT FOUND
  - (C) DENOTES CALCULATED
  - WT DENOTES WITNESS

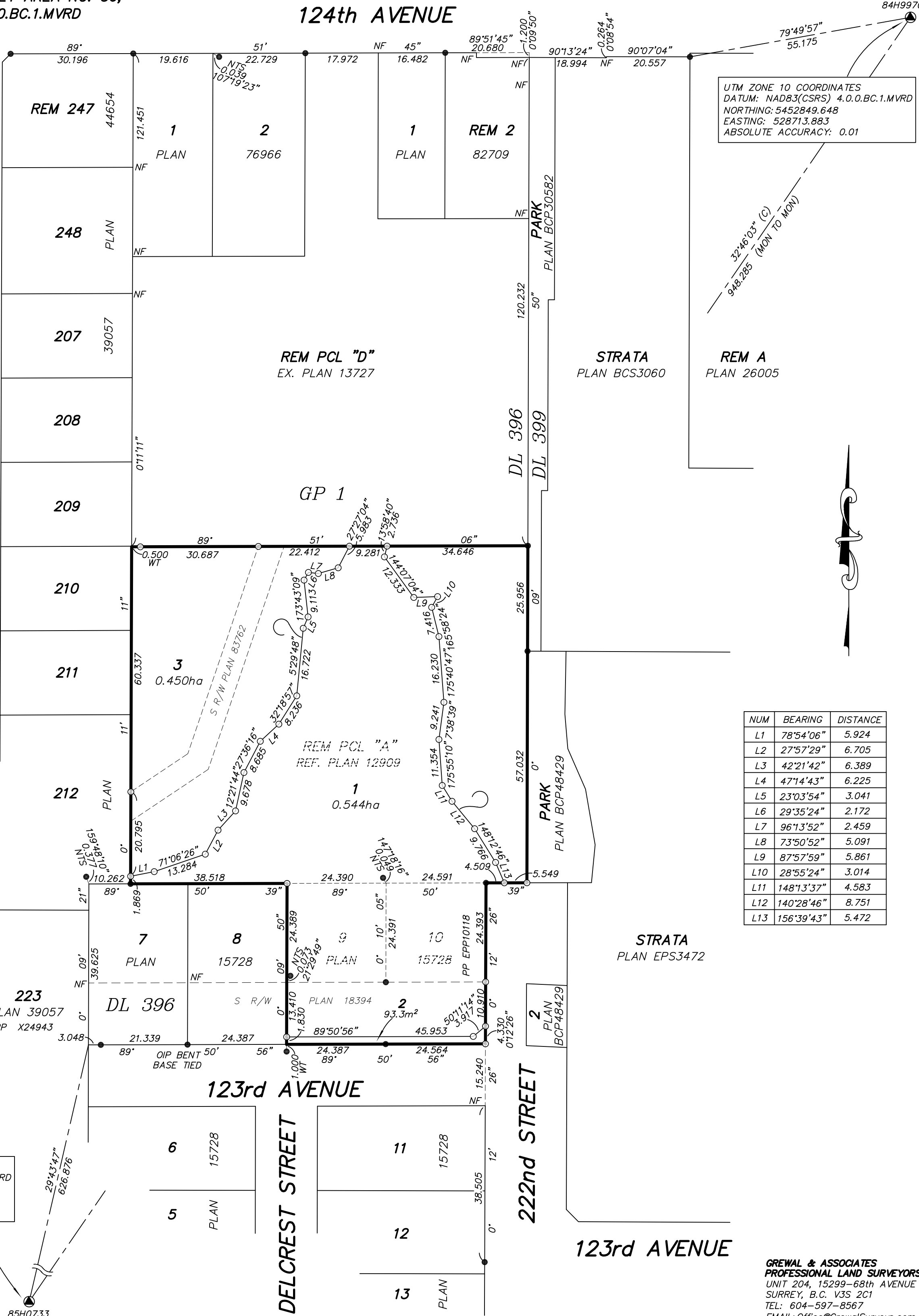
THIS PLAN SHOWS ONE OR MORE WITNESS POSTS  
WHICH ARE NOT SET ON THE TRUE CORNER(S).

UTM ZONE 10 COORDINATES  
DATUM: NAD83(CSRS) 4.0.0.BC.1.MVRD  
NORTHING: 5452052.573  
EASTING: 528200.841  
ABSOLUTE ACCURACY: 0.01

INSPECTED UNDER THE LAND TITLE ACT  
ON THE 9th DAY OF JUNE, 2024.

THE FIELD SURVEY REPRESENTED BY THIS PLAN  
WAS COMPLETED ON THE 7th DAY OF APRIL, 2023.  
LAKHJOT S. GREWAL, B.C.L.S. #809

THIS PLAN LIES WITHIN THE JURISDICTION OF THE APPROVING OFFICER FOR CITY OF MAPLE RIDGE  
THIS PLAN LIES WITHIN THE METRO VANCOUVER REGIONAL DISTRICT



GREWAL & ASSOCIATES  
PROFESSIONAL LAND SURVEYORS  
UNIT 204, 15299-68th AVENUE  
SURREY, B.C. V3S 2C1  
TEL: 604-597-8567  
EMAIL: Office@GrewalSurveys.com  
FILE : 1808-016  
DWG : 1808-016 S1-RV3

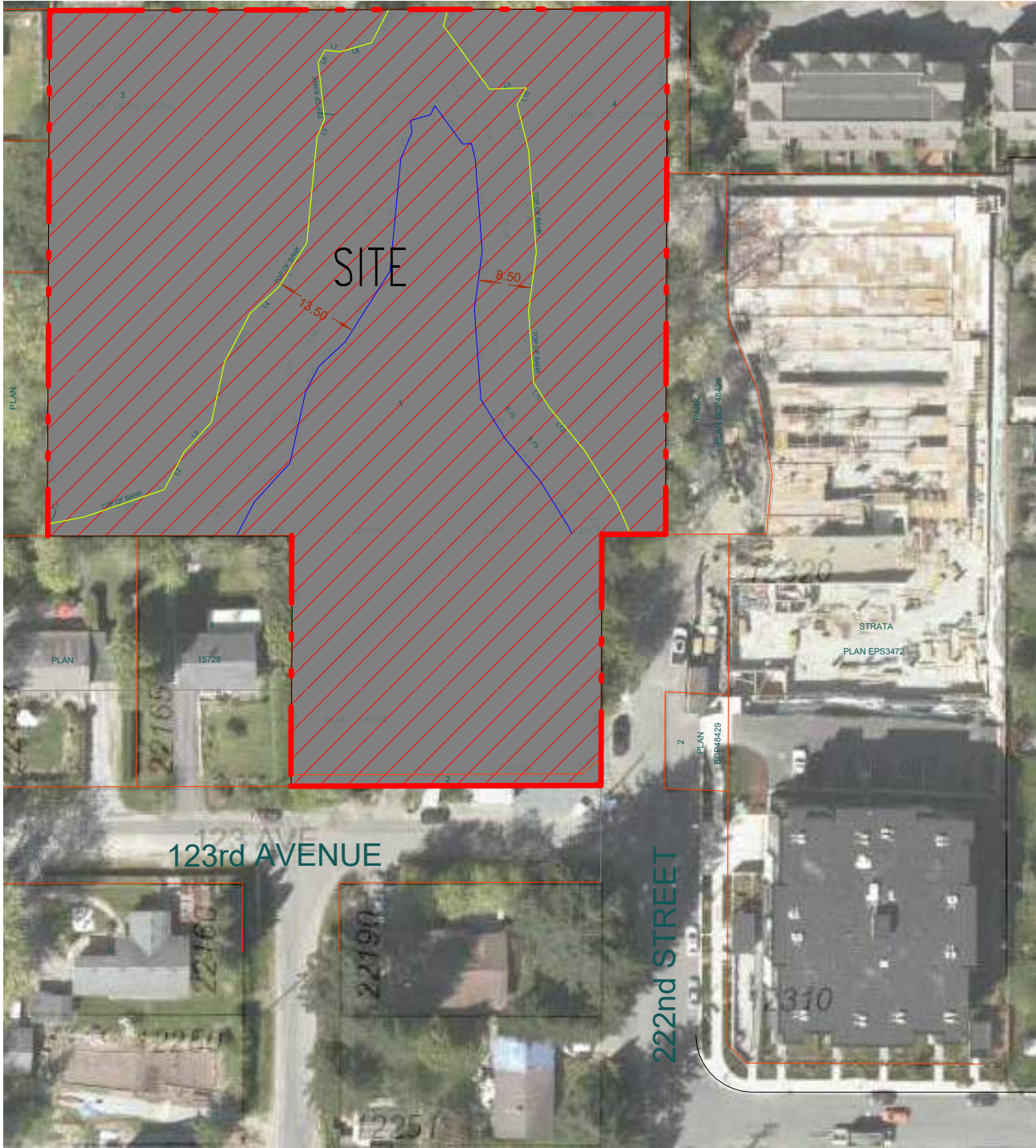


2018-15 222 MEADOWS WALK

12297 222 ST / 22175, 22185 123 AVE MAPLE RIDGE BC.



LOCATION MAP



CONSULTANT INFORMATION

ARCHITECT	
BUSINESS NAME	JM ARCHITECTURE INC.
MAILING ADDRESS	MAIN FLOOR - BLDG.#4 15243-91ST AVENUE SURREY, BC V3R 8P8
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EMAIL ADDRESS	JOE@JMARCHITECTURE.CA
CONTACT PERSON	JOE MINTEN

LANDSCAPE ARCHITECT	
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TELEPHONE NUMBER	604 249 5040
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SURVEYOR	
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EMAIL ADDRESS	
CONTACT PERSON	LUCKY GREWAL

TRAFFIC ENGINEER	
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CONTACT PERSON	KELLY BULLIVANT

ARBORIST	
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TELEPHONE NUMBER	604-783-3732
EMAIL ADDRESS	MEREDITH.MITCHELL@M2LA.COM
CONTACT PERSON	MEREDITH MITCHELL

GEOTECHNICAL ENGINEER	
BUSINESS NAME	ABLE GEOTECHNICAL LTD.
MAILING ADDRESS	15580 79A SURREY BC V3S8R8
TELEPHONE NUMBER	778 995 2404
EMAIL ADDRESS	TEGBIR@ABLEGEO.COM
CONTACT PERSON	TEGBIR BAIWA

ENVIRONMENTAL ENGINEER	
BUSINESS NAME	CSR ENVIRONMENTAL LTD.
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TELEPHONE NUMBER	604.559.7100
EMAIL ADDRESS	MANOUD@CSRENVIRO.COM
CONTACT PERSON	MANOUD BASHI

ELECTRICAL ENGINEER	
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TELEPHONE NUMBER	604 500 8422
EMAIL ADDRESS	MIKE@LEADINGENG.CA
CONTACT PERSON	MIKE SHETREIE

MECHANICAL ENGINEER	
BUSINESS NAME	MEC ENGINEERING CONSULTING LTD
MAILING ADDRESS	UNIT# 4 - 15243 -91 AVE SURREY BC
TELEPHONE NUMBER	604 581 6338
EMAIL ADDRESS	NAV.BRAR@MECEENGINEERING.CA
CONTACT PERSON	NAV BRAR

REISSUED FOR DP  
APRIL 28, 2025

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DP-004	DESIGN BRIEF
DP-005	SHADOW ANALYSIS
DP-006	STREETSCAPE
DP-007	SITE SECTIONS
DP-008	PROJECT DATA
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DP-100B	SITE PLAN SHOWING DEDICATIONS
DP-100C	SITE_LEGAL LOTS & RETAINED TREES
DP-100D	SITE PLAN SHOWING BUILDING SETBACKS
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DP LANDSCAPE DRAWING LIST	
NO.	SHEET TITLE

L1	NOTES & KEYS
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L3	PRESERVE AREA GRADING & WATER MANAGEMENT PLAN
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DP CIVIL DRAWING LIST	
NO.	SHEET TITLE

20040-3	KEY PLAN
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2	SHORING EAST & WEST ELEVATIONS
3	SHORING-SOUTH SECTION
4	NOTES

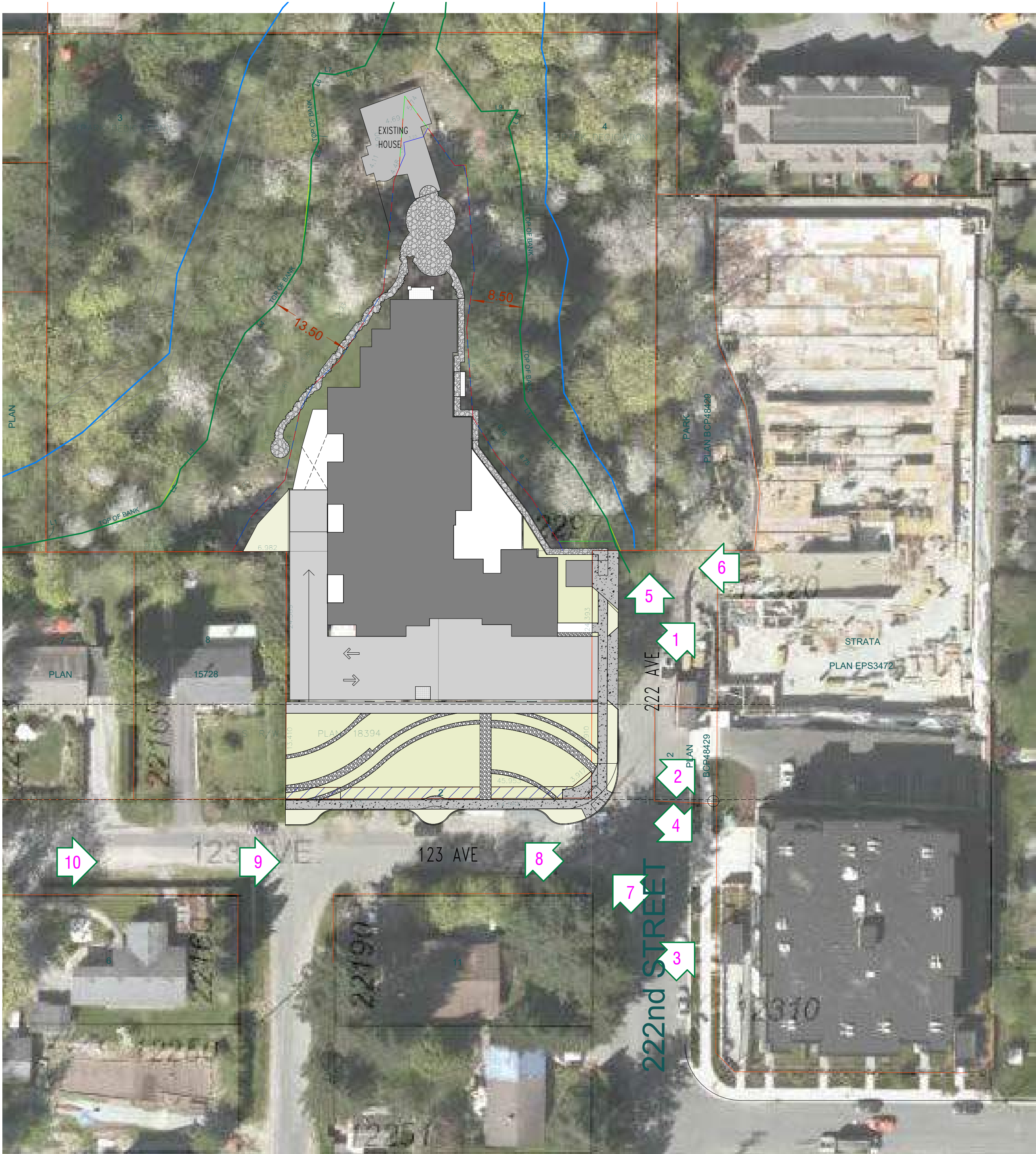
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LD2	FORM C-HABITAT PROTECTION
LD3	FORM C- STORMWATER MANAGEMENT PLAN
LD4	FORM C- TREE PROTECTION
LD5	FORM C - VISITOR PARKING
LD6	FORM C - HOUSING AGREEMENT
LD7	ADP-ROAD DEDICATION PLAN
LD8	ADP-SUBDIVISION PLAN
LD9	SEARCH TITLE- SERCH PID 010-122-818
LD10	SEARCH TITLE- SERCH PID 010-122-885
LD11	SEARCH TITLE- SERCH PID 010-921-935
LD12	SURVEY PLAN CERTIFICATION (SUBDIVISION PLAN)
LD13	SURVEY PLAN CERTIFICATION (ROAD DEDICATION)

SUPPORTING DOCUMENTS  
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222 MEADOWS WALK





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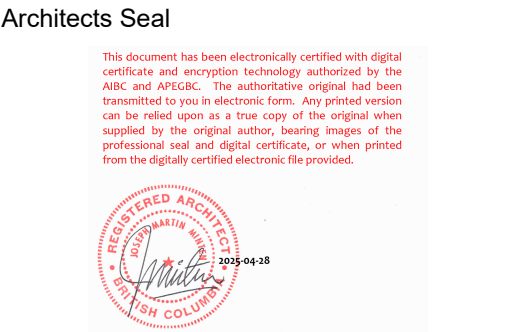
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5	17-02-2023	REISSUED FOR DP	JG
REV	DATE	DESCRIPTION	BY

START DATE		2018-15
PROJECT No	2018-15	
DR.	Author	
CH.	Checker	
SCALE	1" = 40'-0"	



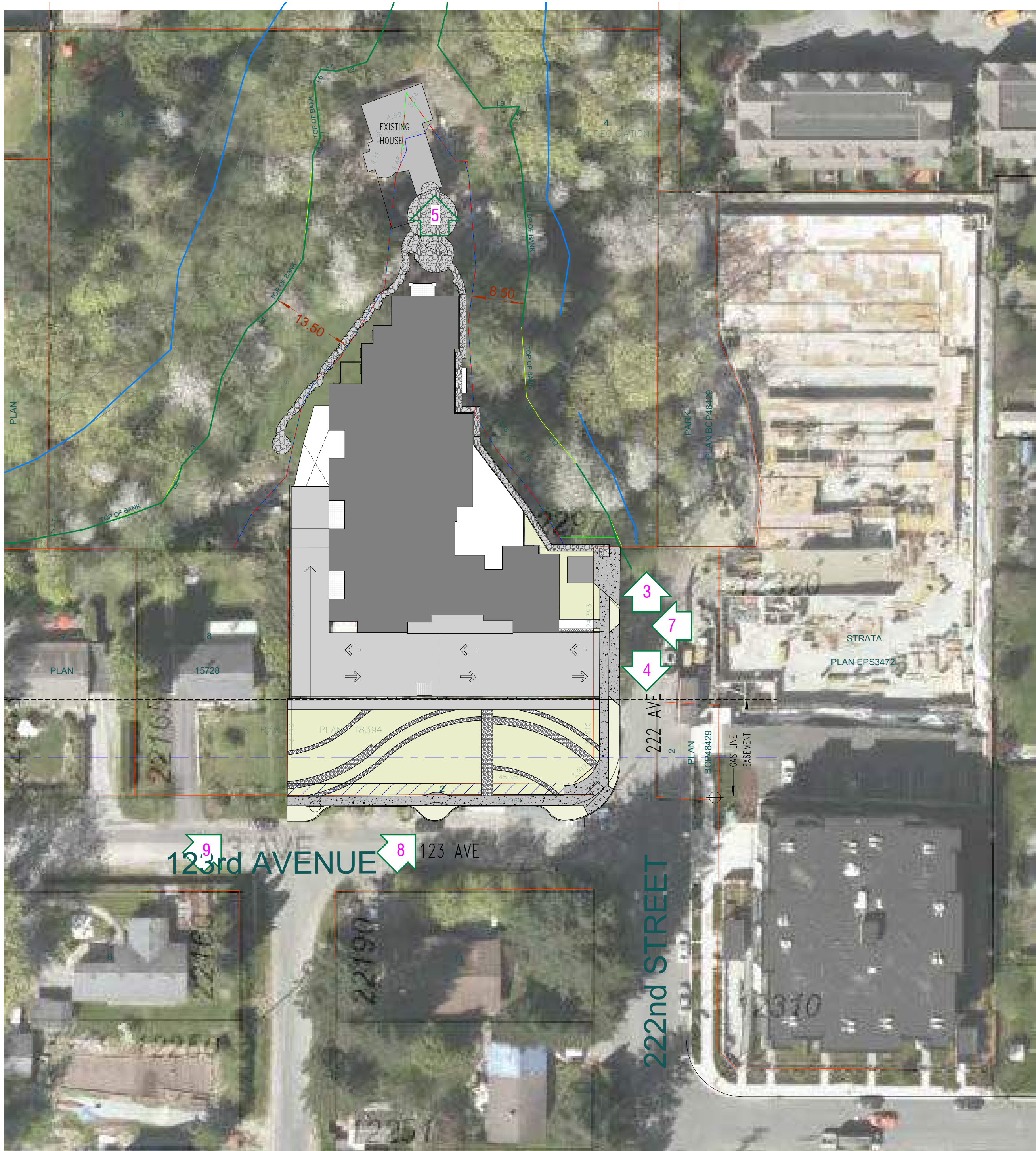
Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

CONTEXT

REV No	14	Project No	2018-15	DP-002
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**START DATE**

PROJECT No 2018-15

DR. Author

CH. Checker

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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

**CONTEXT**

REV No	Project No	DP-003
14	2018-15	



## DESIGN BRIEF

### 01. Natural habitat enhancement and neighbourhood revitalization

The site is located northwest of the intersection of 222 St and 123 Ave. It is a consolidation of 3 lots, 12297 222 ST, 22175 123 AVE and 22185 123 AVE, which are currently single family lots .

Two water courses flow through the north portion of the property and drain into Alouette river.

The development highly respects the existing environmental setting. The habitat enhancement is provided along with the new development. The enhancement not only sets the conservation zone, but also will improve the creek flow by increasing the creek depth and cleaning the lock blocks in the creek.

As an urban infill development, the new development introduces 106 units in total of mixed 1-bedroom, 2-bedroom and 3-bed room unit. The new development will significantly change the appearance of the cul-de-sac area at the end of the 222 St.

### 02. Fit in the town center north view area

The east side of 222 ST is mostly multi-residential buildings. There are two 4-Storey apartments on the opposite side of the site in 222 ST built in recent years.

The new apartment is a transition from the town center to the west residential area. Also, it is a combination of natural environment and urban residential development. The shorter wing of the L shape build forms a smaller façade towards 123 Ave, the longer wing tapers into the natural green space.

Balconies for each unit help breakdown the building mass in several parts with some play in the exterior cladding materials

Two major exterior cladding materials are smooth face concrete block veneer and corrugated metal cladding. The smooth face concrete block veneer wall provides the quality of the building, the metal cladding provides a fresh modern style look. Both materials can be found in the nearby neighbor buildings.

The building roof is a combination of various smaller size pitch roofs. It creates a dynamic outline of the building. The building height is also broken into various height because of the roof forms.

Due to the restriction of the site, the building setback varies in different direction. The building setback is 15.63 meters at south due to the gas line easement, is 42.2 meters to provide the environmental protection.

The main entrance sits in the courtyard at the end of 222 St. A 7m wide driveway leading into a courtyard provides the sense of security and privacy.

The underground parking driveway is located at the west side of the building. So the entire street façade is formed by residence units. It highly reduces the negative impact from underground entrance and blank walls.

### 03. Energy efficiency and environmental design

The building construction aims at achieving high standard of energy efficiency.

The exterior wall assembly core is insulated concrete forms (ICF) wall from first floor to top floor. The ICF wall core provides much higher R value than the code requirement.

The selection of window size provides the tenant sufficient natural light and maintains a lower window-to-wall ratio to prevent heat loss.

The balconies are framed to avoid thermo bridges and the new construction will focus on building airtightness in details.

The mechanical system is designed to use high efficiency heat recovery ventilator and decentralized ventilation systems.

The site is paved by permeable material and maximizes the landscape area to control the stormwater runover. Meanwhile, the roof water is collected for reuse purpose.

### 04. Tenant friendly living environment

As a rental building, the new apartment not only provides various units to fit tenants in different ago groups, but also provide a lot of amenity spaces for tenant convenience.

The amenity spaces include:  
Meeting room, theater room, fitness center, guest-room suites and plenty of storage rooms. In addition, the existing house is to be renovated to a daycare specifically serve the apartment families.

The site grading is to minimize the slope of walkways. The main entrance lobby is lowered to the street level, the walkway has maximum slope of 5%. The width is minimized to 5 feet wide all around the building.

The waterway conservation area is strictly protected by fencing. Meanwhile, the new site also provides enough outdoor usable open space for the tenants. These usable open spaces are carefully chosen so no existing trees will be cut down and no existing environment been disturbed.

The walkway and parking lot are lighted to enhance the safety. The building is designed to avoid dead corners. The unit windows and balconies provide surveillance to the surrounding. Windows are provided at the end of the building corridors and in the staircases.

### 05. CPTED

**Natural Surveillance** is achieved through open and visual connection from within the building and from the site generally. Tenants and building manager are continuously on the site. Lighting shall be chosen for both way finding and security of the property; only down lighting is proposed to protect the night sky. Surveillance cameras will also be installed to monitor the entrance lobby, ground parking area and rear door close to the underground entry ramp.

**Territorial Reinforcement** is achieved by a well-maintained site, clearly defined site entry and exit treatments, water feature and landscaping.

**Natural Access Control** is limited in part by the configuration of the site. The vehicle access to the underground parking is from 222 St to west passing through the ground parking lot and entre. The visitor or drop off is through 222 St to the courtyard. No long-term parking is allowed in courtyard area. Pedestrian access is led by street sidewalk to courtyard and to the main entrance lobby. The entrance lobby is flashing with street level without any steps and steep slope.

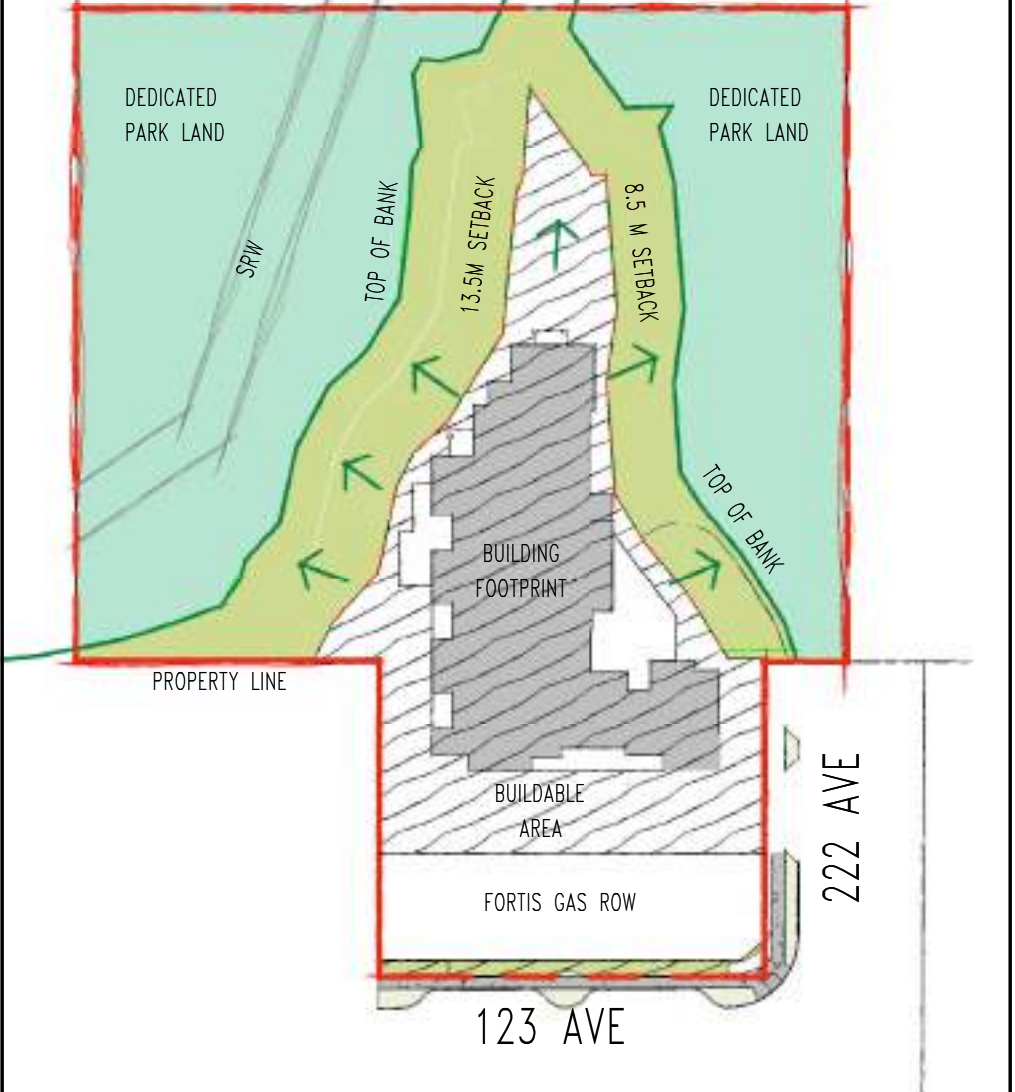
**Maintenance** schedules shall be formulated at the start of operations to ensure the property, building and landscaping are kept in a pristine condition. The property manager shall commission a groundskeeper to maintain the landscape in such a condition that all areas of the site remain visible and free of potential vandalism. Shrubs and trees that require pruning shall be pruned each year and weekly or monthly upkeep shall be maintained throughout the year. On-site lighting shall be durable LED fixtures within easy reach of standard commercial grade maintenance equipment. Lighting shall provide good coverage to ensure no part of the site is left in an unsafe condition. Lighting operations shall be set to a seasonal time schedule. Camera surveillance shall also be within easy reach of commercial grade maintenance equipment.



ELEVATION CONCEPT



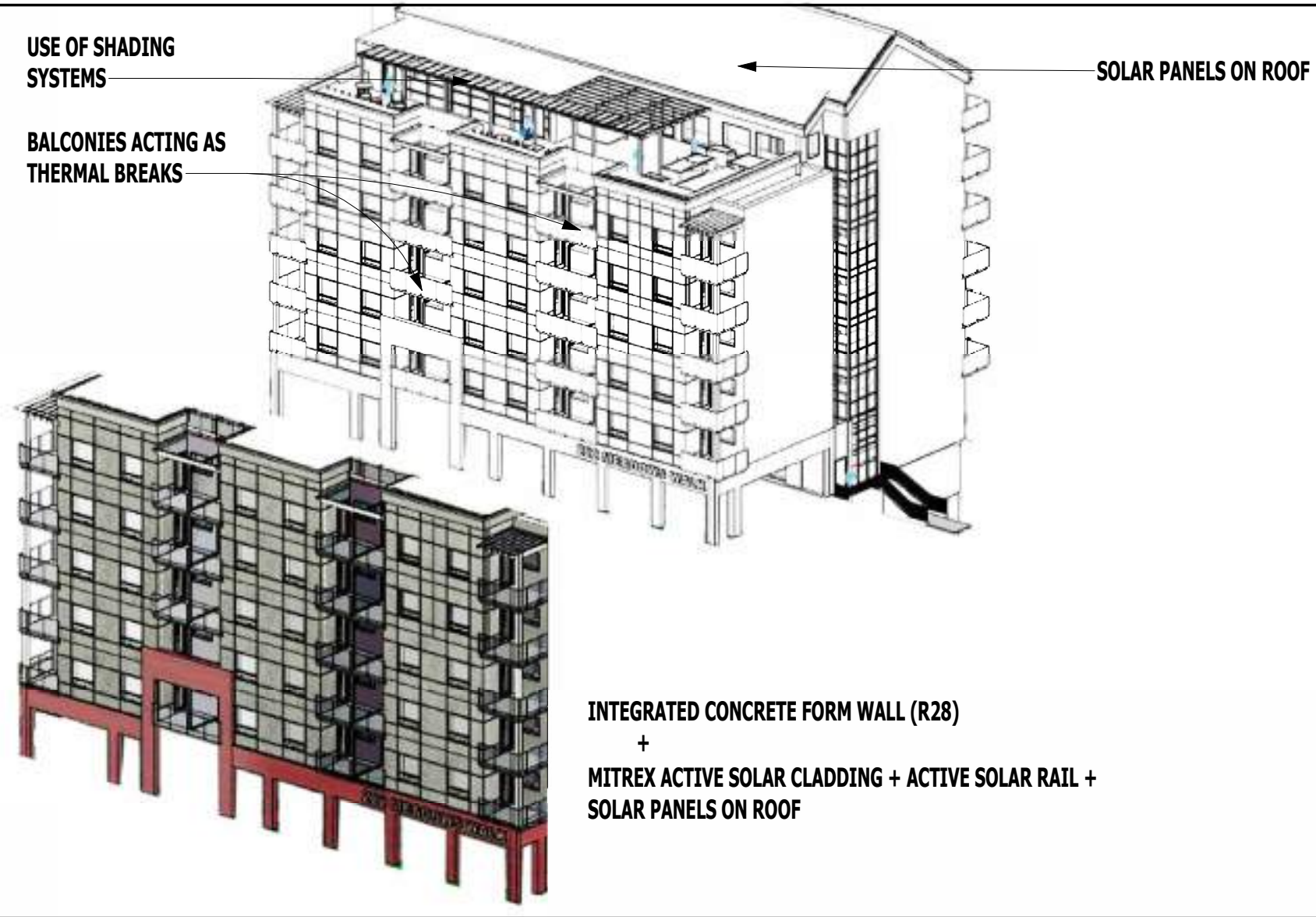
GREEN SPACE & APARTMENT IN THE AREA



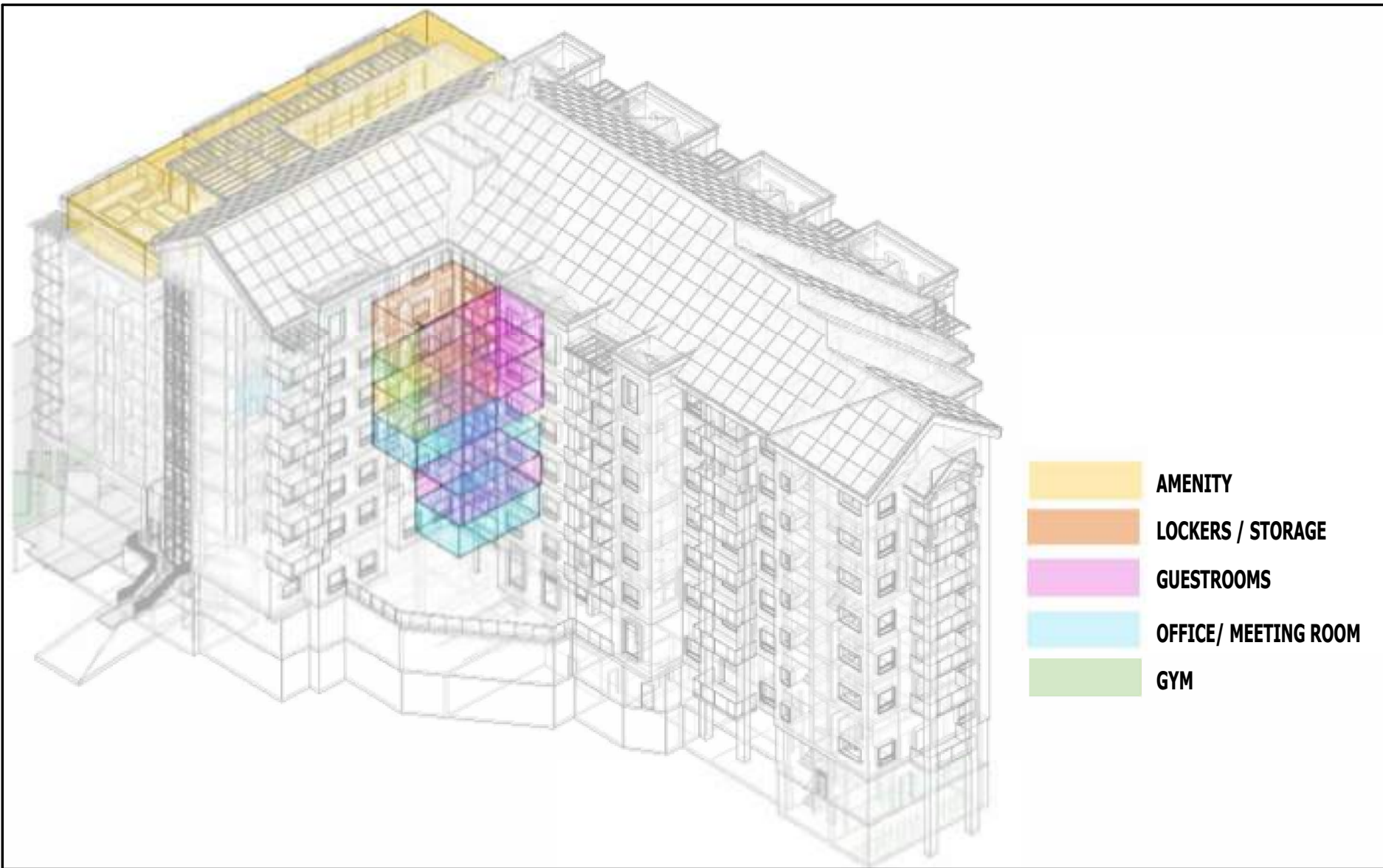
BUILDING SITTING / VIEW CONCEPT



REDUCE THE ROOF MASS AND SIZE



ENERGY-EFFICIENT BUILDING ENVELOPES



AMENITIES FOR TENANTS

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PROJECT No	2018-15
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CH.	Checker
SCALE	As indicated

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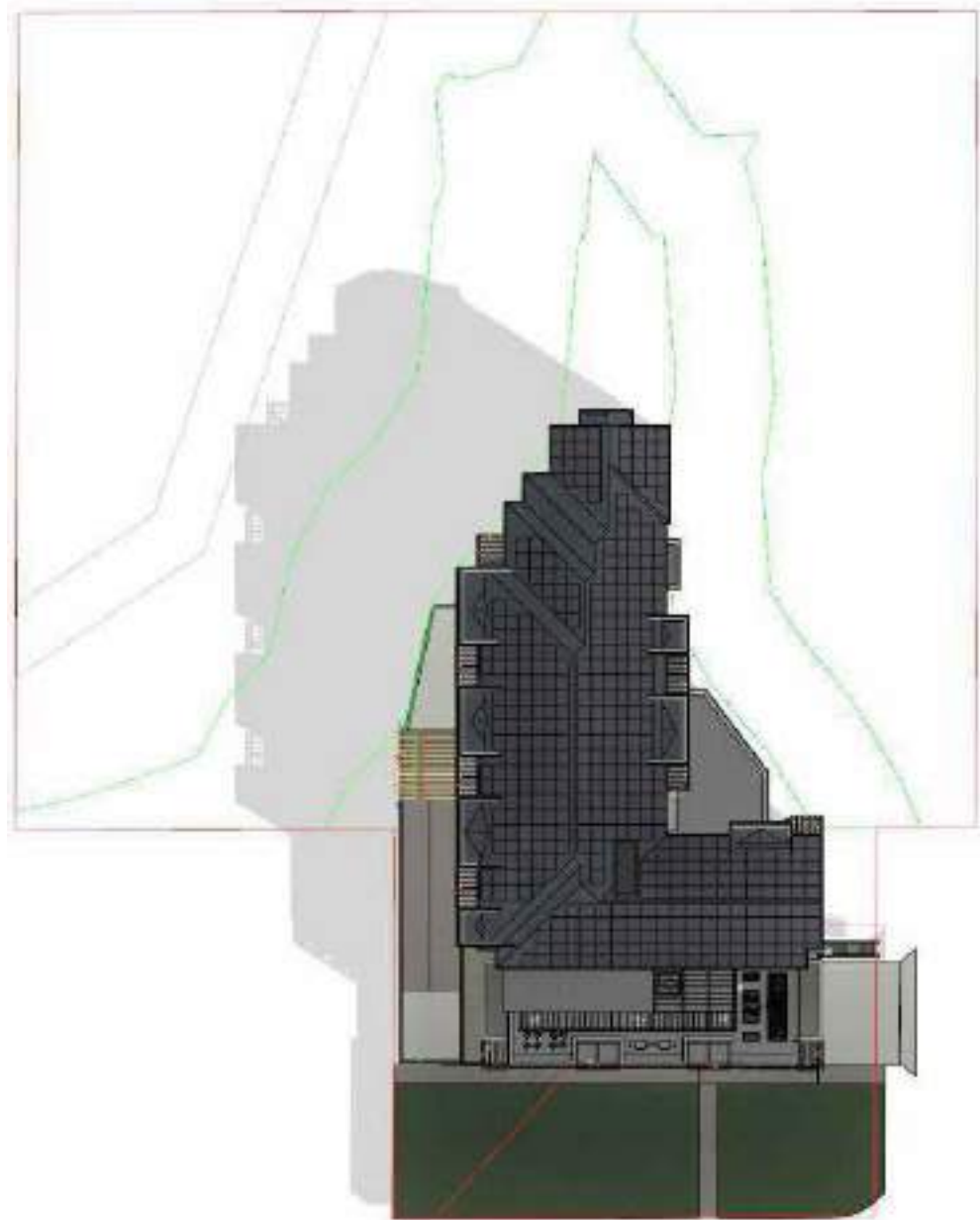
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MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

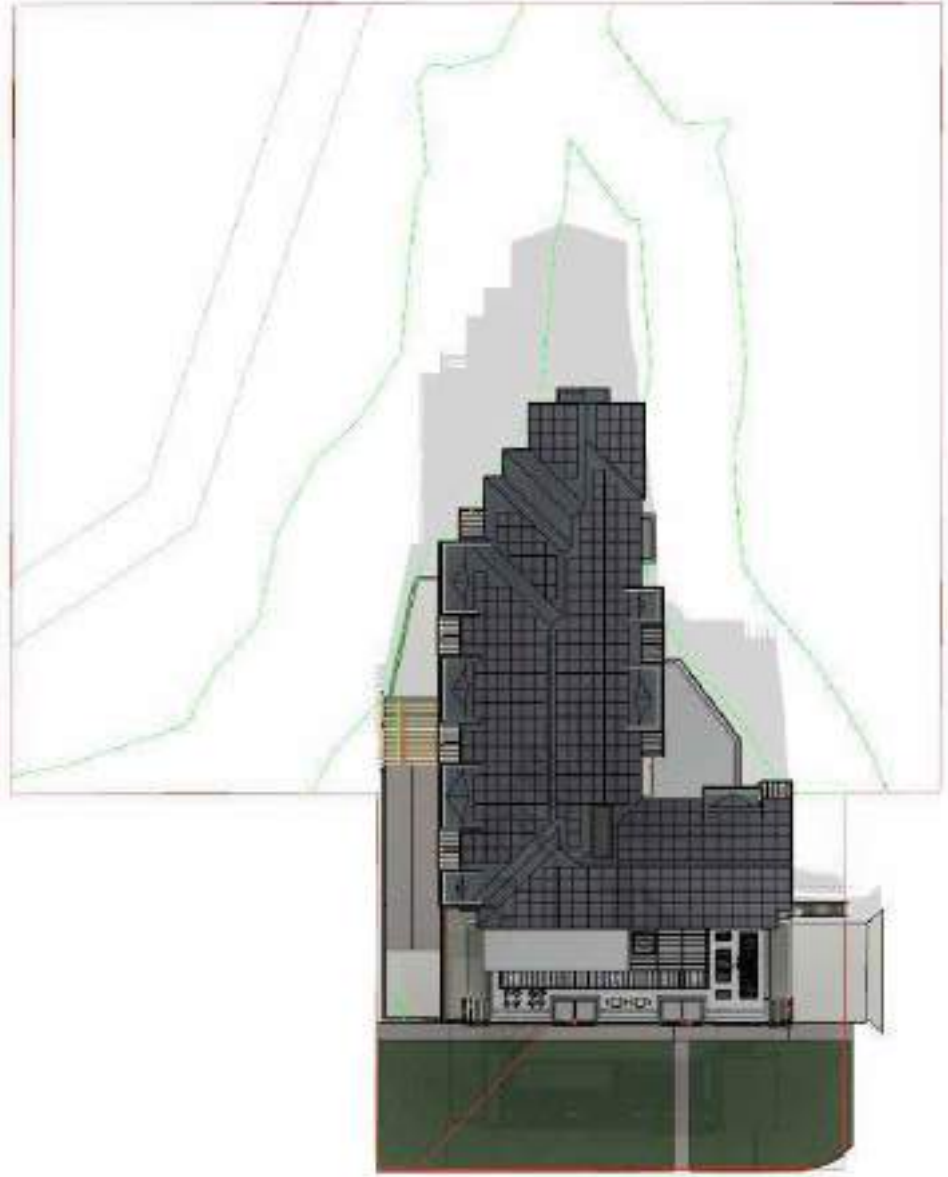
#### DESIGN BRIEF

REV No	14	Project No	2018-15	DP-004
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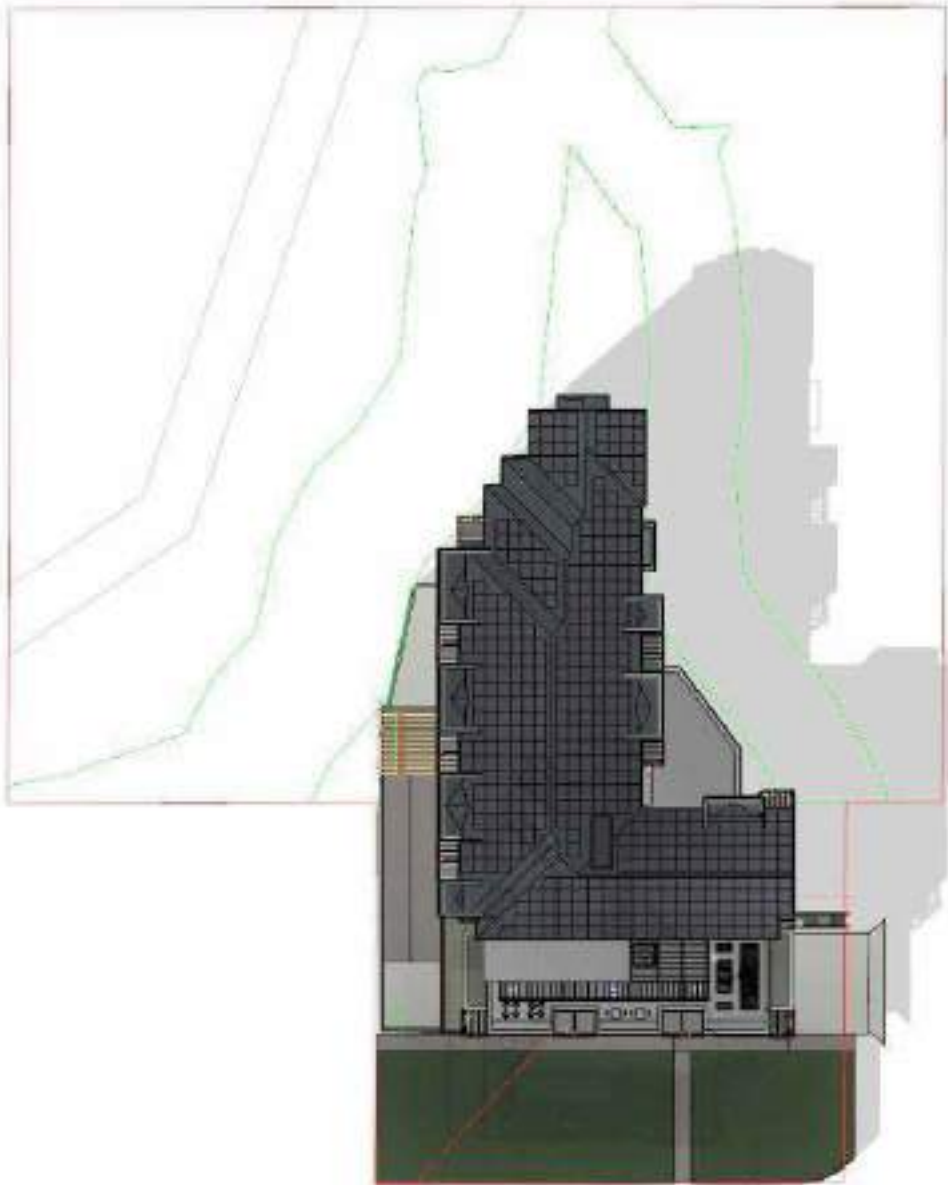




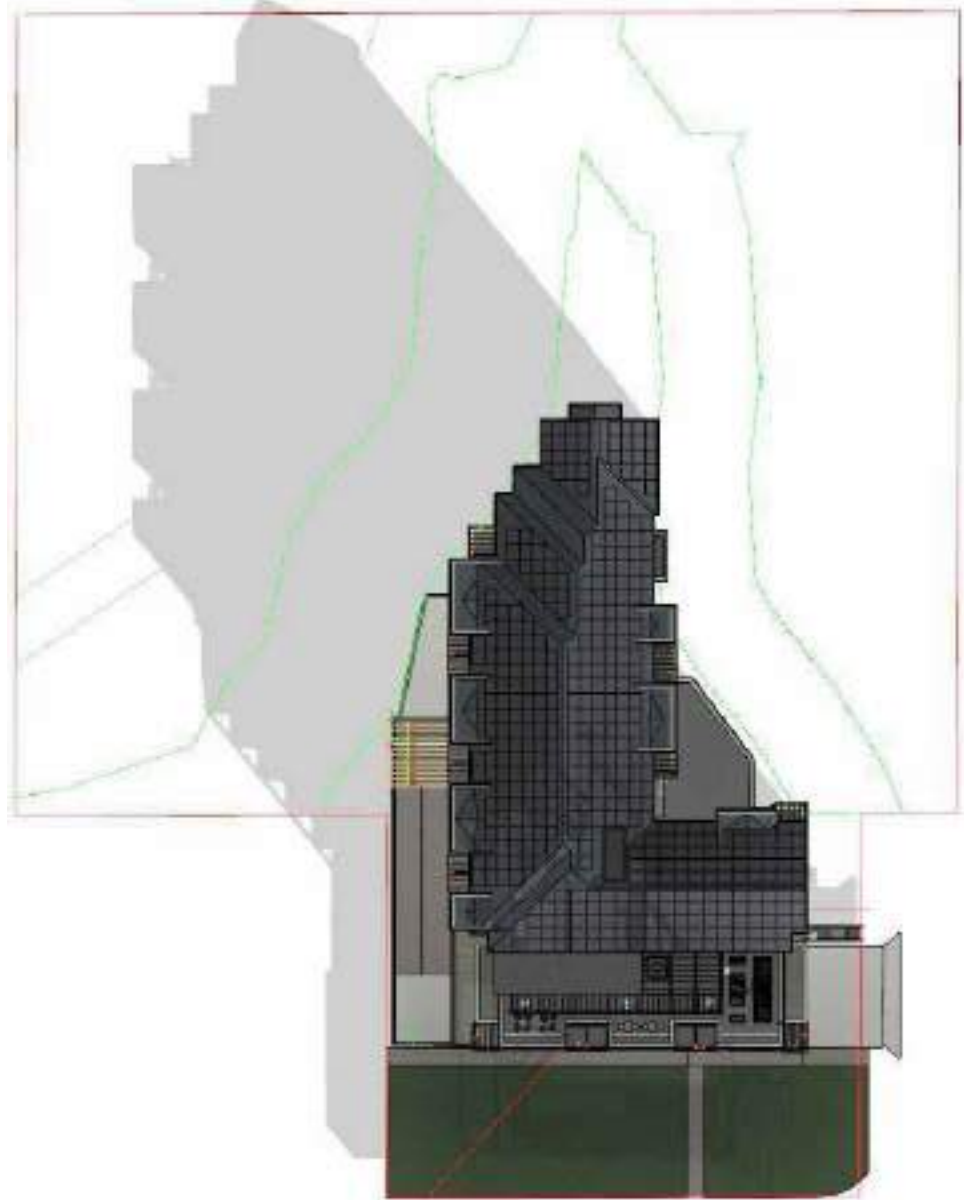
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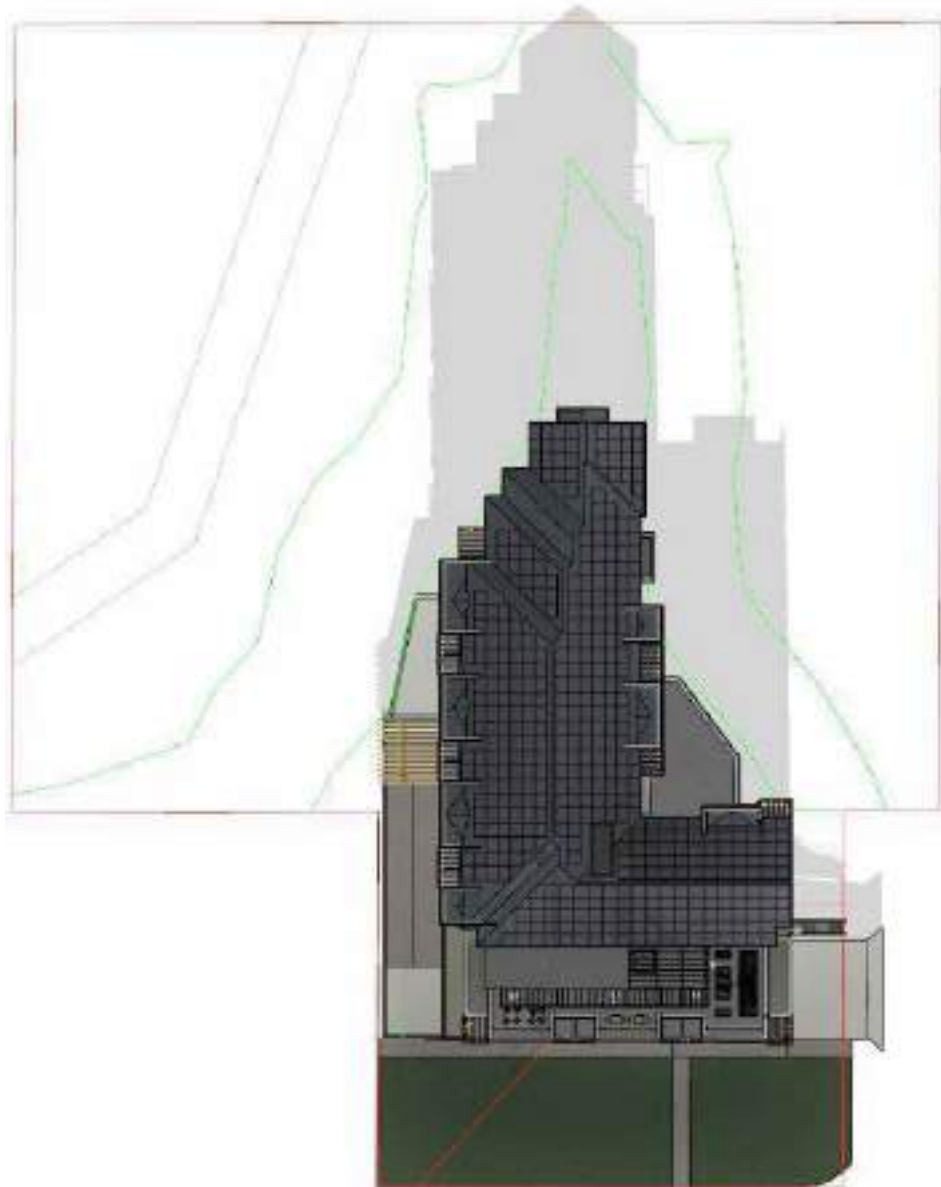
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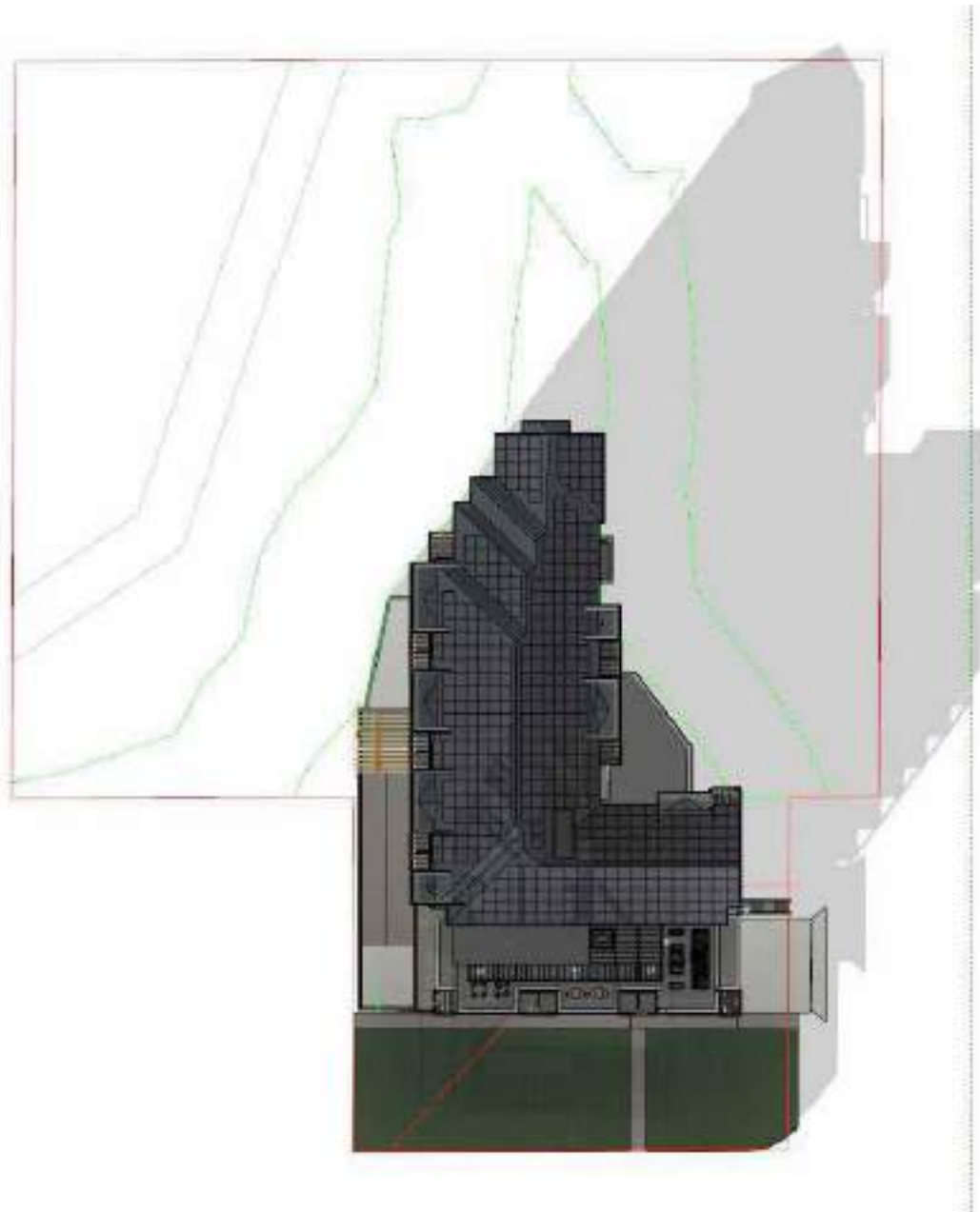
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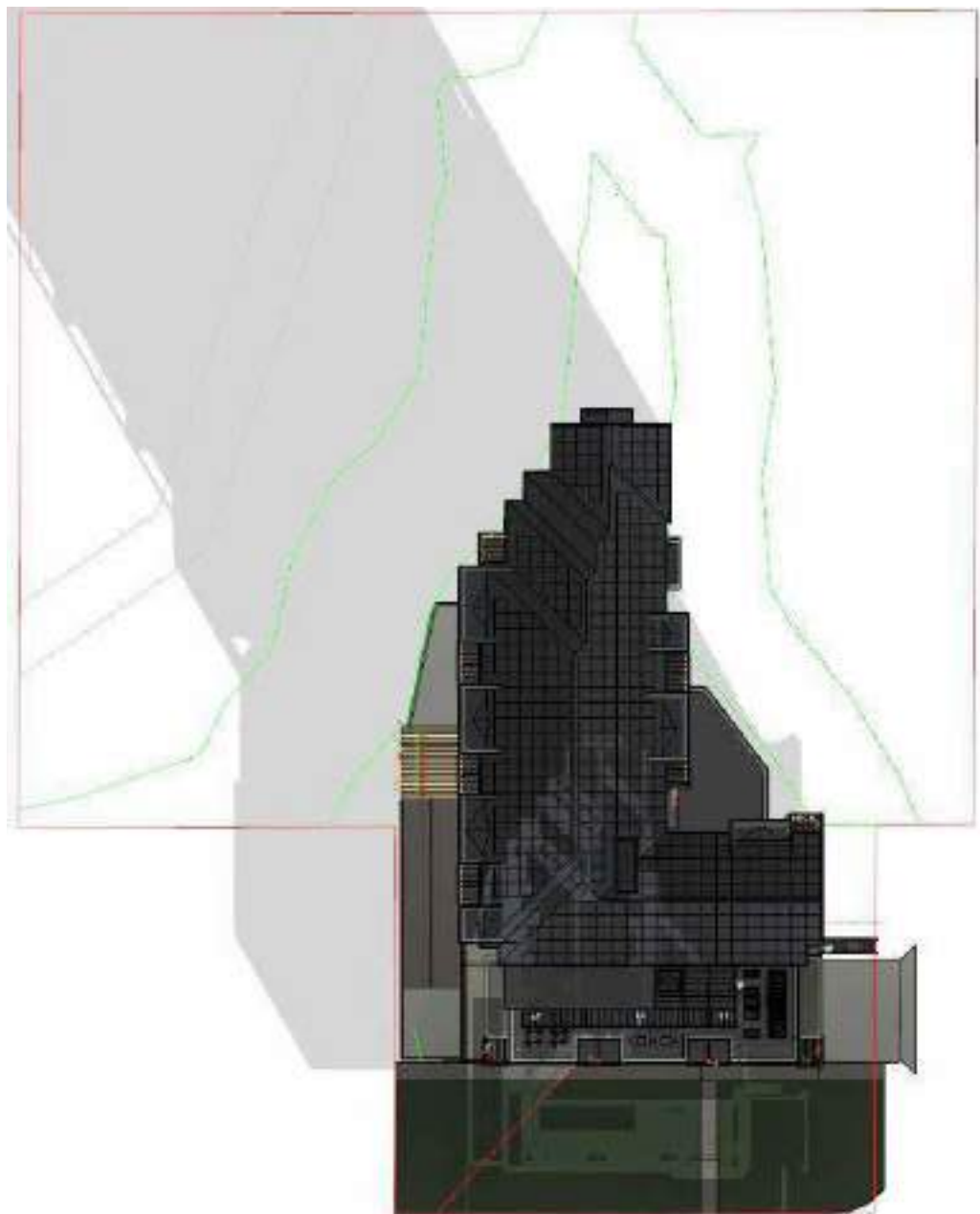
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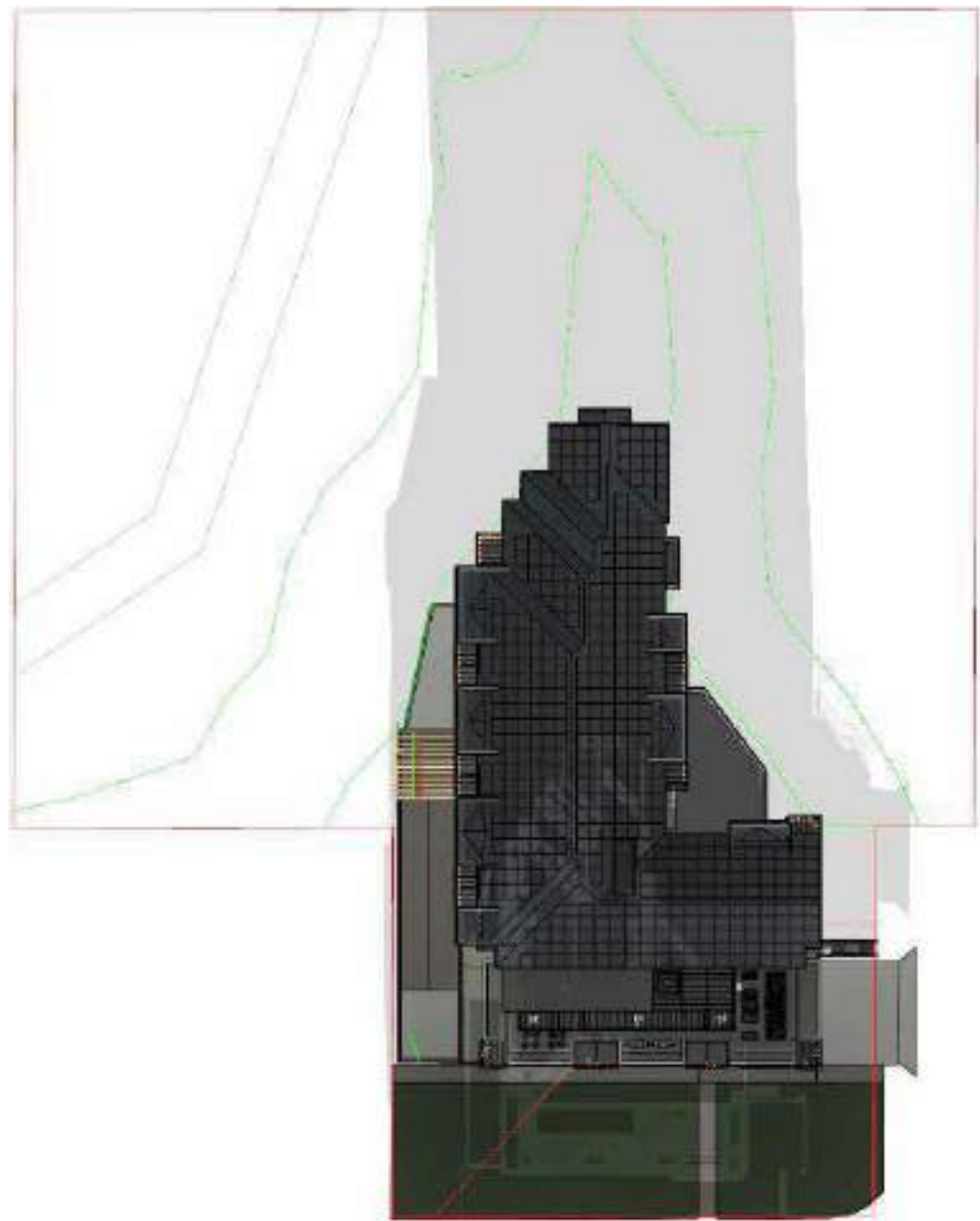
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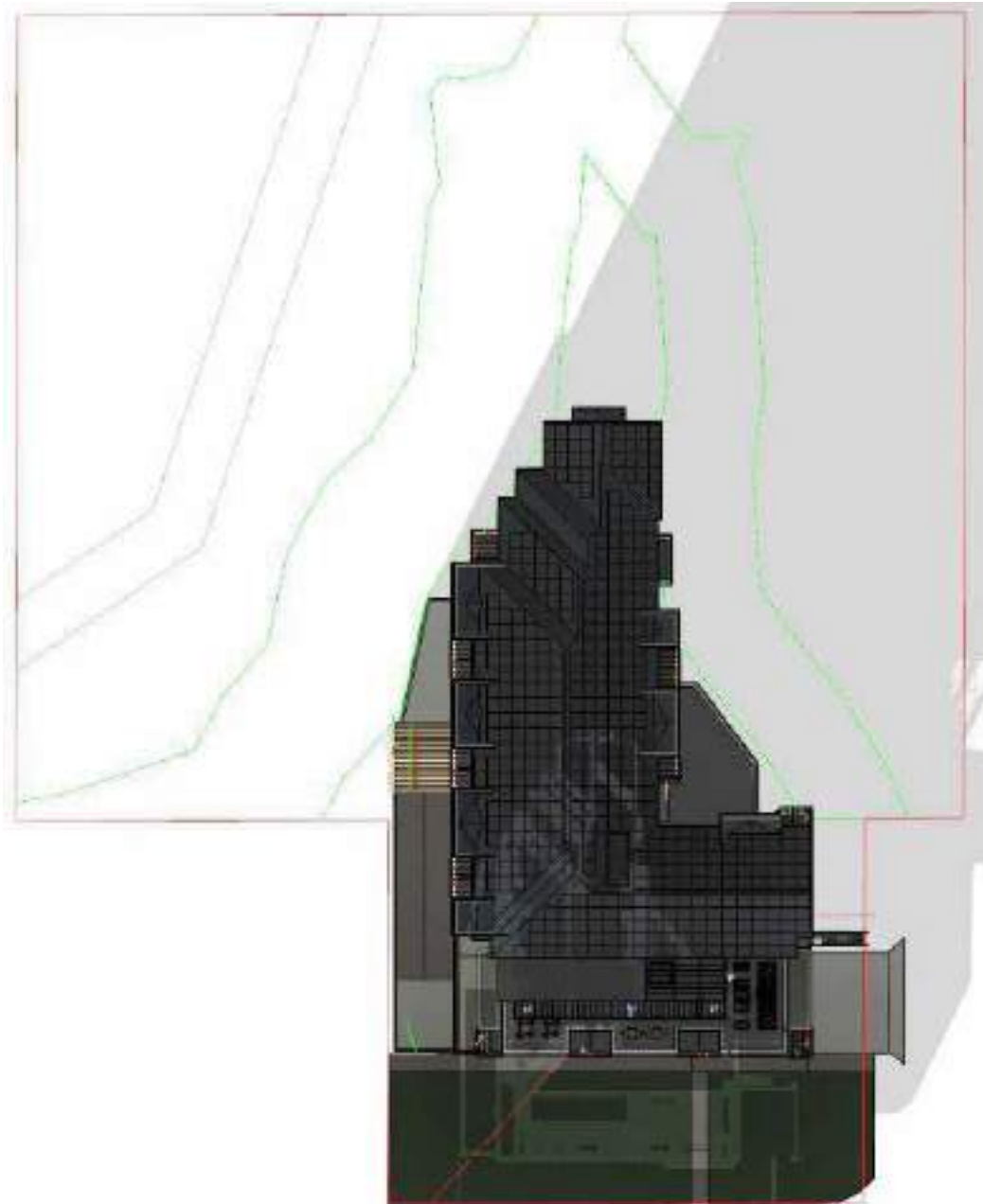
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DECEMBER 21 @ 10:00 AM



DECEMBER 21 @ 12:00 PM



DECEMBER 21 @ 2:00 PM

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**START DATE**

PROJECT No 2018-15

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CH. Checker

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Project Name  
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MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

**SHADOW ANALYSIS**

REV No	11	Project No	2018-15	DP-005
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① SOUTH ELEVATION-STREETSCAPE  
1/16" = 1'-0"



② WEST ELEVATION -STREETSCAPE  
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CH. JMM

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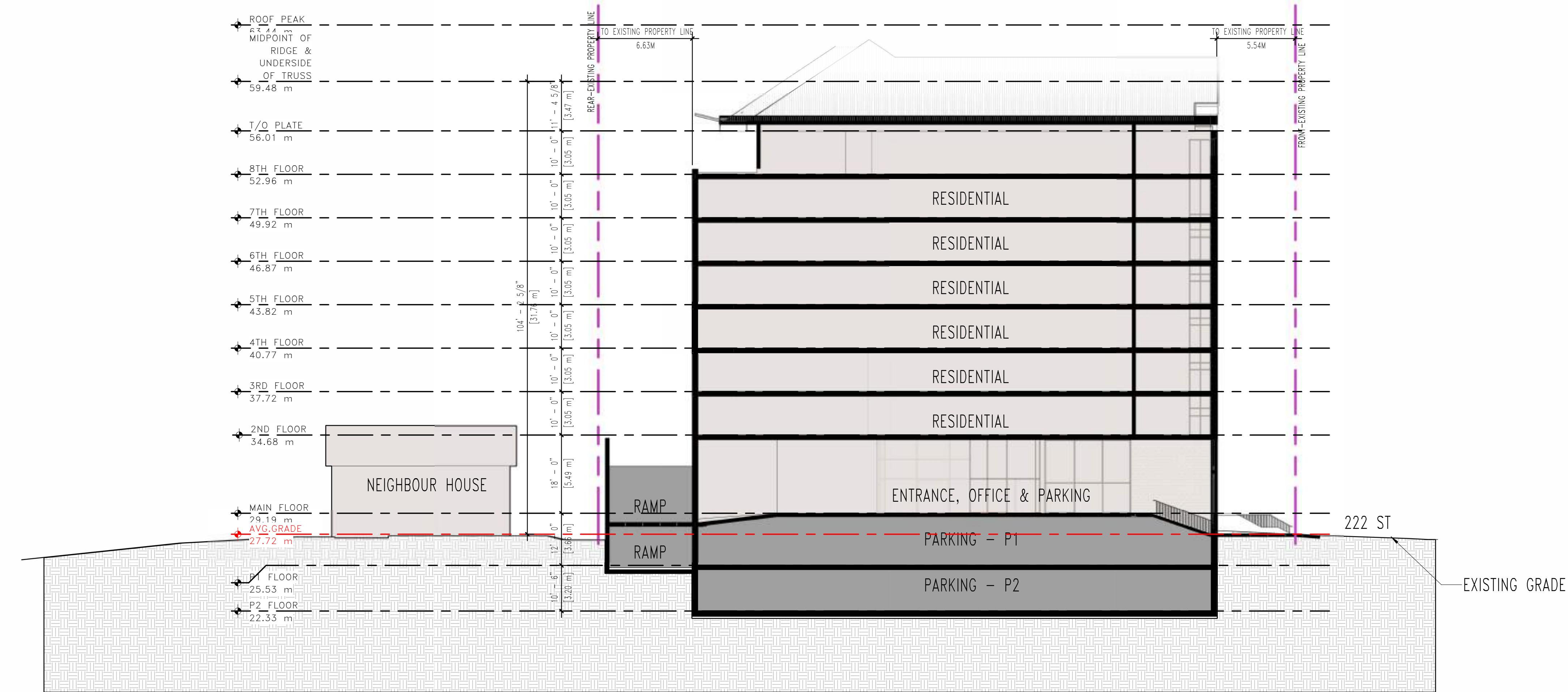
STREETSCAPE

REV No	12	Project No	2018-15	DP-006
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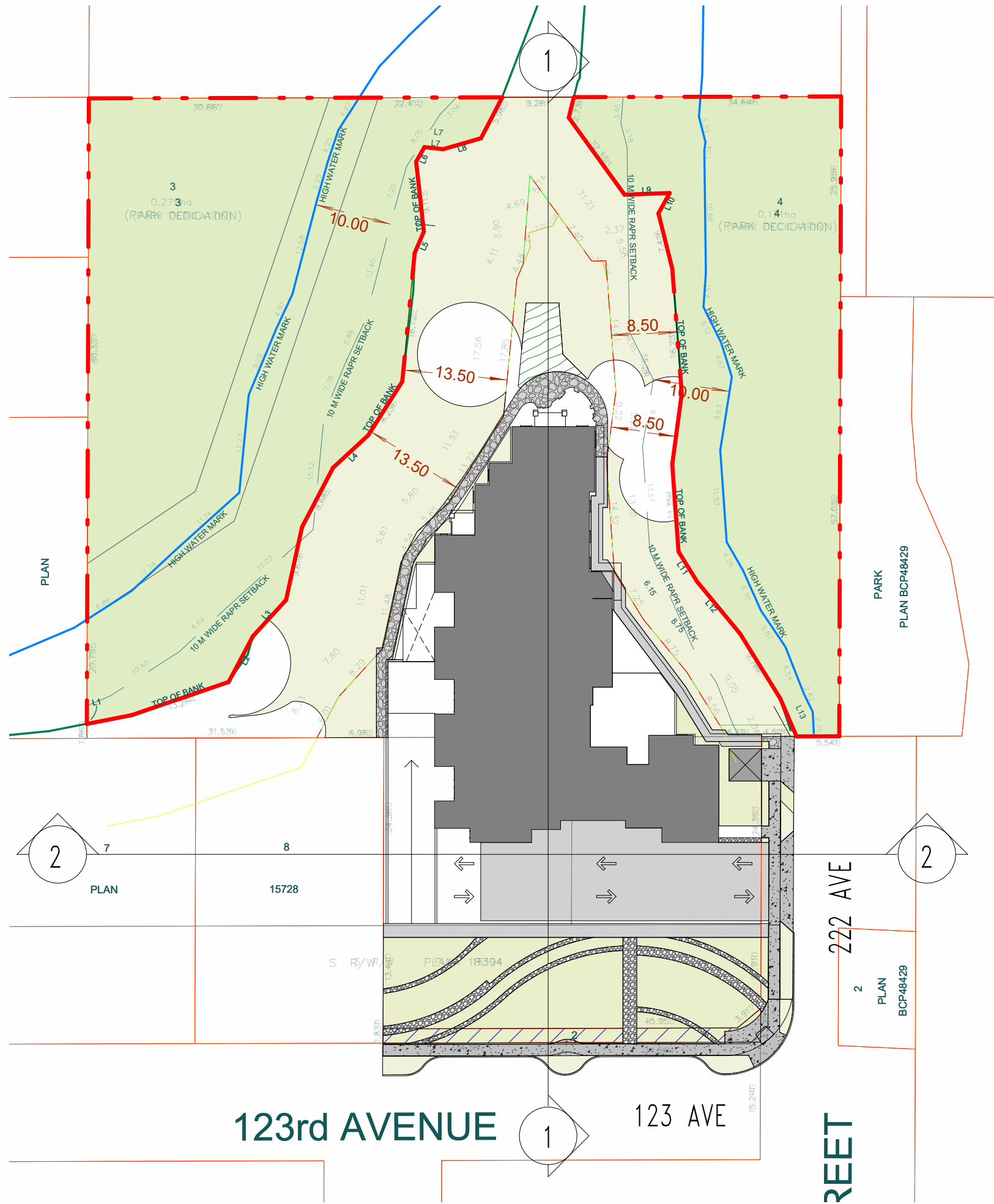




1 SC-NS CROSS SECTION  
1/16" = 1'-0"



2 SC-EW CROSS SECTION 2  
1/16" = 1'-0"



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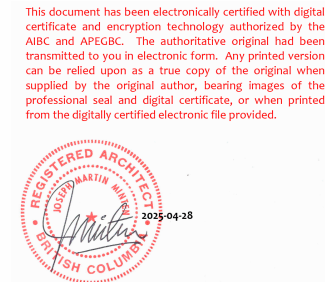
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Project Name  
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**SITE SECTIONS**

REV No	Project No	DP-007
16	2018-15	



BUILDING CODE ANALYSIS - BCBC 2018				
Building Statistics		Proposed building	BCBC 2018 Governing article	
Building Area	1770.99 m2/ 19063 SF		1.4.1.2	
Gross floor Area	11782.54 m2 / 126,827.26 SF			
Building Ht. (storeys)	8 storey			
Basements	2		3.2.2.10	
Facing Number of Streets	1			
Building Size and Construction Relative to Occupancy		Required / Permitted	Proposed	
Major Occupancies:			Group C, Group A2 & Group D	3.2.2.47
Fire Protection System Required (Y/N):	Yes	Yes		3.2.2.47.1
Building height	Any height	8 storey		
Distance from first storey to uppermost floor level		31.76m / 104.20 ft		
Building Area (m2)		1770.99 m2/ 19063 SF		3.2.2.47.2, 3.1.3.1
Construction Type:	Noncombustible	Noncombustible		
Floor Rating:	2 HR	2 HR		
Roof Rating:	1 HR	1 HR		
Loadbearing Rating:	supported assembly	supporting assembly		
Exit Stair	2 HR	2 HR		3.4.4.1
Elevator shaft	2 HR	2 HR		3.5.3.1
Storage garage as a separate building		UNDERGROUND PARKING GROUP F3		3.2.1.2
Floor, Roof, Exterior Wall rating	Noncombustible, 2 HR	Noncombustible, 2 HR		3.2.1.2.(1)

ABOVE CODE ANALYSIS IS BASED ON BCBC 2018 & WILL BE UPDATED ONCE BCBC 2024 DURING THE PROGRESS OF THE PROJECT.

UNIT MATRIX					
UNIT TYPE		UNIT AREA	OUTDOOR AMENITY	# OF UNITS	TOTAL PRIVATE OUTDOOR AMENITY AREA
TYPE A	1 BEDROOM	574.63 SF	61.00 SF	60	3660 SF
TYPE B	1 BEDROOM	678.41 SF	79.00 SF	5	395 SF
TYPE C	2 BEDROOM	1149.26 SF	61.00 SF	18	1098 SF
TYPE D	3 BEDROOM	1356.82 SF	79.00 SF	4	316 SF
TYPE E	2 BEDROOM	892.00 SF	64.00 SF	6	384 SF
TYPE F	2 BEDROOM	930.91 SF	70.00 SF	6	420 SF
TYPE G	3 BEDROOM	1154.06 SF	108.00 SF	7	756 SF
TOTAL				106	7029 SF (653.01 SQ.M.)

UNIT COUNT (AFFORDABLE/MARKET)								
FLOOR	QUANTITY OF UNIT TYPES							
	TYPE A 1BR	TYPE B 1BR	TYPE C 2BR (2LV)	TYPE D 2BR (2LV)	TYPE E 2BR	TYPE F 2BR	TYPE G 3BR	TOTAL
2ND FLOOR	12	1	0	0	0	0	1	14
3RD FLOOR	15	2	0	0	1	1	1	20
4TH FLOOR	15	2	0	0	1	1	1	20
5TH FLOOR	6	0	9	2	1	1	1	20
6TH FLOOR	6	0	—	—	1	1	1	9
7TH FLOOR	6	0	9	2	1	1	1	20
8TH FLOOR	0	0	—	—	1	1	1	3
TOTAL	60	5	18	4	6	6	7	106
PLEASE NOTE: STUDIO GUEST UNITS WILL ONLY BE USED BY THE RESIDENTS.								
COLOR LEGEND								TOTAL
AFFORDABLE RENTAL APARTMENT								54
RENTAL APARTMENT								52

THE DWELLING UNITS ARE DESIGNED TO CMHC ACCESSIBILITY DESIGN STANDARDS WHICH CALL FOR 100% OF THE UNITS TO BE UNIVERSALLY ACCESSIBLE. THERE IS NO REQUIREMENT WITHIN BC BUILDING CODE TO DESIGN THE INTERIOR OF THE UNITS TO AN ACCESSIBILITY STANDARD.

NO. OF CHILDREN FOR DAYCARE
TOTAL DAYCARE FLOOR AREA IS 328.47 SQ.M. ( 3535.62 SQ. FT.)
NO OF CHILDREN IT CAN ACCOMMODATE = 3535.62 / 75 = 47 CHILDREN.

GARBAGE AND RECYCLING AREA							
NUMBER OF RESIDENTIAL UNITS (2 RESIDENTS PER UNIT)	MIXED CONTAINTERS	NEWSPAPERS & MIXED PAPERS (WITHOUT CARDBOARD BIN)	NEWSPAPERS & MIXED PAPERS (WITH CARDBOARD BIN)	GLASS	COMPOSTABLE ORGANICS (HIGH PARTICIPATION)	CARDBOARD BIN	GARBAGE
101 – 110	360 LITRE CART			240 LITRE CART		CUBIC YARD BIN (SIZE)	
	5	11	4	1	5	3 yd3	4 – 4 yd3

ANCILLARY BUILDING – PARKING STRUCTURE SETBACKS			
REQUIRED		PROVIDED	
FRONT LOT LINE – 222 ST.		5.5M	5.59 M
REAR LOAT LINE – WEST		0.5M	0.50 M
INTERIOR LOT LINE – NORTH		42.6M	42.69 M
EXTERIOR LOT LINE – 123 AVE		13.8M	13.88 M

PROJECT DATA			
ZONING DATA	REQUIRED/ PERMISSIBLE PER BYLAW NO. 7812-2021	PROPOSED WITH DEDICATED PARK LAND	VARIANCE
CURRENT ZONE	CD-6-21		
GROSS LOT SIZE (A)		10035.8 SQ.M.	
ROAD DEDICATION (LOT 2) (B)		93.3 SQ.M.	
PARK DEDICATION (LOT 3 & 4) ( C)		4501.6 SQ.M.	
NET LOT AREA		5,440.9 SQ.M. (0.5440 ha) [(1.344 acre)-(B+C)]	
GAS LINE ROW		21.7 SQ.M. [662.41 SQ.M.]	
BASEMENT TO ACCESS PARKLAND			
DENSITY-FAR	2.75	9,580.24 / 5,440.9 = 1.76	
LOT COVERAGE	NOT APPLICABLE	1,767.98/5,440.9 = 32.49 %	
PRINCIPAL USES	AFFORDABLE RENTAL APARTMENT & RENTAL APARTMENT	AFFORDABLE RENTAL AND RENTAL APARTMENT	
ACCESSORY USES	ANCILLARY OFFICE SPACE, GROUP CHILD CARE CENTER ON SECOND FLOOR, BOARDING	BOARDING - ANCILLARY OFFICE SPACE ON MAIN FLOOR - GROUP CHILD CARE ON THE SECOND FLOOR.	
		FOR DAYCARE: INDOOR SPACE - 325 SQ.M.	
		UNCOVERED OUTDOOR PLAY AREA - 105 SQ.M.	
		COVERED OUTDOOR PLAY AREA - 85 SQ.M.	
DEMOLISHED FLOOR AREA		4 BUILDINGS IN TOTAL = 330 SQ.M.	
		PROPOSED	
		13.5M (123 AVE)(SIDE SOUTH)	
SETBACKS		5.5M (222 ST)(FRONT EAST)	VARIANCE REQUIRED
		6.6M (REAR-WEST)	
		42.5M (SIDE-NORTH)	
HEIGHT OF BUILDING (BASED ON AVERAGE GARDE TO ROOF RIDGE)		8 STOREYS (32M)	
		8 STOREYS (31.765 M)	
COMMON OPEN AREA	REQUIRED - 2,000M2 PER CD-6-21	PROVIDED 2005.85 SQ. M.	
		(INCLUDING FORTIS ROW, PRIVATE OUTDOOR AMENITY, OUTDOOR AMENITY, AREA BETWEEN WALKWAY AND GREEN AREA BETWEEN BUILDING AND RESTRICTIVE COVENANT)	
OUTDOOR AMENITY AREA		123.75 SQ.M. ON THE NORTH SIDE OF THE BUILDING	
		219.35 SQ.M. ON 8TH FLOOR	
	REQUIRED - 340M2 PER CD-6-21	TOTAL OUTDOOR AMENITY AREA: 343.08 SQ.M.	
PRIVATE OUTDOOR AREA	5% OF DWELLING UNIT OR 4.6 SQ.M.	5.66 SQ. M. MINIMUM PER DWELLING UNIT	
		TOTAL OF 653.01 SQ.M. (7029 SF)	
		FOR TOTAL DECK AREA REFER TO UNIT MATRIX TABLE	
INDOOR AMENITY AREA	REQUIRED - 510M2	515 SQ.M.	
		(AMENITY PER FLOOR PLUS INDOOR AMENITY ON 8TH FLOOR)	
OFF-STREET PARKING	AS PRESENTED TO COUNCIL		
	REQUIRED 120 STALLS (120-26 AT GRADE +96 STALLS UNDERGROUND)	PROVIDED: 100 STALLS	
		PROVIDED: 20 STALLS AT MAIN FLOOR LEVEL	
		PROVIDED: 2 STALLS (INCLUDED UNDER VISITOR PARKING)	
		PROVIDED: 7 STALLS (INCLUDED UNDER VISITOR PARKING)	
		TOATL NUMBER OF PARKING STALLS REQUIRED = 120 STALLS	PROVIDED :TOTAL 120 STALLS
VEHICLE PARKING PROVIDED	REGULAR PARKING STALLS	99	
	SMALL CAR PARKING STALLS (MAX. 10M x 120 / 10' x 120')	18	VARIANCE REQUIRED
H/C PARKING	ACCESSIBLE PARKING STALLS REQUIRED	2	
		120	
	REQUIRED SPACES	3 (INCLUDED IN THE CALCULATIONS ABOVE)	
		2 STALLS ON THE MAIN FLOOR	
		1 STALL ON LEVEL P1	
BICYCLE PARKING	NO BIKE STALLS REQUIRED	PROVIDED LONG TERM = 32 BIKES	
		BIKE STALL SIZE: 1.8M X 0.6M	
		PROVIDED SHORT TERM = 33 BIKES	
		BIKE STALL SIZE: 1.8M X 0.6M	
		NO SMALL STALLS USED	

NOTE: ALL THE PARKING SPACES & 50% OF VISITOR PARKING SPACES WILL BE PROVIDED WITH ROUGHED-IN INFRASTRUCTURE CAPABLE OF PROVIDING LEVEL 2 CHARGING

NOTE: COVENANT REQUIREMENT  
2. THE GRANTOR SHALL CONSTRUCT AND INSTALL THE FOLLOWING VISITO PARKING SPACES ON THE LAND:  
(a) 15 FULL-TIME VISITOR PARKING SPACES  
(b) 2 OFFICE/VISITOR PARKING SPACES THAT ARE USED AS VISITOR PARKING SPACES AFTER 6PM ON WEEKDAYS AND ALL DAY ON WEEKENDS, AND USED FOR OFFICE PARKING DURING THE REMAINING TIME, AND DAYCARE/VISITOR PARKING SPACES THAT ARE USED AS VISITOR PARKING SPACES AFTER 6PM ON WEEKDAYS AND ALL DAY ON WEEKENDS, AND USED FOR DAYCARE PARKING DURING THE REMAINING TIME. (c) 7

NOTE: OFFSTREET PARKING WILL NOT EXTEND MORE THAN 1.5M ABOVE AVERAGE FINISHED GRADE.

GROSS BUILDING AREA CALCULATIONS									
LEVEL	AREA (sq.ft.)	GROSS FLOOR AREA EXEMPTIONS BREAKDOWN (sq.ft.)						TOTAL AREA	
		CORRIDOR	STAIRS	ELEV./LOBBY	AMEN./STOR./MEETING/GYM	BIKE STOR.	MAIL ROOM	TOTAL	(sq.ft.) (sq.m.)
MAIN FLOOR PLAN	18,752.14	272.90	359.16	1,836.07		1,014.72	152.72	3,635.57	15,116.57 1404.36
2ND FLOOR PLAN	16,336.45	1,659.48	359.16	334.74		435.15		2,788.53	13,547.92 1258.63
3RD FLOOR PLAN	16,153.00	1,620.90	359.16	334.74		855.20		3,170.00	12,983.00 1206.15
4TH FLOOR PLAN	16,153.00	1,620.90	359.16	334.74		855.20		3,170.00	12,983.00 1206.15
5TH FLOOR PLAN	16,153.00	1,620.90	359.16	334.74		855.20		3,170.00	12,983.00 1206.15
6TH FLOOR PLAN	16,153.00	1,620.90	359.16	334.74		855.20		3,170.00	12,983.00 1206.15
7TH FLOOR PLAN	16,153.00	1,620.90	359.16	334.74		855.20		3,170.00	12,983.00 1206.15
8TH FLOOR PLAN	13,524.93	1,578.60	359.16	334.74				3,982.69	9,542.24 886.50
TOTAL AREA	129,378.52					1,710.19			103,121.73 9,580.24
TOTAL GROSS BUILDING AREA									

DENSITY-FAR	1.78	FSR = TOTAL GROSS AREA / LOT AREA	
NET LOT AREA	5,440.9 SQ.M.	FSR	1.76

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25	29-07-2024	REISSUED FOR ADP REVIEW	JG
24	27-06-2024	REISSUED FOR ADP REVIEW	JG
23	14-06-2024	REISSUED FOR REZONING	JG
22	12-06-2024	REISSUED FOR REZONING	JG
21	07-06-2024	ISSUED FOR CITY REVIEW	JG
20	04-06-2024	ISSUED FOR REZONING	JG
REV	DATE	DESCRIPTION	BY

## START DATE

PROJECT No 2018-15

DR. SP

CH. JMM

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Project Name

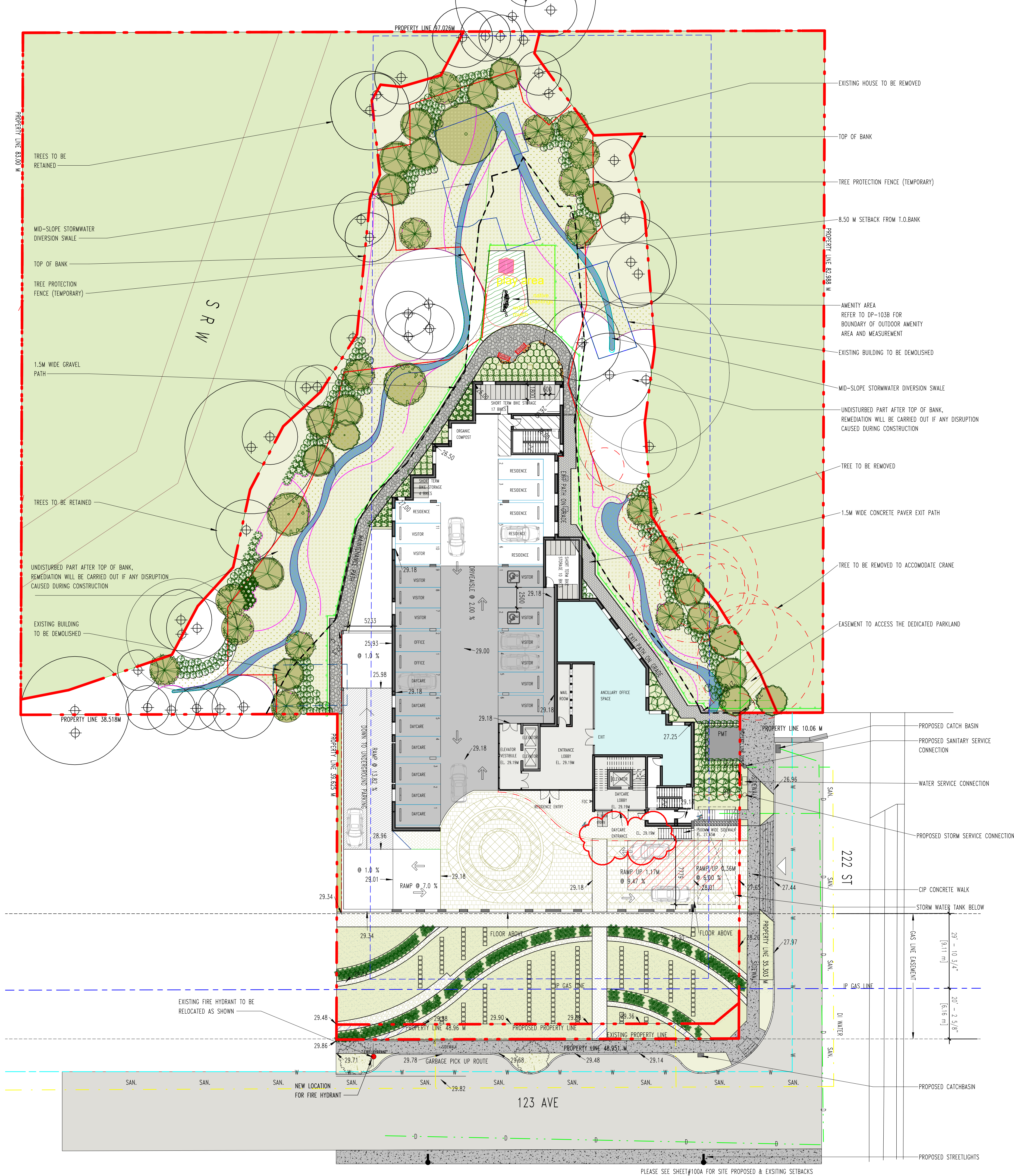
222 MEADOWS WALK  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

## PROJECT DATA

REV No	28	Project No	2018-15	DP-008
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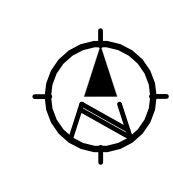




- NOTE:
- TREES TO BE REMOVED
  - TREES TO BE RETAINED

NOTE: AN EFFICIENT AND EFFECTIVE LIGHTING PLAN FOR THE COVERED DRIVE ASILE WILL BE PROPOSED AT THE BUILDING PERMIT STAGE

SITE PLAN  
SCALE: 1:250  
Page 485 of 690



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24	04-09-2024	REISSUED FOR ADP REVIEW	JG
23	27-06-2024	REISSUED FOR ADP REVIEW	JG
22	4-06-2024	REISSUED FOR REZONING	JG
21	12-06-2024	REISSUED FOR REZONING	JG
20	04-06-2024	ISSUED FOR REZONING	JG

START DATE	
PROJECT No	2018-15
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CH.	JMM
SCALE	1 : 250



Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

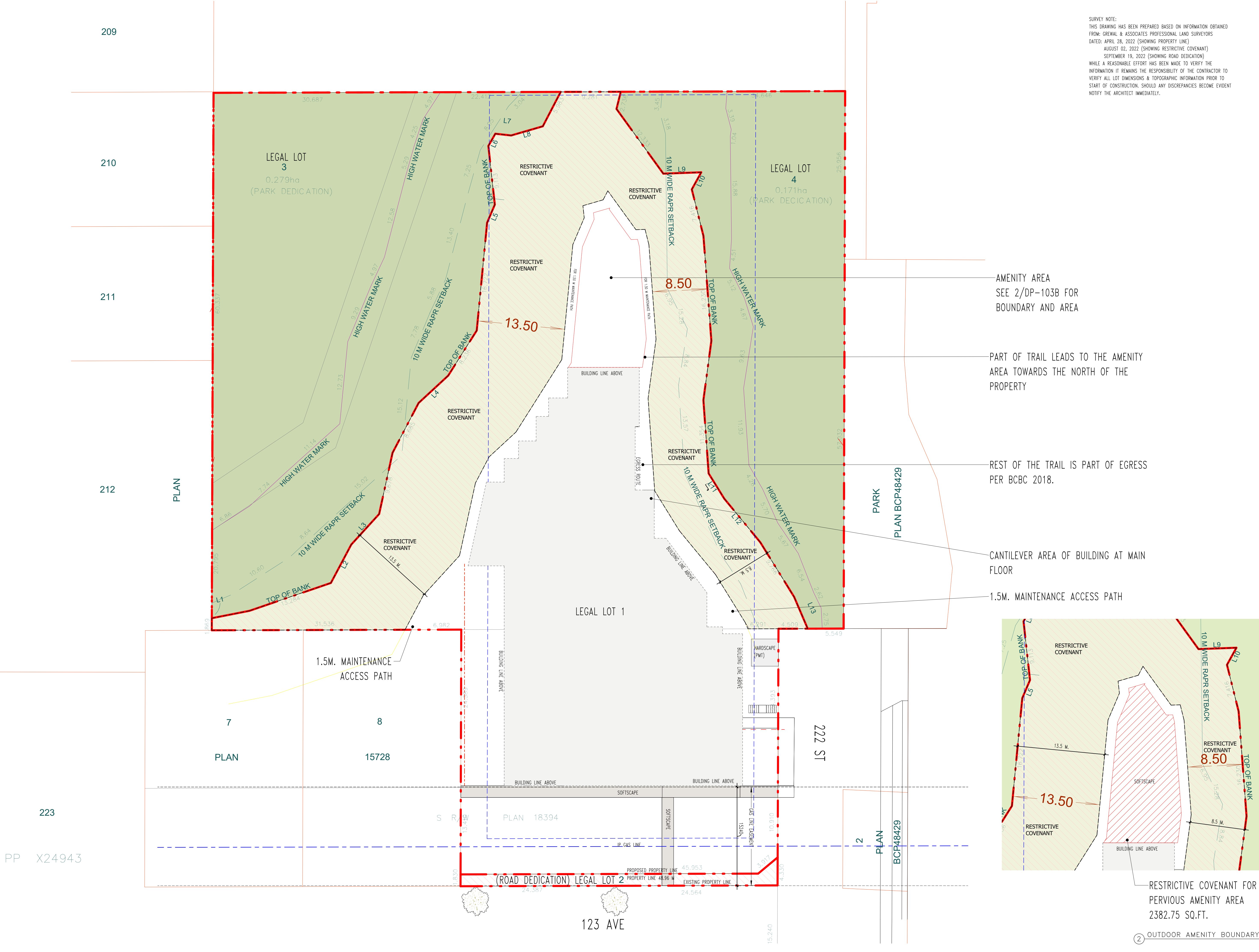
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SITE PLAN

REV No	26	Project No	2018-15	DP-100A
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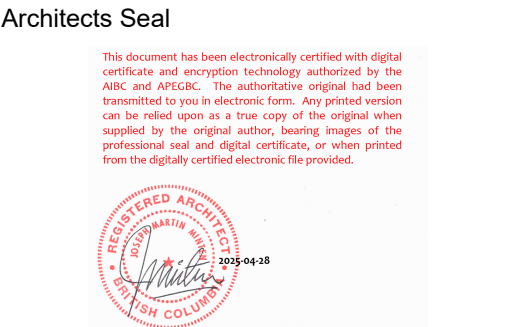
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3	29-04-2024	REISSUED FOR DP	JG
2	22-04-2024	REISSUED FOR DP & ADP	JG
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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

SITE PLAN SHOWING DEDICATIONS

REV No	9	Project No	2018-15	DP-100B
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2	22-04-2024	REISSUED FOR DP & ADP	JG
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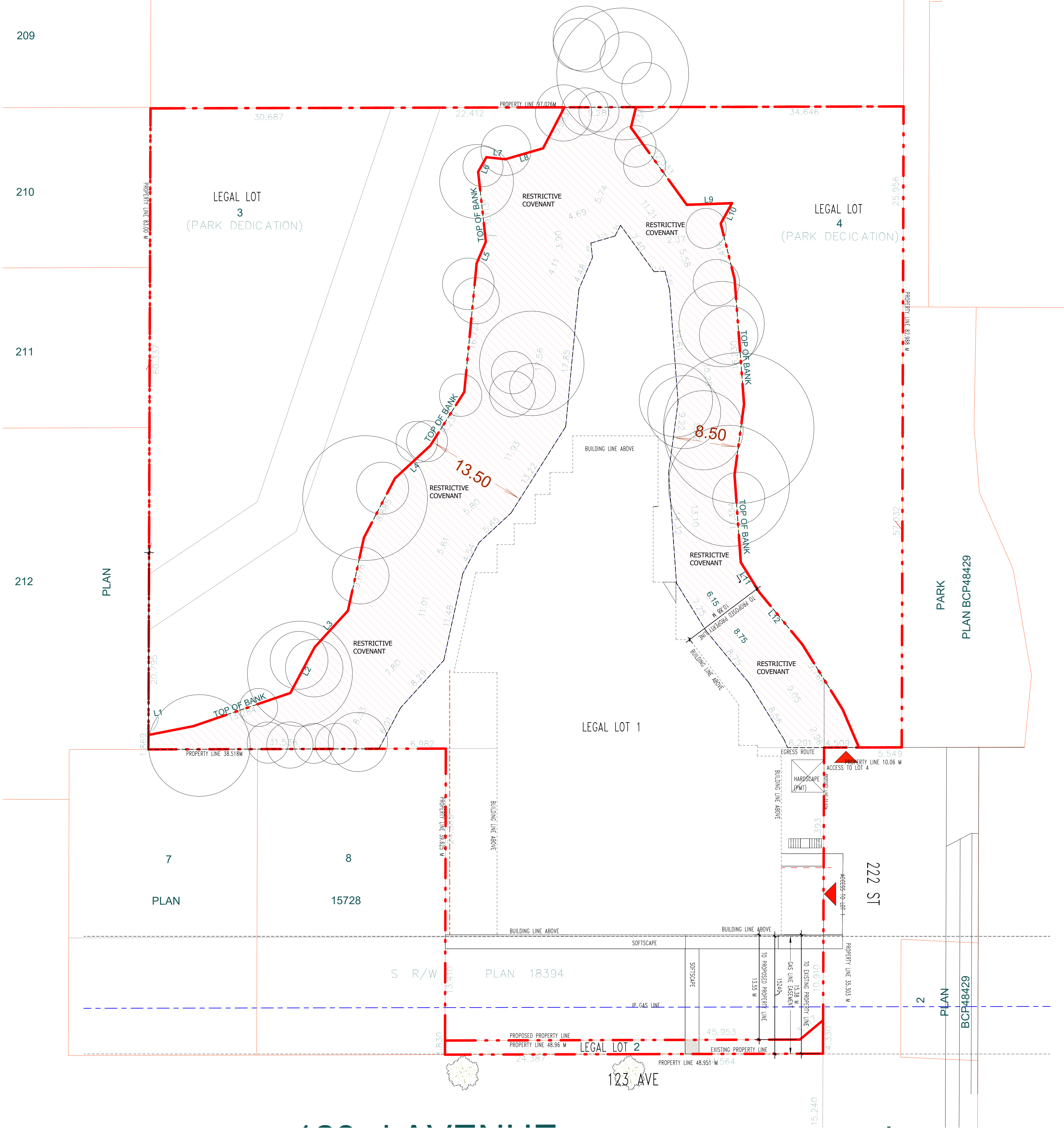
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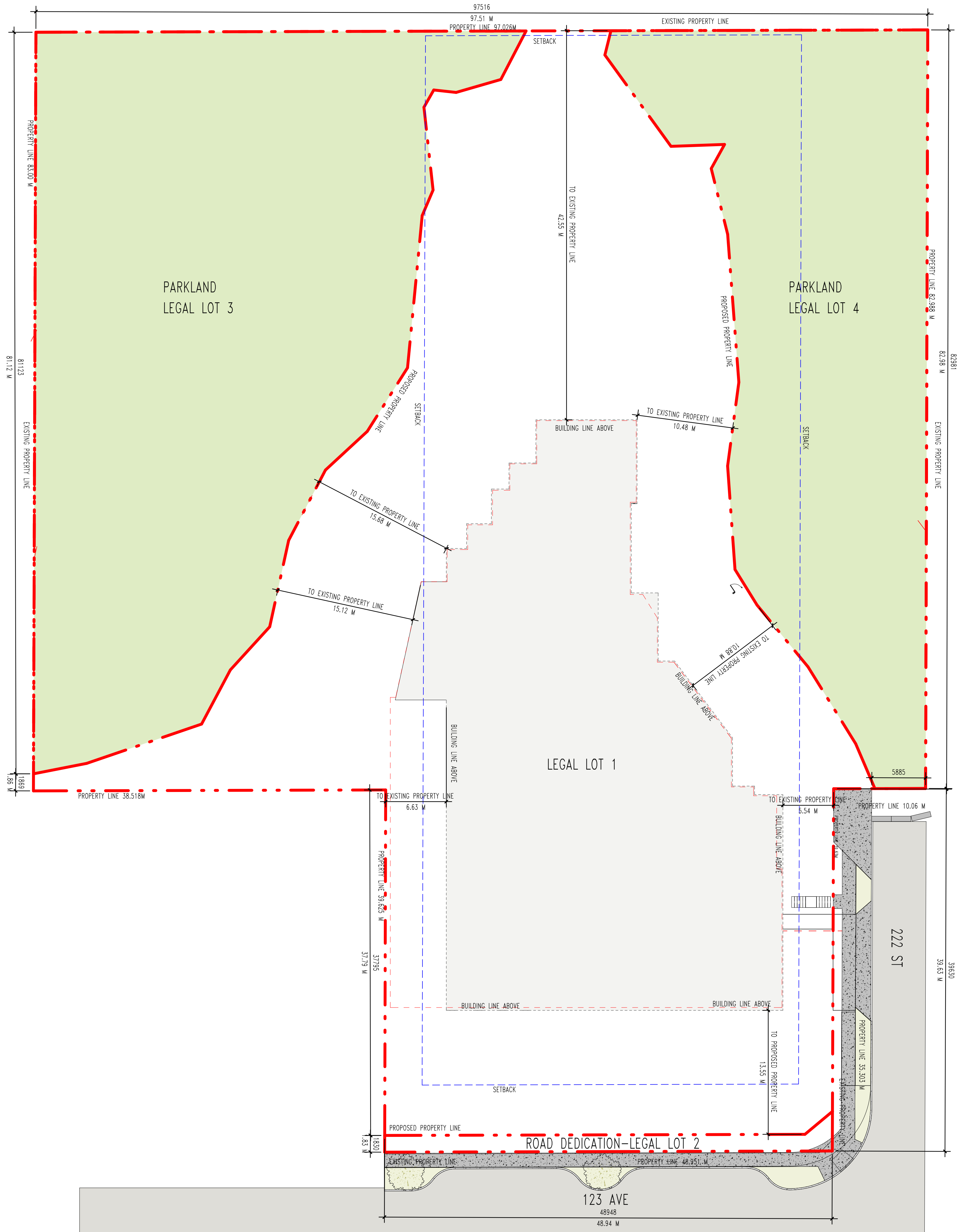
Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
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SITE_LEGAL LOTS & RETAINED TREES		
REV No	Project No	
9	2018-15	DP-100C

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7	12-06-2024	REISSUED FOR REZONING	JG
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5	03-06-2024	REISSUED FOR DP	JG
4	29-05-2024	REISSUED FOR ADP REVIEW	JG
3	29-04-2024	REISSUED FOR DP	JG
2	22-04-2024	ISSUED FOR DP & ADP	JG
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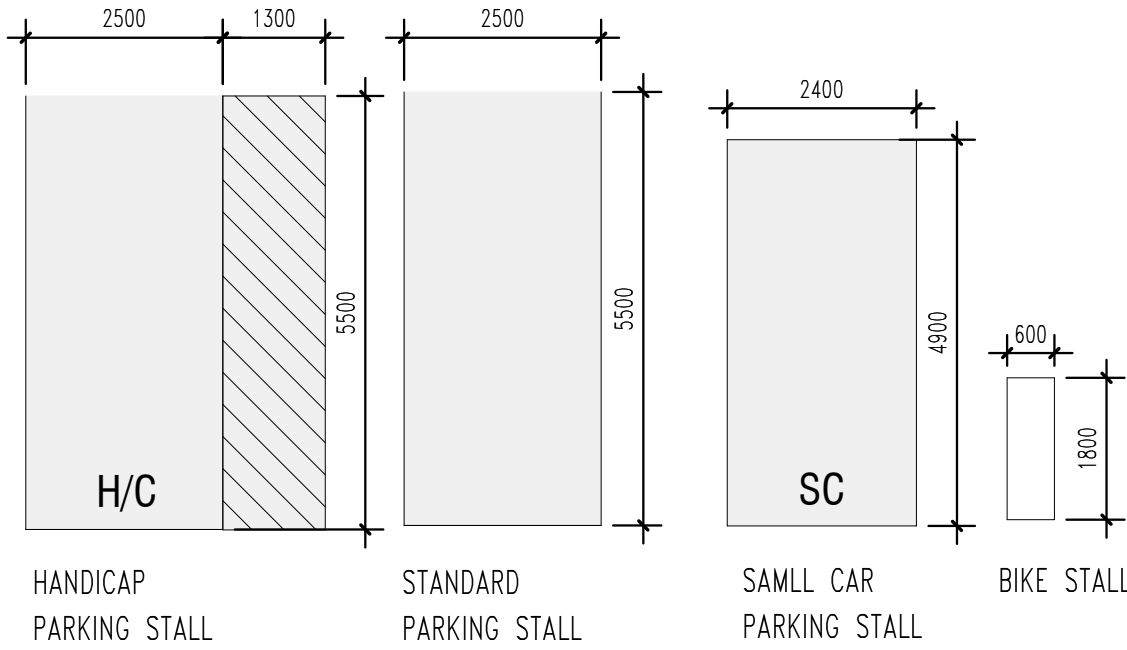
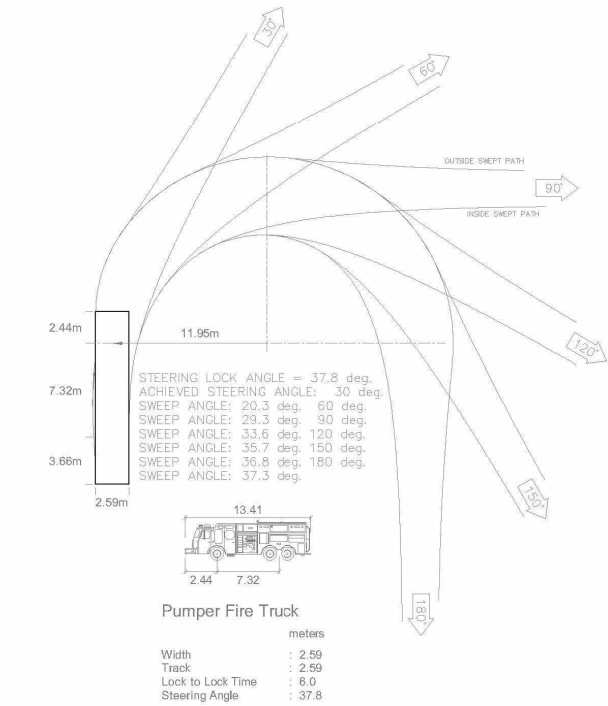
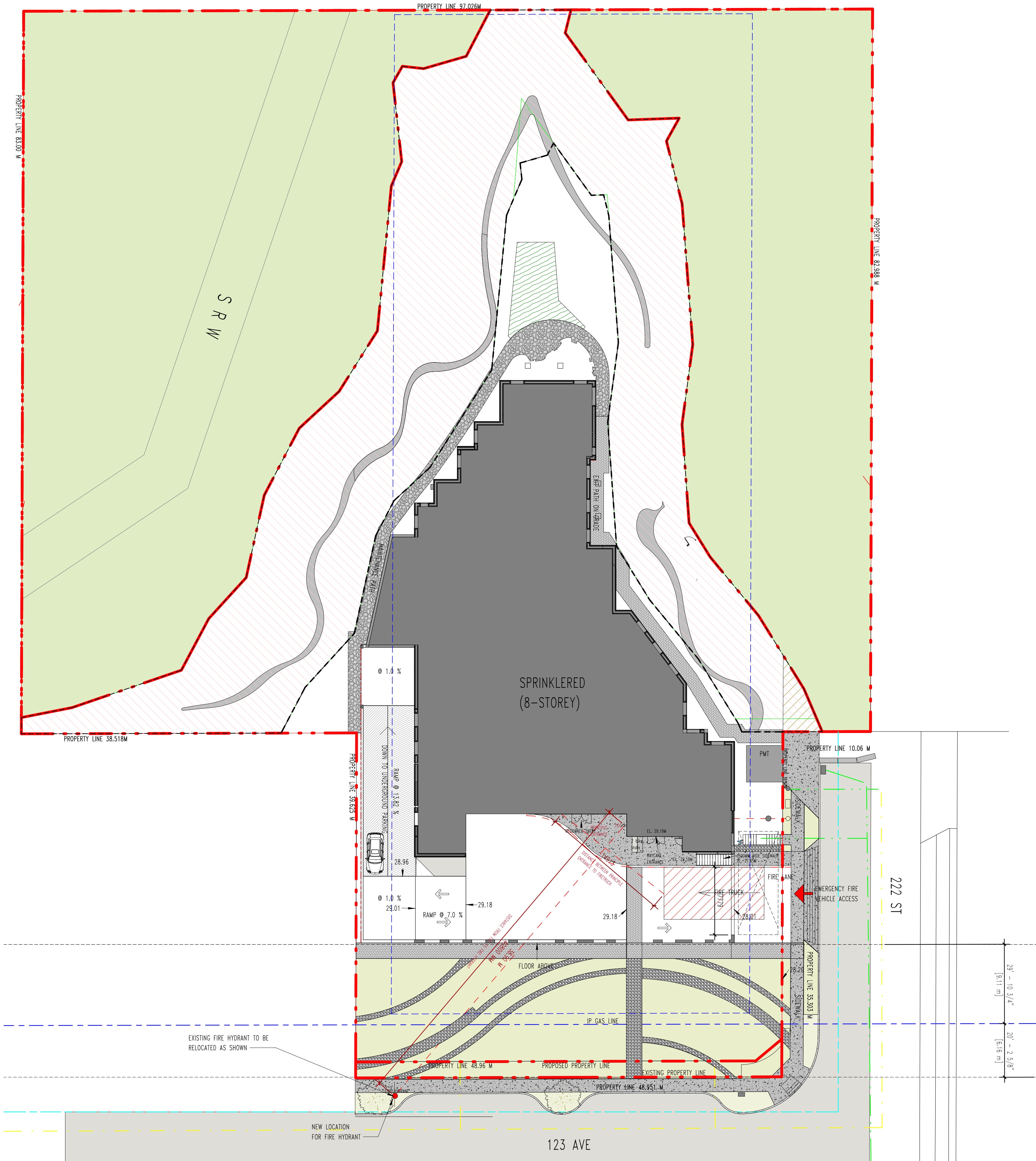
Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

SITE PLAN SHOWING  
BUILDING SETBACKS

REV No	10	Project No	2018-15	DP-100D
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14	14-06-2024	REISSUED FOR REZONING	JG
13	12-06-2024	REISSUED FOR REZONING	JG
12	04-06-2024	ISSUED FOR REZONING	JG
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9	22-04-2024	ISSUED FOR DP & ADP	JG
8	30-11-2023	REISSUED FOR DP	JG
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## START DATE

PROJECT No 2018-15

DR. SP

CH. JMM

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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

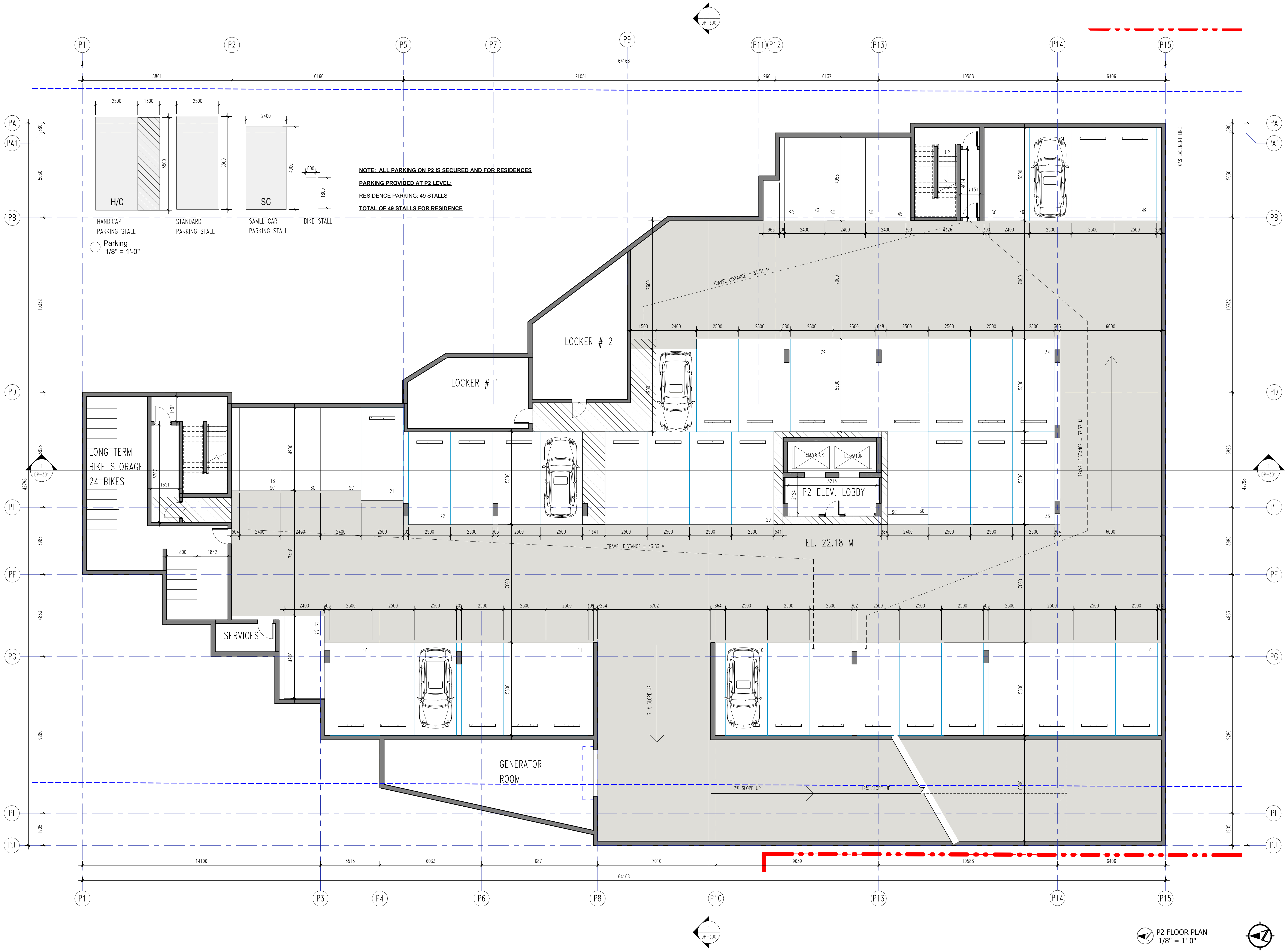
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## SITE PLAN - FIRE TRUCK ACCESS

REV No 16 Project No 2018-15 DP-100E

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16	27-06-2024	REISSUED FOR ADP REVIEW	JG
15	14-06-2024	REISSUED FOR REZONING	JG

START DATE  
PROJECT No 2018-15  
DR. SP  
CH. JMM  
SCALE 1/8" = 1'-0"



Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

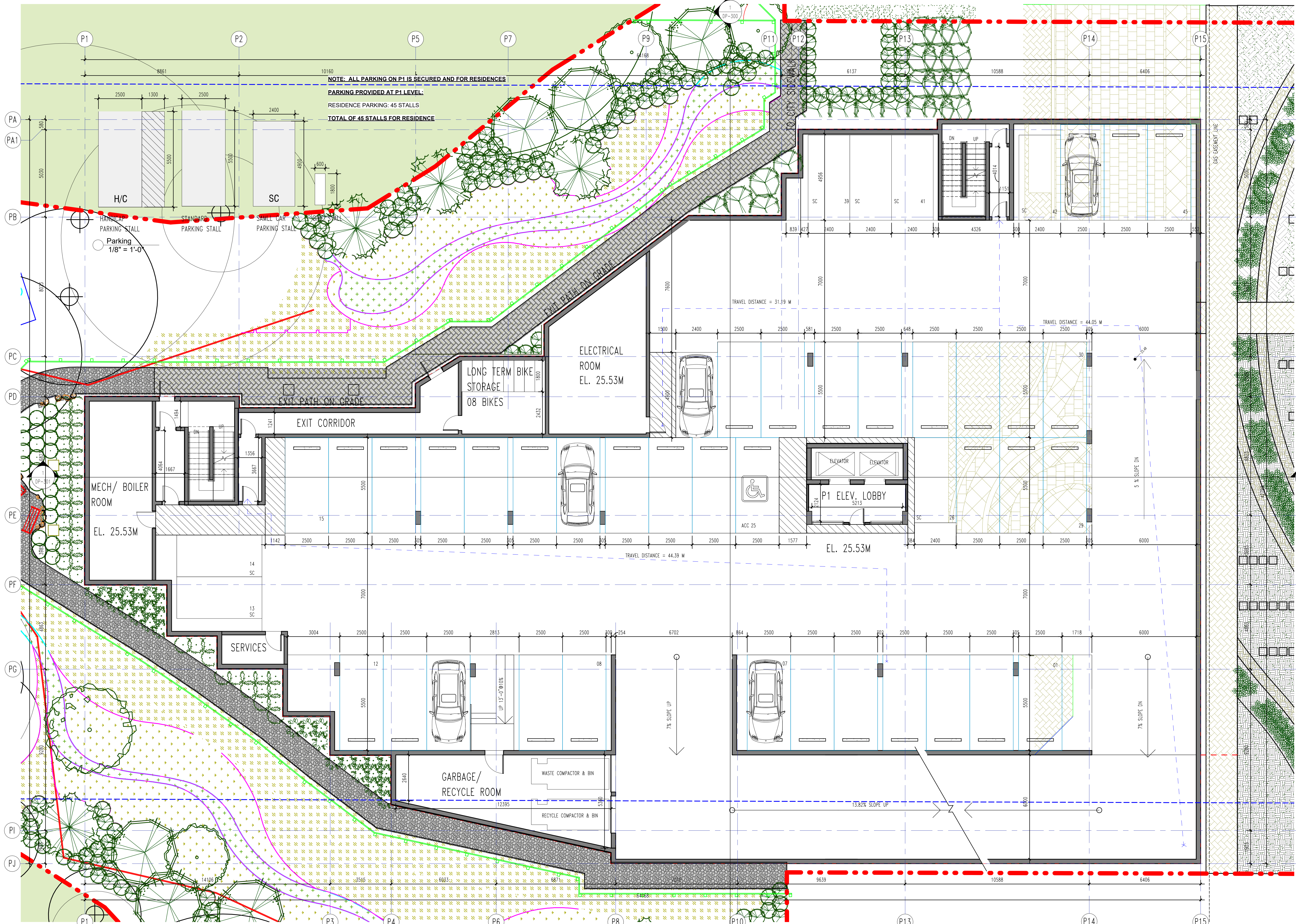
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P2 FLOOR PLAN

REV No	20	Project No	2018-15	DP-101
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1 P1 FLOOR PLAN  
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15	12-06-2024	REISSUED FOR REZONING	JG

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CH. JMM  
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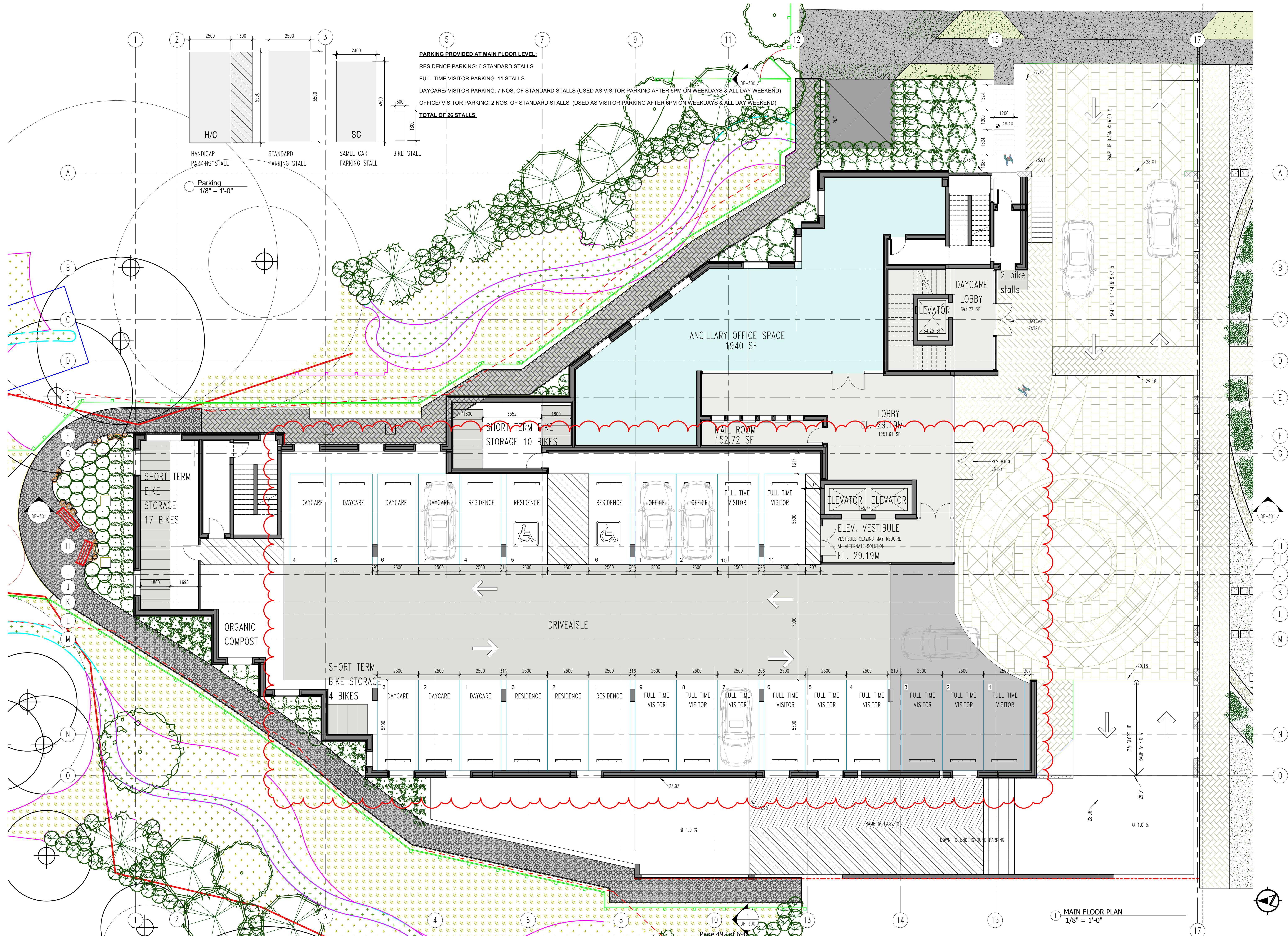
Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

**P1 FLOOR PLAN**

REV No	Project No	DP-102
21	2018-15	





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START DATE		2018-15
PROJECT No	2018-15	
DR.	SP	
CH.	JMM	
SCALE	1/8" = 1'-0"	



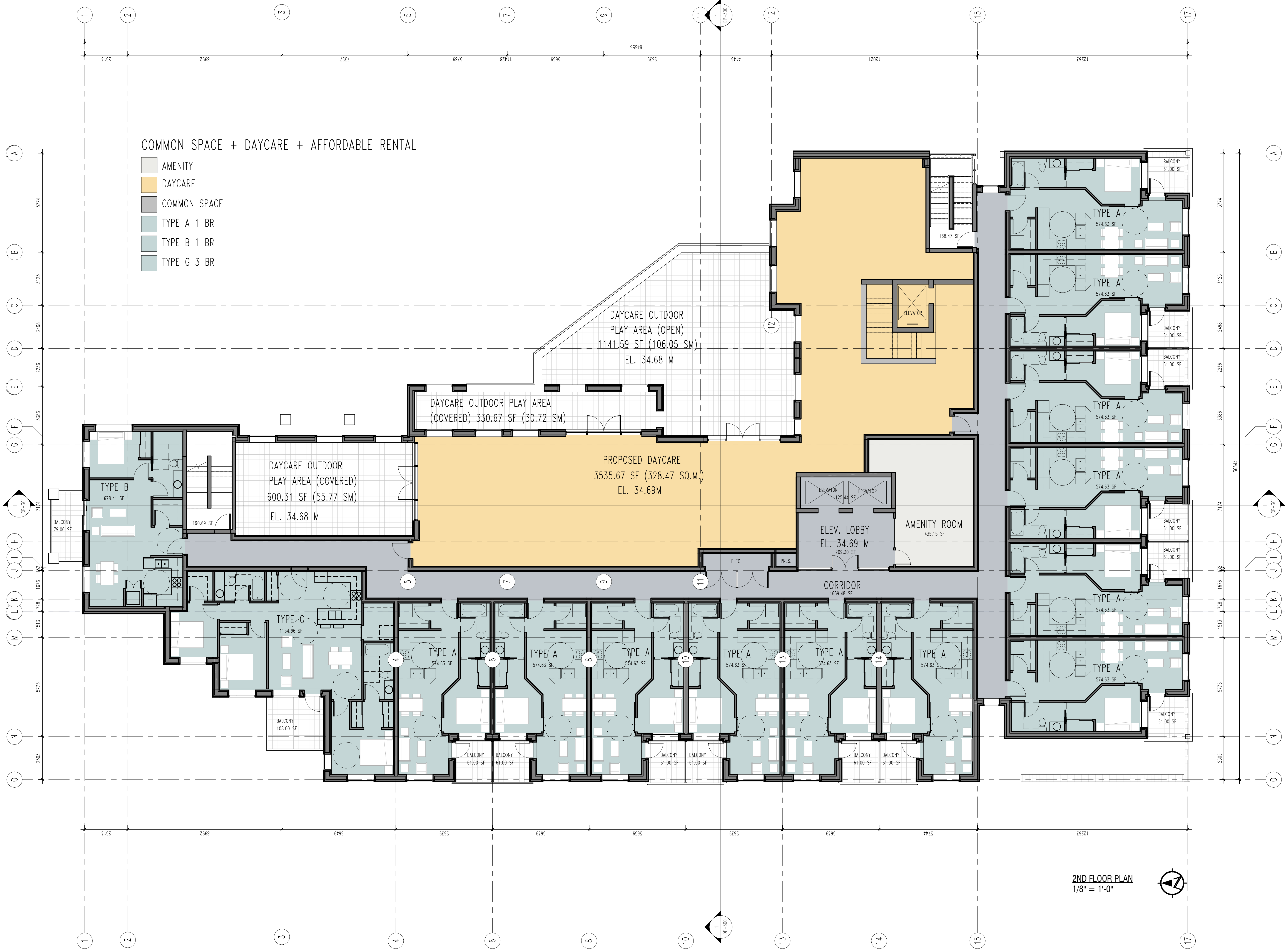
Project Name  
**222 MEADOWS WALK**  
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Client:  
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**MAIN FLOOR**

REV No	21	Project No	2018-15	DP-103
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8	15-02-2024	REISSUED FOR DP	JG
7	02-08-2023	REISSUED FOR DP	JG

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PROJECT No 2018-15  
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**222 MEADOWS WALK**  
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Client:  
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2nd FLOOR PLAN

REV No	16	Project No	2018-15	DP-104
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8	22-04-2024	ISSUED FOR DP & ADP	JG	
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REV	DATE	DESCRIPTION		BY

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DR.	SF
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CH.	JMM
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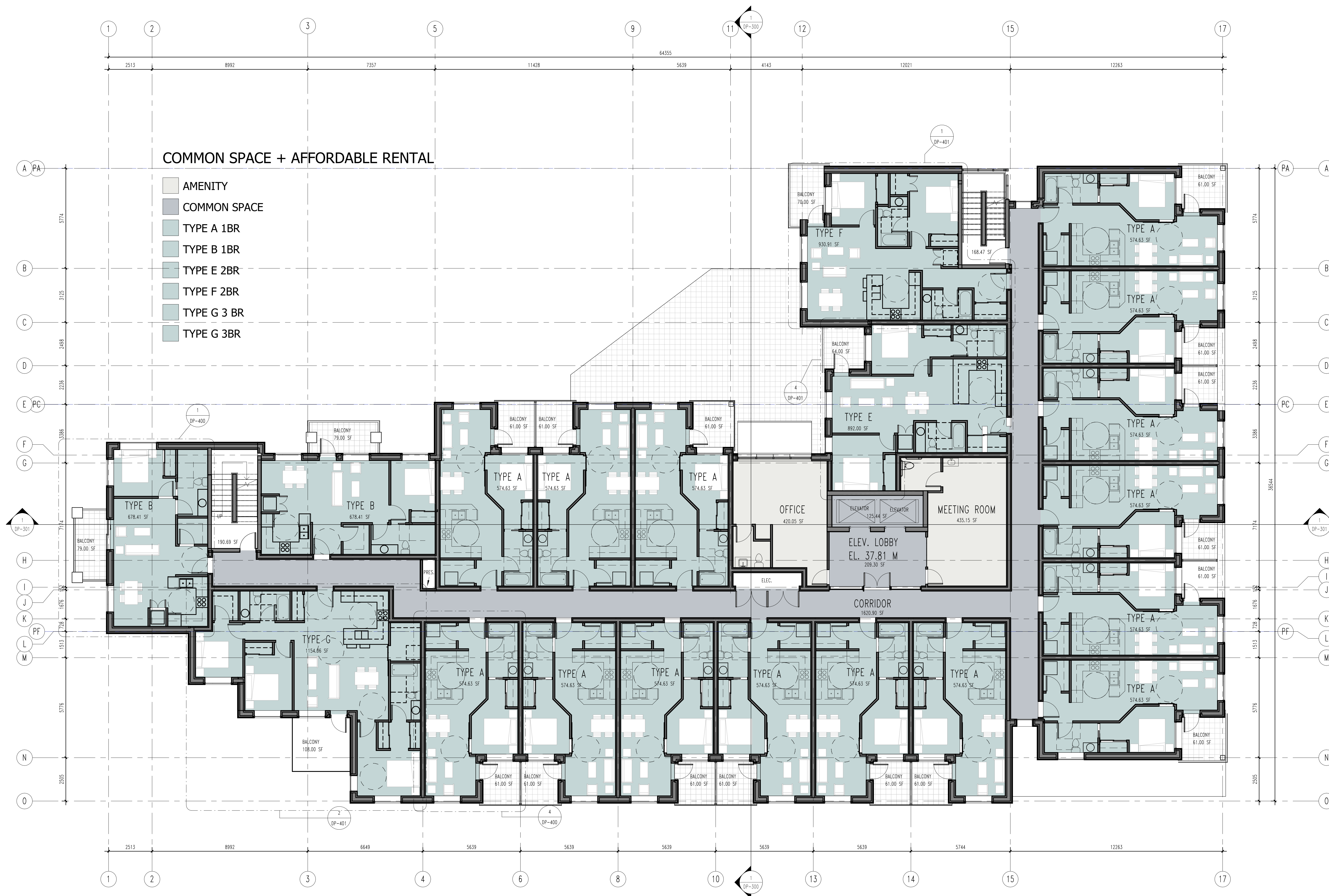
3RD FLOOR PLAN

3RD FLOOR PLAN

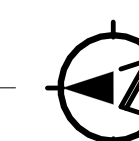
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REV No.	15	Project No.	2018-15	DP-105
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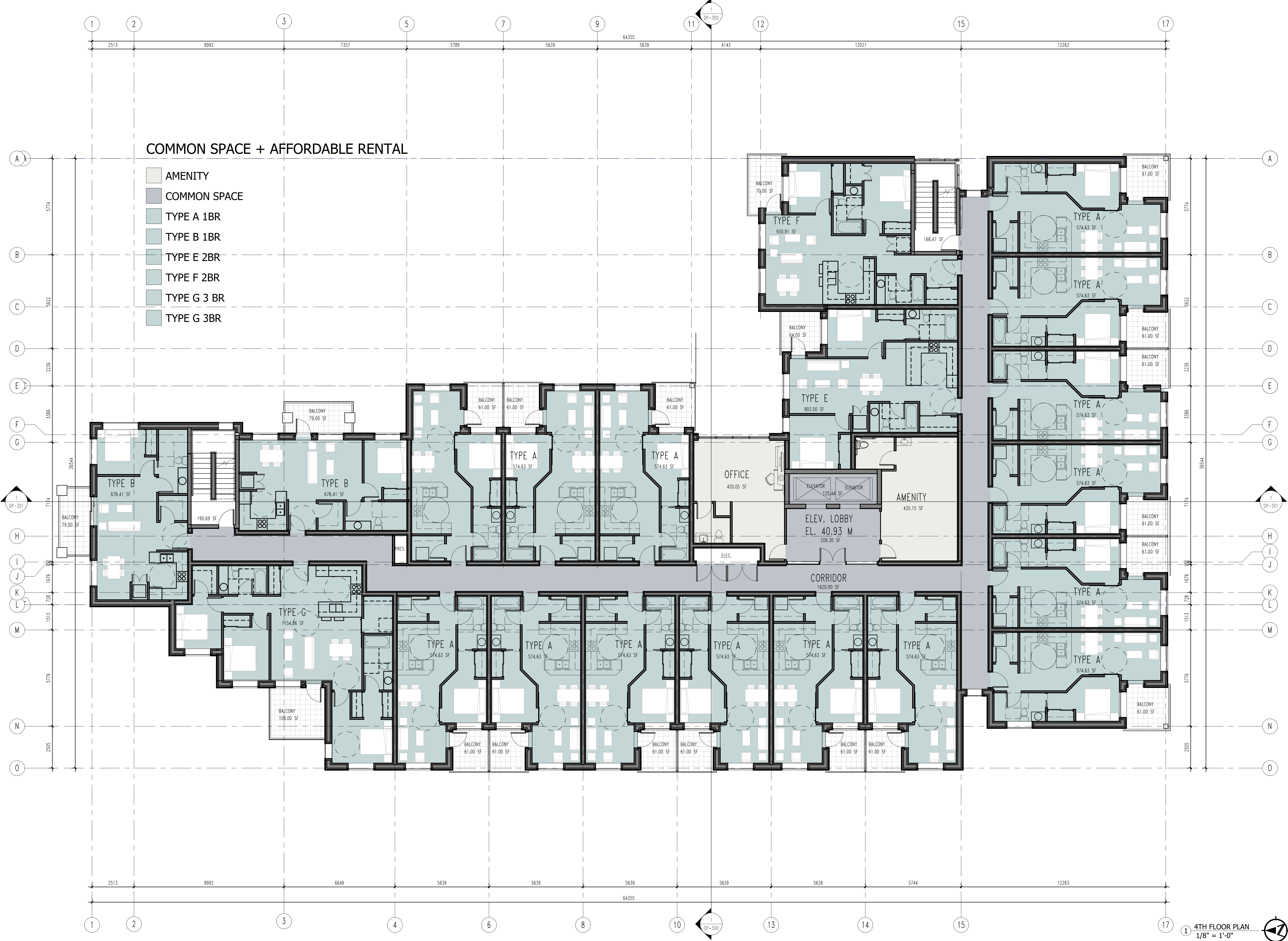
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① 3RD FLOOR PLAN  
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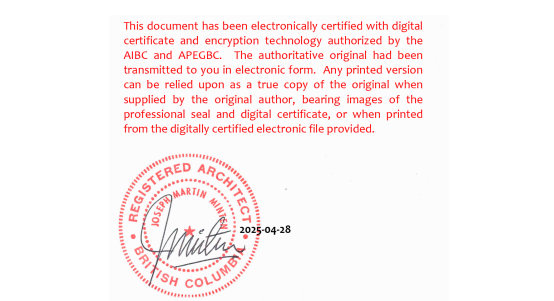
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START DATE  
PROJECT No 2018-15  
DR. SP  
CH. JMM  
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Architects Seal



Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
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4TH FLOOR PLAN

REV No	Project No	DP-106
16	2018-15	DP-106

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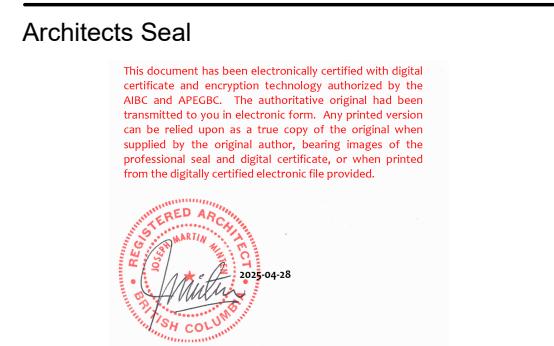
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7	17-02-2023	REISSUED FOR DP	JG

START DATE  
PROJECT No 2018-15  
DR. SP  
CH. JMM  
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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
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**5TH FLOOR PLAN**

REV No	Project No	DP-107
16	2018-15	DP-107

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START DATE	
PROJECT No	2018-15
DR.	SF
CH.	JMM
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Client:  
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6TH FLOOR PLAN

REV No.	16	Project No.	2018-15	DP-108
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REV	DATE	DESCRIPTION	BY

PROJECT No	2018-15
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7TH FLOOR PLAN

REV No.	16	Project No.	2018-15	DP-109
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6	20-03-2021	REISSUED FOR ADP	TJ
REV	DATE	DESCRIPTION	

PROJECT No	2018-15
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DR. \_\_\_\_\_ SE

CH	Chapman
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CH. \_\_\_\_\_ Checker \_\_\_\_\_

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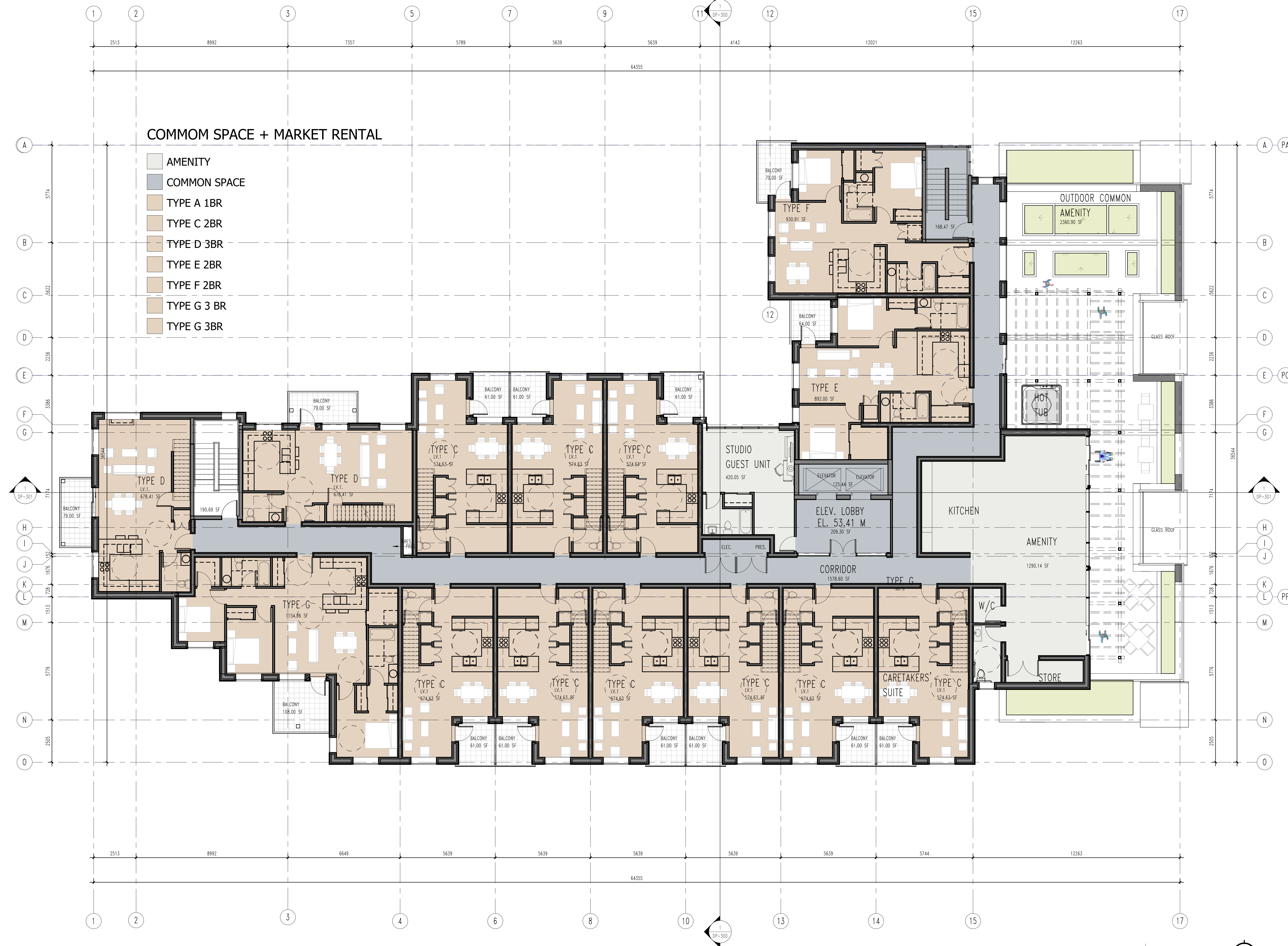


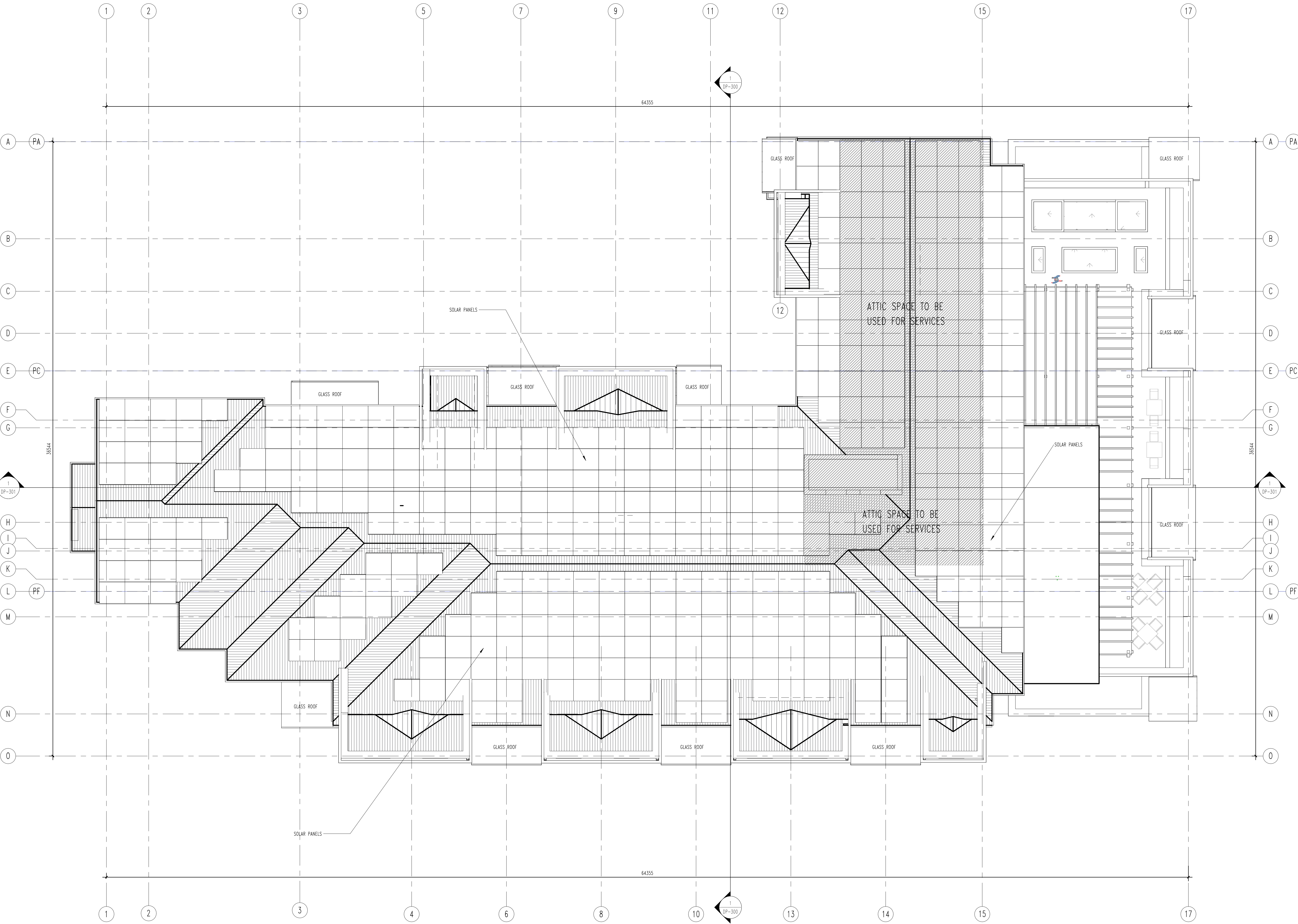
Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

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8TH FLOOR PLAN

REV No.	15	Project No.	2018-15	DP-110
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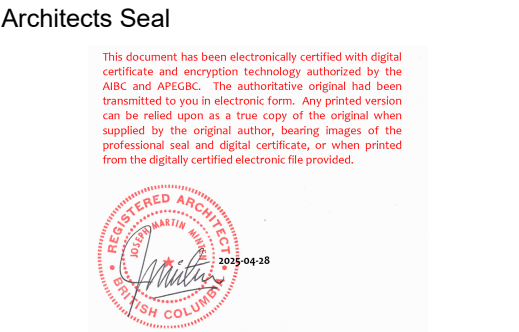
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11	12-06-2024	REISSUED FOR REZONING	JG
10	14-06-2024	ISSUED FOR REZONING	JG
9	29-05-2024	ISSUED FOR ADP REVIEW	JG
8	29-04-2024	ISSUED FOR DP	JG
7	22-04-2024	ISSUED FOR DP & ADP	JG
6	02-08-2023	REISSUED FOR DP	JG

START DATE	
PROJECT No	2018-15
DR.	SP
CH.	Checker
SCALE	1/8" = 1'-0"



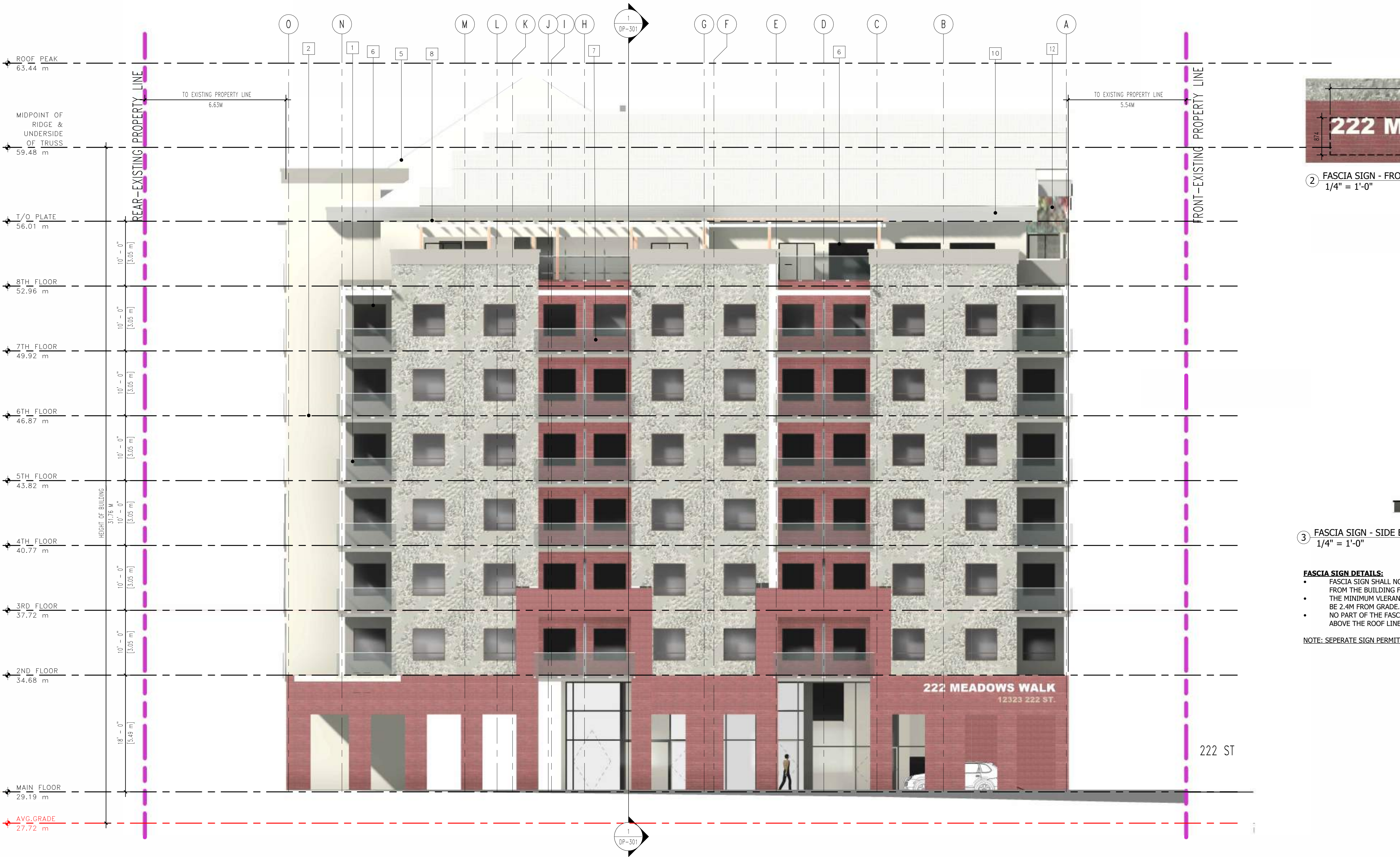
Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

**ROOF PLAN**

REV No	Project No	DP-111
14	2018-15	





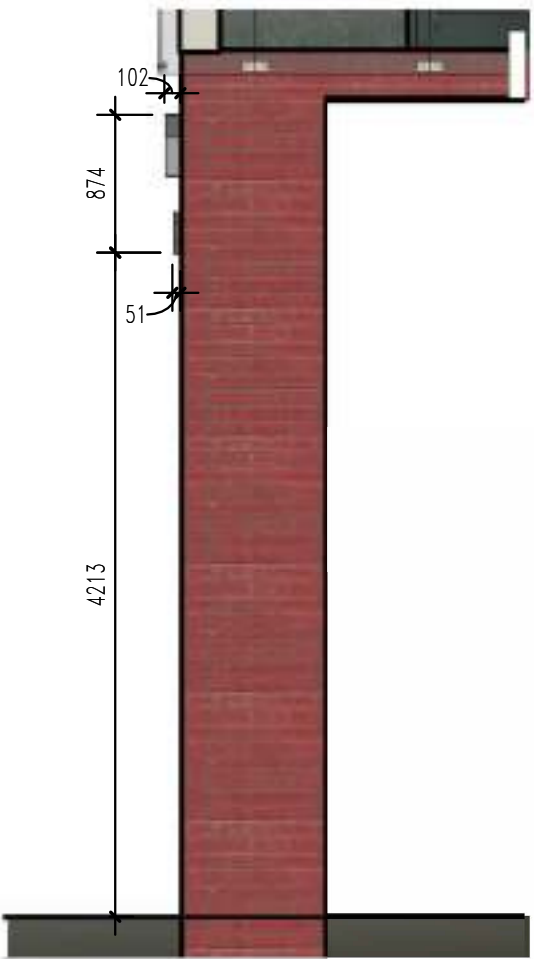
1 SOUTH ELEVATION  
1/8" = 1'-0"

### FINISH SCHEDULE

1 EXTERNAL CLADDING 1 ACTIVE MITREX SOLAR PANELS TO MATCH BENJAMIN MOORE COVENTRY GRAY HC-169	2 EXTERNAL CLADDING 2 ACTIVE MITREX SOLAR CLADDING TO MATCH BENJAMIN MOORE BACKGROUND - AMHERST GRAY - HC-167	3 EXETRNAL CLADDING 3 CLADIFY- BRICK ADMIRAL RED VELOUR	4 ROOF RIB STEEL ROOF PANEL CASCADIA PVDF - OLD ZINC GREY	5 SOLAR PANEL ON ROOF MITREX TO MATCH CASCADIA PVDF OLD ZINC GREY RIB STEEL PANEL	6 WINDOW / CURTAIN WALL FRAME ALUMINUM ANODIZED DARK GREY	7 ACTIVE MITREX SOLAR RAIL RAIL PANEL GLASS	8 SOFFIT / TRELLIS / BEAM CASCADIA PVDF ACORN	9 COLUMN CLADDING NON SOLAR CASCADIA STEEL - REGENT GREY	10 FACIA BOARD ALUMINUM PAINTED	11 EXTERNAL CLADDING 4 ACTIVE MITREX SOLAR CLADDING TO MATCH BENJAMIN MOORE BACKGROUND - AMHERST GRAY - HC-167 WITH LEAVES PATTERN	12 EXTERNAL CLADDING 4 ACTIVE MITREX SOLAR CLADDING WITH MURAL



2 FASCIA SIGN - FRONT ELEVATION  
1/4" = 1'-0"



3 FASCIA SIGN - SIDE ELEVATION  
1/4" = 1'-0"

#### FASCIA SIGN DETAILS:

- FASCIA SIGN SHALL NOT PROJECT BEYOND 300MM FROM THE BUILDING FACE.
- THE MINIMUM VLERANCE OF A FASCIA SIGN SHALL BE 2.4M FROM GRADE.
- NO PART OF THE FASCIA SIGN SHALL PROJECT ABOVE THE ROOF LINE

NOTE: SEPERATE SIGN PERMIT IS REQUIRED.

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15	04-06-2024	ISSUED FOR REZONING	JG
14	29-05-2024	REISSUED FOR ADP REVIEW	JG
13	29-04-2024	ISSUED FOR DP	JG
12	22-04-2024	ISSUED FOR DP & ADP	JG
11	15-02-2024	REISSUED FOR DP	JG
10	30-11-2023	REISSUED FOR DP	JG

#### START DATE

PROJECT No 2018-15

DR. JVS

CH. JMM

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#### Project Name

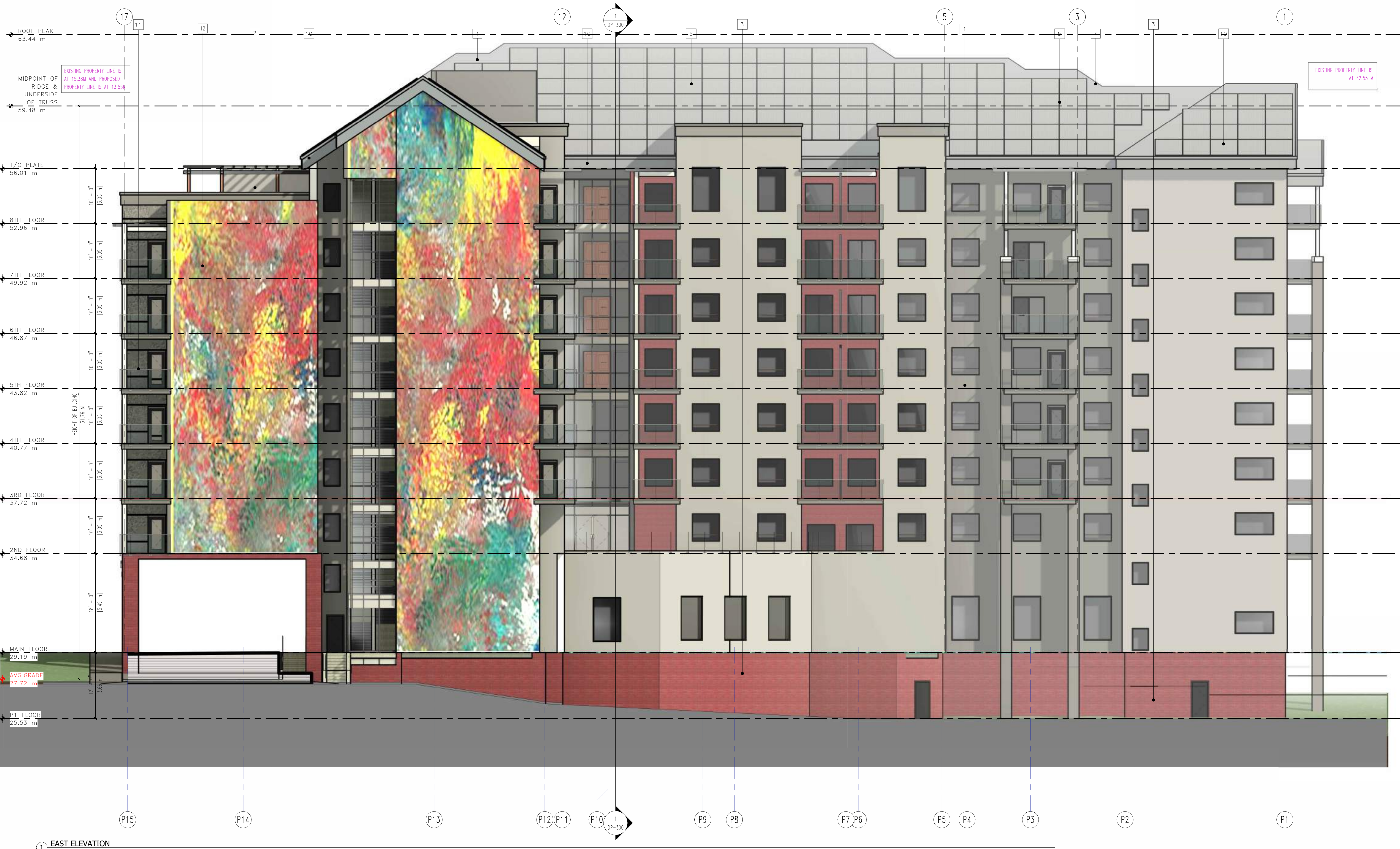
222 MEADOWS WALK  
MAPLE RIDGE, BC

Client: SIMPLEX MEADOWS DEVELOPMENT CORP.

#### SOUTH ELEVATION

REV No 19	Project No 2018-15	DP-200
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1 EAST ELEVATION  
1/8" = 1'-0"

FINISH SCHEDULE

1	2	3	4	5	6	7	8	9	10	11	12
EXTERNAL CLADDING 1 ACTIVE MITREX SOLAR PANELS TO MATCH BENJAMIN MOORE COVENTRY GRAY HC-169	EXTERNAL CLADDING 2 ACTIVE MITREX SOLAR CLADDING TO MATCH BENJAMIN MOORE BACKGROUND - AMHERST GRAY - HC-167	EXTERNAL CLADDING 3 CLADIFY BRICK ADMIRAL RED VELOUR	ROOF RIB STEEL ROOF PANEL CASCADIA PVDF - OLD ZINC GREY	SOLAR PANEL ON ROOF MITREX TO MATCH CASCADIA PVDF OLD ZINC GREY RIB STEEL PANEL	WINDOW / CURTAIN WALL FRAME ALUMINUM ANODIZED DARK GREY	ACTIVE MITREX SOLAR RAIL RAIL PANEL GLASS	SOFFIT / TRELLIS / BEAM CASCADIA PVDF ACORN	COLUMN CLADDING NON SOLAR CASCADIA STEEL - REGENT GREY	FACIA BOARD ALUMINUM PAINTED	EXTERNAL CLADDING 4 ACTIVE MITREX SOLAR CLADDING TO MATCH BENJAMIN MOORE BACKGROUND - AMHERST GRAY - HC-167 WITH LEAVES PATTERN	EXTERNAL CLADDING 4 ACTIVE MITREX SOLAR CLADDING WITH MURAL

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10	22-04-2024	ISSUED FOR DP & ADP	JG
9	15-02-2024	REISSUED FOR DP	JG
8	02-08-2023	REISSUED FOR DP	JG

START DATE	
PROJECT No	2018-15
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CH.	JMM
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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

EAST ELEVATION

REV No	17	Project No	2018-15	DP-201
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1 WEST ELEVATION  
1/8" = 1'-0"

### FINISH SCHEDULE

1	2	3	4	5	6	7	8	9	10	11	12
EXTERNAL CLADDING 1 ACTIVE MITREX SOLAR PANELS TO MATCH BENJAMIN MOORE COVENTRY GRAY HC-169	EXTERNAL CLADDING 2 ACTIVE MITREX SOLAR CLADDING TO MATCH BENJAMIN MOORE BACKGROUND - AMHERST GRAY - HC-167	EXTERNAL CLADDING 3 CLADIFY - BRICK ADMIRAL RED VELOUR	ROOF RIB STEEL ROOF PANEL CASCADIA PVDF - OLD ZINC GREY	SOLAR PANEL ON ROOF MITREX TO MATCH CASCADIA PVDF OLD ZINC GREY RIB STEEL PANEL	WINDOW / CURTAIN WALL FRAME ALUMINUM ANODIZED DARK GREY	ACTIVE MITREX SOLAR RAIL RAIL PANEL GLASS	SOFFIT / TRELLIS / BEAM CASCADIA PVDF ACORN	COLUMN CLADDING NON SOLAR CASCADIA STEEL - REGENT GREY	FACIA BOARD ALUMINUM PAINTED	EXTERNAL CLADDING 4 ACTIVE MITREX SOLAR CLADDING TO MATCH BENJAMIN MOORE BACKGROUND - AMHERST GRAY - HC-167 WITH LEAVES PATTERN	EXTERNAL CLADDING 4 ACTIVE MITREX SOLAR CLADDING WITH MURAL

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11	22-04-2024	ISSUED FOR DP & ADP	JG
10	15-02-2024	REISSUED FOR DP	JG

START DATE	
PROJECT No	2018-15
DR.	Author
CH.	Checker
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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

### WEST ELEVATION

REV	19	Project No 2018-15	DP-202
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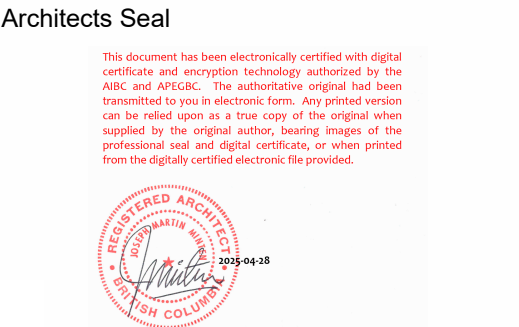
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8	15-02-2023	REISSUED FOR DP	JG

START DATE	
PROJECT No	2018-15
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Project Name  
**222 MEADOWS WALK**  
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Client:  
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**NORTH ELEVATION**

REV No	16	Project No	2018-15	DP-203
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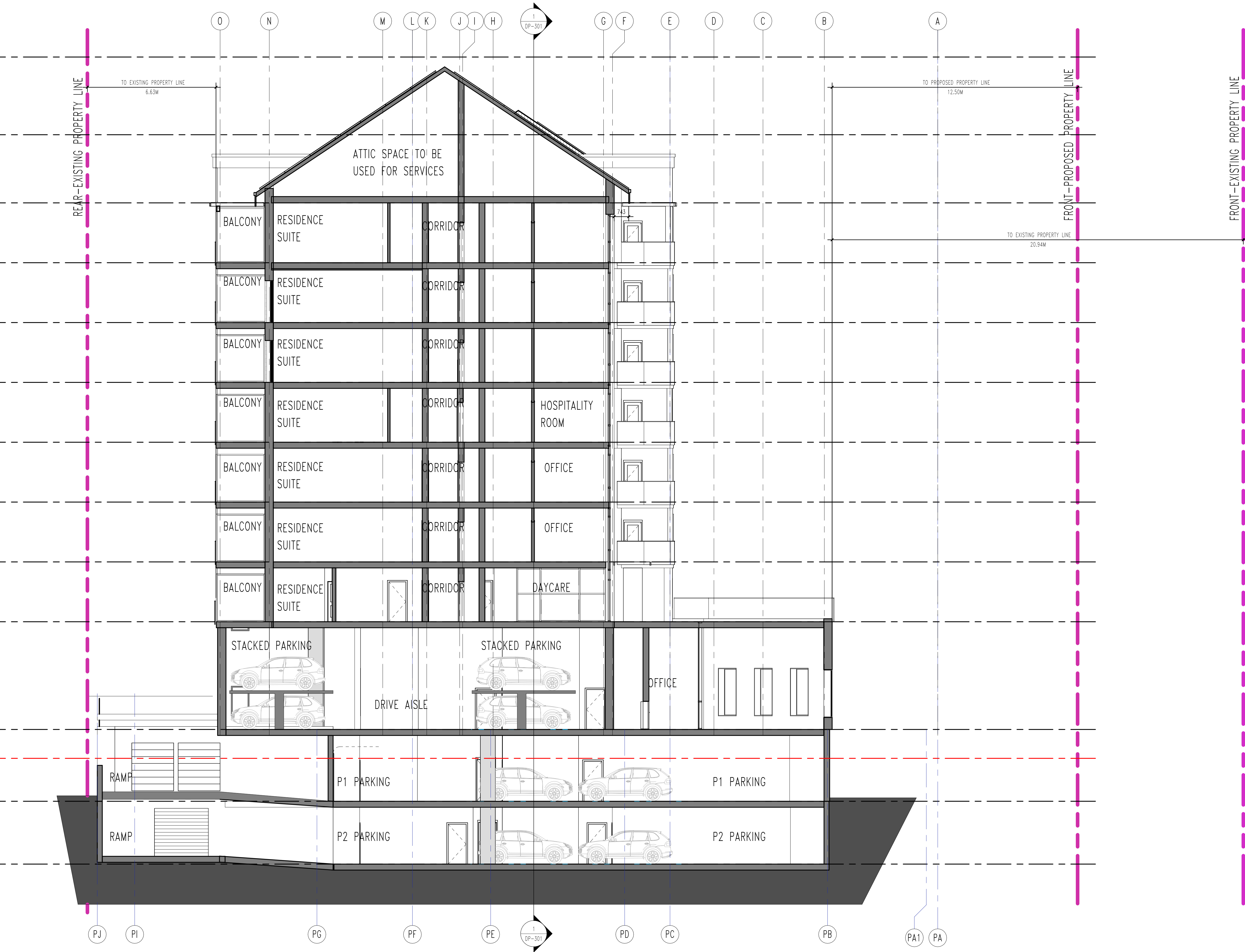
4/28/2025 3:38:19 PM

1 NORTH ELEVATION  
1/8" = 1'-0"

**FINISH SCHEDULE**

1	2	3	4	5	6	7	8	9	10	11	12
EXTERNAL CLADDING 1 ACTIVE MITREX SOLAR PANELS TO MATCH BEHAVIUM MOORE COVENTRY GRAY HC-169	EXTERNAL CLADDING 2 ACTIVE MITREX SOLAR CLADDING TO MATCH BEHAVIUM MOORE BACKGROUND - AMHERST GRAY - HC-167	EXTERNAL CLADDING 3 CLADIFY-BRICK ADMIRAL RED VELOUR	ROOF RIB STEEL ROOF PANEL CASCADIA PVOF - OLD ZINC GREY	SOLAR PANEL ON ROOF MITREX TO MATCH CASCADIA PVOF OLD ZINC GREY RIB STEEL PANEL	WINDOW / CURTAIN WALL FRAME ALUMINUM ANODIZED DARK GREY	ACTIVE MITREX SOLAR RAIL RAIL PANEL GLASS	SOFFIT / TRELLIS / BEAM CASCADIA PVOF ACORN	COLUMN CLADDING NON SOLAR CASCADIA STEEL - REGENT GREY	FACIA BOARD ALUMINUM PAINTED	EXTERNAL CLADDING 4 ACTIVE MITREX SOLAR CLADDING TO MATCH BEHAVIUM MOORE BACKGROUND - AMHERST GRAY - HC-167 WITH LEAVES PATTERN	EXTERNAL CLADDING 4 ACTIVE MITREX SOLAR CLADDING WITH MURAL





1 SECTION AA  
1/8" = 1'-0"

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9	15-02-2024	REISSUED FOR DP	JG

START DATE	
PROJECT No	2018-15
DR.	JVS
CH.	JMM
SCALE	1/8" = 1'-0"

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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

**BUILDING SECTION A**

— A

REV No	15	Project No	2018-15	DP-300
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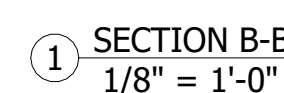


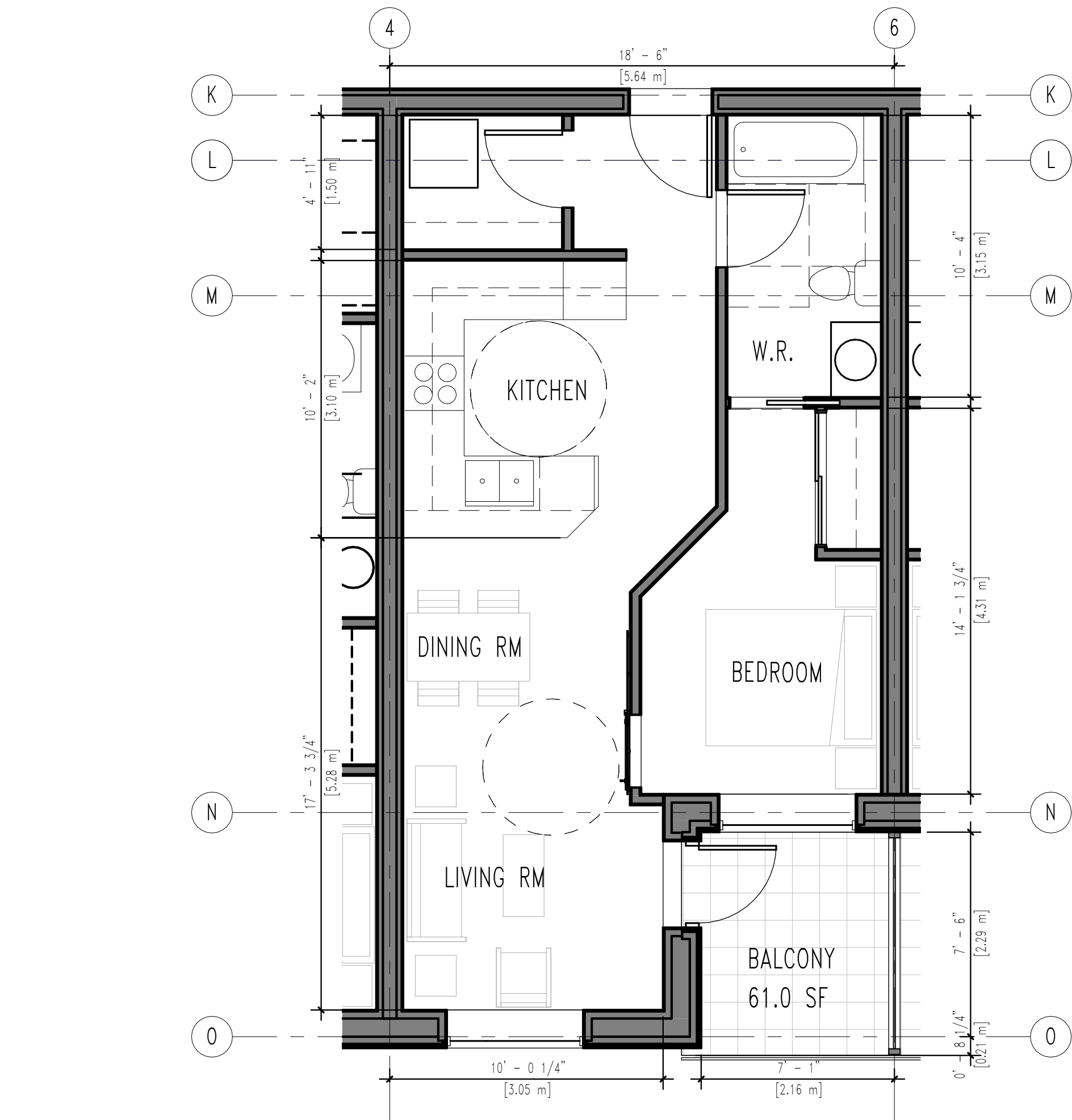
START DATE	
PROJECT No	2018-15
DR.	Author
CH.	Checker
SCALE	1/8" = 1'-0"
Architects Seal	

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

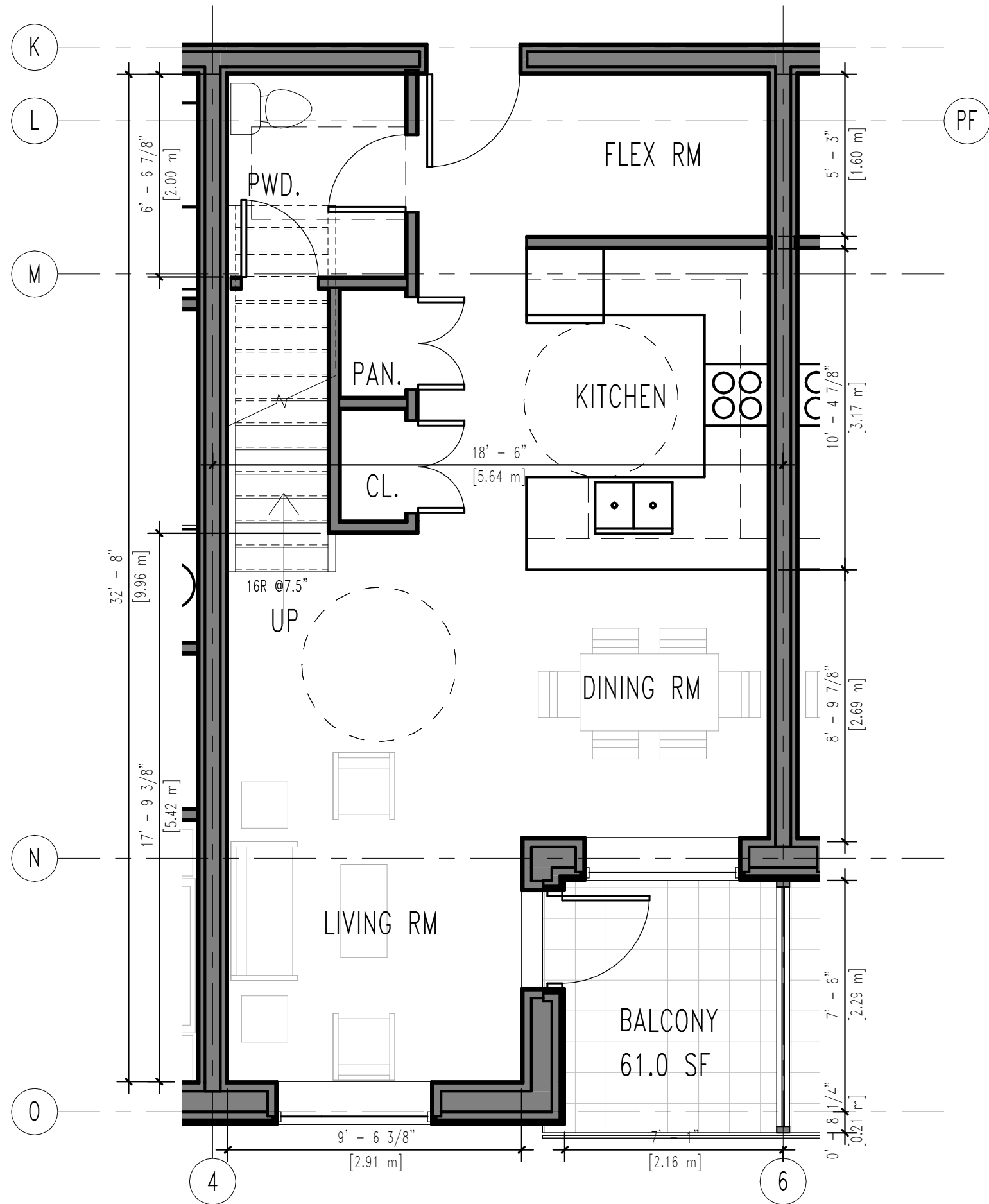
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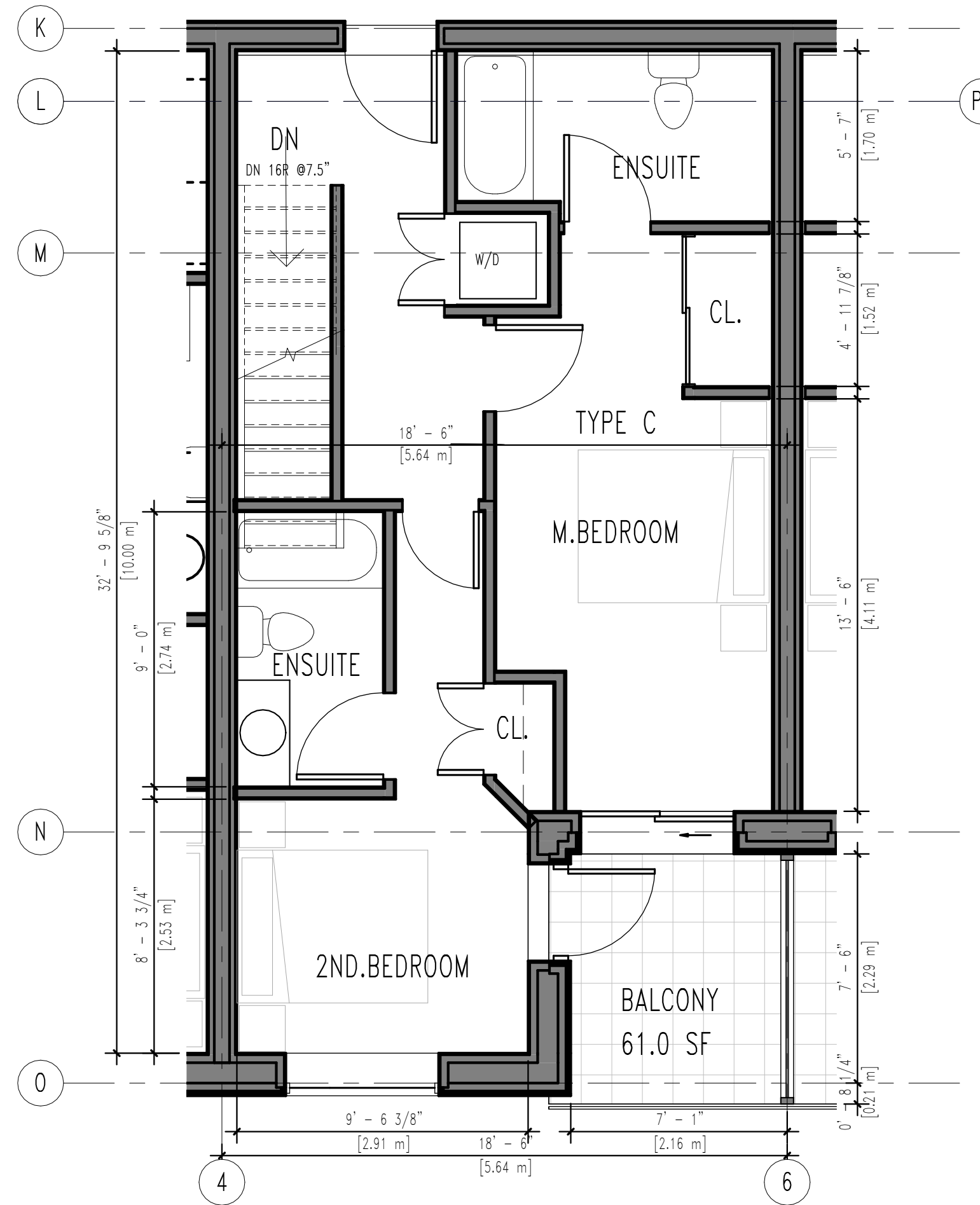




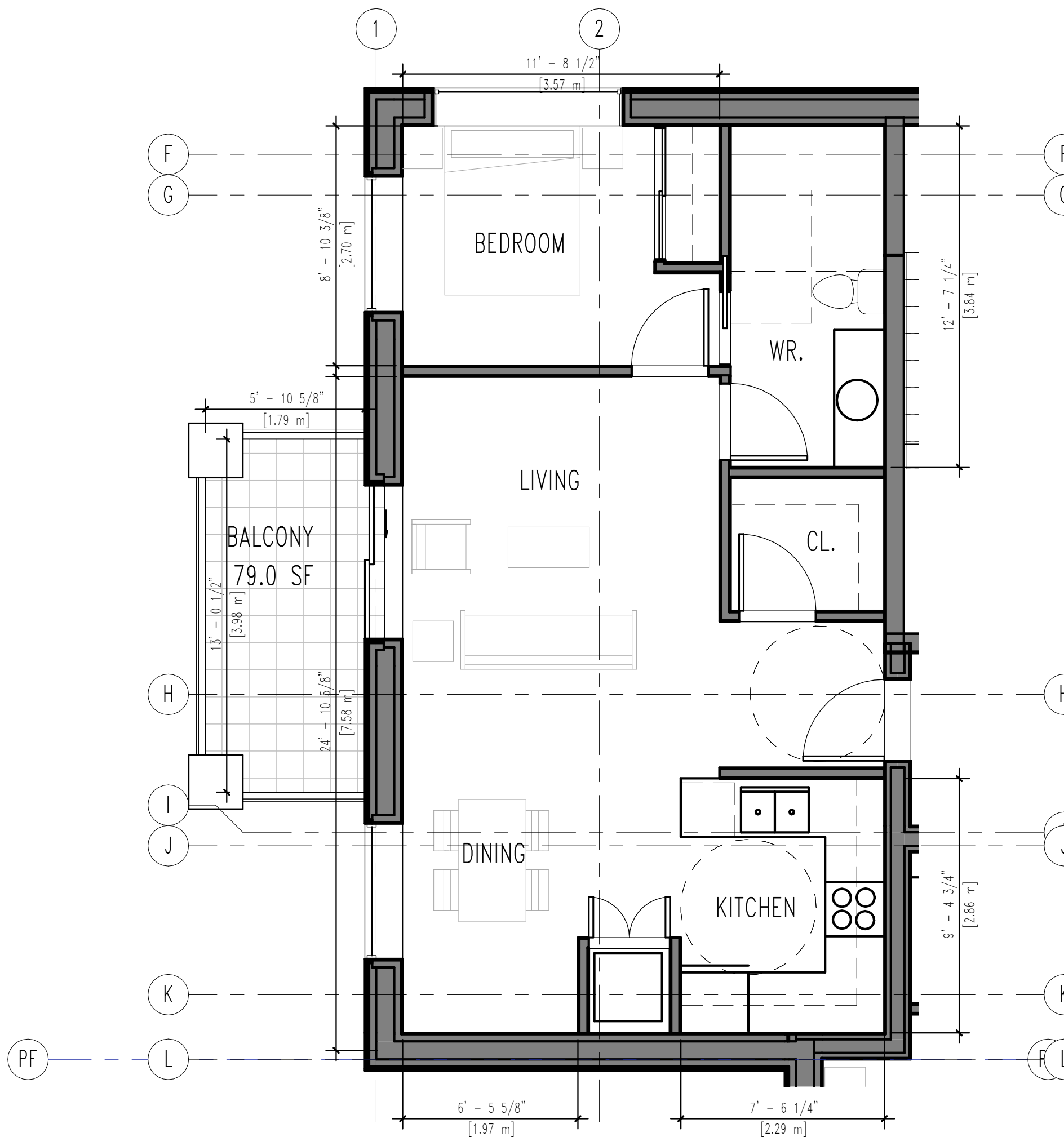
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1/4" = 1'-0"



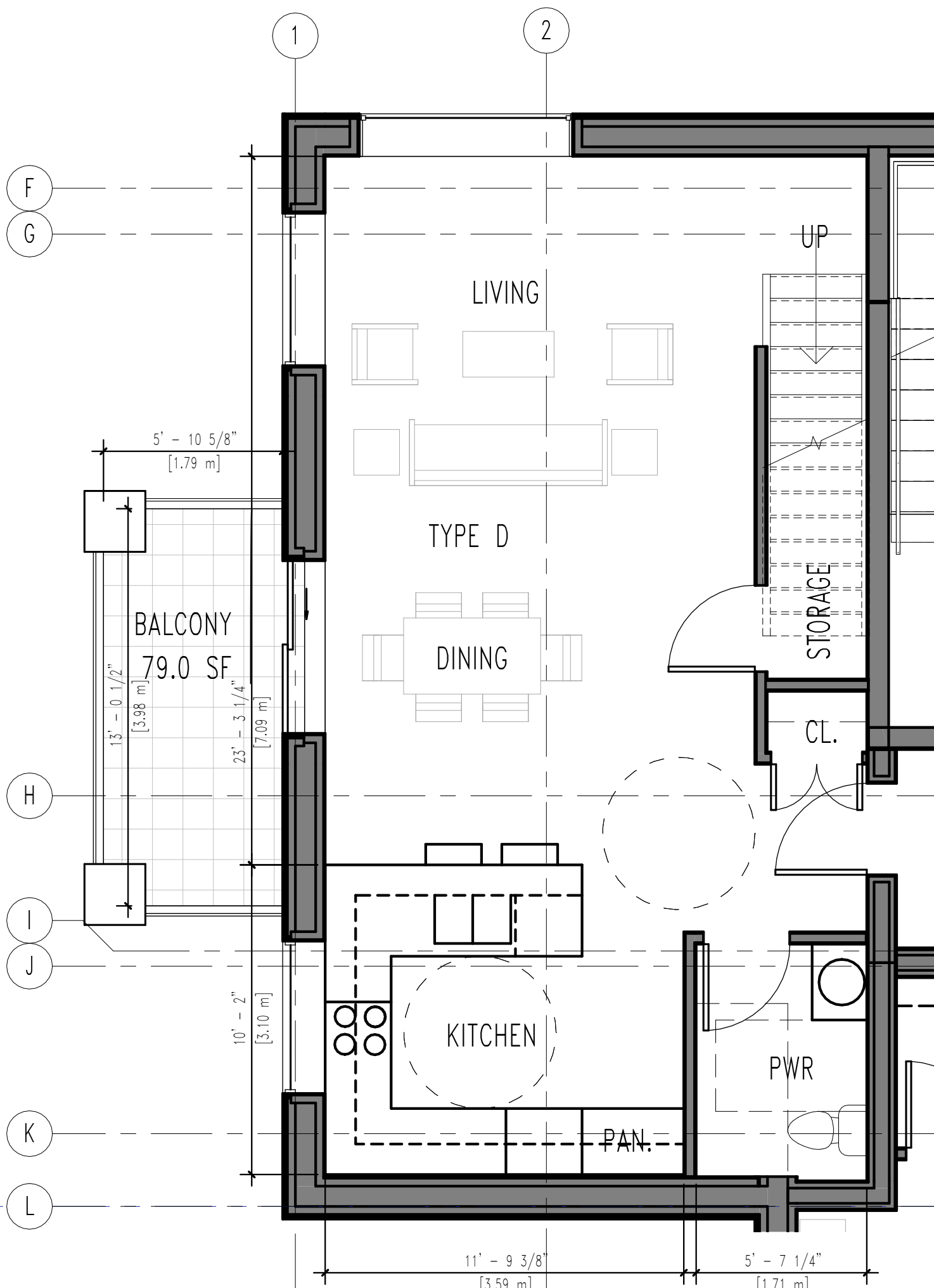
2 TYPE C - 2 BEDROOM - LOWER FLOOR 1127.61SF  
1/4" = 1'-0"



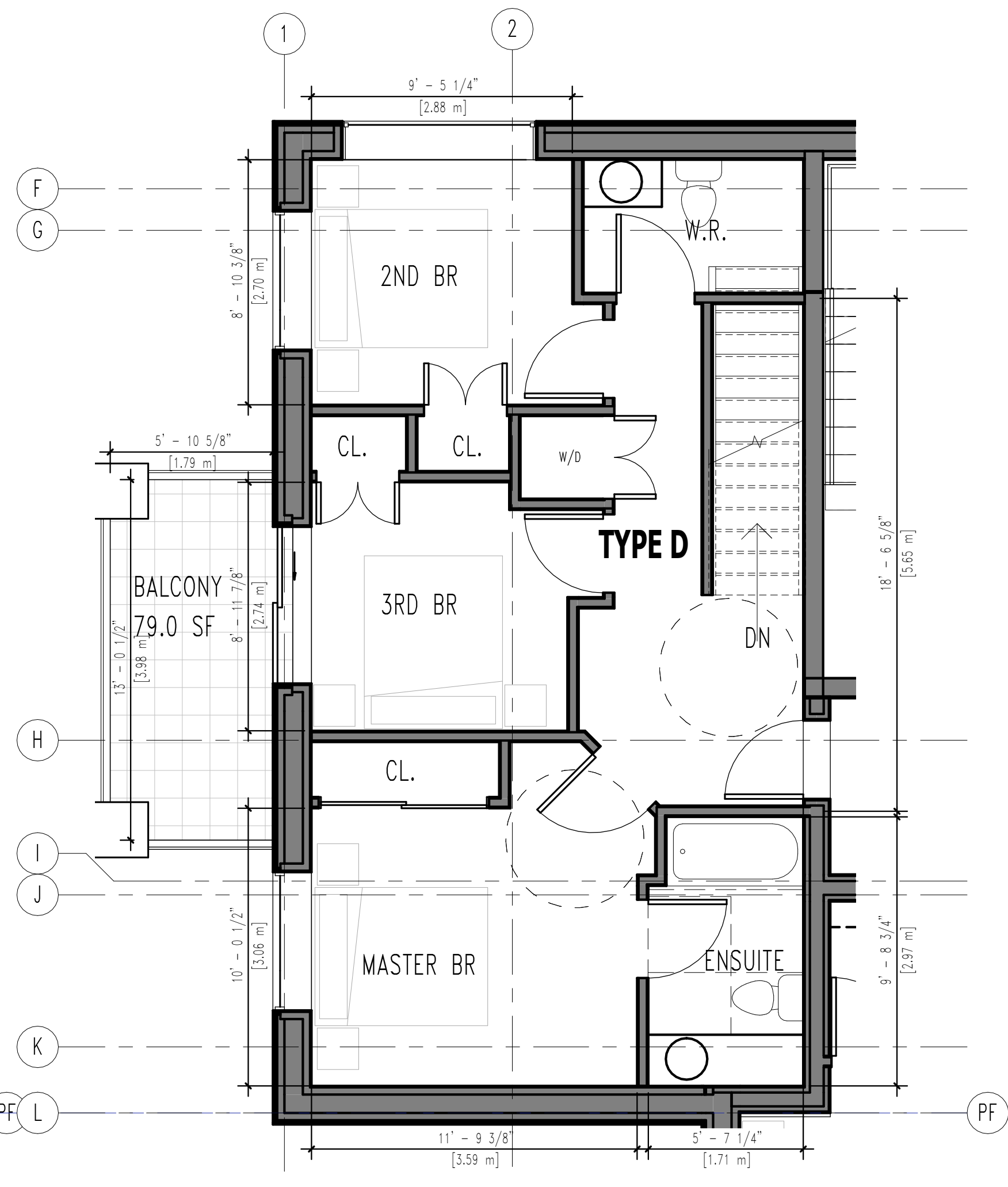
3 TYPE C - 2 BEDROOM - UPPER FLOOR  
1/4" = 1'-0"



1 TYPE B - 1 BEDROOM 696.16SF  
1/4" = 1'-0"



5 TYPE D - 3 BEDROOM LOWER FLOOR 1363.95 SF  
1/4" = 1'-0"



6 TYPE D - 3 BEDROOM UPPER FLOOR  
1/4" = 1'-0"

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6	22-04-2024	ISSUED FOR DP & ADP	JG
5	17-02-2023	REISSUED FOR DP	JG
4	20-08-2021	REISSUED FOR ADP	TJ

START DATE  
PROJECT No 2018-15  
DR. SP  
CH. JMM  
SCALE 1/4" = 1'-0"



Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

ENLARGED UNIT PLANS

REV No 13 Project No 2018-15 DP-400



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10	12-06-2024	REISSUED FOR REZONING	JG
9	04-06-2024	ISSUED FOR REZONING	JG
8	29-05-2024	REISSUED FOR ADP REVIEW	JG
7	29-04-2024	REISSUED FOR DP	JG
6	22-04-2024	ISSUED FOR DP & ADP	JG
5	17-02-2023	REISSUED FOR DP	JG
4	20-08-2021	REISSUED FOR ADP	TJ

START DATE	
PROJECT No	2018-15
DR.	SP
CH.	JMM
SCALE	1/4" = 1'-0"

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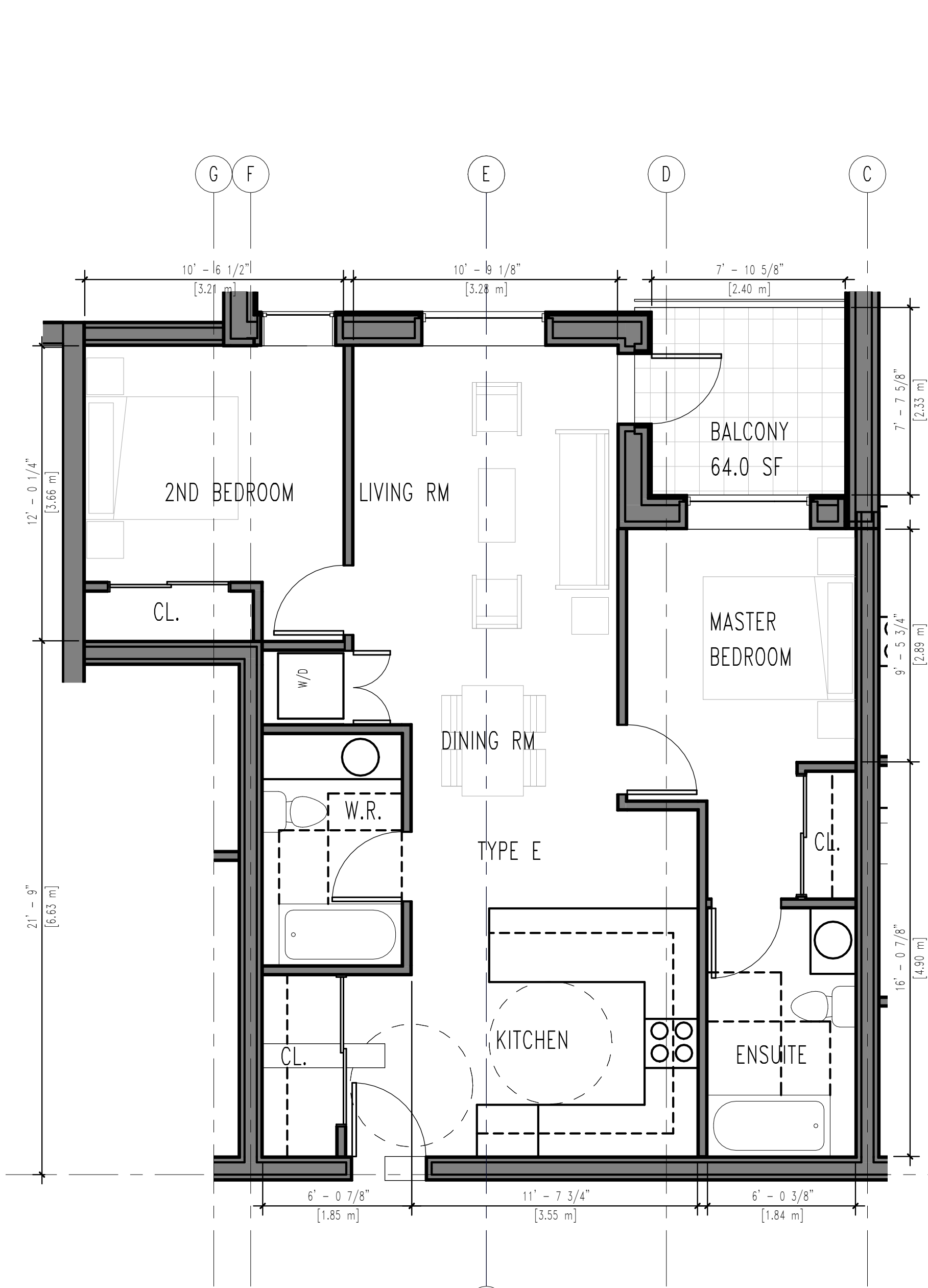
Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

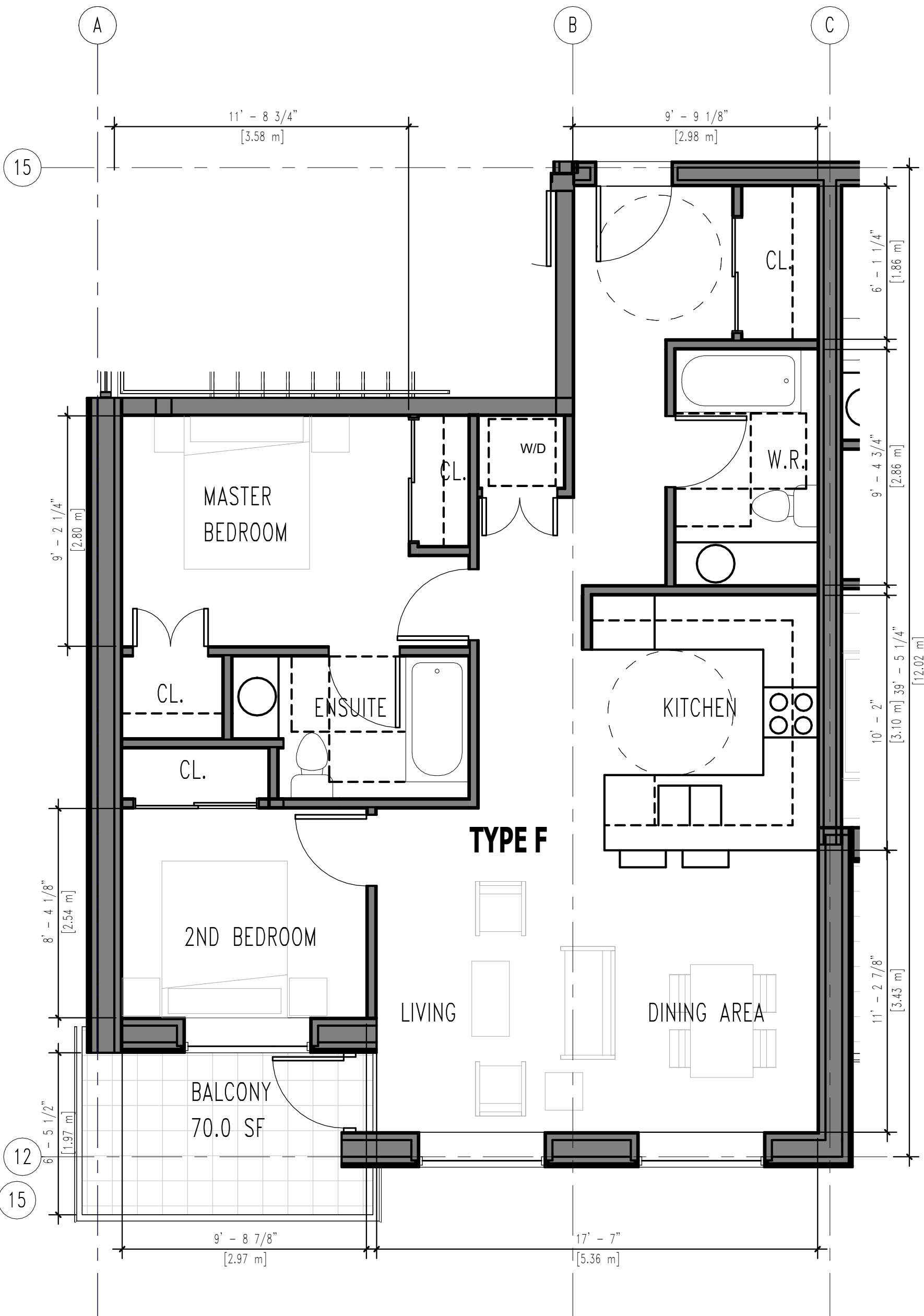
ENLARGED UNIT PLANS

REV No	13	Project No	2018-15	DP-401
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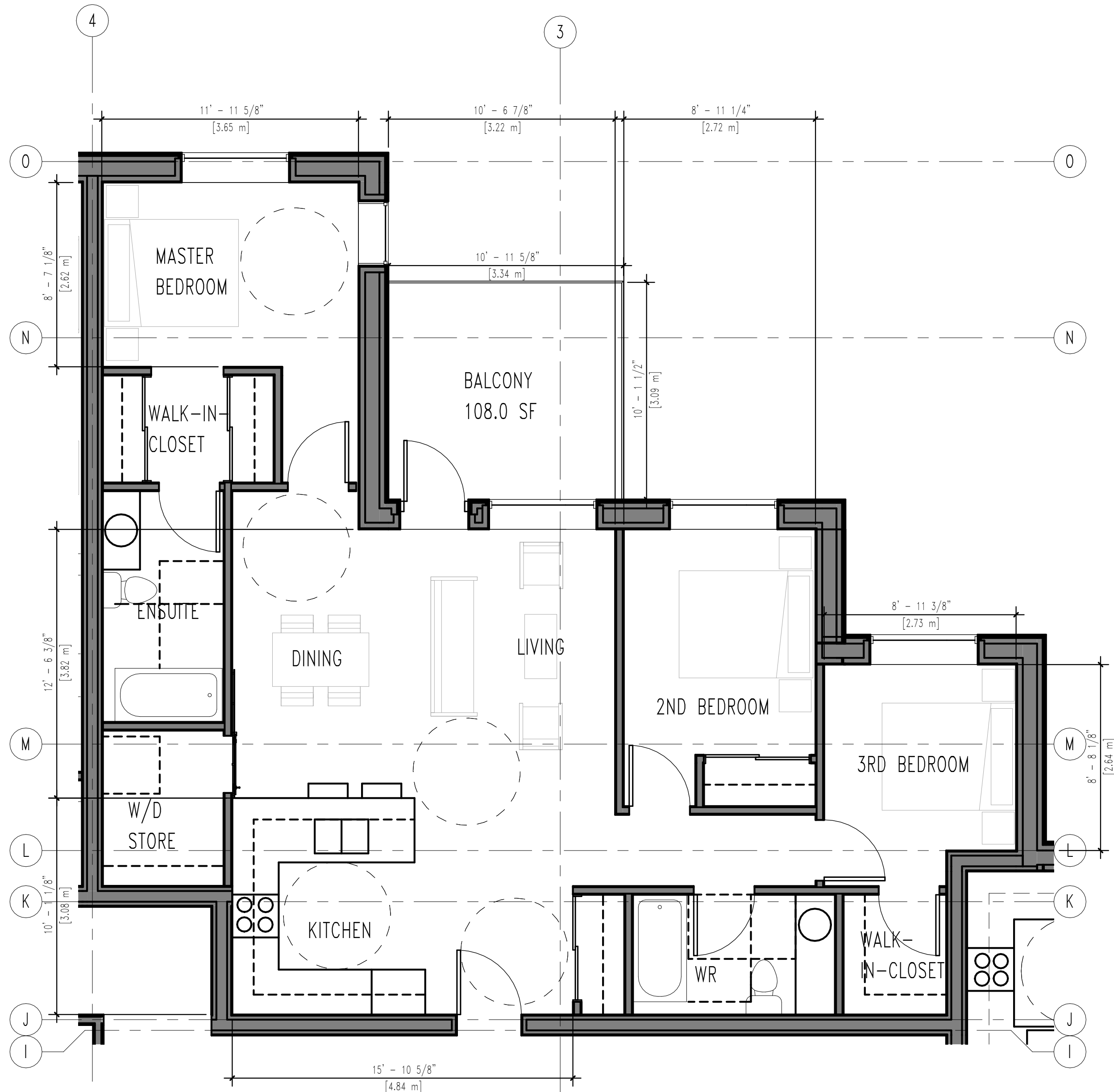
4/28/2025 3:38:29 PM



④ TYPE E - 2 BEDROOM 903.17SF  
1/4" = 1'-0"



① TYPE F 2 BEDROOM 944.60 SF  
1/4" = 1'-0"



② TYPE G - 3 BEDROOM 1173.84SF  
1/4" = 1'-0"

UNIT MATRIX					
UNIT TYPE	UNIT AREA	OUTDOOR AMENITY	# OF UNITS	TOTAL PRIVATE OUTDOOR AMENITY AREA	
TYPE A	1 BEDROOM	574.63 SF	61.00 SF	60	3660 SF
TYPE B	1 BEDROOM	678.41 SF	79.00 SF	5	395 SF
TYPE C	2 BEDROOM	1149.26 SF	61.00 SF	18	1098 SF
TYPE D	3 BEDROOM	1356.82 SF	79.00 SF	4	316 SF
TYPE E	2 BEDROOM	892.00 SF	64.00 SF	6	384 SF
TYPE F	2 BEDROOM	930.91 SF	70.00 SF	6	420 SF
TYPE G	3 BEDROOM	1154.06 SF	108.00 SF	7	756 SF
TOTAL			106		7029 SF (653.01 'SO.M.)





WEST SIDE VIEW OF THE BUILDING



BIRD'S EYE VIEW OF THE DEVELOPMENT



NORTH-WEST SIDE VIEW OF THE BUILDING



SOUTH-EAST VIEW OF THE BUILDING



CLOSE UP VIEW OF THE SOLAR CLADDING



VIEW OF THE ROOF-TOP AMENITY



VIEW OF THE CHILDREN PLAY AREA FOR THE DAYCARE

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REV	DATE	DESCRIPTION	BY
15	28-04-2025	REISSUED FOR DP	JKA
14	33-07-2024	REISSUED FOR ADP REVIEW	JG
13	27-06-2024	REISSUED FOR ADP REVIEW	JG
12	14-06-2024	REISSUED FOR REZONING	JG
11	12-06-2024	REISSUED FOR REZONING	JG
10	14-06-2024	ISSUED FOR REZONING	JG
9	29-05-2024	REISSUED FOR ADP REVIEW	JG
8	29-04-2024	REISSUED FOR DP	JG
7	22-04-2024	ISSUED FOR DP & ADP	JG
6	02-08-2023	REISSUED FOR DP	JG

**START DATE**

PROJECT No 2018-15

DR. SP

CH. JMM

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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

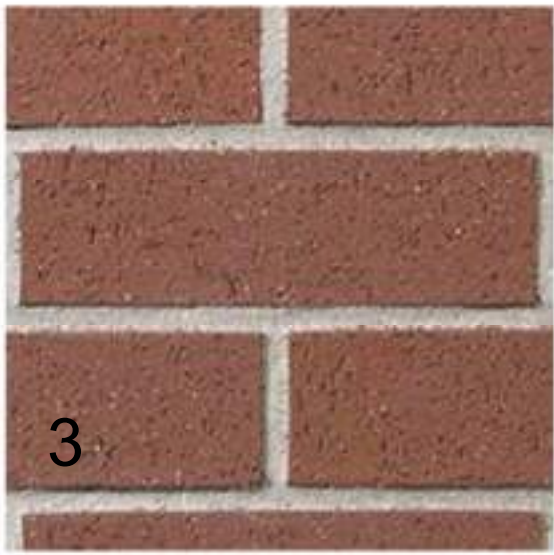
3D VIEWS

REV No	15	Project No	2018-15	DP- 900
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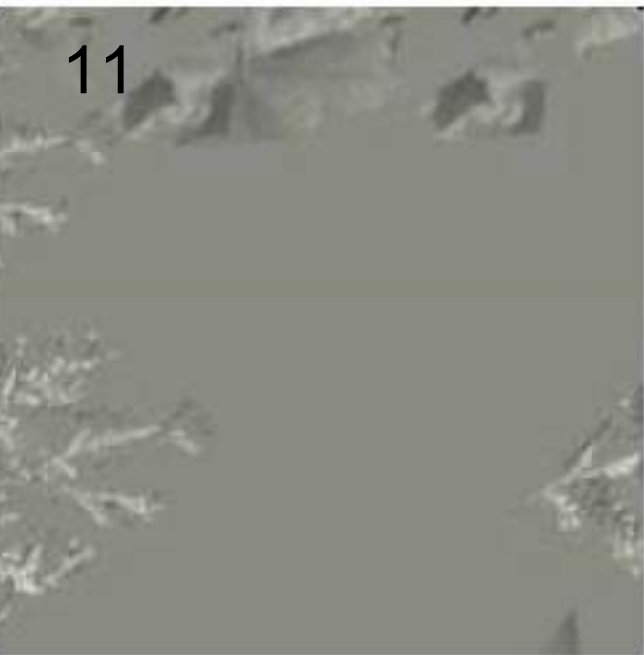
EXTERIOR CLADDING

MITREX  
Active Solar  
Panel  
Colour to match  
Benjamin Moore  
Amherst Gray

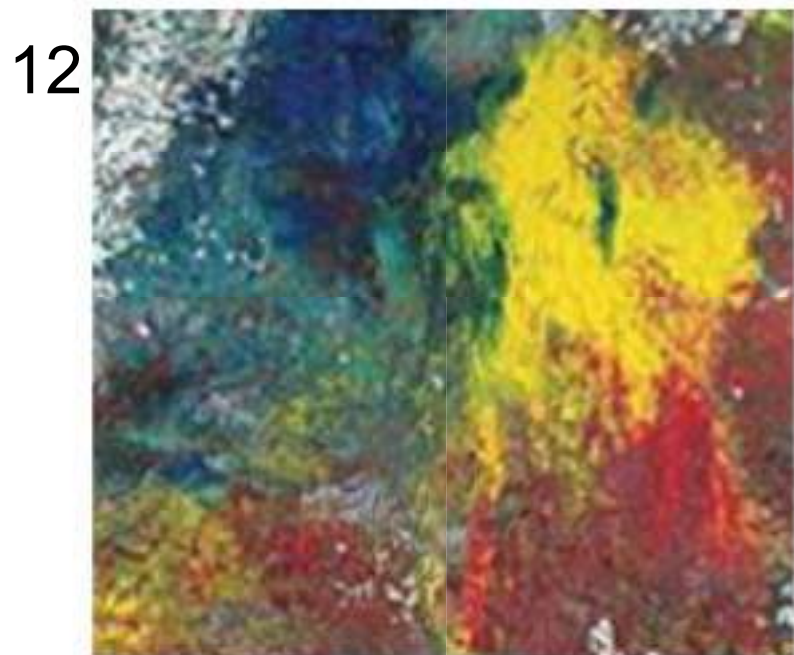


CLADIFY  
Brick Cladding  
Admiral Red Velour

MITREX  
Active Solar  
Panel  
Colour to match  
Benjamin Moore  
Coventry Gray



MITREX  
Active Solar  
Panel  
Colour to match  
Benjamin Moore  
Amherst Gray  
with leaves



MITREX  
Active Solar  
Panel  
Artwork

ROOFING



CASCADIA - Rib steel roof panel  
PVDF - Old Zinc Grey

5 METRIX  
Solar panel  
To match Cascadia - Rib steel roof panel  
PVDF - Old Zinc Grey

ALUMINIUM FRAMES



SOFFIT/TRELLIS/BEAM



Cascadia -PVDF Acorn

BALCONY SOLAR GLASS RAIL



MITREX  
Active Solar panel

Column Cladding



CASCADIA  
Regent Grey



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5	22-04-2024	ISSUED FOR DP & ADP	JG
4	02-08-2023	REISSUED FOR DP	JG

START DATE	
PROJECT No	2018-15
DR.	Author
CH.	Checker
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Project Name  
**222 MEADOWS WALK**  
MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

MATERIAL BOARD

REV No	12	Project No	2018-15	DP- 901
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1 EAST VIEW@ 222 ST FINAL Copy 1



2 SOUTH WEST VIEW @ 123 AVE Copy 1



4 ROOF TOP VIEW- Copy 1



3 AERIAL VIEW Copy 1

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7	22-04-2024	ISSUED FOR DP & ADP	JG
6	02-08-2023	REISSUED FOR DP	JG
5	17-02-2023	REISSUED FOR DP	TJ
4	15-03-2021	REISSUED FOR DP	TJ
3	09-03-2021	ISSUED FOR CLIENT	TJ
2	28-08-2020	ISSUED FOR DP	SP
1	20-08-2020	FOR CLIENT REVIEW	SP

START DATE

PROJECT No 2018-15

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Project Name  
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Client:  
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3D VIEWS WITH REVIT VIEWS

REV No	Project No	DP- 902
10	2018-15	





VIEW FROM 222 ST.

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2	28-04-2025	REISSUED FOR DP	JKA
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REV	DATE	DESCRIPTION	BY

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PROJECT No 2018-15

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MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

3D VIEW-1

REV No	2	Project No	2018-15	DP- 903
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BIRD'S EYE VIEW OF THE DEVELOPMENT

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1	03-07-2024	REISSUED FOR ADP	JG
REV	DATE	DESCRIPTION	BY

**START DATE**

PROJECT No	2018-15
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MAPLE RIDGE, BC

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3D VIEW-2

REV No	2	Project No	2018-15	DP- 904
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SOUTH-WEST VIEW OF THE BUILDING

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Project Name  
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MAPLE RIDGE, BC

Client:  
SIMPLEX MEADOWS DEVELOPMENT CORP.

3D VIEW-3

REV No	2	Project No	2018-15	DP- 905
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VIEW OF THE CHILDREN PLAY AREA FOR THE DAYCARE



VIEW OF THE ROOF-TOP AMENITY



CLOSE UP VIEW OF THE SOLAR CLADDING



WEST SIDE VIEW OF THE BUILDING



NORTH-WEST SIDE VIEW OF THE BUILDING

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REV	DATE	DESCRIPTION	BY

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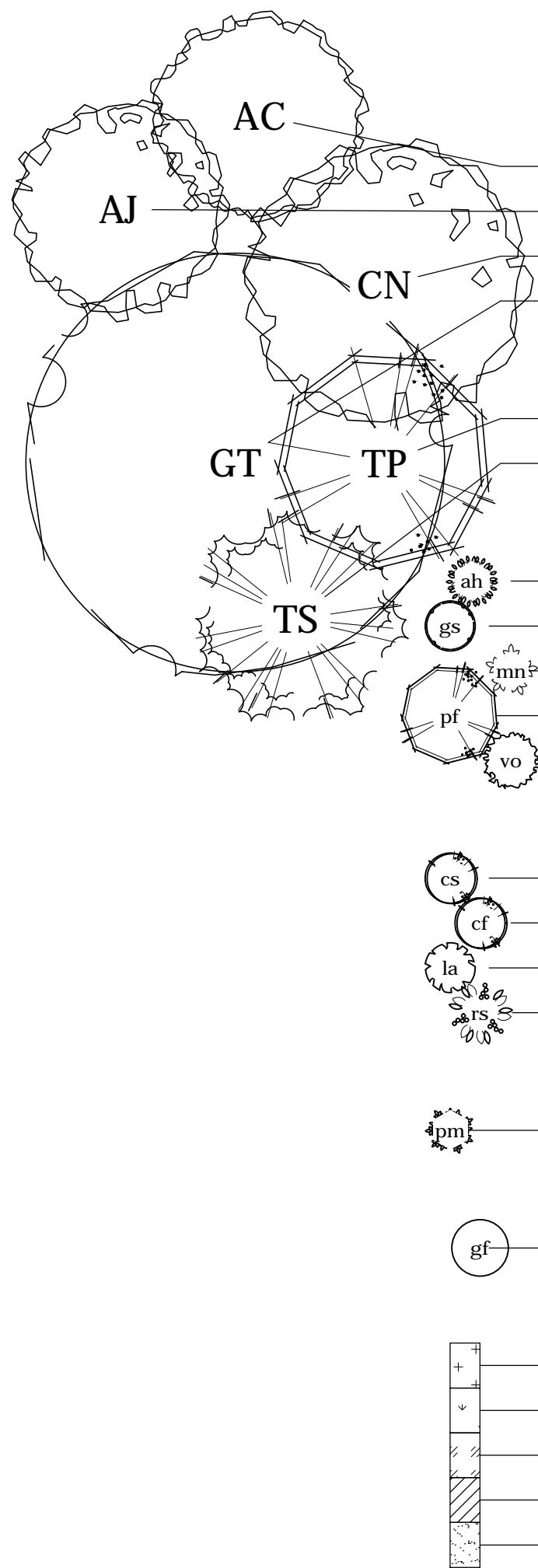
Client:  
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3D VIEW-4

REV No	2	Project No	2018-15	DP- 906
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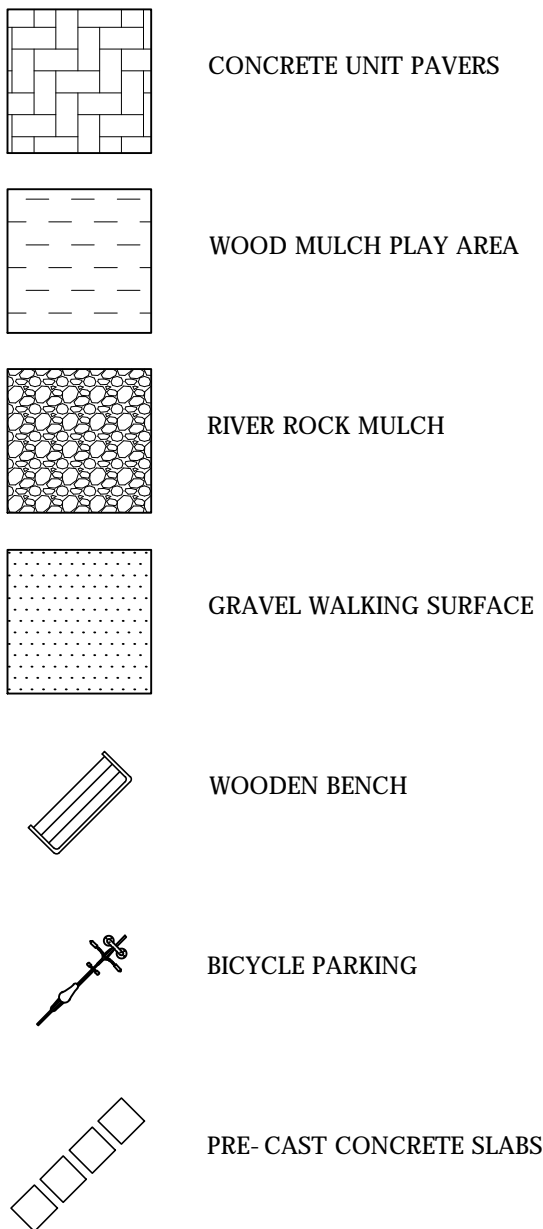
PLANT PALLET



PLANT LIST

DECIDUOUS TREES							
CODE	QTY	SCIENTIFIC NAME	COMMON NAME	SIZE	COMMENTS	MATURE SIZE	SPACING
AC	5	Acer circinatum	Vine Maple	2.5m clump	B&B min. 60cm root ball dia.	4.5m height 4.5m spread	as shown
AJ	3	Acer palmatum 'Sango Kaku'	Coral Bark Maple	2 m clump	B&B min. 50cm root ball dia.	4.5m height 4.5m spread	as shown
CN	2	Cornus 'Eddies White Wonder'	White Wonder Dogwood	6 cm cal.	150cm standard B&B min. 50cm root ball dia.	6.0m height 4.5m spread	as shown
GT	1	Gleditsia triacanthos	Honey Locust	6 cm cal	150cm standard B&B min. 50cm root ball dia.	20m height 20.0m spread	as shown
CONIFEROUS TREES							
CODE	QTY	SCIENTIFIC NAME	COMMON NAME	SIZE	COMMENTS	MATURE SIZE	SPACING
TP	14	Thuja plicata	Western Red Cedar	2.5 m B&B		40m height 15m width	as shown
TS	16	Tsuga heterophylla	Western Hemlock	3.5m B&B	B&B min. 50cm root ball dia.	50m height 15m width	as shown
BROADLEAF EVERGREEN SHRUBS							
CODE	QTY	SCIENTIFIC NAME	COMMON NAME	SIZE	COMMENTS	MATURE SIZE	SPACING
ah	52	Azalea japonica 'Hino Crimson'	Crimson Japanese Azalea	#2 pot	minimum 20cm height	100cm height 120cm width	60cm
gs	24	Gaultheria shallow	Salal	#1 pot	minimum 30cm height - heavy	100cm height 150cm width	60cm
mn	212	Mahonia nervosa	Cascade Mahonia	#2 pot	minimum 30cm spread	60cm height 60cm width	75cm
pf	28	Photinia f. 'Little Red Robin'	Christmas Berry	#5 pot	minimum 60cm height	150cm height 100cm width	150cm
vo	42	Vaccinium ovatumThunderbird'	Evergreen Huckleberry	#2 pot	minimum 20cm height	180cm height 150cm width	75cm
DECIDUOUS EVERGREEN SHRUBS							
CODE	QTY	SCIENTIFIC NAME	COMMON NAME	SIZE	COMMENTS	MATURE SIZE	SPACING
cs	88	Cornus sericea	Red Osier Dogwood	#2 pot	minimum 50cm height	200cm height 200cm width	75cm
cf	165	Cornus sericea 'Flaviramea'	Yellowtwig Dogwood	#2 pot	minimum 50cm height	200cm height 200cm width	75cm
la	16	Lavandula angustifolia	English Lavender	#2 pot	minimum 20cm height	70cm height 70cm width	75cm
rs	47	Ribes sanguineum	Flowering Currant	#2 pot	minimum 50cm height	300cm height 20cm width	75cm
FERNS							
CODE	QTY	SCIENTIFIC NAME	COMMON NAME	SIZE	COMMENTS	MATURE SIZE	SPACING
pm	30	Polystichum munium	Swordfern	#1 pot	minimum 30cm height	100cm height 100cm width	60cm
VINES							
CODE	QTY	SCIENTIFIC NAME	COMMON NAME	SIZE	COMMENTS	MATURE SIZE	SPACING
gf	7	Lonicera 'Gold Flame'	Gold FlameHoneysuckle	#5 pot			
GROUND COVERS							
CODE	QTY	SCIENTIFIC NAME	COMMON NAME	SIZE	COMMENTS	MATURE SIZE	SPACING
cij	375	Carex Iris Juncus Mix	Pacific Pleading Heart	plug	50% Carex densa + 10% Carex vulphnoidea + 10% Iris douglassiana + 30% Juncus haiticus		50cm o.c.
cdo	1040	Cornus Dcentra Oxalis Mix	Redwood Sorrel/Bleeding Heart	plug	25% Cornus canadensis + 25% Dcentra formosa + 50% Oxalis oregana		50cm o.c.
oo	2412	Gaultheria Mahonia Rubus Mix	Redwood Sorrel	plug	30% Gaultheria shallon + 40% Mahonia repens + 10% Rubus spectabilis + 20% Vaccitium vitis- idaea		50cm o.c.
arc	48	Arctostaphylos uva- ursi	Kinnikinnick	10cm pot			30cm o.c.
la		Lawn	Shade Tolerant Blend				

HARD LANDSCAPE



GENERAL NOTES

- COMPLIANCE, GOVERNMENT - ALL WORK SHALL BE COMPLETED TO THE SATISFACTION OF THE CITY OF MAPLE RIDGE AND SHALL COMPLY WITH ALL STANDARDS AND REGULATIONS OF THE SAME. CONTRACTORS SHALL ENSURE COMPLIANCE OF ALL WORK PRIOR TO INSTALLATION.
- COMPLIANCE, STANDARD - ALL MATERIAL, SHIPPING PROCEDURES, AND INSTALLATION OF LANDSCAPE ELEMENTS SHALL CONFORM TO LATEST EDITION OF THE CANADIAN LANDSCAPE STANDARD AVAILABLE AT COMMENCEMENT OF WORK.
- COMPLIANCE, MMCD - ALL WORKS SHALL CONFORM TO THE LATEST MMCD DETAILS AND SPECIFICATIONS AVAILABLE AT COMMENCEMENT OF WORK WHERE NO SPECIFIC DETAIL, NOTE OR SPECIFICATION IS PROVIDED IN THE DRAWING SET OR ATTACHED MATERIALS.
- EXISTING SERVICES - REFER TO ENGINEERING DRAWINGS FOR LOCATIONS OF ALL BURIED SERVICES. CONTRACTOR IS RESPONSIBLE FOR OBTAINING AND PAYING FOR ALL UTILITIES LOCATIONS AND ANY AND ALL COSTS WHICH ARISE FROM DAMAGE TO SERVICES CAUSED BY ANY ACT OR FAILURE TO ACT OF THE CONTRACTOR.
- COORDINATION - THESE DRAWINGS SHALL BE TAKEN TO BE PART OF THE LARGER SET OF DRAWINGS AND SPECIFICATIONS, WHETHER DIRECTLY ATTACHED OR NOT, ISSUED FOR THE CONSTRUCTION OF THE ASSOCIATED BOULEVARD WORKS. THESE DRAWINGS SHALL BE READ IN CONCERT WITH SUCH DOCUMENTATION AND, WHERE CONFLICTS ARISE, THE CONTRACTOR SHALL IMMEDIATELY REPORT SUCH CONFLICTS TO THE LANDSCAPE ARCHITECT. THE WORK IS TO BE DONE IN COORDINATION WITH THE CONTRACTOR OR CONTRACTORS ENGAGED IN CONSTRUCTION OF SUCH ADJACENT WORKS.
- CONCRETE, FORMS - THE CONTRACTOR SHALL COMPARE DIMENSIONS PROVIDED WITH SITE CONDITIONS AND IMMEDIATELY REPORT ANY DISCREPANCIES TO THE LANDSCAPE ARCHITECT AND THE ENGINEER. ONCE COMPLETED THE CONTRACTOR SHALL CALL FOR INSPECTION OF THE FORMS PRIOR TO PLACING STEEL REINFORCEMENT. CONTACT LANDSCAPE ARCHITECT A MINIMUM OF 48 HOURS PRIOR TO DESIRED INSPECTION TIME.
- CONCRETE, REINFORCEMENT - ALL CONCRETE REINFORCEMENT SHALL BE INSPECTED AND APPROVED BY THE STRUCTURAL ENGINEER PRIOR TO POURING OF CONCRETE. CONTACT STRUCTURAL ENGINEER A MINIMUM OF 48 HOURS PRIOR TO DESIRED INSPECTION TIME.
- GRADING, GENERAL - THE CONTRACTOR SHALL ESTABLISH GRADES SUCH THAT THERE IS, IN ALL AREAS, POSITIVE DRAINAGE TO AN ADJACENT DRAIN. MINIMUM SLOPES SHALL BE AS FOLLOWS:

PEDESTRIAN PAVED AREAS	1%
VEHICULAR PAVED AREAS	1%
LAWN AREAS	2%
PLANTED BEDS	5%
- GRADING, ALLOWANCES AND TOPSOIL PLACEMENT - OVERALL DEPTH OF PLANTING MEDIUM AFTER SETTLEMENT SHALL BE:

GRASS AREAS	150 MM
SHRUB BEDS	450 MM
GROUND COVER AREAS	300 MM
TREES	600 MM
- ALL SOIL SHALL BE TESTED NOT MORE THAN 14 DAYS PRIOR TO INSTALLATION ON SITE. TESTING SHALL BE SPECIFIC TO THIS WORK. CONTRACTOR SHALL AMEND THE SOIL ACCORDING TO THE RECOMMENDATIONS OF THE SOILS TESTING LAB.
- GRADING, BUILDING FINISHED FLOOR & CORNER ELEVATIONS - SEE ARCHITECTURAL DRAWINGS.
- PLANT MATERIAL, QUANTITIES - CONTRACTOR SHALL CONFIRM ALL PLANT MATERIAL QUANTITIES. ANY DISCREPANCIES BETWEEN THE PLANT LIST AND THE DRAWING SHALL BE REPORTED TO THE CONSULTANT. IN CASE OF DISCREPANCY CONTRACTOR SHALL INSTALL PLANT MATERIAL AS PER DRAWING. IN CIRCUMSTANCES WHERE THERE IS A DISCREPANCY BETWEEN GROUND COVER QUANTITY AND SPACING THE SPACING REQUIREMENT WILL PREVAIL.
- PLANT MATERIAL, SUBSTITUTIONS - NO SUBSTITUTIONS TO PLANT LIST SHALL BE MADE BY CONTRACTOR WITHOUT PRIOR WRITTEN AUTHORIZATION FROM CONSULTANT.
- PLANT MATERIAL, HEALTH - ALL PLANT MATERIALS MUST BE DISEASE FREE FROM CERTIFIED NURSERIES.
- MULCH - ORNAMENTAL PLANTING AREAS ALONG THE ROAD FRONTAGES AND IN THE PAVING AREAS SHALL BE MULCHED WITH 50 MM OF HEM/FIR BARK MULCH. THE USE OF CEDAR MULCH OR ANY MIX CONTAINING CEDAR IS NOT PERMITTED. NATURESCAPED AREAS SHALL NOT BE MULCHED.

DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
2024- 04- 05	D.P.	M
2024- 04- 18	A.D.P.	N
2024- 04- 29	D.P.	O
2024- 06- 13	D.P.	P
2024- 06- 26	D.P.	Q

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Consultants

Architectural: JMArchitecture Inc.  
Arborist: -  
Civil:  
Structural:  
Mechanical:  
Electrical:

**DVS** Donald V. S. Duncan  
DEVELOPMENT CONSULTANT  
LANDSCAPE ARCHITECT

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New Westminster BC  
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Maple Ridge #  
2018- 255- RZ/DP

Q Revision	Project Leader
	D.Duncan
Revision	Drawn
	D.Duncan

prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**NOTES & KEYS**

Check Scale (may be photo reduced)  
0 1inch 0 10mm

Project No. 20- 0071

Drawing No. L1- 00




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---

Architectural: JMArchitecture Inc.  
Arborist: -  
Civil:  
Structural:  
Mechanical:  
Electrical:



	Project Leader D.Duncan
	Drawn D.Duncan

# Drawing Title

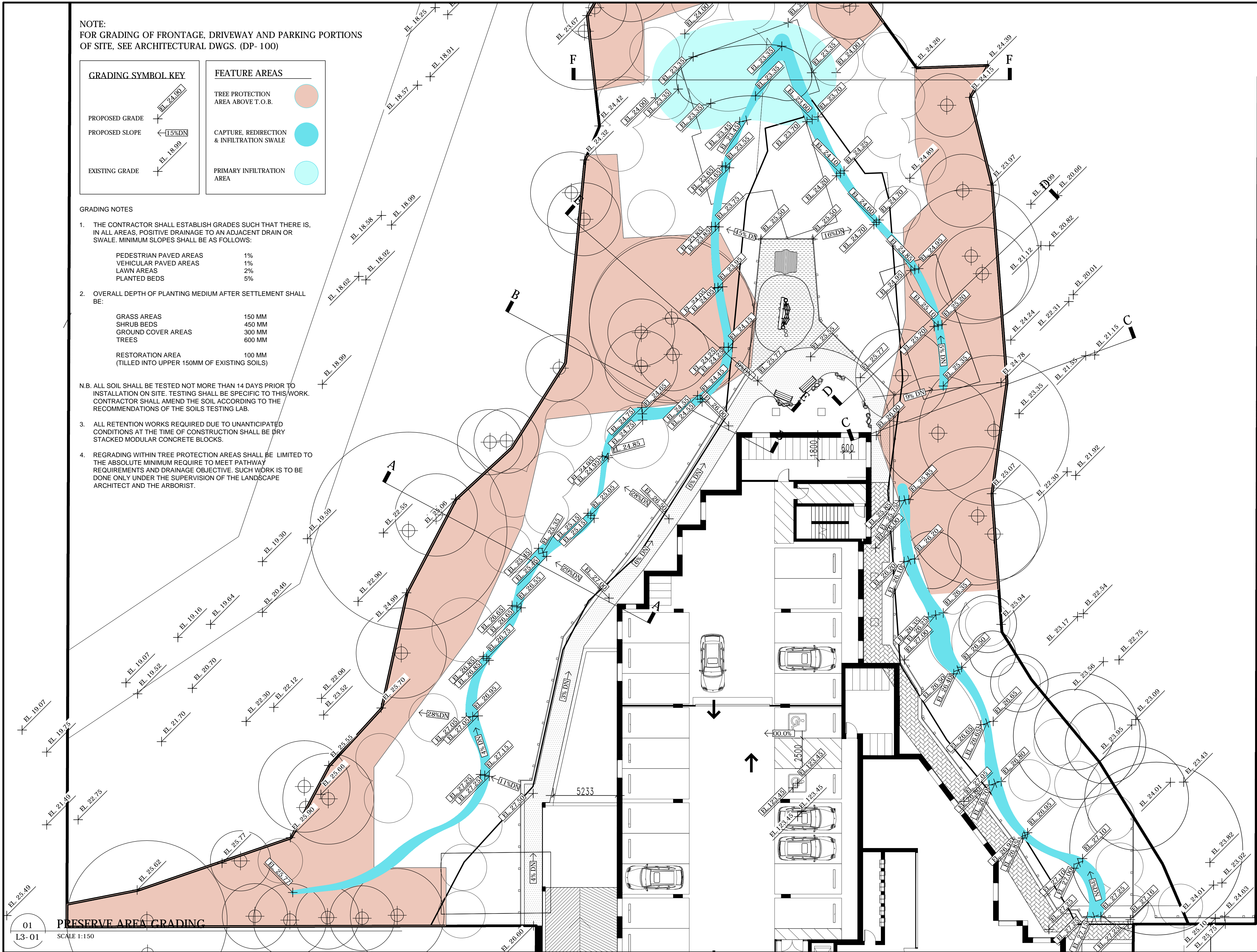
## HARD SITE PLAN

Drawing No. **L2- 00**





PLOT DATE: June 26, 2024 TIME: 6:21 PM FULL PATH AND FILENAME: D:\PROJECTS\220-0071 222 ST MR 500- DELIV\LAND 01 - TB\L3-01.DWG PLOTSTYLE TABLE: ----



NOTE:  
FOR GRADING OF FRONTAGE, DRIVEWAY AND PARKING PORTIONS  
OF SITE, SEE ARCHITECTURAL DWGS. (DP- 100)

GRADING SYMBOL KEY		FEATURE AREAS	
PROPOSED GRADE		TREE PROTECTION AREA ABOVE T.O.B.	
PROPOSED SLOPE		CAPTURE, REDIRECTION & INFILTRATION SWALE	
EXISTING GRADE		PRIMARY INFILTRATION AREA	

#### GRADING NOTES

1. THE CONTRACTOR SHALL ESTABLISH GRADES SUCH THAT THERE IS, IN ALL AREAS, POSITIVE DRAINAGE TO AN ADJACENT DRAIN OR SWALE. MINIMUM SLOPES SHALL BE AS FOLLOWS:

PEDESTRIAN PAVED AREAS	1%
VEHICULAR PAVED AREAS	1%
LAWN AREAS	2%
PLANTED BEDS	5%

2. OVERALL DEPTH OF PLANTING MEDIUM AFTER SETTLEMENT SHALL BE:

GRASS AREAS	150 MM
SHRUB BEDS	450 MM
GROUND COVER AREAS	300 MM
TREES	600 MM

RESTORATION AREA (TILLED INTO UPPER 150MM OF EXISTING SOILS)	100 MM
---	--------

- N.B. ALL SOIL SHALL BE TESTED NOT MORE THAN 14 DAYS PRIOR TO INSTALLATION ON SITE. TESTING SHALL BE SPECIFIC TO THIS WORK. CONTRACTOR SHALL AMEND THE SOIL ACCORDING TO THE RECOMMENDATIONS OF THE SOILS TESTING LAB.

3. ALL RETENTION WORKS REQUIRED DUE TO UNANTICIPATED CONDITIONS AT THE TIME OF CONSTRUCTION SHALL BE DRY STACKED MODULAR CONCRETE BLOCKS.

4. REGRADING WITHIN TREE PROTECTION AREAS SHALL BE LIMITED TO THE ABSOLUTE MINIMUM REQUIRE TO MEET PATHWAY REQUIREMENTS AND DRAINAGE OBJECTIVE. SUCH WORK IS TO BE DONE ONLY UNDER THE SUPERVISION OF THE LANDSCAPE ARCHITECT AND THE ARBORIST.

01  
L3-01

PRESERVE AREA GRADING

SCALE 1:150

DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
2024- 04- 05	D.P.	M
2024- 04- 18	A.D.P.	N
2024- 04- 29	D.P.	O
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2024- 06- 26	D.P.	Q

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#### Consultants

Architectural: JMArchitecture Inc.  
Arboret: -  
Civil: -  
Structural: -  
Mechanical: -  
Electrical: -

**DV SD** Donald V. S. Duncan  
DEVELOPMENT CONSULTANT  
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Maple Ridge #  
2018-255- RZ/DP

Revision	Project Leader
	D.Duncan
Revision	Drawn
	D.Duncan

prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC  
Drawing Title  
**PRESERVE AREA GRADING**

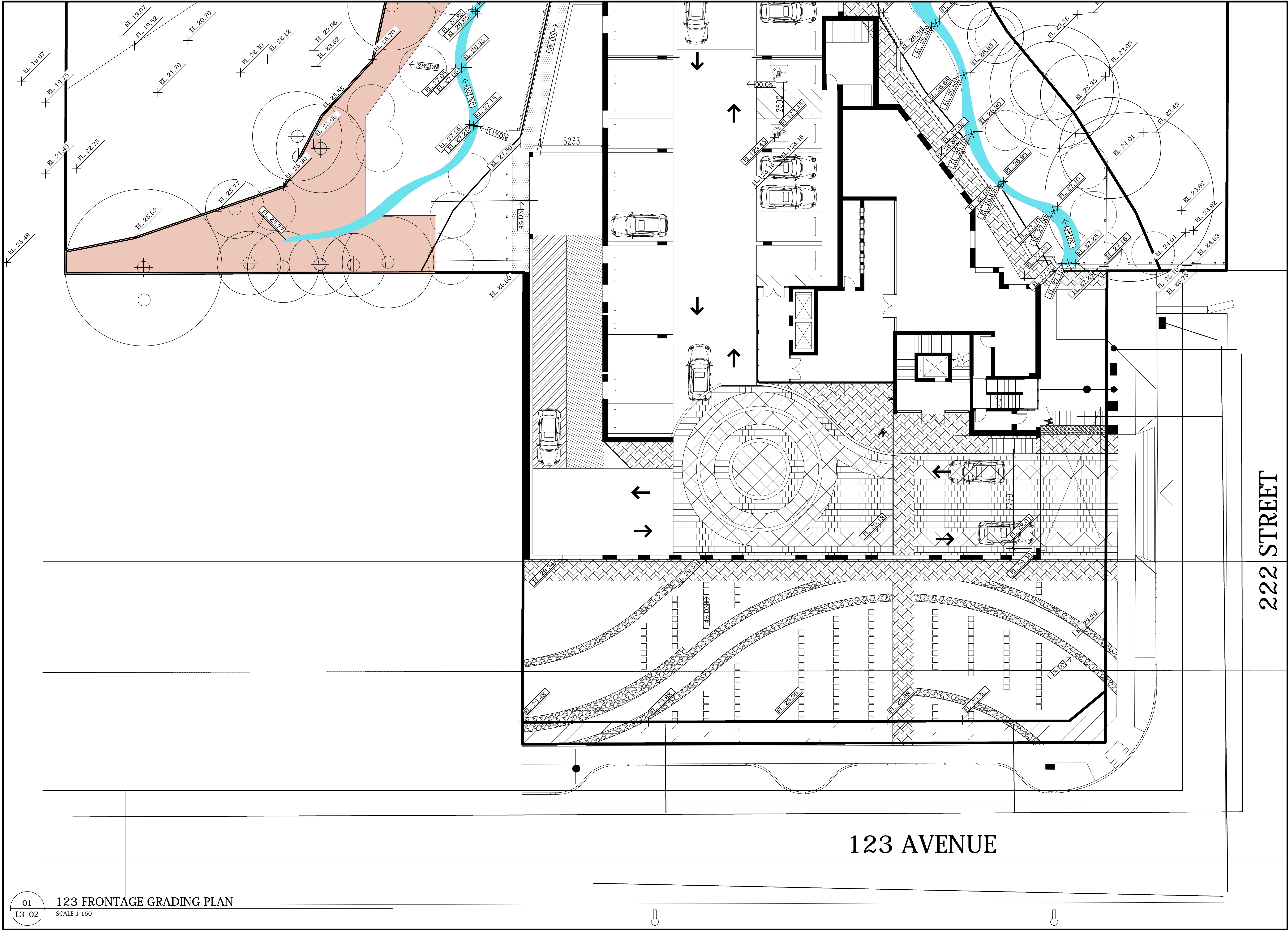
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Project No. 20-0071

Drawing No. L3-01



PLOT DATE: June 26, 2024 TIME: 6:55 PM FULL PATH AND FILENAME: D:\PROJECTS\20-0071 222 ST MR 500- DELIV\LAND 01 - TB\L3-02.DWG PLOTSYLE TABLE: ----



01  
L3-02

123 FRONTAGE GRADING PLAN  
SCALE 1:150

222 STREET

123 AVENUE

DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
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2024- 06- 26	D.P.	Q

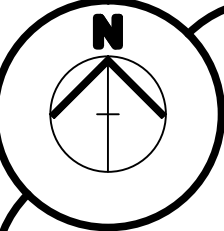
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Consultants

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Arborist: -  
Civil: -  
Structural: -  
Mechanical: -  
Electrical: -



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Maple Ridge #  
2018-255- RZ/DP

Project Leader  
D.Duncan  
Drawn  
D.Duncan

prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**GRADING PLAN**

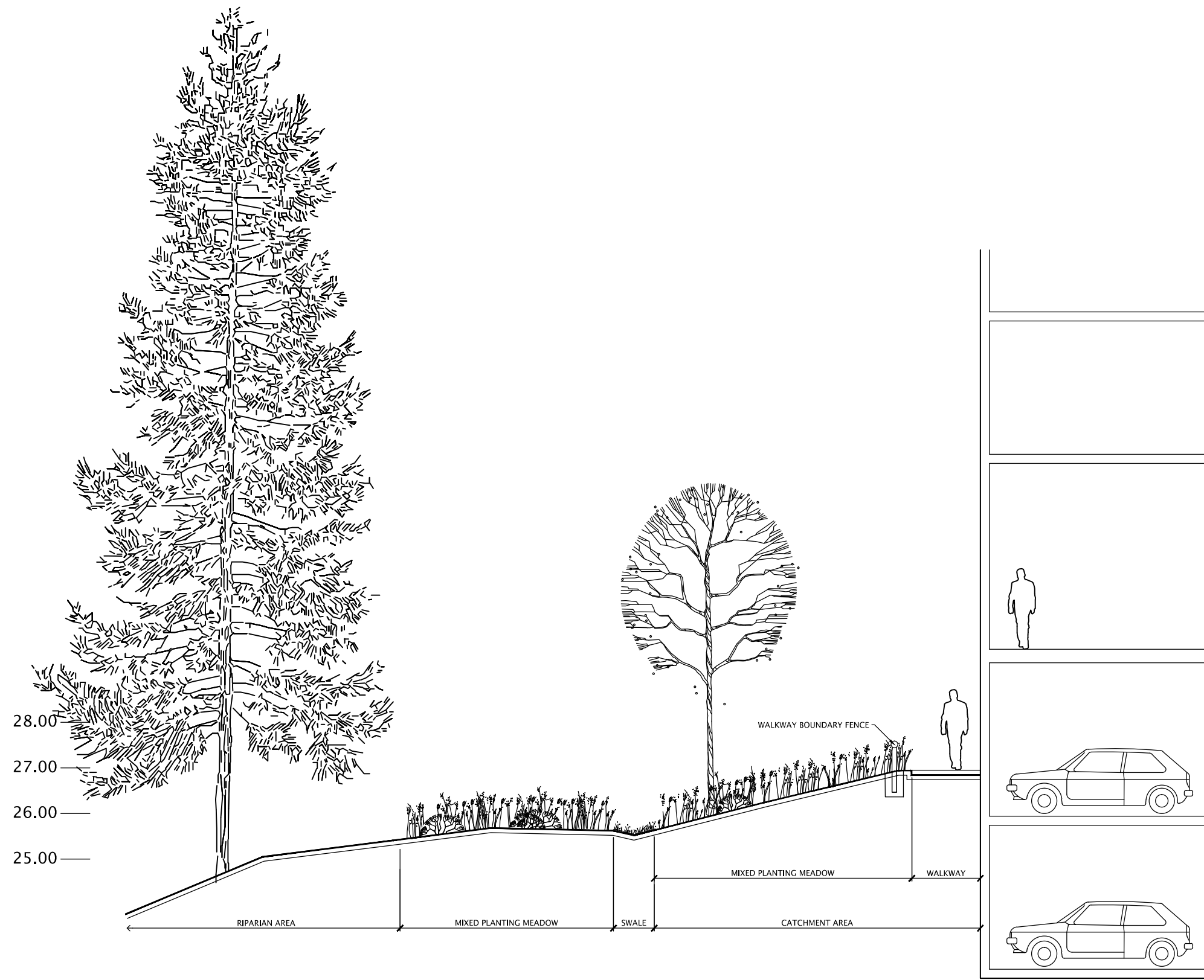
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Project No. 20-0071

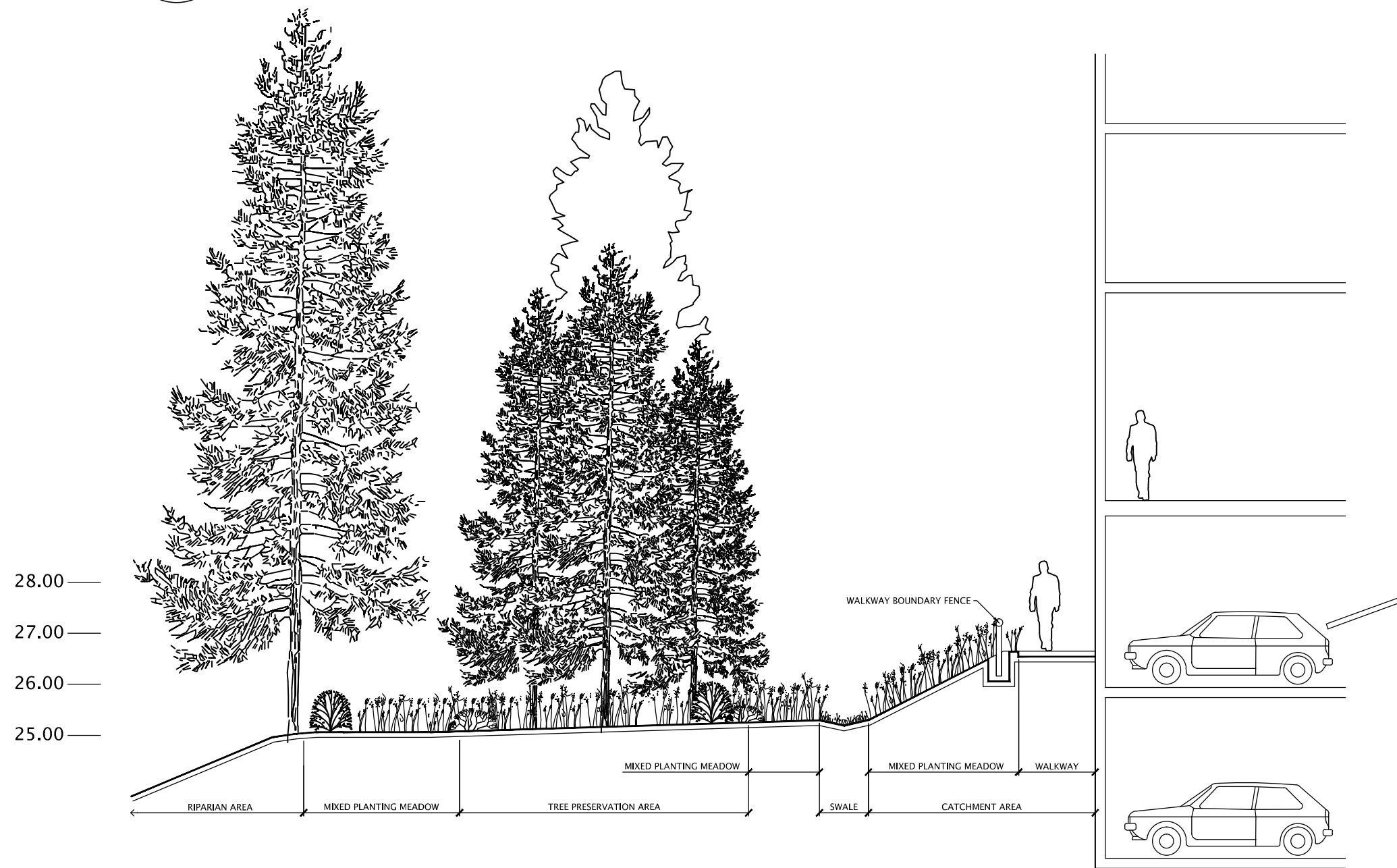
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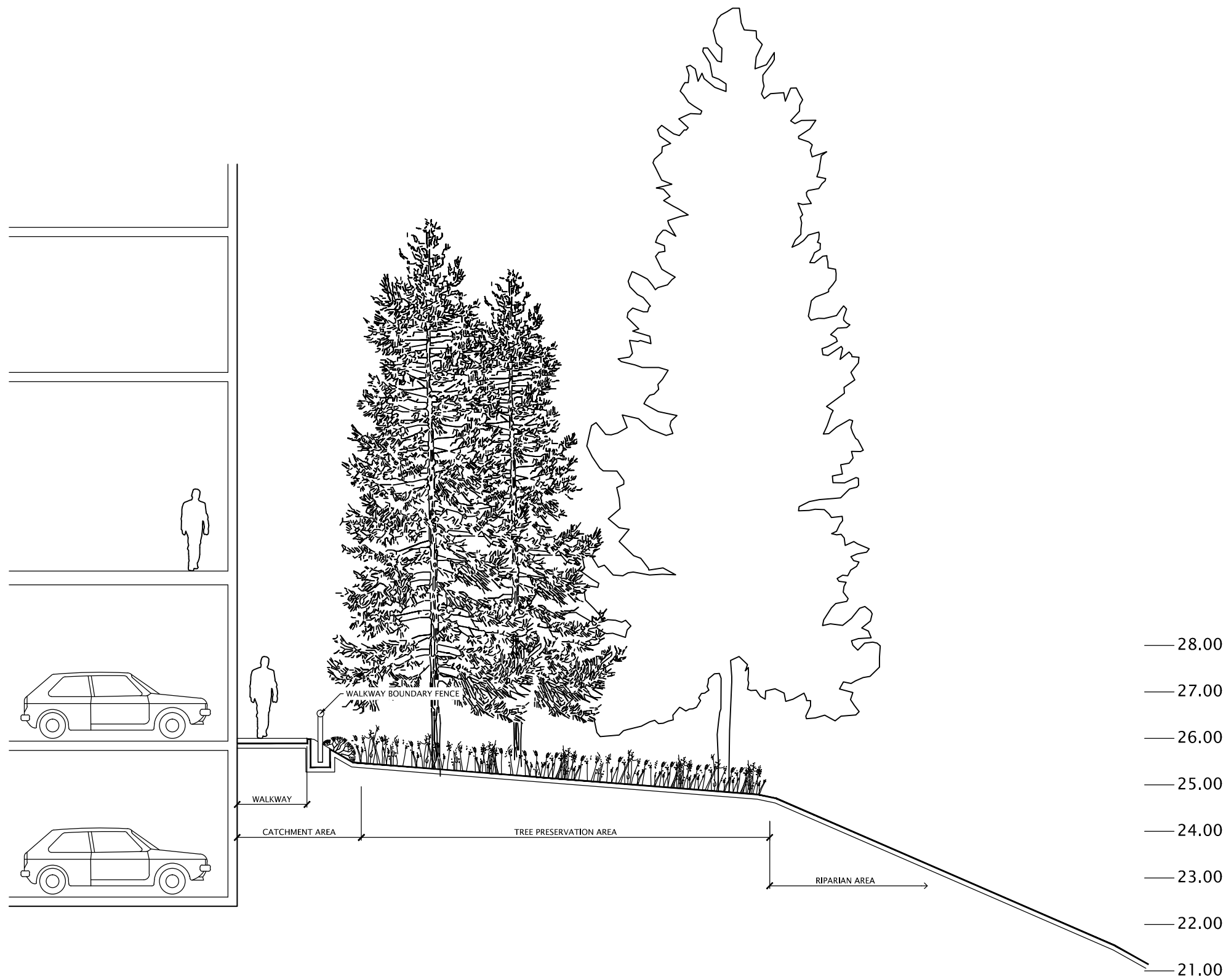
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01  
L3-00 SECTION A-A  
SCALE 1:100



02  
L3-00 SECTION B-B  
SCALE 1:100



03  
L3-00 SECTION C-C  
SCALE 1:100

DATE	ISSUED FOR	REV
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Arborist: -  
Civil:  
Structural:  
Mechanical:  
Electrical:

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Maple Ridge #  
2018- 255- RZ/DP

Revision	Project Leader
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Revision	Drawn
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**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

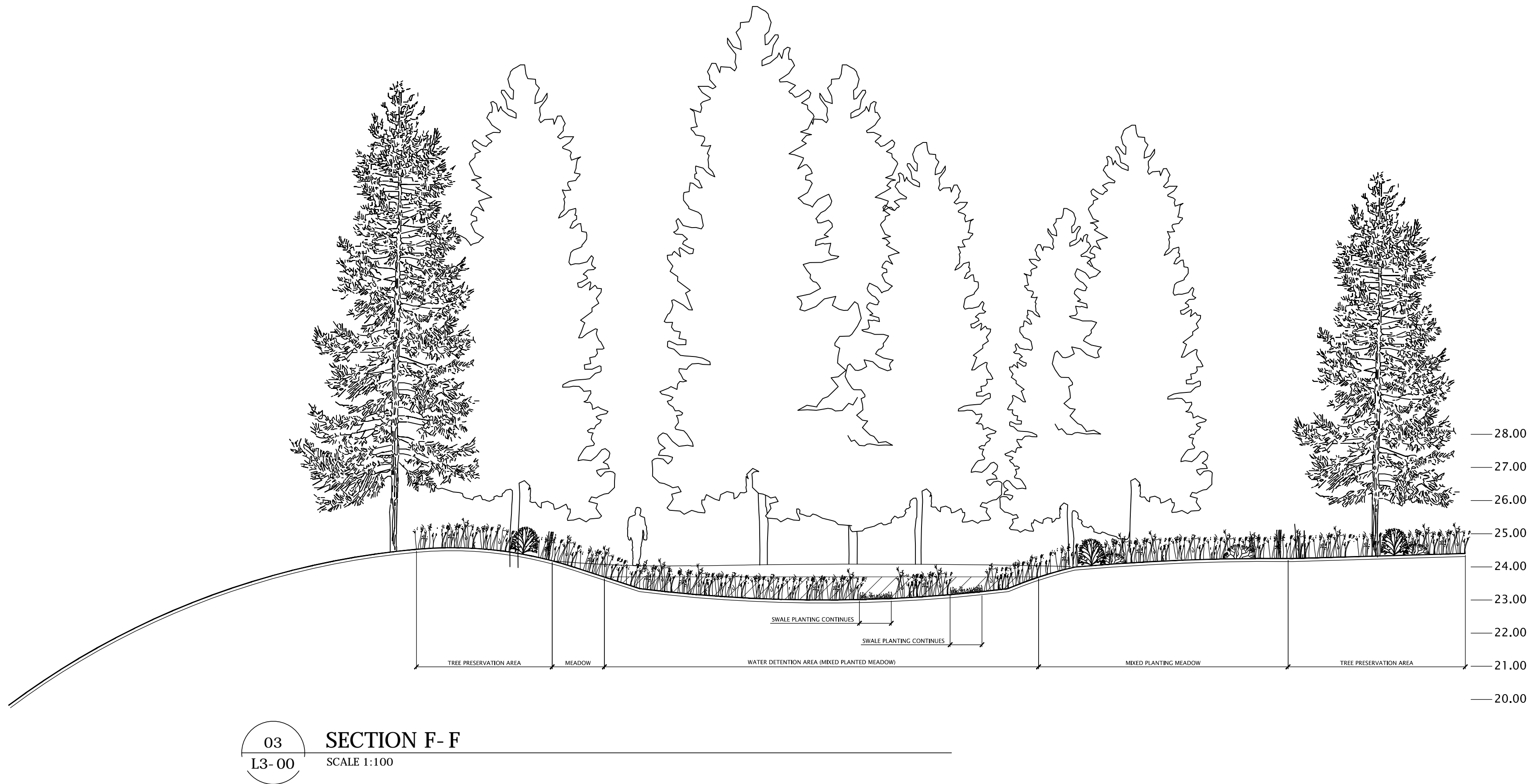
Drawing Title  
**SECTIONS**

Check Scale (may be photo reduced)  
0 1inch 0 10mm

Project No. 20- 0071

Drawing No. L3- 10

PLOT DATE: June 26, 2024 TIME: 6:25 PM FULL PATH AND FILENAME: D:\PROJECTS\20-0071 222 ST MR 500- DELIV\LAND\01-TB\L3-11.DWG PLOTSTYLE TABLE: ----



DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
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Consultants

Architectural: JMArchitecture Inc.  
Arborist: -  
Civil: -  
Structural: -  
Mechanical: -  
Electrical: -

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Maple Ridge #  
2018- 255- RZ/DP

Revision	Project Leader
	D.Duncan
Revision	Drawn
	D.Duncan

prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**SECTIONS**

Check Scale (may be photo reduced)  
0 1inch 0 10mm

Project No. 20-0071

Drawing No. L3- 11



PLOT DATE: June 26, 2024 TIME: 6:25 PM FULL PATH AND FILENAME: D:\PROJECTS\20-0071 222 ST MR 500- DELIV\LAND 01 - TB\L3-21.DWG PLOTSTYLE TABLE: -----

01  
L3-21

WATER MANAGEMENT PLAN - NORTHERN AREA

SCALE 1:150

NOTE:  
FOR GRADING OF FRONTAGE, DRIVEWAY AND PARKING PORTIONS  
OF SITE, SEE ARCHITECTURAL DWGS. (DP- 100)

INFILTRATION DEPRESSION  
AVERAGE DEPTH 650mm  
APPROXIMATE AREA 110m<sup>2</sup>

DRAINAGE SWALE AT BASE  
OF CATCHMENT AREA  
4% SLOPE TO NORTH

CATCHMENT AREA  
SLOPES TO  
DRAINAGE SWALES

DRAINAGE SWALE AT BASE  
OF CATCHMENT AREA  
4% SLOPE TO NORTH

DRAINAGE SWALE AT BASE  
OF CATCHMENT AREA  
5% SLOPE TO NORTH

SUB- SURFACE PIPE CONNECTION  
BETWEEN EAST DRAINAGE SWALES  
100mm DIA. PVC DRAIN LINE

CATCHMENT AREA  
SLOPES TO  
DRAINAGE SWALES

DRAINAGE SWALE AT BASE  
OF CATCHMENT AREA  
3% SLOPE TO NORTH

DATE	ISSUED FOR	REV
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2024- 04- 29	D.P.	O
2024- 06- 13	D.P.	P
2024- 06- 26	D.P.	Q

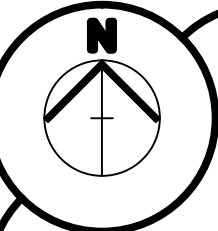
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Consultants

Architectural: JMArchitecture Inc.  
Arborist: -  
Civil:  
Structural:  
Mechanical:  
Electrical:



**DV SD** Donald V. S. Duncan  
DEVELOPMENT CONSULTANT  
LANDSCAPE ARCHITECT

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Maple Ridge #  
2018- 255- RZ/DP

Revision	Project Leader
	D.Duncan
Revision	Drawn
	D.Duncan

prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**WATER MANAGEMENT  
NORTHERN AREA**

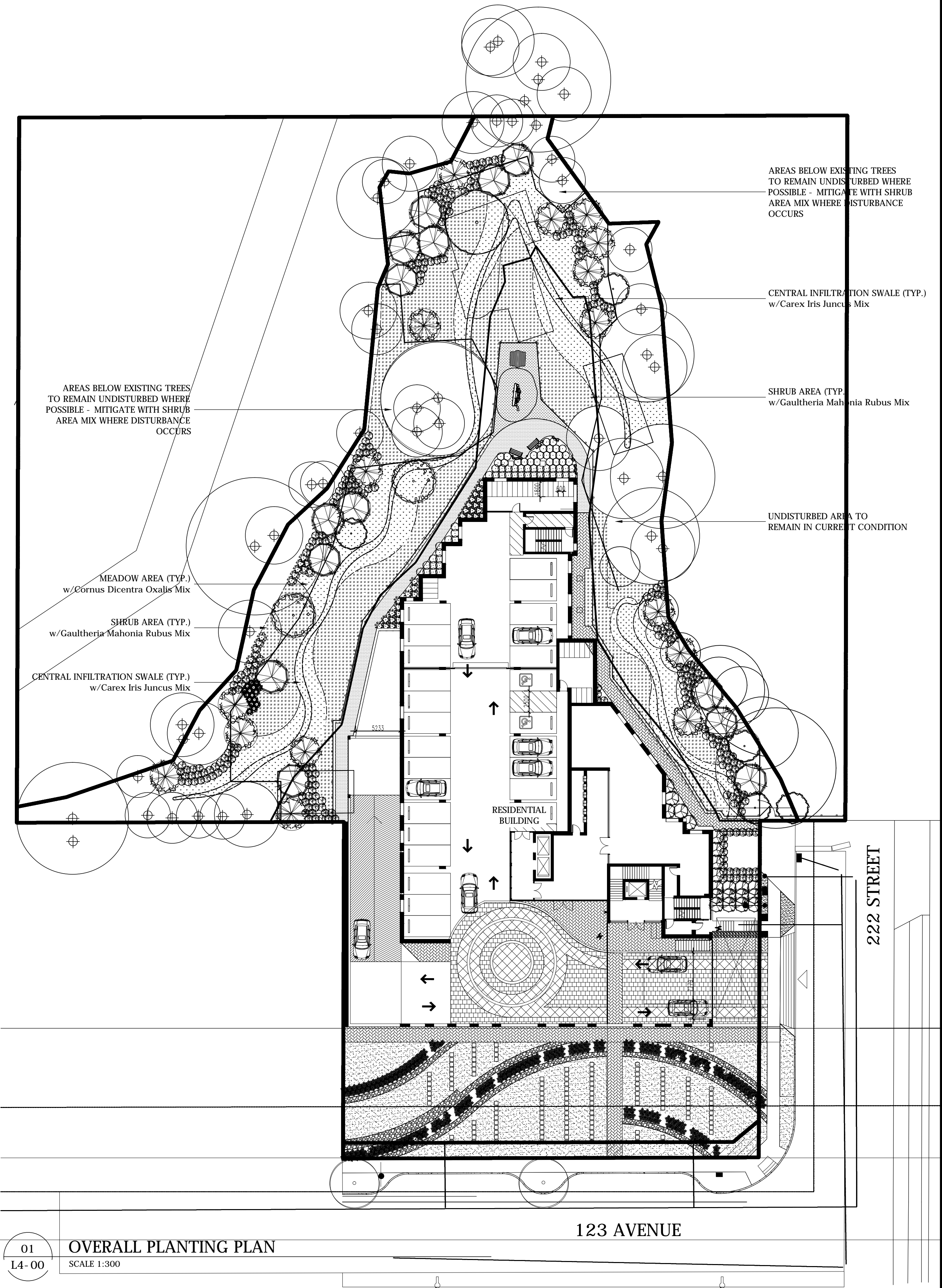
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0 1inch 0 10mm

Project No. 20-0071

Drawing No. L3- 21



PLOT DATE: June 26, 2024 TIME: 6:56 PM FULL PATH AND FILENAME: D:\PROJECTS\20-0071 222 ST MR 500- DELIV\LAND 01 - TB\L4-00.DWG PLOTSYLE TABLE: -----



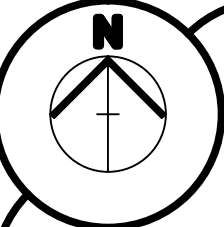
DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
2024- 04- 05	D.P.	M
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Consultants  
Architectural: JMArchitecture Inc.  
Arborist: -  
Civil:  
Structural:  
Mechanical:  
Electrical:



**DV SD** Donald V. S. Duncan  
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Revision	Project Leader
	D.Duncan
Revision	Drawn
	D.Duncan

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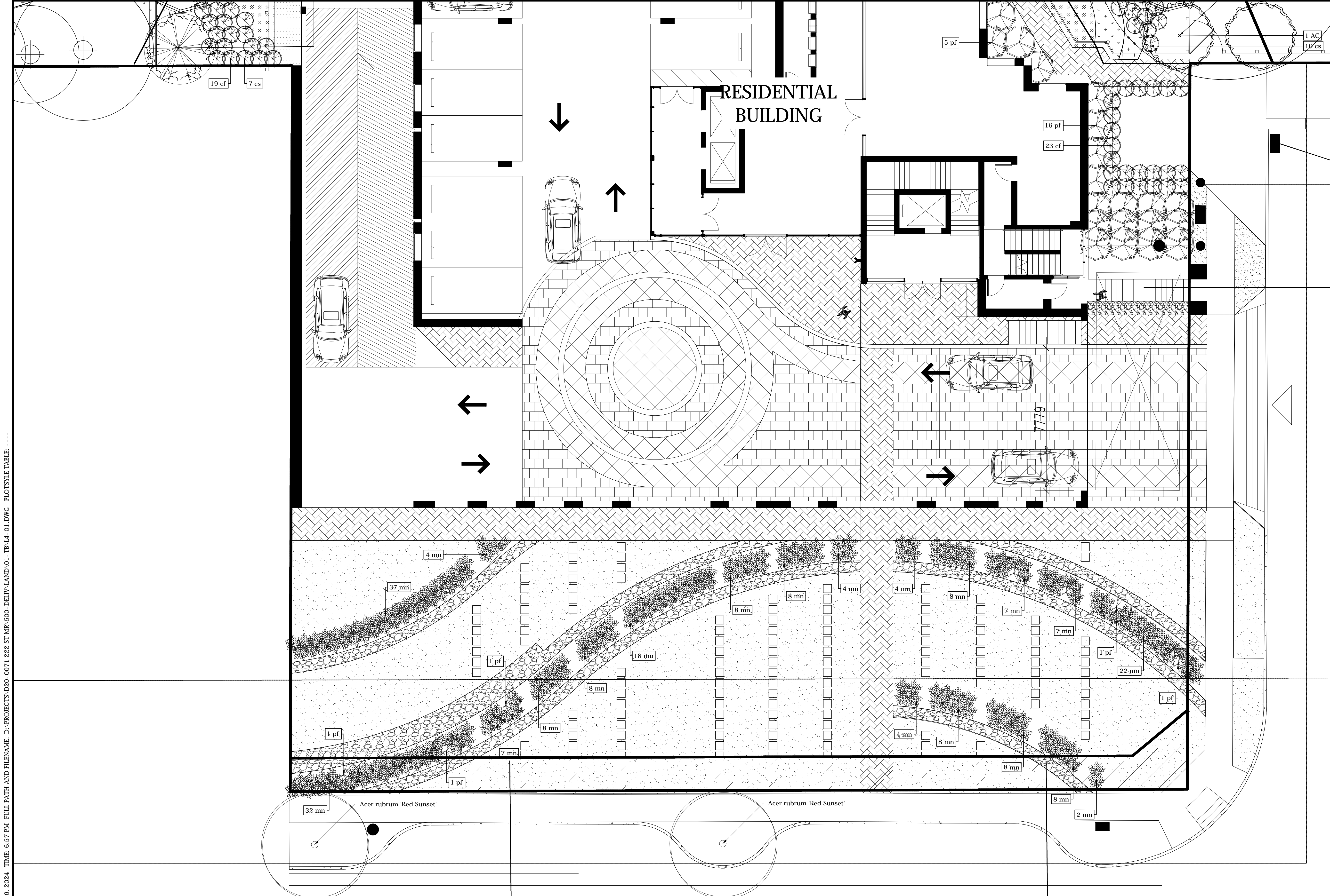
Drawing Title  
**OVERALL PLANTING PLAN**

Check Scale (may be photo reduced)  
0 1inch 0 10mm

Project No. 20-0071

Drawing No. L4- 00





PLOT DATE: June 26, 2024 TIME: 6:57 PM FULL PATH AND FILENAME: D:\PROJECTS\20-0071 222 ST MR 500- DELIV\LAND 01 - TB L4- 01.DWG PLOTSYLE TABLE: ----

01  
L4-01

PLANTING PLAN - SOUTHERN PORTION

SCALE 1:100

DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
2024- 04- 05	D.P.	M
2024- 04- 18	A.D.P.	N
2024- 04- 29	D.P.	O
2024- 06- 13	D.P.	P
2024- 06- 26	D.P.	Q

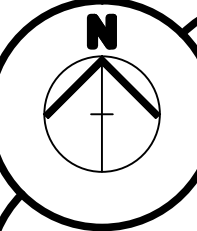
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Arborist: -  
Civil: -  
Structural: -  
Mechanical: -  
Electrical: -



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DEVELOPMENT CONSULTANT  
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Revision	Project Leader
	D.Duncan
Revision	Drawn
	D.Duncan

prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**PLANTING PLAN**

Check Scale (may be photo reduced)  
0 1inch 0 10mm

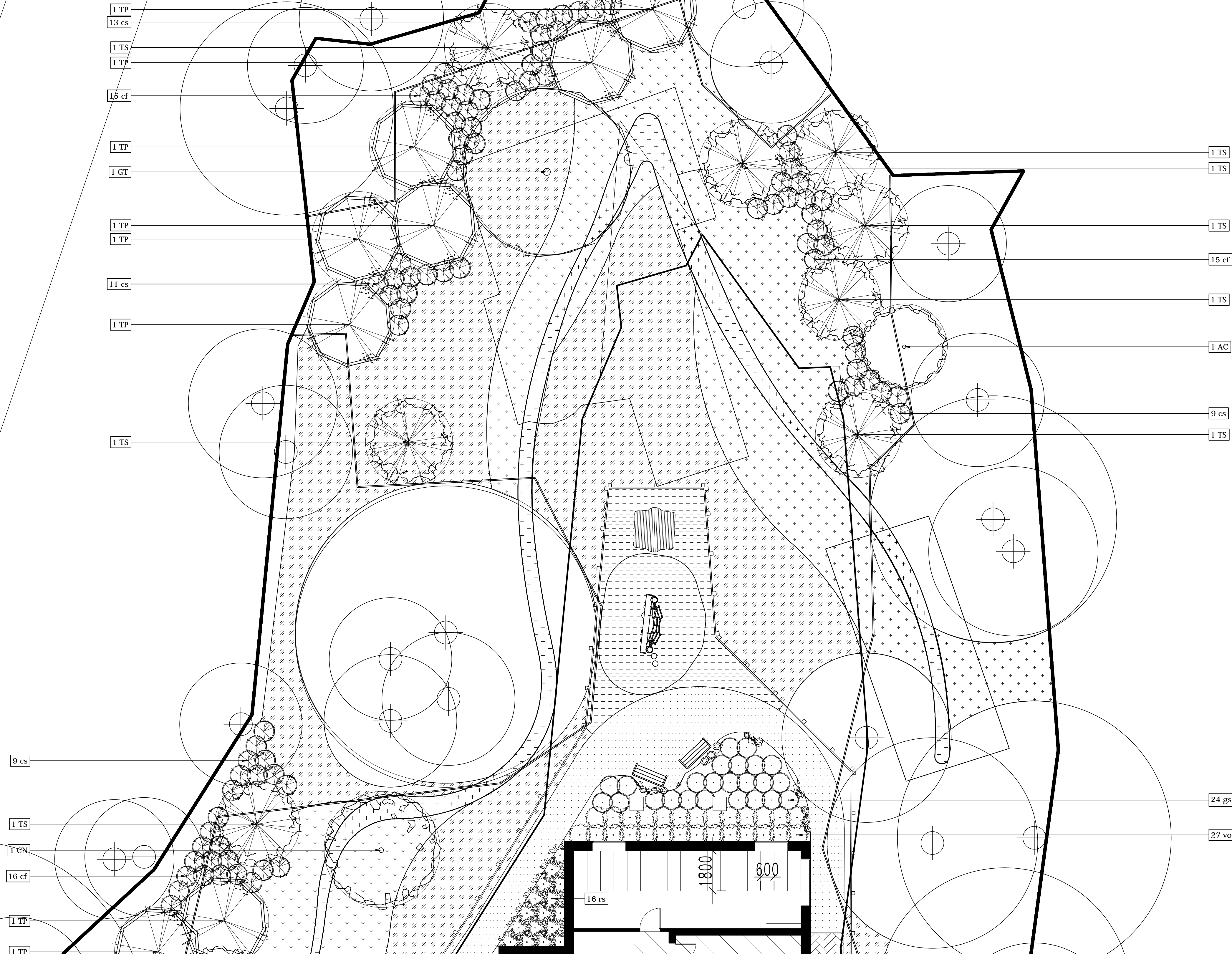
Project No. 20-0071

Drawing No. L4- 01









DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
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2024- 04- 29	D.P.	O
2024- 06- 13	D.P.	P
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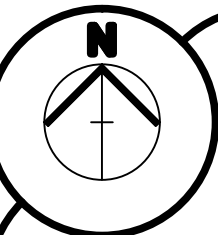
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## Consultants

Architectural: JMArchitecture Inc  
Arborist: -  
Civil:  
Structural:  
Mechanical:  
Electrical:



**DV  
SD** **Donald V. S. Duncan**  
DEVELOPMENT CONSULTANT  
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Maple Ridge #  
2018- 255- RZ/DP

<div style="font-size: 2em; font-weight: bold; text-align: center;">Q</div> <div style="text-align: center;">Revision</div>	Project Leader D.Duncan
	Drawn D.Duncan

prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**PLANTING PLAN**

Check Scale (may be photo reduced) 0 1inch 0 10mm

Project No. 20-0071

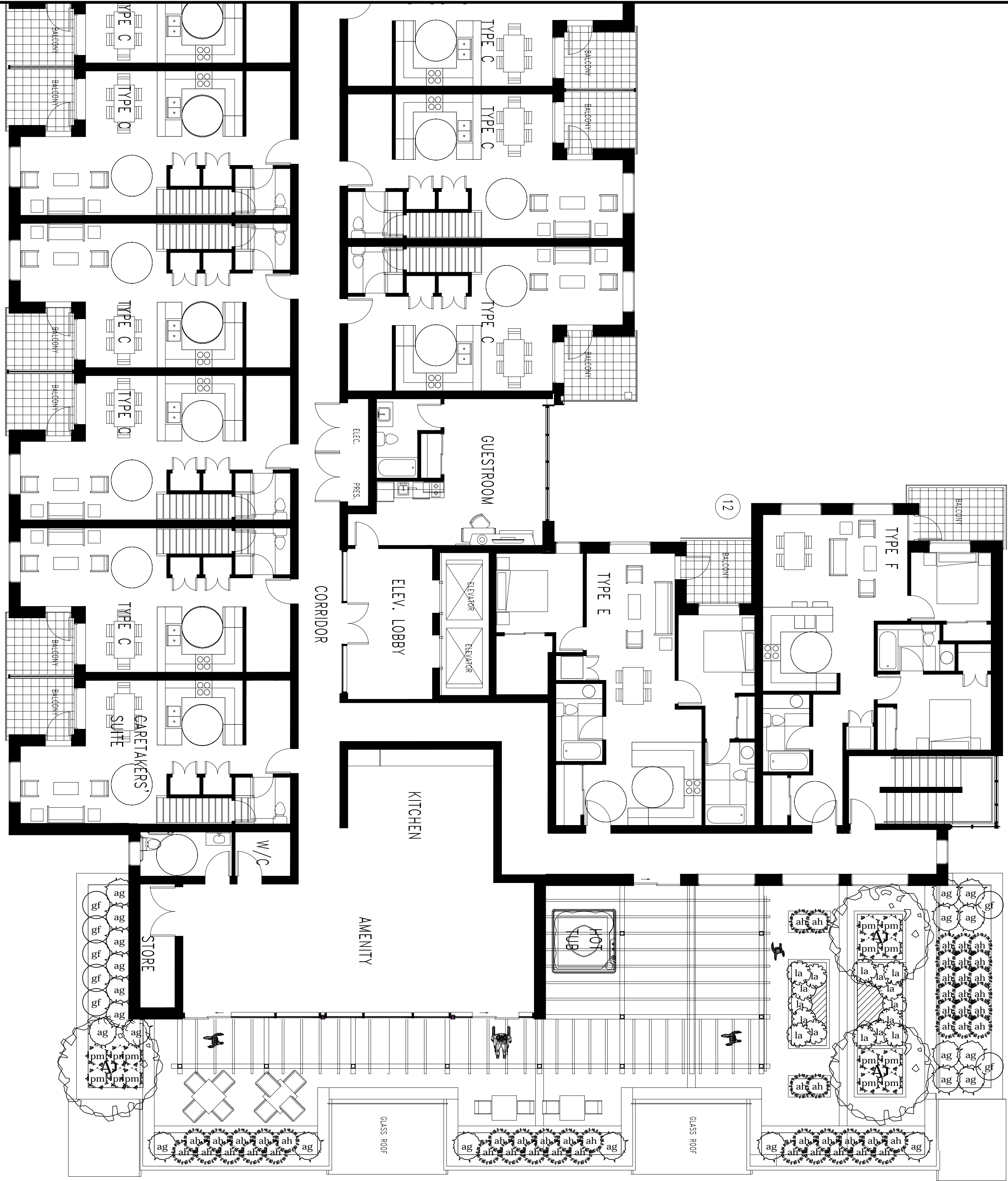
Drawing No. L4- 03

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01  
L4-04

PLANTING PLAN - EIGHTH FLOOR PATIO

SCALE 1:100



DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
2024- 04- 05	D.P.	M
2024- 04- 18	A.D.P.	N
2024- 04- 29	D.P.	O

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Consultants

Architectural: JMArchitecture Inc.  
Arborist: -  
Civil:  
Structural:  
Mechanical:  
Electrical:

**DV SD** Donald V. S. Duncan  
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dvsduncan@gmail.com

Maple Ridge #  
2018-255- RZ/DP

Revision	Project Leader
	D.Duncan
Revision	Drawn
	D.Duncan

prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**PLANTING PLAN**

Check Scale (may be photo reduced)  
0 1inch 0 10mm

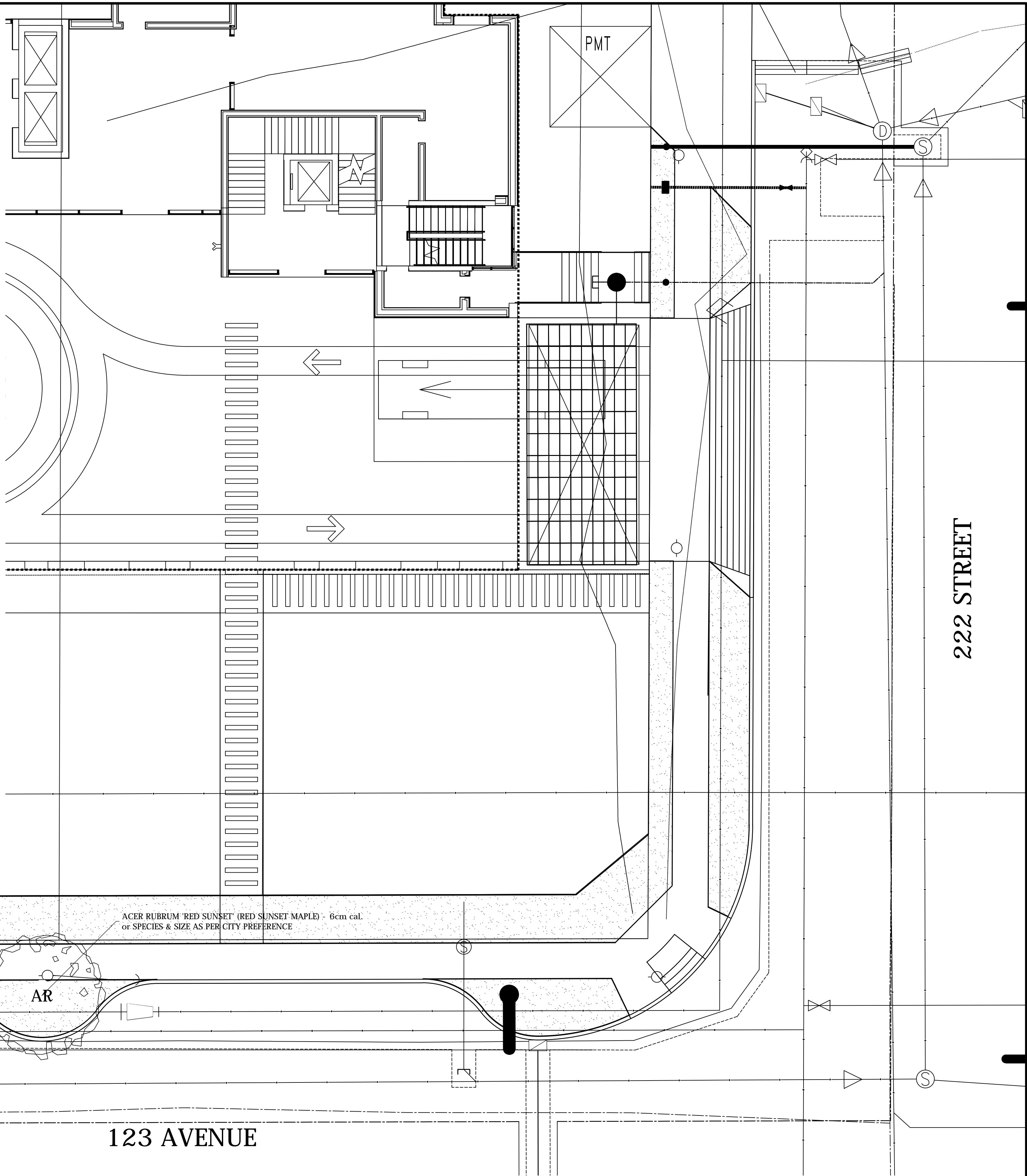
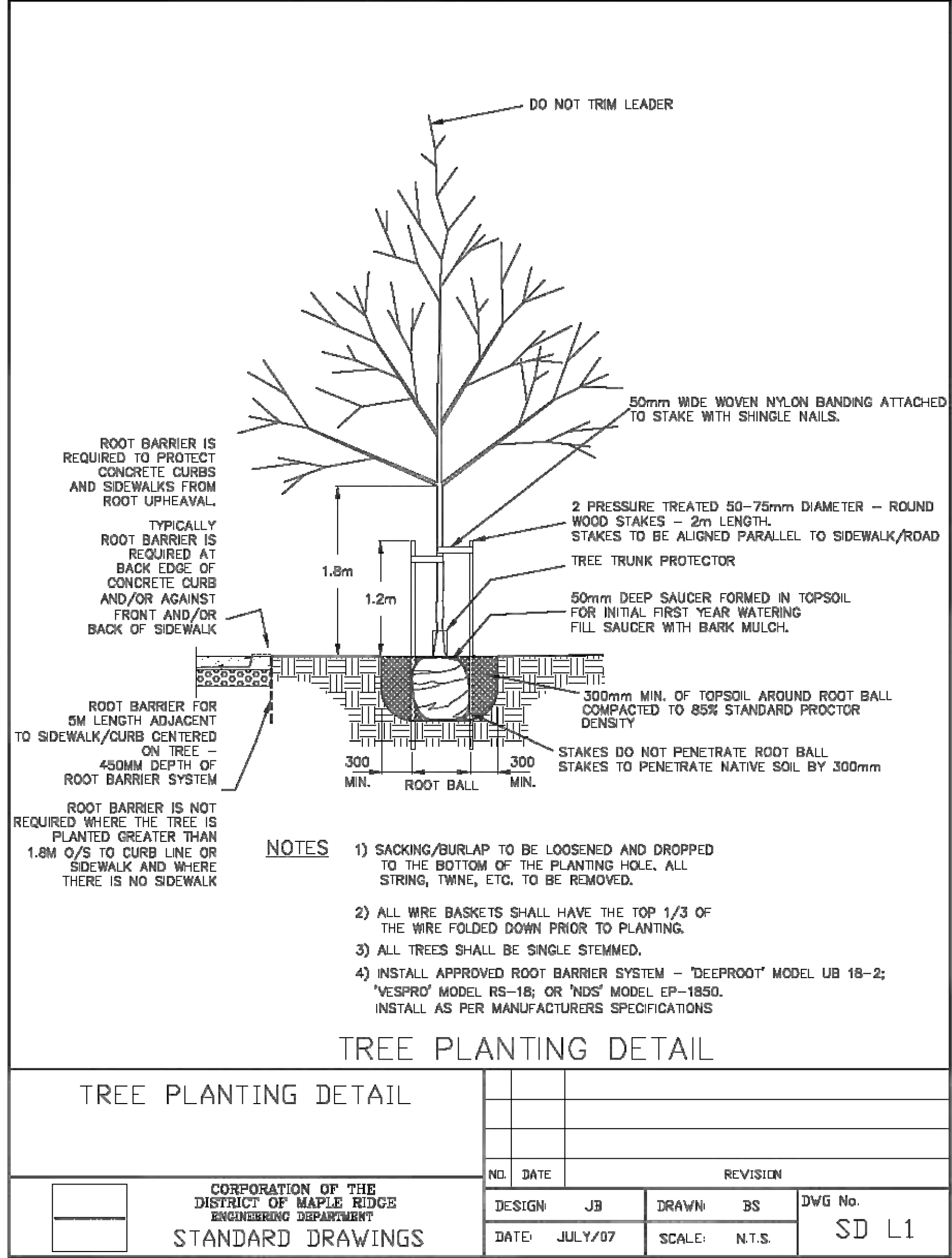
Project No. 20-0071

Drawing No. L4-04



PLOT DATE: June 26, 2024 TIME: 6:32 PM FULL PATH AND FILENAME: D:\PROJECTS\20-0071 222 ST MR 500- DELIV\LAND 01 - TB L4- 05.DWG PLOTSTYLE TABLE: -----

Schedule "A"  
Street Tree Planting Detail



DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
2024- 04- 05	D.P.	M
2024- 04- 18	A.D.P.	N
2024- 04- 29	D.P.	O
2024- 06- 13	D.P.	P
2024- 06- 26	D.P.	Q

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Civil:  
Structural:  
Mechanical:  
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Maple Ridge #  
2018-255- RZ/DP

Revision	Project Leader
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Revision	Drawn
	D.Duncan

prepared for:  
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12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**STREET TREE PLAN**

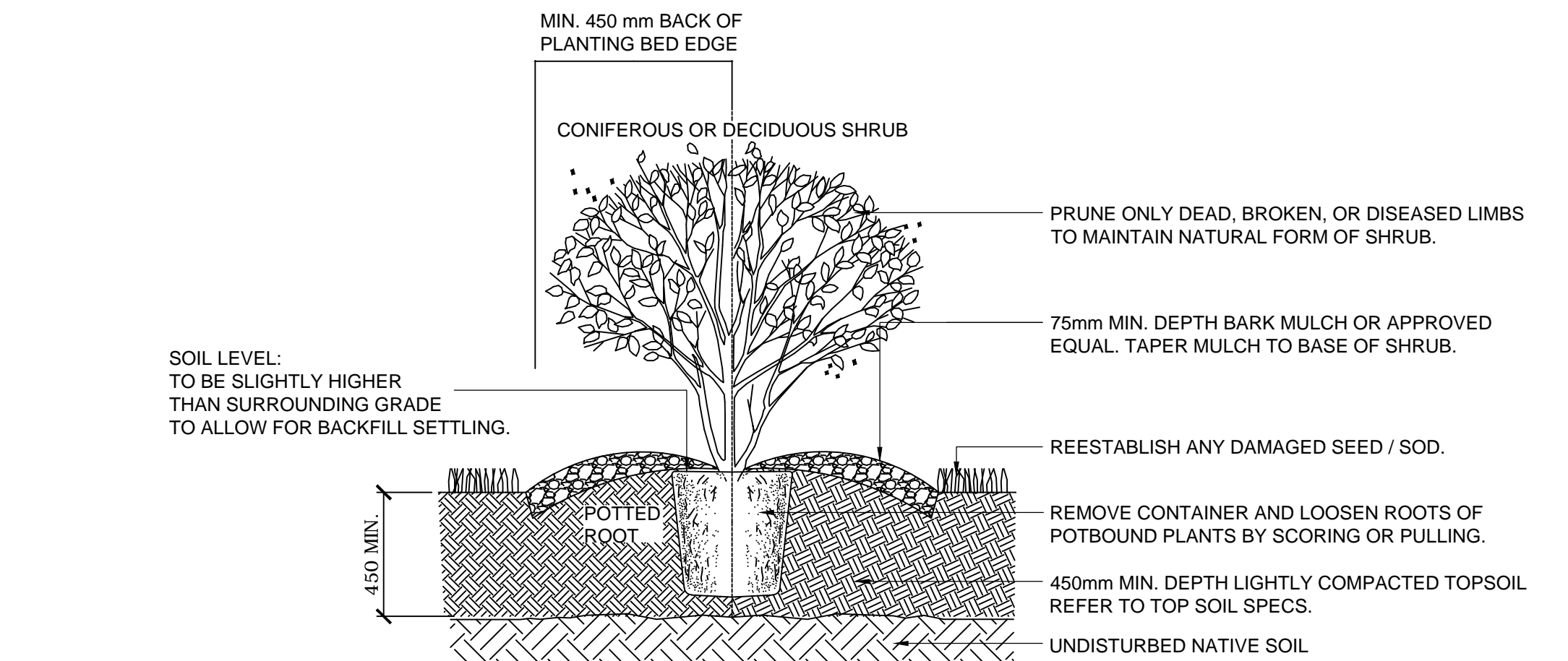
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Project No. 20-0071

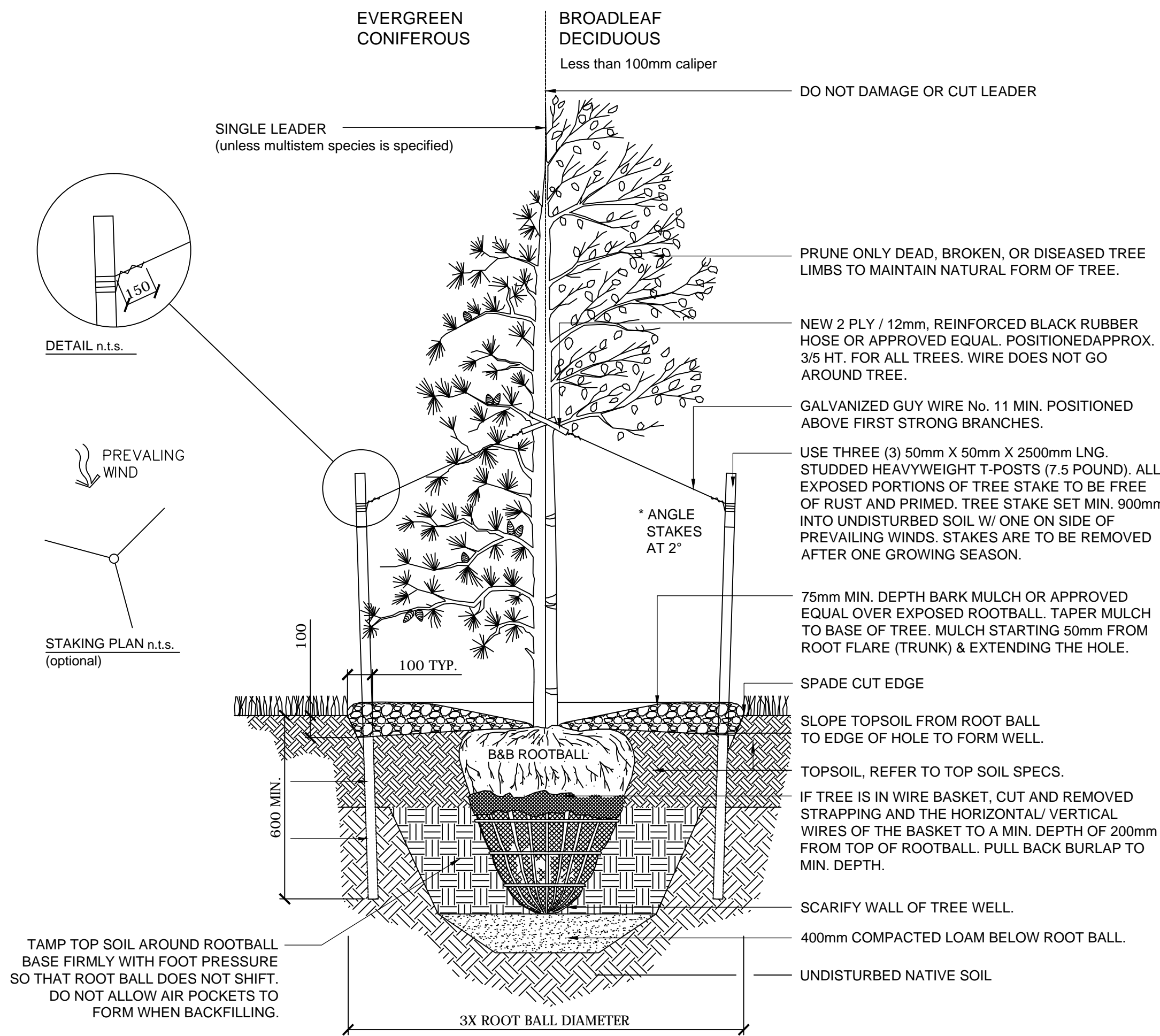
Drawing No. L4- 05

01  
L4- 05  
PLANTING PLAN - PUBLIC BOULEVARD  
SCALE 1:100

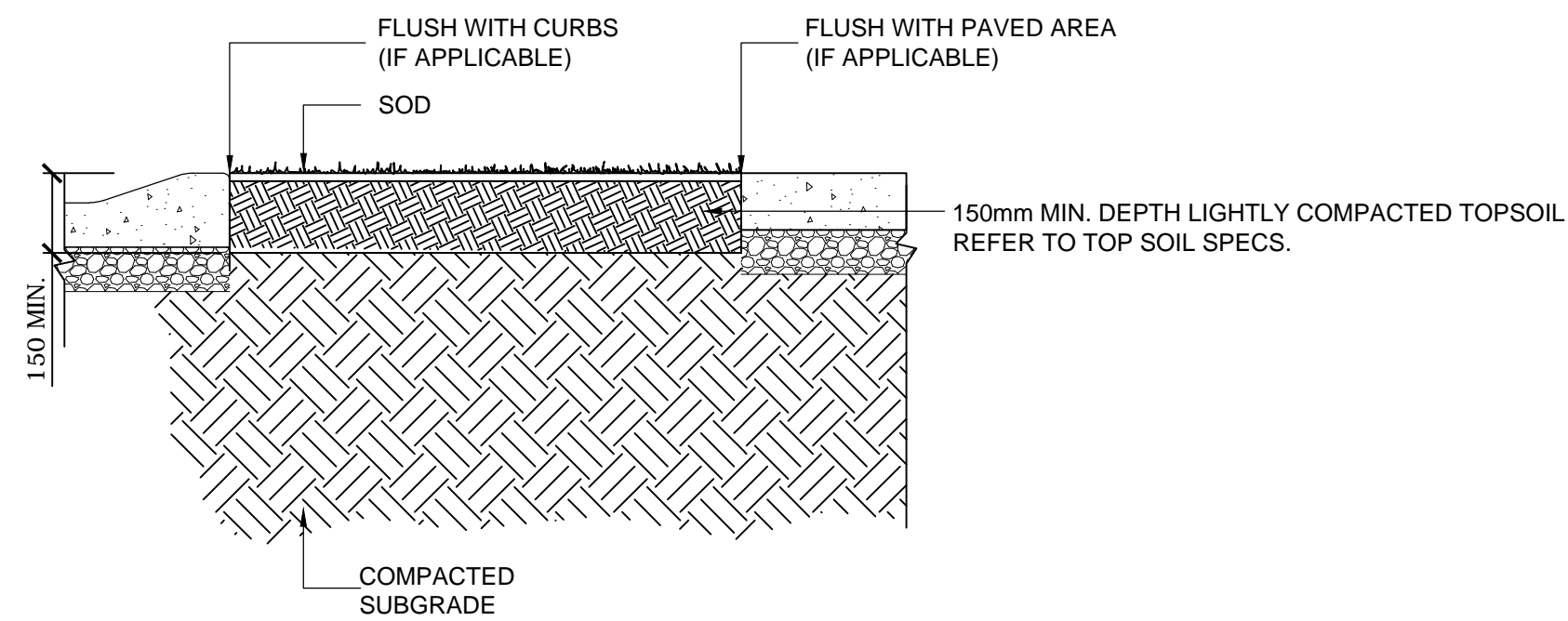
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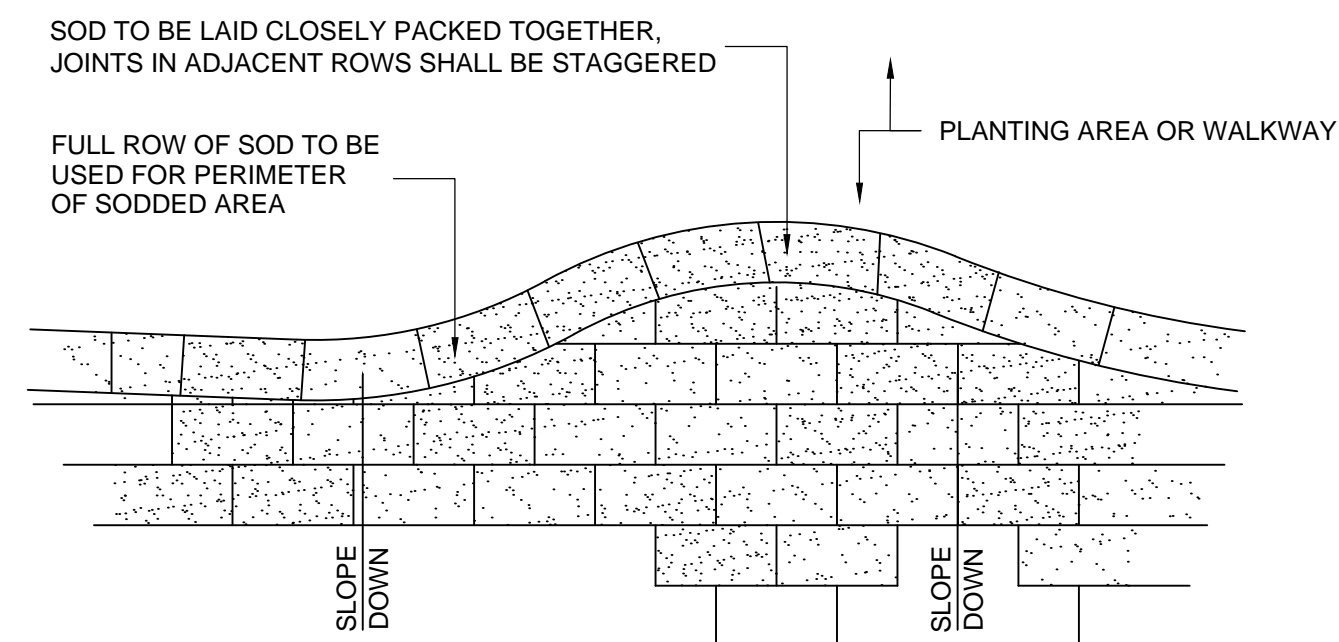
01 SHRUB INSTALLATION  
L5-01 SCALE 1:20



02 TREE INSTALLATION  
L5-01 SCALE 1:20

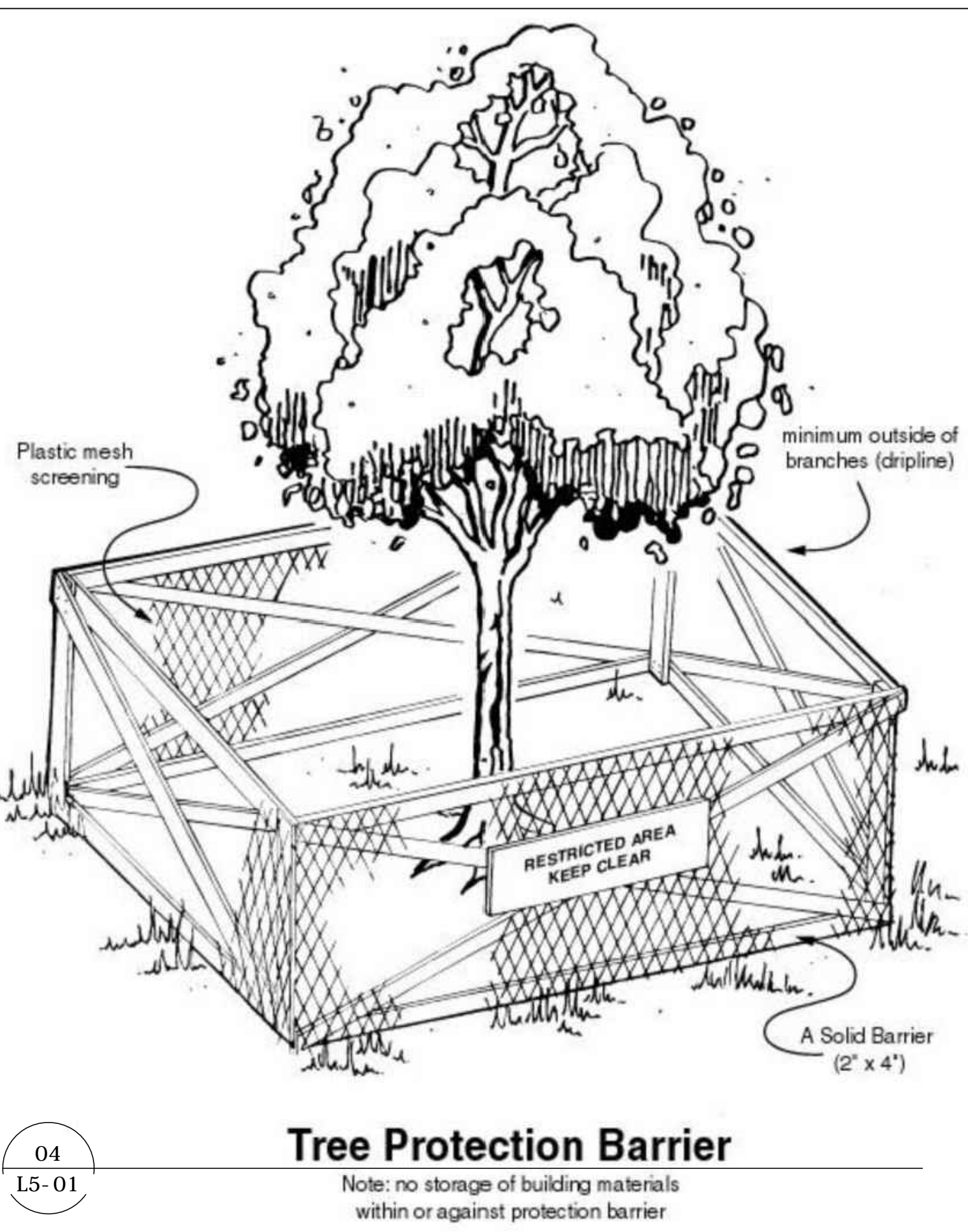


SECTION THROUGH SOD



PLAN VIEW OF SOD LAYOUT AND EDGING  
NOT TO SCALE

03 GRASS INSTALLATION  
L5-01 SCALE 1:20



04  
L5-01

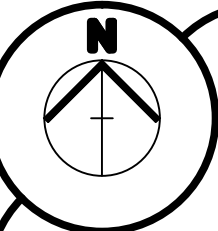
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2024- 04- 18	A.D.P.	N
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Maple Ridge #  
2018-255- RZ/DP

Project Leader	D.Duncan
Drawn	D.Duncan

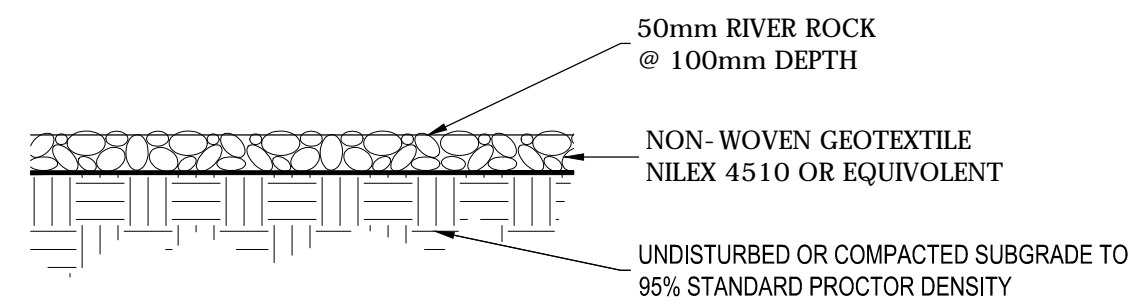
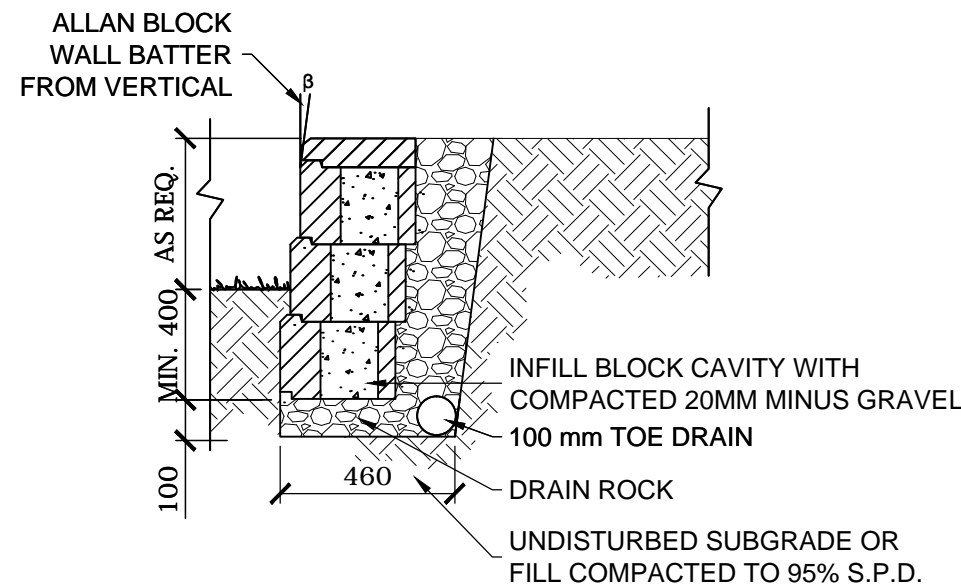
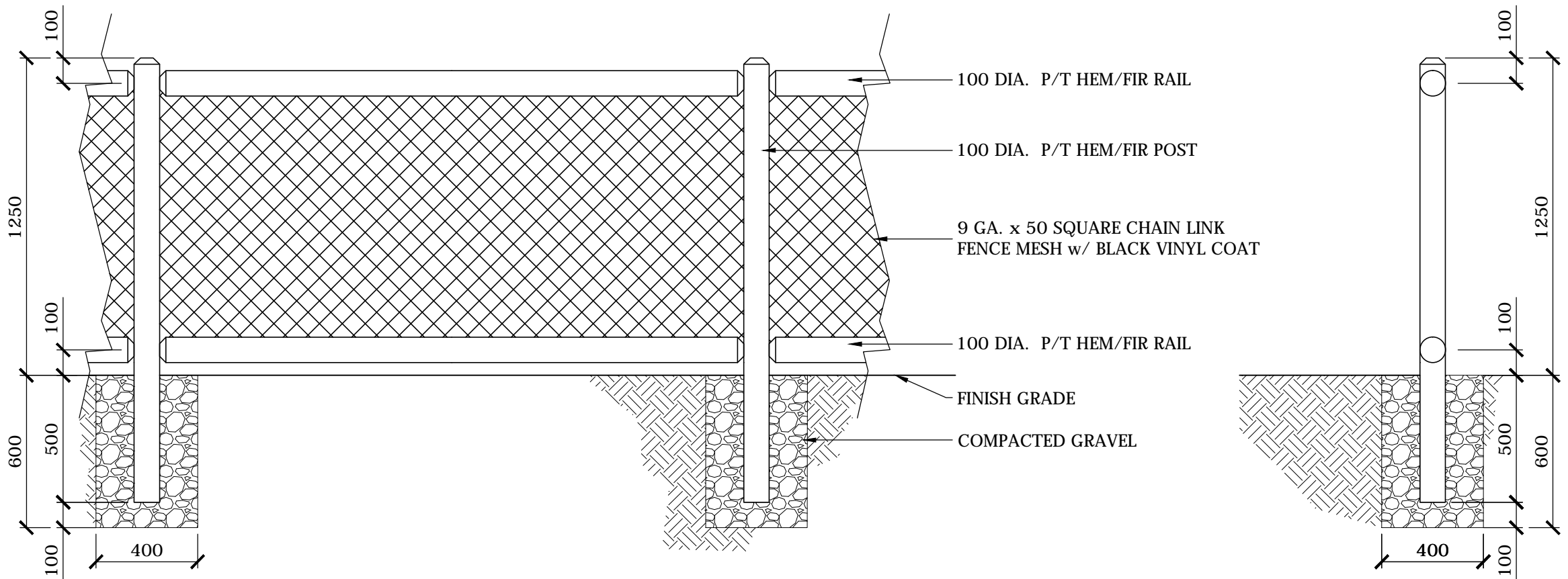
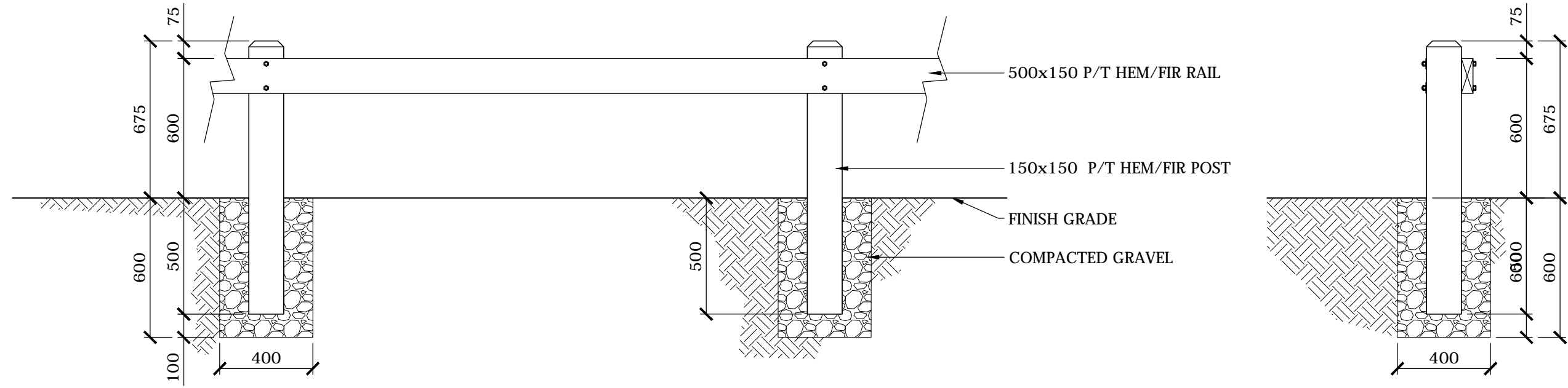
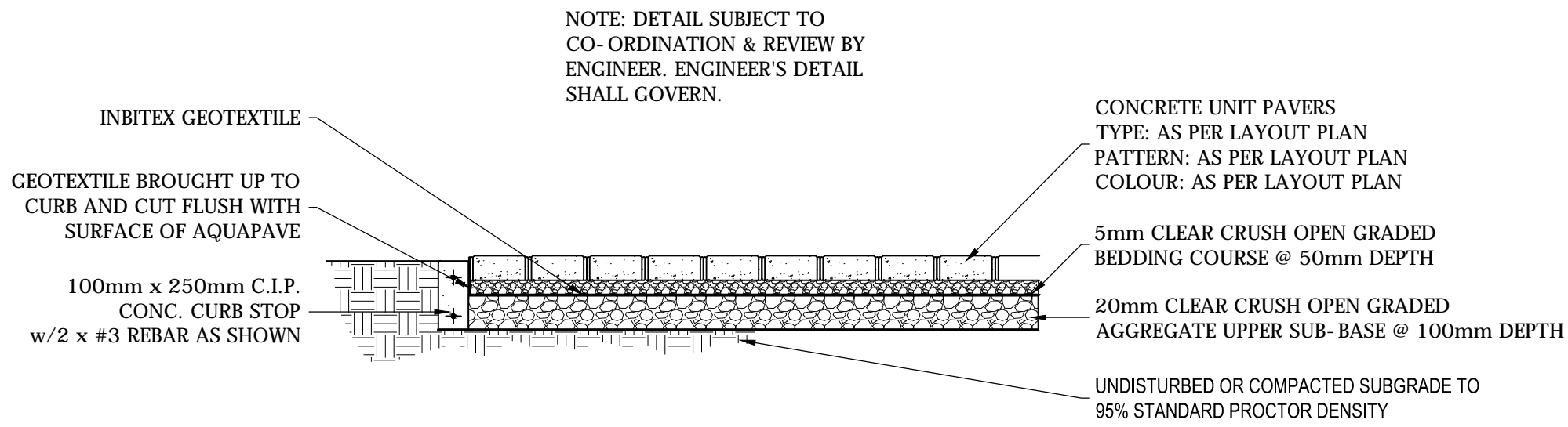
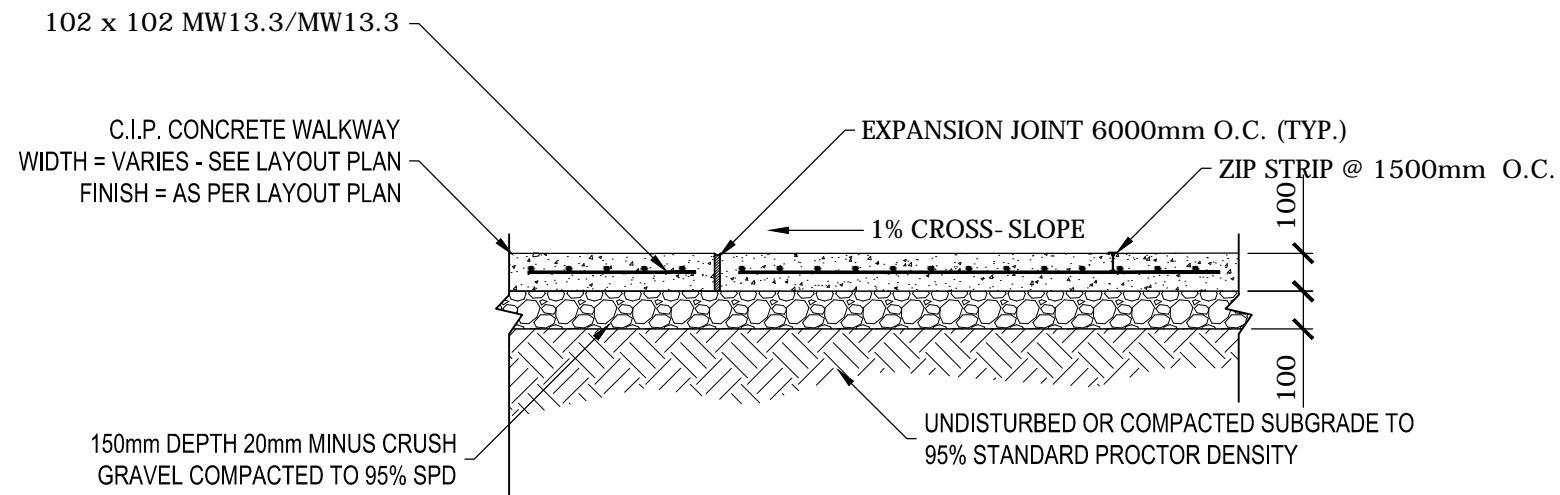
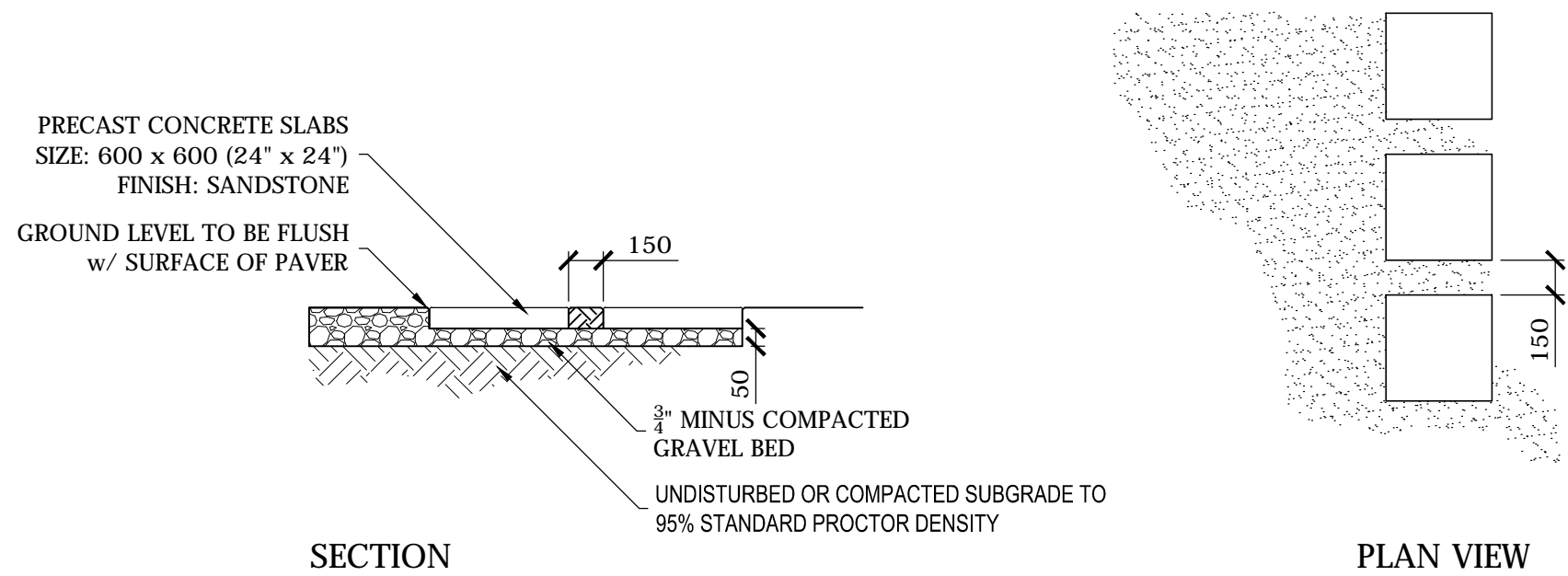
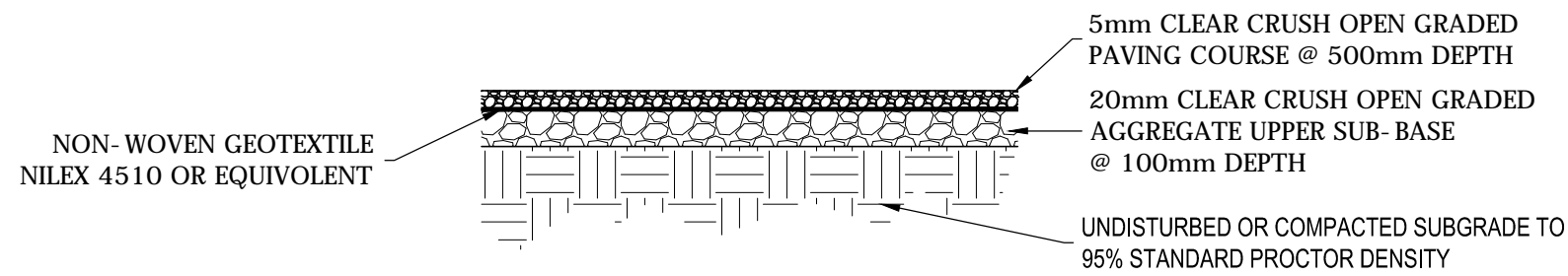
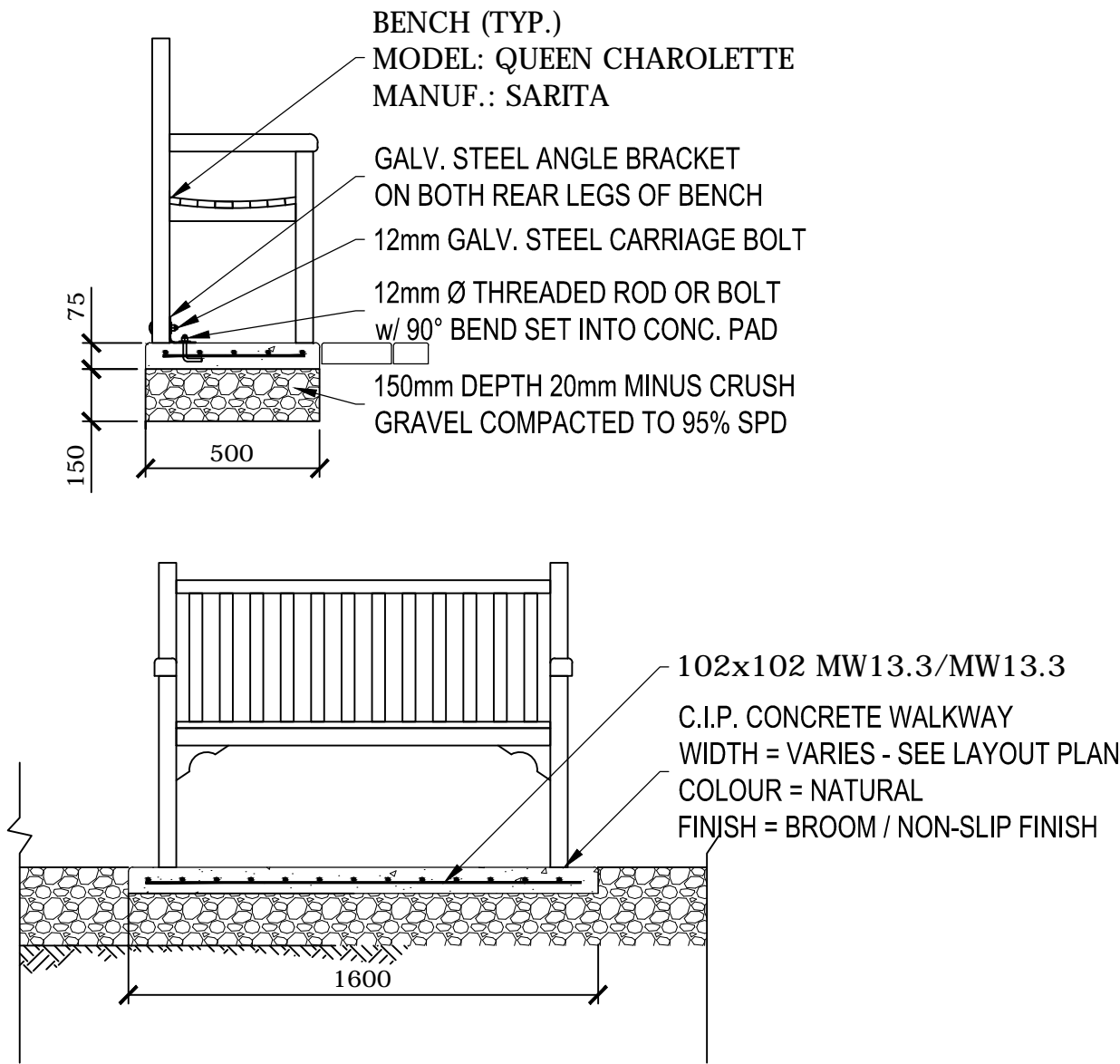
prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC  
Drawing Title  
**DETAILS**

Check Scale (may be photo reduced)  
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Project No. 20-0071  
Drawing No. L5-01



PLOT DATE: June 26, 2024 TIME: 6:37 PM FULL PATH AND FILENAME: D:\PROJECTS\20-0071 222 ST MR 500- DELIV\LAND 01 - TB L5- 02.DWG PLOTSTYLE TABLE: ----



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2024- 04- 05	D.P.	M
2024- 04- 18	A.D.P.	N
2024- 04- 29	D.P.	O
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Civil:	-
Structural:	-
Mechanical:	-
Electrical:	-

**DV SD Donald V. S. Duncan**  
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Maple Ridge #  
2018- 255- RZ/DP

Revision	Project Leader
	D.Duncan
Revision	Drawn
	D.Duncan

prepared for:  
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12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**DETAILS**

Check Scale (may be photo reduced)  
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Project No. 20- 0071

Drawing No.  
**L5- 02**



PLOT DATE: June 26, 2024 TIME: 6:38 PM FULL PATH AND FILENAME: D:\PROJECTS\20-0071 222 ST MR 500- DELIV\LAND 01 - TB\L5- 03.DWG PLOTSYLE TABLE: - - - -



01  
L5- 03

KOMPAN PLAY HUT NRO404  
N.T.S.



02  
L5- 03

SUTTLE RECREATION CLIMBER  
N.T.S.

DATE	ISSUED FOR	REV
2023- 11- 23	D.P.	L
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Maple Ridge #  
2018- 255- RZ/DP

Revision	Project Leader
	D.Duncan
Revision	Drawn
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prepared for:  
**Residential Development**  
12297 222 St/22175, 22185 123 Ave  
Maple Ridge BC

Drawing Title  
**DETAILS**

Check Scale (may be photo reduced)  
0 1inch 0 10mm

Project No. 20- 0071

Drawing No. L5- 03



**Advisory Design Panel's Resolution/Comments and Applicant's Responses – May 15, 2024**

Application 2020-309-DP in relation to the properties located at 12297 227 Street, 22175 123 Avenue, and 22185 123 Avenue was reviewed by the Advisory Design Panel (ADP) during their meeting on May 15, 2024. The ADP resolutions/comments and applicants' responses are provided below.

**Development Permit No: 2020-309-DP / 12297 222 ST, 22175 123 AVE, 22185 123 AVE**

Note: V. Nguyen recused herself from the agenda item due to a conflict of interest.

The Chair welcomed the project team to the meeting. The project team presented the development plans and answered questions from the Panel.

R/2024-ADP-012

It was moved and seconded

**That the following concerns be addressed and digital versions of revised drawings and memo be submitted to Planning staff; and further the Planning staff forward this on to the Advisory Design Panel for information and review.**

**CITY REQUESTED COMMENTS FROM ADP**

*Please comment on the overall form and character of this development and identify issues that the applicant should be aware of.*

**Architectural Comments:**

- Parking: Ensure parking level stairs/elevator vestibules conform to code. Some appear to meet code, and some have yet to be considered.
- Design rendering: Consider using a more realistic rendering to illustrate that the building is highly technical, as described. In particular, the cladding in the renderings appears as concrete, though they are described as fully solar panels, which makes it difficult to determine whether the building fits into the local urban fabric.

- Pedestrian access: Consider better pedestrian access to the front lobby of the building residences to encourage public transportation use.
- Lighting: Consider addressing the covered area in front of the recessed lobby with better lighting and a safer cross walk with center landscape median.
- Design rendering: Consider including a sample picture of the solar panels to provide a more realistic understanding of the materials. Also, consider using a more vibrant color of solar panel.
- Vehicle charging stations: Consider installing charge stations for electric vehicles with the anticipated excess electricity generated by the solar panels.

**Landscape Comments:**

- Overall landscape: Consider alignment of the swale/planting/and post/rail fence. Some areas of the fence cut through the swale.



## JM Architecture Inc.

Registered in BC - AB  
Joe M. Minten - principal  
Architect AIBC | AAA | MRAIC

29 May 2024

City of Maple Ridge  
11995 Haney Place  
Maple Ridge, B.C.  
Planning Department

Attention: Maryam Lotfi – Planner  
Re: 2019-255 RZ

Please accept this itemized response to the Advisory Design Panels review comments of May 15, 2024  
We trust this letter and the accompanying architecture and landscape plans address the intent of the review comments.

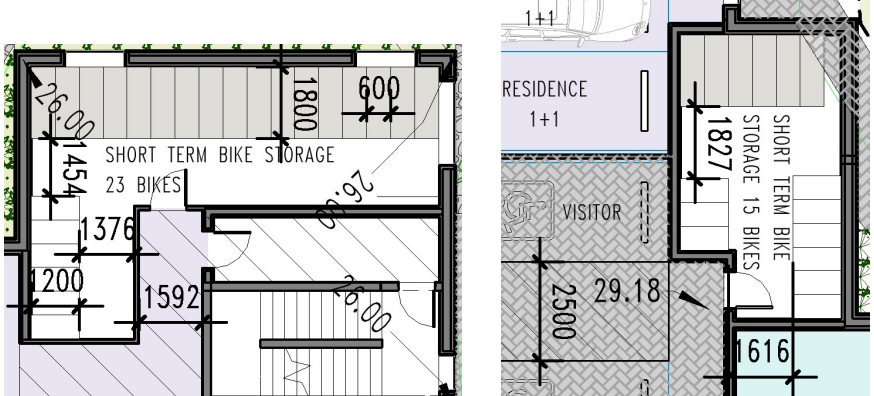
ADP Comments		May 15, 2024	File No.2019- 255-RZ
Architectural Comments			
Item No.	City of Maple Ridge Comment	Design Team Response	Reference Document
A	Parking: Ensure parking level stairs/elevator vestibules conform to code. Some appear to meet the code, and some have yet to be considered.	<b>JMA:</b> Parking level stair & elevator vestibules are code compliant.	Refer to Architectural sheets # DP-102 & DP-102_P1 & P2 Floor Plan
B	Design rendering: Consider using a more realistic rendering to illustrate that the building is highly technical as described. In particular, the cladding in the renderings appear as concrete, though they are described as fully solar panel, which makes it difficult to determine whether the building fits into the local	<b>JMA:</b> As instructed by our client, the work for the 3d-renders is in progress and you should have the renderings sometime next week.	

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joe@jmarchitecture.ca- E  
[www.jmarchitecture.ca](http://www.jmarchitecture.ca)

	urban fabric.		
C	Pedestrian access: Consider better pedestrian access to the front lobby of the building residences to encourage public transportation use.	<b>JMA:</b> Sidewalks have been proposed from 222 St. and 123 Ave leading to the daycare entrance & the residence main entrance lobby.	Please refer to Architectural drawing sheet # DP-100A_Site Plan
D	Lighting: Consider addressing the covered area in front of the recessed lobby with better lighting and a safer cross walk with center landscape median	<b>JMA:</b> Noted. An efficient and effective lighting plan for the covered drive aisle will be proposed at the building permit stage.	
E	Design rendering: Consider including a sample picture of the solar panels to provide a more realistic understanding of the materials. Also, consider using a more vibrant color of solar panel.	<p><b>JMA:</b> Please use the links below to refer to more realistic images of solar cladding and solar railing used in different parts of Canada by Mitrex, the solar cladding company for the project.</p> <p>For Solar Cladding:</p> <p><a href="https://www.mitrex.com/projects/capital-towers">https://www.mitrex.com/projects/capital-towers</a></p> <p><a href="https://www.mitrex.com/projects/hospital-in-alberta">https://www.mitrex.com/projects/hospital-in-alberta</a></p> <p><a href="https://www.mitrex.com/projects/carrtera-avonhead">https://www.mitrex.com/projects/carrtera-avonhead</a></p> <p><a href="https://www.mitrex.com/projects/midland-courthouse">https://www.mitrex.com/projects/midland-courthouse</a></p> <p><a href="https://www.mitrex.com/projects/1451-wellington-st">https://www.mitrex.com/projects/1451-wellington-st</a></p> <p>For Solar Railing:</p> <p><a href="https://www.mitrex.com/projects/1154-wilson-ave">https://www.mitrex.com/projects/1154-wilson-ave</a></p>	
F	Vehicle charging stations: Consider installing charge stations for electric vehicles with the	<b>JMA:</b> Noted. We will review this in detail at a later stage.	



	anticipated excess electricity generated by the solar panels.		
<b>Landscape Comments</b>			
H	Overall landscape: Consider alignment of the swale/planting/and post/rail fence. Some areas of the fence cut through the swale.	<b>DVS:</b> The alignment of the infiltration swale will be adjusted to avoid contact with the fence.	
I	Site Permeability: Designs suggest that the ground materials are all permeable. Consider providing design to the granular pathway and a more attractive approach to the youth recreation area.	<b>DVS:</b> The current alignment of the trail, which sits against the foundation of the building, is mandated by Planning and intended to minimize impact of the areas to be naturalized. If Planning revises its directive and prefers that the trail be moved away from the building foundation and given a more meandering character, as proposed in our previous design, our design will be amended accordingly.	
J	Overall landscape: Consider working with the City, within their restrictions, to improve the north landscape site as it is very dull and there is room for improvement.	<b>DVS:</b> The landscape architect will entertain any substantive suggestions the City wishes to make regarding the north landscape. He will make himself available for discussions at the convenience of city staff.	
K	Youth recreation area: Consider improvements as there is very little play value there and a lot of lost opportunity.	<b>DVS:</b> The recreation area was designed based on the discussions with Planning that appeared to request an absolutely minimal installation of structures in this area. The landscape architect will entertain any substantive suggestions the City wishes to make regarding the recreation area. He will make himself available for discussions at the convenience of city staff.	

L	Front driveway entrance round-about: The spiral pattern is a little on point. Consider using different paving materials and/or including another small plant bed.	<b>DVS:</b> Additional colours will be added to the paving pattern to accentuate the design.	
M	Bike Racks: Consider whether the front entrance bike rack area is tight.	<p><b>DVS:</b> The location of the bike racks will be reviewed but do meet current requirements for clearances.</p> <p><b>JMA:</b> Please refer to the detailed measurements shown in the images below. All the short-term bike parking spaces adhere to zoning bylaws. Also, there are a couple of bicycle parking stalls near the daycare entrance.</p>	Refer to Architectural Drawing Sheet # DP-100A_Site Plan.
			
N	Outdoor seating: Consider whether to use material other than wood benches as they cannot be secured which may be a concern in a rental building.	<b>DVS:</b> The wooden benches will be secured with two steel angle brackets as per detail 01/L5-02 of the submitted set.	
O	Pathway materials: Consider whether to use a smaller granular material such as pea gravel rather than river rock as the path will have movement and larger rocks may be difficult to walk on.	<b>DVS:</b> No pathways are specified as river rock. Pathways will be constructed of 5mm minus clear crush over a 20mm clear crush base as per detail 02/L5-02 of the submitted set. The bands of river rock in the front lawn are ornamental elements edging planting beds. A central concrete walkway is provided through the bands leading from the 123 Avenue frontage to the autocourt.	



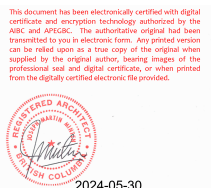
## JM Architecture Inc.

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Architect AIBC | AAA | MRAIC

P	Trees near the covenant: Once Fortis BC assigns a consultant to work with the project team, there will likely be some potential for trees outside of the 10 meter mark. Consider including trees in this area with the input of the Fortis BC consultant.	<b>DVS:</b> Previous information provided by and communications with Fortis have indicated that trees will not be allowed in this area. However, we will review the matter with Fortis again.	
Q	Plant list: The size of some of the species on the plant list appear to be out of proportion to availability, such as lavender pot size 5 seems large, and red and yellow dogwoods appear to be small.	<b>DVS:</b> Container sizes will be reviewed and adjusted as necessary.	
R	Daycare parking: Consider denoting a walkway from the daycare parking spaces to the front door of the daycare.	<b>DVS:</b> This issue will be addressed by architecture.	
S	Landscape materials: Consider introducing a coil form into the landscape.	<b>DVS:</b> Inclusion of such a form will be considered.	

Please confirm acceptance of this report by reply email.

Best Regards



Joe Minten Architect AIBC | AAA | MRAIC  
PRINCIPAL

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joe@jmarchitecture.ca- E  
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### **Advisory Design Panel's Resolution/Comments and Applicant's Responses – June 19, 2024**

Application 2020-309-DP in relation to the properties located at 12297 227 Street, 22175 123 Avenue, and 22185 123 Avenue was reviewed by the Advisory Design Panel (ADP) during their meeting on June 19, 2024. The ADP resolutions/comments and applicants' responses are provided below.

#### **6.1. Development Permit No: 2020-309-DP / 12297 222 ST, 22175 123 AVE, 22185 123 AVE**

Note: V. Nguyen recused herself from agenda item 7.1 due to a conflict of interest.

R/2024-ADP-015

It was moved and seconded

**That the following concerns be addressed, and digital versions of revised drawings & memo be submitted to Planning staff; and further the Planning staff forward this on to the Advisory Design Panel for information and review.**

#### **Architectural Comments:**

- Elevator Vestibule: Recommend including elevator vestibule in parking lot; ensure vestibules conform to code.
- Design Renderings: Recommend providing more realistic design renderings to provide a better sense of proportion.
- Roof Line Aesthetic: Recommend reconsidering roof line to follow current design trends.
- Solar Panels: Difficult to tell what the solar panels look like. Recommend including sample picture of solar panels to provide more realistic rendering of the materials. Further, reconsider monochromatic colour used for the solar panels by adding colour variation.
- Pedestrian Access to Front Lobby: Consider including median to allow pedestrians access to travel to the lobby.
- Lighting: Recommend giving more consideration for the recessed area to be lit properly.



**Landscape Comments:**

- Pedestrian Access to Daycare: Recommend a stronger pedestrian connection from vehicle drop off area to daycare.
- Front Driveway Entrance round-about: Consider making this area safer and more pedestrian friendly

~~27 June 2024~~

30 August 2024 - revised

City of Maple Ridge  
11995 Haney Place  
Maple Ridge, B.C.  
Planning DepartmentAttention: Maryam Lotfi – Planner  
Re: 2019-255 RZ

Please accept this itemized response to the Advisory Design Panels review comments of June 19, 2024  
We trust this letter and the accompanying architecture and landscape plans address the intent of the review comments.

ADP Comments		June 19, 2024	File No.2019-255-RZ
Architectural Comments			
Item No.	ADP Comments	Design Team Response	Reference Document
A	Elevator Vestibule: Recommend including an elevator vestibule in the parking lot; ensure vestibules conform to code.	<b>JMA:</b> Added. The main floor plan has been updated to accommodate an elevator vestibule lobby.	Refer to Architectural sheets # DP-100 & DP-101
B	Design Renderings: Recommend providing more realistic design renderings to provide a better sense of proportion.	<b>JMA: Revised renderings are attached to the ADP package. The option to combine brick and solar cladding is to establish a strong natural material at the base of the building and to accent the front façade. The solar paneling is proposed in matt and gloss finishes and is adopted for the high sustainability it provides in terms of energy contribution to the development.</b>	Provided imagery
C	Roof Line Aesthetic: Recommend reconsidering the roof line to follow current design trends.	<b>JMA: The roof slope is both a design decision and a program requirement from the owner to ensure the inclusion of a durable, long-lasting roof. It also supports the solar array at this level. There is also a roof deck facing south which supports an outdoor amenity space.</b>	
D	Solar Panels: Difficult to tell what the solar panels	<b>JMA: same as item #B</b>	



	look like. Recommend including sample pictures of solar panels to provide a more realistic rendering of the materials.		
E	Pedestrian Access to Front Lobby: Consider including a median to allow pedestrians access to travel to the lobby.	<b>DVS:</b> Drivers will have a direct line to the drop-off parking area and will be slowing to navigate a turn at this location. In view of the good visibility and anticipated low speed of travel, we anticipate that any pedestrian crossing the drive aisle in this location will be readily identified by drivers. Therefore, we consider a specific crosswalk to be unnecessary. A band of pedestrian paving, matching the sidewalk, now crosses the auto court. This should serve to remind drivers of the presence of pedestrians in the area.	
F	Lighting: Recommend giving more consideration for the recessed area to be lit properly.	<b>JMA:</b> Noted. An efficient and effective lighting plan for the covered drive aisle will be proposed at the building permit stage.	
<b>Landscape Comments</b>			
H	Pedestrian Access to Daycare: Recommend a stronger pedestrian connection from the vehicle drop-off area to daycare.	<b>DVS:</b> Drivers will have a direct line to the dropoff parking area and will be slowing to navigate a turn at this location. In view of the good visibility and anticipated low speed of travel, we anticipate that any pedestrian crossing the drive aisle in this location will be readily identified by drivers. Therefore, we consider a specific crosswalk to be unnecessary.	
I	Front Driveway Entrance round-about: Consider making this area safer and more pedestrian-friendly.	<b>DVS:</b> A band of pedestrian paving, matching the sidewalk, now crosses the auto court. This should serve to remind drivers of the presence of pedestrians in the area.	

Please confirm acceptance of this report by reply email.

Best Regards

Joe Minten Architect AIBC | AAA | MRAIC  
PRINCIPAL

This document has been electronically certified with digital certificate and encryption technology authorized by the AIBC and APECBC. The authoritative original had been transmitted to you in electronic form. Any printed version can be relied upon as a true copy of the original when supplied by the original author, bearing images of the professional seal and digital certificate, or when printed from the digitally certified electronic file provided.



2024-08-30

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### **Advisory Design Panel's Resolution/Comments and Applicant's Responses – September 18, 2024**

Application 2020-309-DP in relation to the properties located at 12297 227 Street, 22175 123 Avenue, and 22185 123 Avenue was reviewed by the Advisory Design Panel (ADP) during their meeting on September 18, 2024. The ADP resolutions/comments and applicants' responses from the architect in [blue](#) are provided below.

#### **7.1. Development Permit No: 2020-309-DP/ 12297 222 ST, 22175 123 AVE, 22185 123 AVE R/2024-ADP-024**

It was moved and seconded

**That the application be supported as presented and the applicant proceed to Council.**

#### **CITY REQUESTED COMMENTS FROM ADP**

##### **Architectural Comments:**

- Consider introducing another shade of copper siding for more variation. [We can consider a variation in colouring dependent upon the availability from the manufacturer. We will do that at the building permit application submission knowing that the package will also be vetted through the planning department. At best these would be slight variations only and dependent very much on the cost to alter the manufacturing of the solar cladding system.](#)
- Reconsider playground structure size: playground structure appears too large for the area depicted. [The design of the playground equipment was scrutinized at that time.](#)
- A more modern style would be beneficial, though there may be limitations due to the buildings intended use. [The architectural expression of the building will not change at this time.](#)

##### **Landscape Comments:**

The panel had no further landscaping comments.



**PUBLIC INFORMATION MEETING REPORT**  
**JUNE 13 2024**  
**130-22470 Dewdney Trunk Road Maple Ridge**

The public Information meeting for 12297 222 Street and 22175 & 22185 123 Avenue No. 2019-255 RZ was held at the Maple Ridge Public Library on June 13 2024 from 5:30 to 7:45 PM.

In Attendance for the development were [ ] from Simplex Meadows Development Ltd., [ ] from JM Architecture, [ ] [ ] from Donald VS Duncan Development Consultant Landscape Architect, [ ] from Quad Lock Air Foam, and [ ] [ ] from Civic Visions Inc.

We presented 17 foam display boards showing all aspects of this development and landscape profile.

It showed the complexity of building on this site and addressed the need for the significant riparian setback areas required to meet Provincial and Civic requirements.

The foam insulation sample was presented to show how this development plans to achieve the superior R-Value of insulation and how it will resist the conductive flow of heat.

Samples of the Solar panels were presented to show how this development will be clad with the ability to generate vast sources of clean energy.

Eleven members of the public signed in, representing 13 people.

Seven written comments were received. Five of the comments were in opposition with four of those comments being opposed to the height of the building.

One comment expressed concern related to parking, increased traffic and crime.

Two of the comments were strongly in favor of this development, with one of the two expressing a need to house “people with needs and limitations physically and financially.”

Another supporter was pleased with how we were “protecting the natural features on the site” she went on to state that, “I hope Maple Ridge approves this development ASAP.”

Those expressing negative concerns lived in close proximity to the development.

Please contact me, should you require anything further pertaining to this engagement.

Civic Visions Inc



**CITY OF MAPLE RIDGE  
DEVELOPMENT PERMIT NO. 2020-309-DP  
MULTI-FAMILY**

**TO: JM ARCHITECTURE JOSEPH M MINTON,  
15243 91 AVENUE  
SURREY BC V3R 8P8**  
(the "Permittee")

1. This Development Permit (the "Permit") is issued subject to compliance with all the Bylaws of the City of Maple Ridge (the "City") applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Permit applies to, and only to, those lands within the City described below and any and all buildings, structures, and other development thereon:

**LOT 1 DISTRICT LOT 396 GROUP 1 NEW WESTMINSTER DISTRICT PLAN EPP124350  
(PID: 032-401-850)**  
(the "Lands")

3. As a condition of the issuance of this Permit, Council will be holding the security set out below to ensure that development, including landscape works is carried out in accordance with the terms and conditions of this Permit. The condition of the posting of the security is that should the Permittee fail to carry out the development hereby authorized, according to the terms and conditions of the Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors. Any surplus shall be paid over to the Permittee upon verification by the Director of Planning of the City, or their designate, that the development has been completed in accordance with the terms and conditions of this Permit. There will be filed accordingly:
  - a) an Irrevocable Letter of Credit in the amount of \$107, 599.21; or
  - b) a Certified Cheque in the amount of \$107, 599.21.
4. The Lands described herein shall be developed strictly in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

5. If the Permittee does not substantially commence the development permitted by this Permit within 2 years of issuance, this Permit shall lapse.
6. In the event that this Permit lapses, the Permittee may request refund of the security described in Paragraph 3, and the City shall make such refund.
7. This Permit is not a Building Permit or a Tree Permit.
8. Nothing set out in this Permit, nor the issuance of this Permit by the City, is intended to interfere with the statutory or common law property rights of any third parties and this Permit will not in any way restrict or release the Permittee from any obligations they may owe to any third parties in relation to such statutory or common law property rights.
9. Any construction, development, or other actions that the Permittee undertakes in relation to this Permit must be done and completed in such a manner that does not violate the statutory or common law property rights of any third parties, and the Permittee will assume full and sole responsibility for resolving any third party issues or disputes over statutory or common law property rights that may arise in relation to such construction, development, or other actions taken by the Permittee. Any proposed resolutions of third-party issues or disputes by the Permittee will be entirely independent from the City's approval process and the City will not be involved in any such resolutions.
10. The City will not be held liable or responsible in any way if the Permittee does not comply with any obligations they may owe any third parties in relation to statutory or common law property rights.
11. The Permittee acknowledges and agrees that upon issuance of this Permit by the City to the Permittee, the Permittee agrees to release and indemnify the City and its employees, directors, agents, servants, and officers, from and against all actions, claims, demands, proceedings, suits, losses, damages, costs and expenses (including any such ongoing costs or expenses as they come due) of whatsoever kind or nature, including, without limiting the generality of the foregoing, in respect of any death, loss, injury, or damage to any person or property, that may arise in any way out of or in connection with the issuance and use of this Permit.



**ISSUED** on the                      day of                      , 2025.

---

**CORPORATE OFFICER**

The following attachments are attached hereto:

Appendix A: Location Map

Appendix B: Aerial Photo

Appendix C: Proposed Architectural Plans

Appendix D: Proposed Landscape Plans

## Transit-Oriented Areas: Land Use Planning Process Update

### Recommendation:

**THAT Council receive the report titled “Transit-Oriented Areas: Land Use Planning Process Update” dated May 20, 2025, for Information.**

### Report Purpose and Summary Statement:

This report presents initial land use concepts for the three Transit-Oriented Areas in Maple Ridge.

### Previous Council Action:

On June 25, 2024, Council endorsed the Transit-Oriented Areas Land Use Planning Process.

### Strategic Alignment:

Liveable Community;

### Communications:

A public Open House, to present the initial land use concepts is scheduled to occur in early June. It will be advertised on the City’s website, through social media platforms, and in the local newspaper.

### Applicable Legislation/ Bylaw/Policy:

*Housing Statutes (Transit-Oriented Areas) Amendment Act, 2023 (Bill 47)*



## **Transit-Oriented Areas: Land Use Planning Process Update**

### **BACKGROUND:**

#### **Provincial Legislation:**

On November 30, 2023 the Provincial Government gave Royal Assent to *2023 Housing Statutes (Transit-Oriented Areas) Amendment Act* (Bill 47). This legislation required municipalities to designate, by bylaw, 'Transit-Oriented Areas' near transit hubs by June 30, 2024.

At the [June 4, 2024 Committee of the Whole Meeting](#), Council considered and introduced the *Maple Ridge Transit-Oriented Area Designation Bylaw No. 8001-2024* to designate three areas as 'Transit-Oriented Areas' (TOAs) as required by Provincial regulation. The City's [Transit-Oriented Area Designation Bylaw](#) was adopted by Council on June 25, 2024, and identifies the following as TOAs:

- Haney Transit Exchange,
- Port Haney West Coast Express Station, and
- Maple Meadows West Coast Express Station.

*Bylaw No. 8001-2024* does not modify the land use designations of the identified properties, it simply contains the maps showing the boundaries of each TOA. It does not impact the zoning or land use designations of the properties identified within the TOA.

The Provincial legislation restricts a local government's abilities to require residential off-street parking, except for parking for people with disabilities, within those TOAs. Associated amendments to both the City's Zoning Bylaw and Off-Street Parking and Loading Bylaw were also brought forward at the same Council meetings and came into effect by June 30, 2024.

#### **Transit-Oriented Areas:**

TOAs are defined as areas within a prescribed distance from a transit station, and are intended to be developed as areas of mixed-use, complete communities. With Bill 47, the Province introduced development density requirements within the TOAs, which apply to parcels that are partially or wholly within a 400 m catchment area surrounding the identified Transit Hub. The requirements only apply to those parcels that were zoned at the time of passing to allow residential uses. This includes mixed-use zones where commercial, employment, institutional, or other uses are permitted alongside residential uses.

For the three TOAs within Maple Ridge (Figure 1, Attachment A), the prescribed densities within 200 m and 400 m of the Transit Hubs are summarized in Table 1.

Figure 1. Maple Ridge TOAs

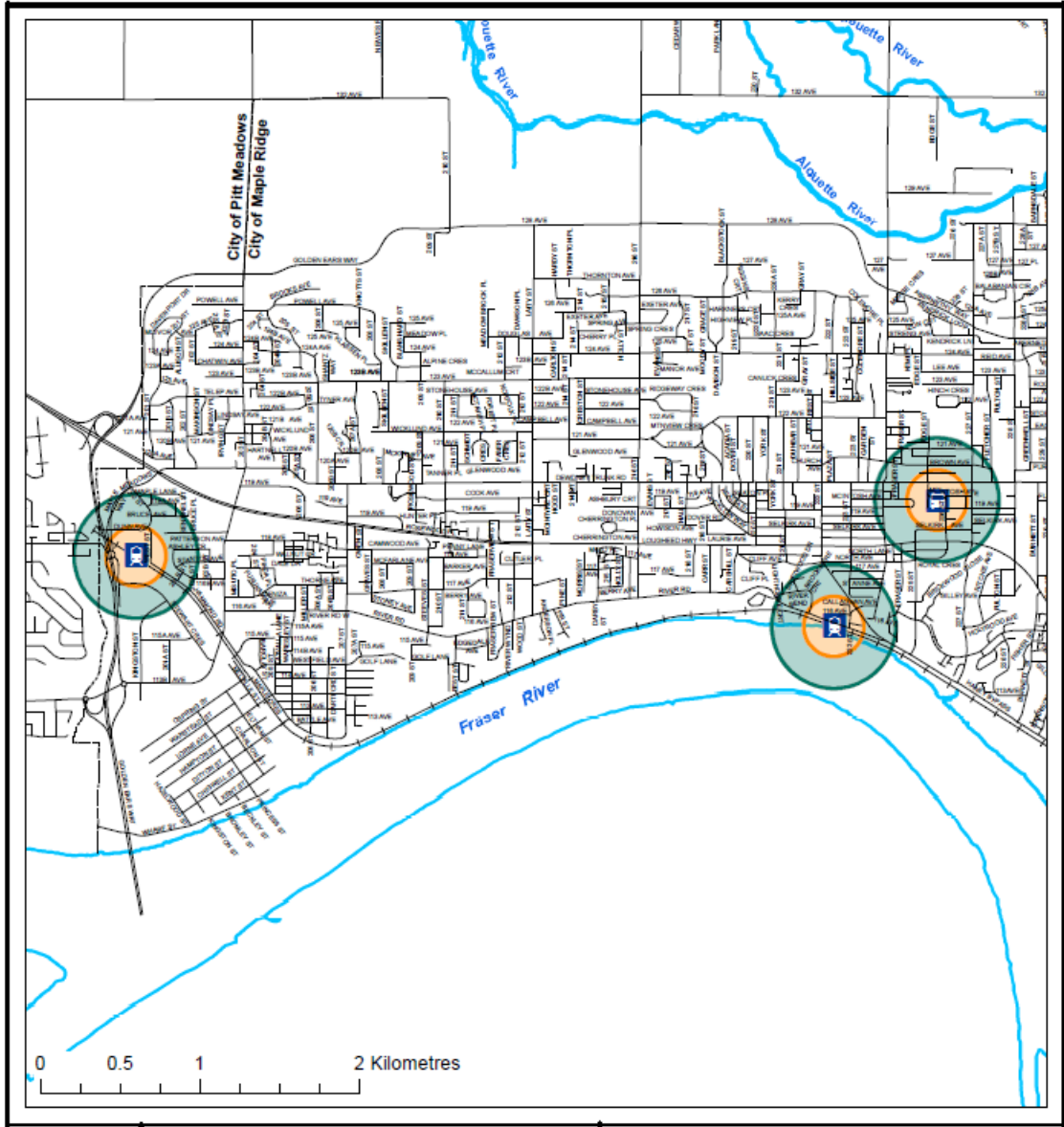


Table 1. Prescribed TOA Densities

	Distance from Transit Hub	Minimum Allowable Height (Number of Storeys)	Minimum Allowable Density (FAR/FSR)
<b>Tier 4</b>	< 200 m	12	Up to 4.0
<b>Tier 5</b>	201 – 400 m	8	Up to 3.0



### **Transit-Oriented Area Land Use Planning Process:**

As the City's Transit-Oriented Area Designation Bylaw did not modify the land use designations of the identified properties, Council endorsed a Transit-Oriented Areas Land Use Planning Process to undertake a targeted land use review of the three TOAs. This process is intended to consider the need to update and refine the land uses within the TOAs, as well as the adjacent land uses. The process includes:

- A review of relevant City policies, and consider the updated land use and housing economic analyses received through the updated Housing Needs Report (2024);
- A review of community amenities, transportation networks, site constraints and opportunities;
- Prepare and refine land use concepts; and
- Solicit community input through a public consultation process.

The outcomes of the initial work of the TOA Land Use Planning Process is summarized in this report and is being provided to Council for information.

### **ANALYSIS:**

The initial land use concepts for the three Transit-Oriented Areas in Maple Ridge are described below and presented in the attachments.

#### **Haney Transit Exchange Transit-Oriented Area:**

The Haney Transit Exchange TOA is located in the heart of Maple Ridge within the Town Centre Area Plan (Figure 2). The TOA includes all properties within a 400 m radius of the Haney Transit Exchange. This area represents most of the City's Central Business District, as well as its cultural, recreational, and institutional centre. Most commercial businesses are also spread out around this civic node. The surrounding context includes:

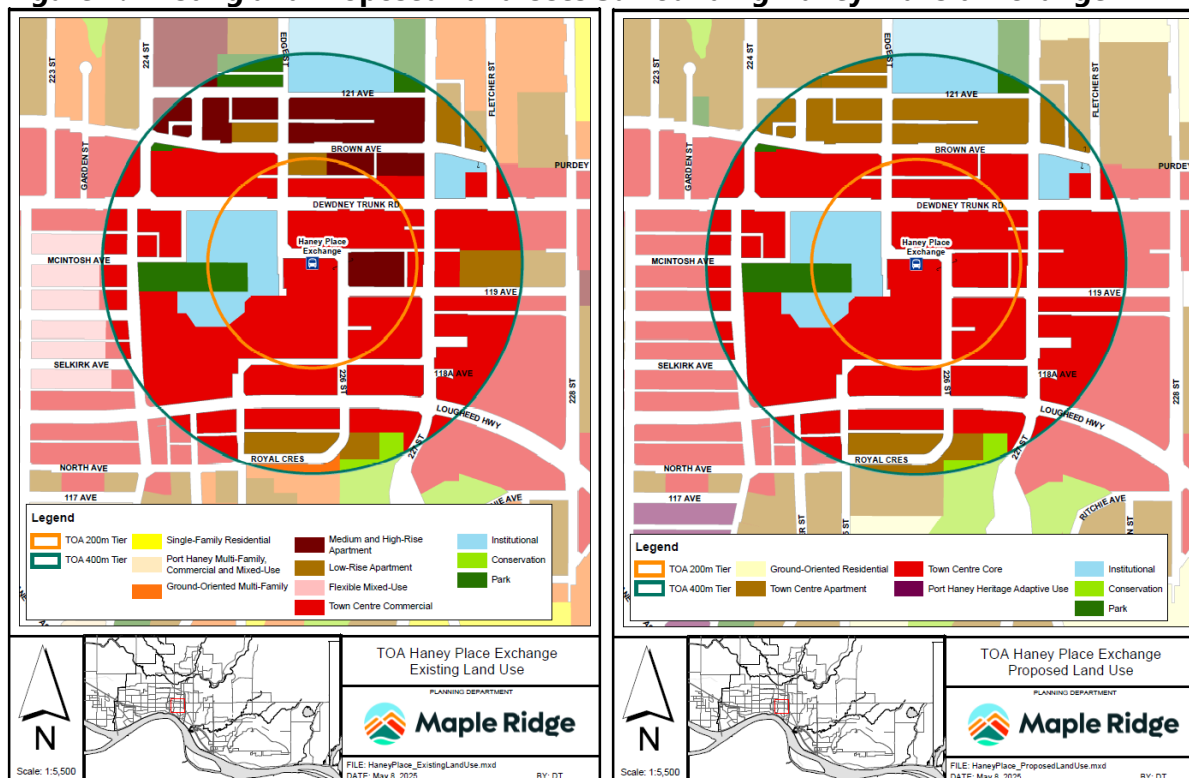
- North: A mix of residential (of different densities), institutional along with some parks,
- East: A mix of residential and commercial along Dewdney Trunk Road.
- South: A mix of residential and conservation areas.
- West: A mix of commercial along Dewdney Trunk Road, residential and flexible mixed use.

#### Proposed Land Use Updates:

Several land use updates are proposed to the Haney Transit Exchange TOA to comply with the new provincial requirements for Transit-Oriented Areas (Figure 2, Table 2). The proposed land use changes reinforce the concentration of density around the Haney Transit Exchange while supporting the creation of a high quality, vibrant and walkable urban built environment. The proposed changes include updating the Town Centre Area Plan land use designations with a new naming convention to reinforce the following concepts:

- Creating a vibrant downtown core by concentrating population and employment growth within a defined mixed use downtown core area centred around the bus loop station.
- Improving pedestrian connectivity through enhancements to the pedestrian infrastructure and mid-block crossings to create a more pleasant and walkable area. Closing the gaps in pedestrian and cycling infrastructure will become increasingly important as the area grows.

**Figure 2. Existing and Proposed Land Uses Surrounding Haney Transit Exchange**



**Table 2. Haney Transit Exchange TOA Proposed Land Use Designations**

Land Use Designation	Proposed Updates
Town Centre Apartment	<ul style="list-style-type: none"> <li>Mid-rise residential buildings that support a range of multi-unit housing near services and parks.</li> <li>Building heights will range between 8–12 storeys, depending on location.</li> <li>This designation may also support local commercial and community spaces.</li> </ul>
Town Centre Core	<ul style="list-style-type: none"> <li>Located on the central core area around the bus loop exchange supporting tower-and-podium forms to establish high quality urban design.</li> <li>Building heights will be flexible, and can exceed 12 storeys.</li> </ul>

In addition to the land use designations noted above, a new Ground-Oriented Residential designation is proposed for the Town Centre Area Plan. This designation is intended to capture low-rise, ground-oriented forms of housing, including duplexes, triplexes, courtyard homes and townhouses, that will be located along the edges of the area, to provide a transitional built form from the more urban style of development within the core to the adjacent established, lower-density neighbourhoods. The existing Institutional, Parks and Conservation land use designations are not proposed to change.



The proposed land use updates within the Haney Transit Exchange (Figure 2, Attachment B) include changes to the land uses within the TOA as well as to the properties adjacent to the TOA, particularly to the north, to better align the land uses. This will bring higher density forms of development, such as apartments and townhouses, farther north than what is envisioned in the Town Centre Area Plan today. It also includes a redistribution of park spaces from within the TOA to outside of the TOA. The proposed concept plan for this TOA focuses on reinforcing the area's mixed-use character while supporting transit-oriented growth.

### **Port Haney Transit-Oriented Area:**

The Port Haney TOA is located near the southern edge of the Town Centre Area Plan and adjacent to the Fraser River (Figure 3). The Transit Hub (i.e., West Coast Express station) is accessed from River Road and is situated on the south side of the CPKC rail line. The surrounding context includes:

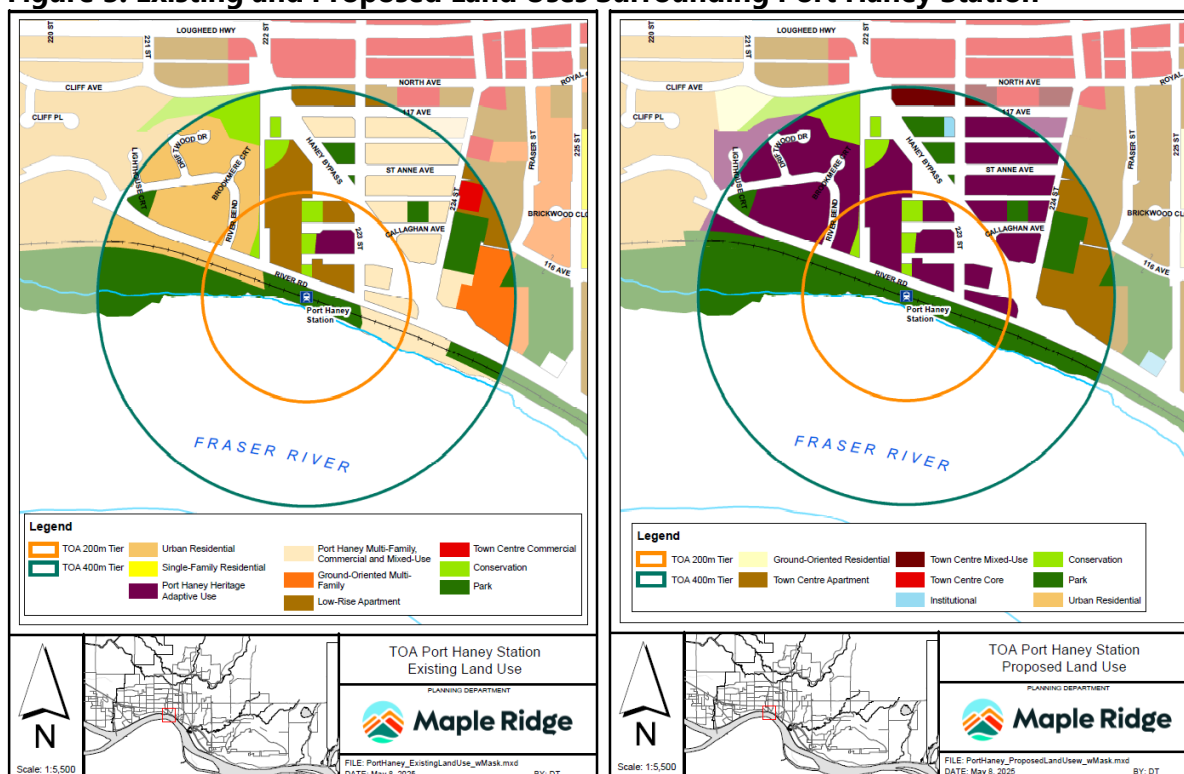
- North: A mix of residential, commercial, and mixed-use development, along with some natural open space and conservation areas.
- East: Residential uses, parks, and commercial development, with a heritage landmark situated near the intersection of 224 Street and 116 Avenue.
- South: The Fraser River, railway infrastructure, and a combination of park and conservation lands form the southern boundary.
- West: River Bend residential neighbourhood and adjacent conservation lands, including park space and conservation areas.

### Proposed Land Use Updates:

The following land use updates are proposed to the Port Haney TOA to comply with the new provincial requirements (Figure 3, Table 3). The proposed land uses reinforce the area's historic heritage waterfront character while supporting thoughtful, transit-oriented growth. The proposed changes include updating the Town Centre Area Plan with the land use designations noted above, as well as introducing some Port Haney specific land uses, while recognizing its heritage value. The proposed changes include updating the land use designations within the Port Haney Area to reinforce the following concepts:

1. Introduce a balanced mix of uses to foster a vibrant, livable community with a blend of residential, commercial, and cultural spaces.
2. Support a complete, walkable neighbourhood through a connected block structure, enhanced pedestrian environments, and integration with transit access.
3. Enhance public access to the waterfront by identifying opportunities for new walkways, open spaces, and public viewpoints along the Fraser River, connecting residents and visitors to the riverfront.
4. Continue to celebrate Port Haney's historic character through design guidelines, heritage-sensitive mixed-use development, and public realm improvements connecting key heritage sites.

**Figure 3. Existing and Proposed Land Uses Surrounding Port Haney Station**



**Table 3. Port Haney TOA Proposed Land Use Designations**

Land Use Designation	Proposed Updates
Town Centre Apartment	<ul style="list-style-type: none"> <li>Mid-rise residential buildings that support a range of multi-unit housing near services and parks.</li> <li>Building heights will range between 8–12 storeys, depending on location.</li> <li>This designation may also support local commercial and community spaces.</li> </ul>
Town Centre Mixed-Use	<ul style="list-style-type: none"> <li>Mid-rise mixed-use buildings that support a range of uses while promoting active street frontages with retail, office, and residential above.</li> </ul>
Port Haney Heritage Adaptive Use	<ul style="list-style-type: none"> <li>A flexible land use designation intended to accommodate a range of development forms that reflect the area’s historic character and future transit-oriented potential.</li> <li>Built form and land use will vary by three sub-areas, with specific policies to be developed for: <ul style="list-style-type: none"> <li>River Bend Neighbourhood: Primarily residential (up to 8 storeys), with potential for limited commercial where servicing allows.</li> </ul> </li> </ul>



	<ul style="list-style-type: none"> <li>○ Port Haney Station: A mix of park, public space, and mixed-use development (up to 12 storeys) near the WCE station.</li> <li>○ Port Haney Heritage Village: Heritage-sensitive mixed-use area (up to 8 storeys), focused on reinforcing existing character through form, scale, and public space integration.</li> <li>• Building heights will range between 8–12 storeys, depending on location.</li> <li>• This designation may also support employment-generating uses or job-supportive spaces in select locations, where appropriate.</li> </ul>
--	--

The proposed land use updates within the Port Haney TOA (Figure 3, Attachment C) will support better and more effective planning surrounding the Port Haney West Coast Express station. The proposed concept plan for this TOA focuses on balancing density requirements with the preservation of heritage identity, improved public access to the Fraser River, and a connected, walkable urban structure centred around the Transit Hub. The Town Centre Area Plan boundary is proposed to be extended to the west to include parcels located within the TOA.

### **Maple Meadows Transit-Oriented Area:**

The Maple Meadows West Coast Express station is located near the western edge of the Hammond Area Plan (Figure 4). The station is located on the north side of the CPKC rail line and features a large parking area for commuters and several transit stops with connections to Haney Place, Coquitlam, and Langley. The surrounding context includes:

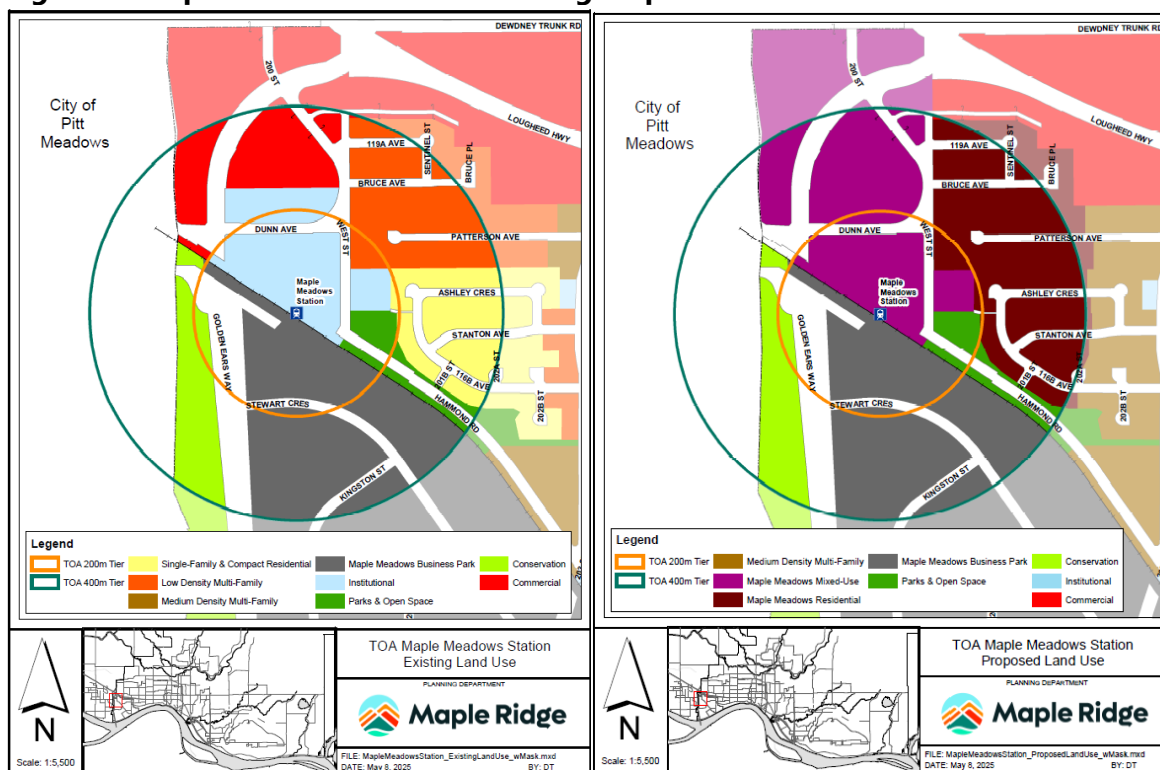
- North: Commercial uses, including car dealerships and retail stores.
- East: Place of worship and residential neighbourhoods.
- South: Maple Meadows Industrial Business Park.
- West: Meadowtown Shopping Centre and residential neighbourhoods in Pitt Meadows.

### Proposed Land Use Updates:

The following land use updates are proposed to the Hammond Area Plan to comply with the new provincial requirements for TOAs (Figure 4, Table 4). The proposed updates include two new land use designations that align with the provincial requirements that increase the capacity for housing and supporting mixed-use development near Maple Meadows Station.

Updated policies are also being proposed for the existing land use designations to provide an appropriate transition between the Hammond Area Plan and the Lougheed Transit Corridor Area Plan to the north, which is currently being developed.

**Figure 4. Proposed Land Uses Surrounding Maple Meadows Station**



**Table 4. Maple Meadows TOA Proposed Land Use Designations**

Land Use Designation	Proposed Updates
Maple Meadows Mixed-Use	<ul style="list-style-type: none"> <li>• New designation to allow higher-density commercial, employment, institutional, and residential uses (mixed-use).</li> <li>• Maximum 8-12 storeys in height, depending on the distance from Maple Meadows Station.</li> <li>• Applied to parcels currently designated for commercial and institutional uses within the Transit-Oriented Area.</li> </ul>
Maple Meadows Residential	<ul style="list-style-type: none"> <li>• New designation to allow higher-density residential uses (e.g., apartments).</li> <li>• Maximum 8-12 storeys in height, depending on the distance from Maple Meadows Station.</li> <li>• Applied to parcels currently designated for residential uses within the Transit-Oriented Area.</li> </ul>
Medium Density Multi-Family	<ul style="list-style-type: none"> <li>• Existing designation that allows townhouse, rowhouse, and apartment housing forms.</li> <li>• Updated to increase the maximum building height for apartments from 4 to 6 storeys when located near the Transit-Oriented Area or Lougheed Transit Corridor.</li> </ul>



	<ul style="list-style-type: none"> <li>• Updated to allow apartments with accessory commercial uses (e.g., cafes, restaurants, daycares), where requirements for traffic management and off-street parking and loading are met.</li> <li>• Applied to parcels currently designated for residential uses that are adjacent to the Transit-Oriented Area (i.e., west of 203 Street) or the Lougheed Transit Corridor (i.e., along the north edge of the Hammond Area Plan).</li> </ul>
--	--

The proposed land use updates within the Hammond Area Plan (Figure 4, Attachment D) will support better and more effective planning surrounding the Maple Meadows Station. These changes ensure that new development in the TOA is subject to the same policies and guidelines. The Hammond Area Plan boundary is proposed to be extended to the north to include parcels located on both sides of Maple Meadows Way.

## TOA LAND USE CONSIDERATIONS:

While Bill 47 does enable certain development heights and densities for the parcels in the TOA areas, it is important to note the following considerations:

- While local governments are required to align their Official Community Plan with the new provincial legislation for Transit-Oriented Areas, there may be instances where an individual site cannot achieve all the allowable density or building height. This could include sites with size, shape, and land constraints, such as irregularly shaped parcels next to cul-de-sacs. In these instances, higher-density development may require land assemblies.

The proposed land use updates for the TOAs do not guarantee that the maximum density is achievable or feasible for the existing parcels. Details surrounding the maximum building height and floor space ratio would be determined through the City's development review process once the respective rezoning application is submitted.

- A change in land use designation does not mean that (re)development is permitted outright or that the site is 'pre-zoned' for construction, a rezoning application will still be required. The new legislation also does not change the City's existing rezoning process, application fees, staff review, or Council approval. It also doesn't remove the City's ability to establish conditions of approval for the application, such as protections for existing tenants, or necessary infrastructure and servicing upgrades.
- While the City cannot require applicants to include a minimum amount of residential vehicle parking spaces in a TOA, parking can still be required for accessibility stalls and bicycles. In other types of non-residential uses or buildings, like commercial (e.g., a retail store) and institutional (e.g., a school), the City's parking requirements are still in effect.
- Parcels within a TOA with a heritage designation or heritage revitalization agreement are exempt from the densities and height enabled under the Bill 46 legislation.

- Parcels within a TOA are exempt from the zoning regulations that permit between 3 to 6 units under the Bill 44 (SSMUH) legislation.
- Portions of the TOAs may be located within the Fraser River floodplain or other flood hazard areas, including the Fraser River Escarpment. Development applications within identified hazard areas may be exempt from the densities and height provisions, are subject to additional studies, and must comply with applicable local and provincial regulations for hazard areas.

## **NEXT STEPS:**

The next step in the Transit-Oriented Area Land Use Planning Process is to share the initial land use concepts, incorporating any Council feedback, to the public at an upcoming Open House. The Open House is anticipated to be scheduled in early June and will be advertised on the City's website, through social media platforms, and in the local newspaper.

The public process is an opportunity to introduce the TOA concept brought into effect by the Province to the community, as well as to provide the opportunity for input on the proposed land uses for the three TOAs.

Following the public engagement, Official Community Plan Amending Bylaws will be prepared over the summer and presented to Council in the fall of 2025 for consideration. In accordance with provincial regulation, local governments, including the City, have until December 31, 2025 to update their Official Community Plans to align with the recent provincial legislation.

## **STRATEGIC ALIGNMENT:**

The update of the three Transit-Oriented Areas as set out in the TOA Land Use Planning Process aligns with the Liveable Community pillar within the 2023-2026 Council Strategic Plan.

## **FINANCIAL IMPACT:**

This work falls within the Planning Department's 2025 Work Plan.



## CONCLUSION:

This report provides a summary of the proposed land use changes for the parcels within and adjacent to three Transit-Oriented Areas in Maple Ridge (i.e., the Haney Transit Exchange, Port Haney, and Maple Meadows), shares initial land use concepts for each, and outlines an engagement plan to seek public comments.

Prepared by:

"Elona Saro"

Elona Saro, Community Planner 1

"Mahshid Rezaei"

Mahshid Rezaei, Community Planner 1

"Jeff White"

Jeff White, Community Planner 2

### Attachments:

- (A1) Map of Transit-Oriented Areas in Maple Ridge
- (A2-4) Haney Place Exchange, Port Haney Station, Maple Meadows Station Transit-Oriented Areas
- (B1-3) Proposed Land Use Maps for the Haney Place Exchange Transit Exchange Transit-Oriented Area
- (C1-3) Proposed Land Use Maps for Port Haney West Coast Express Station Transit-Oriented Area
- (D1-3) Proposed Land Use Maps for Maple Meadows West Coast Express Station Transit-Oriented Area

## Report Approval Details

Document Title:	Transit Oriented Area Update.docx
Attachments:	<ul style="list-style-type: none"><li>- Attachment A1 - Map of Transit-Oriented Areas in Maple Ridge.pdf</li><li>- Attachment A2 - Haney Place Exchange Transit-Oriented Area.pdf</li><li>- Attachment A3 - Port Haney Station Transit-Oriented Area.pdf</li><li>- Attachment A4 - Maple Meadows Station Transit-Oriented Area.pdf</li><li>- Attachment B1 - HaneyPlace_ExistingLandUse_wMask_NoParcels.pdf</li><li>- Attachment B2 - HaneyPlace_ProposedLandUse_wMask_NoParcels.pdf</li><li>- Attachment B3 - HaneyPlace_ProposedLandUse_MRStyle.pdf</li><li>- Attachment C1 - PortHaney_ExistingLandUse_wMask_NoParcels.pdf</li><li>- Attachment C2 - PortHaney_ProposedLandUsew_wMask_NoParcels.pdf</li><li>- Attachment C3 - PortHaney_ProposedLandUse_MRStyle.pdf</li><li>- Attachment D1 - MapleMeadowsStation_ExistingLandUse_wMask_NoParcels.pdf</li><li>- Attachment D2 - MapleMeadowsStation_ProposedLandUse_wMask_NoParcels.pdf</li><li>- Attachment D3 - MapleMeadowsStation_ProposedLandUse_MRStyle.pdf</li></ul>
Final Approval Date:	May 13, 2025

This report and all of its attachments were approved and signed as outlined below:

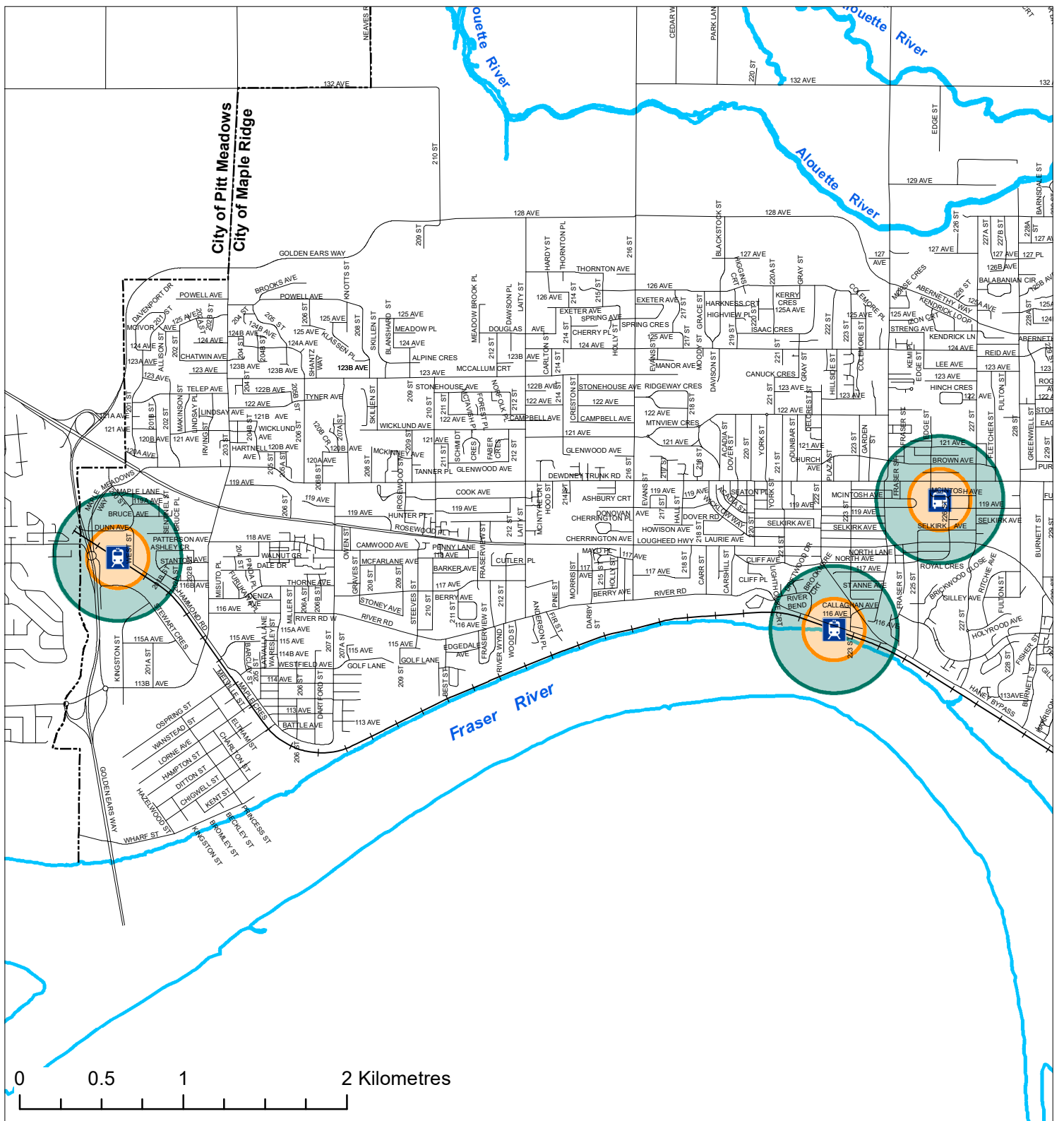
Amanda Grochowich, Manager of Community Planning

James Stiver, Director of Planning and Building

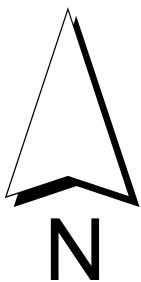
Carolyn Mushata, Director of Legislative Services and Corporate Officer

Scott Hartman, Chief Administrative Officer





0 0.5 1 2 Kilometres



## Legend

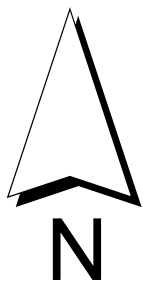
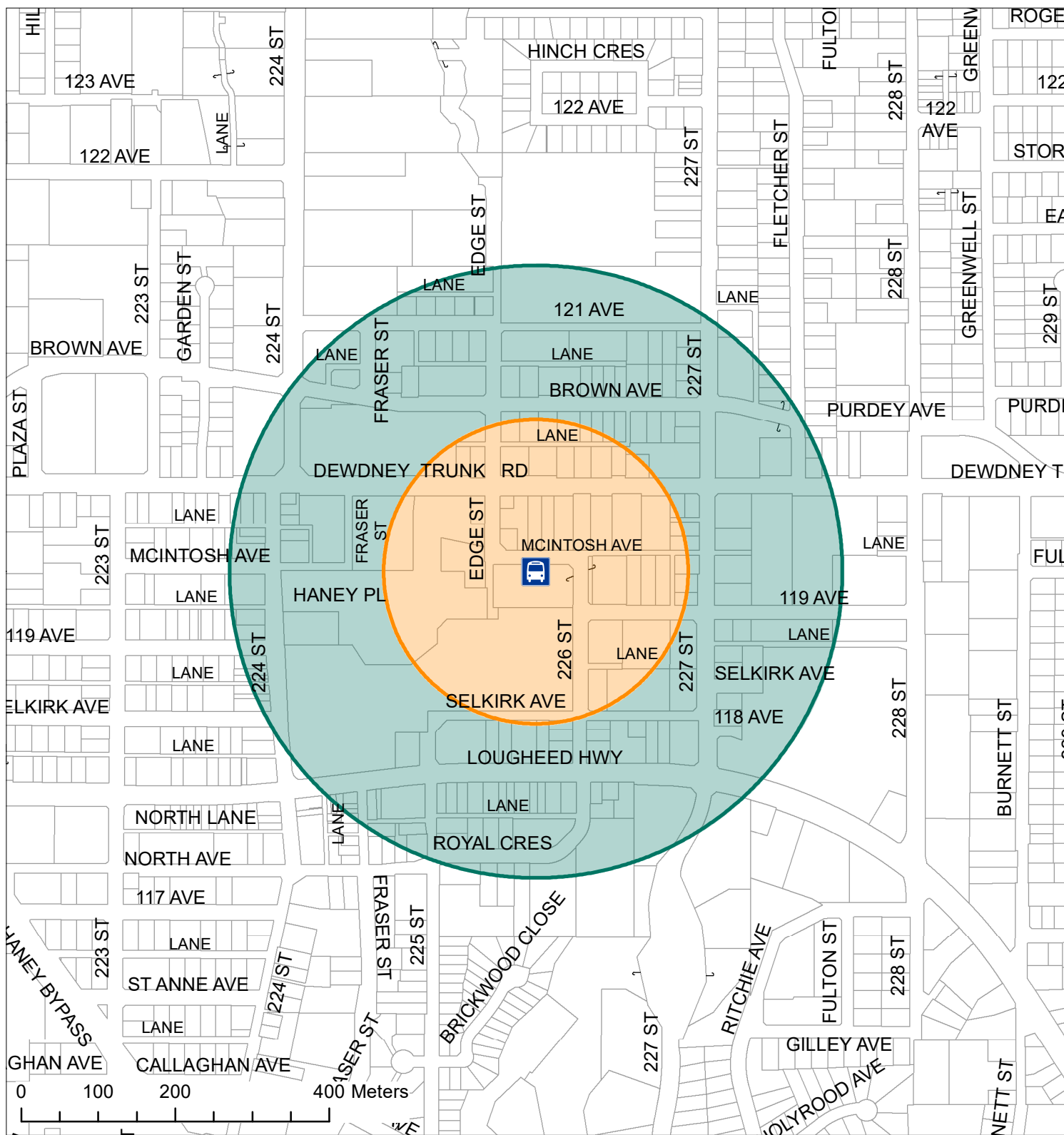
-  Bus Exchange
-  West Coast Express Station
-  200 Metre Tier
-  400 Metre Tier

Bylaw No, 7600-2019  
Schedule 'H' (1 of 4)  
Overview

PLANNING DEPARTMENT



**Maple Ridge**



Scale: 1:7,000

## Legend



Haney Place Exchange



200 Metre Tier



400 Metre Tier

Bylaw No, 7600-2019  
Schedule 'H' (2 of 4)  
Haney Place Exchange

PLANNING DEPARTMENT

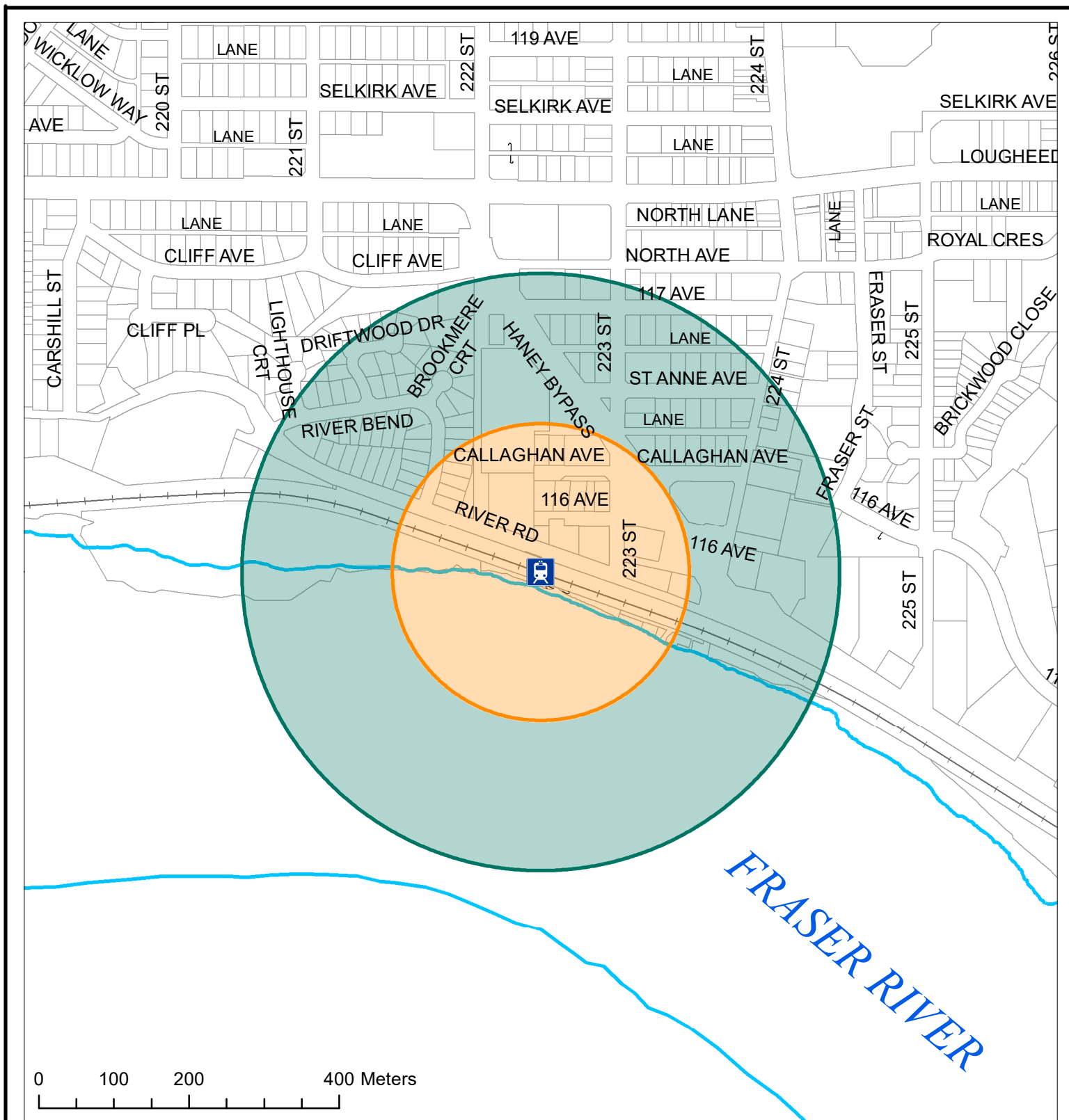


**Maple Ridge**

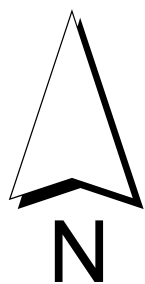
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DATE: Mar 4, 2025

BY: DT





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Scale: 1:7,000

## Legend



Port Haney Station



200 Metre Tier



400 Metre Tier

Bylaw No. 7600-2019  
Schedule 'H' (3 of 4)  
Port Haney Station

PLANNING DEPARTMENT

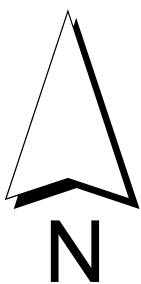
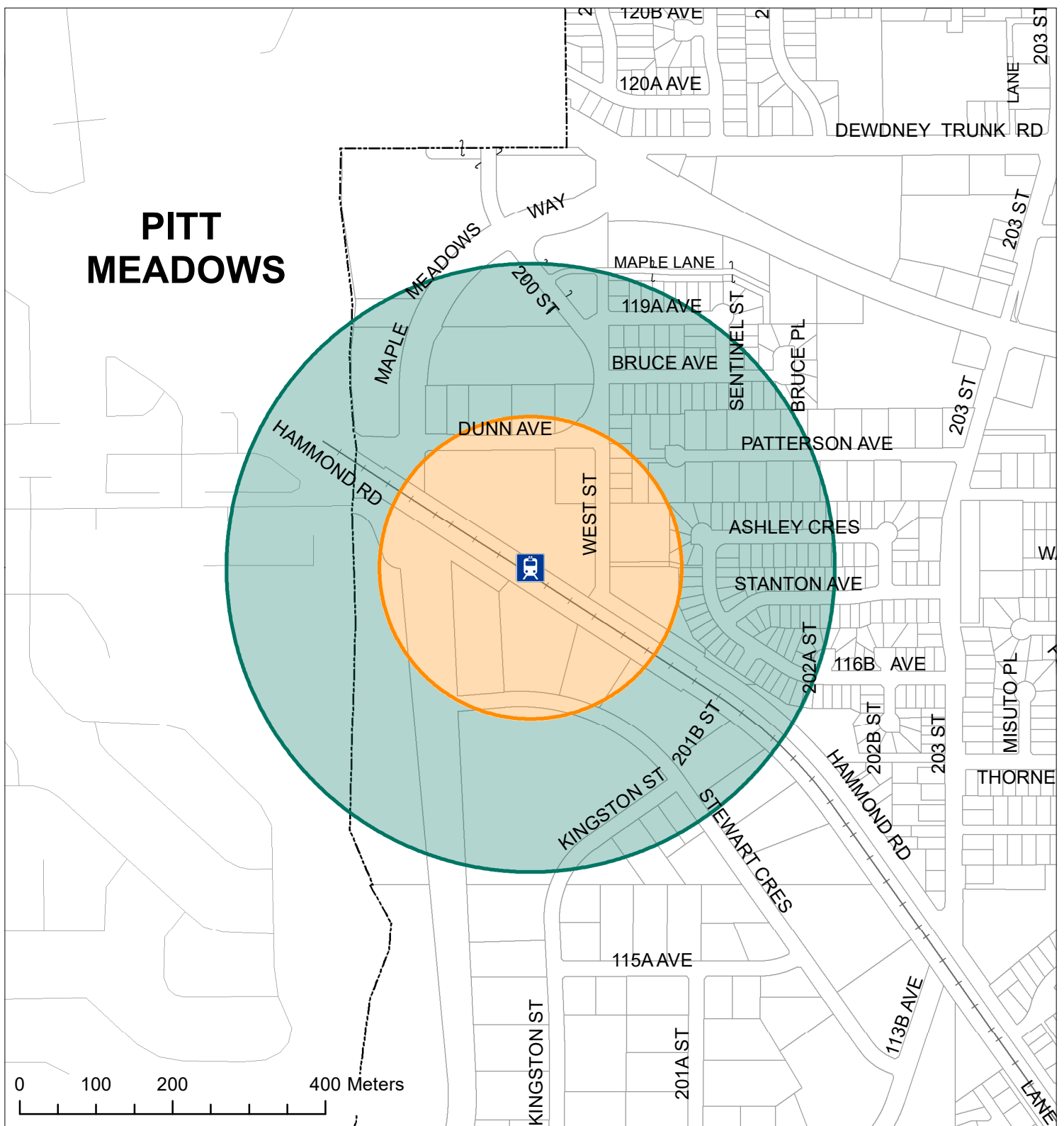


**Maple Ridge**

FILE: ScheduleH\_PortHaneyStation.mxd  
DATE: Mar 4, 2025

BY: DT

# PITT MEADOWS



Scale: 1:7,000

## Legend



Maple Meadows Station



200 Metre Tier



400 Metre Tier

Bylaw No, 7600-2019  
Schedule 'H' (4 of 4)  
Maple Meadows Station

PLANNING DEPARTMENT

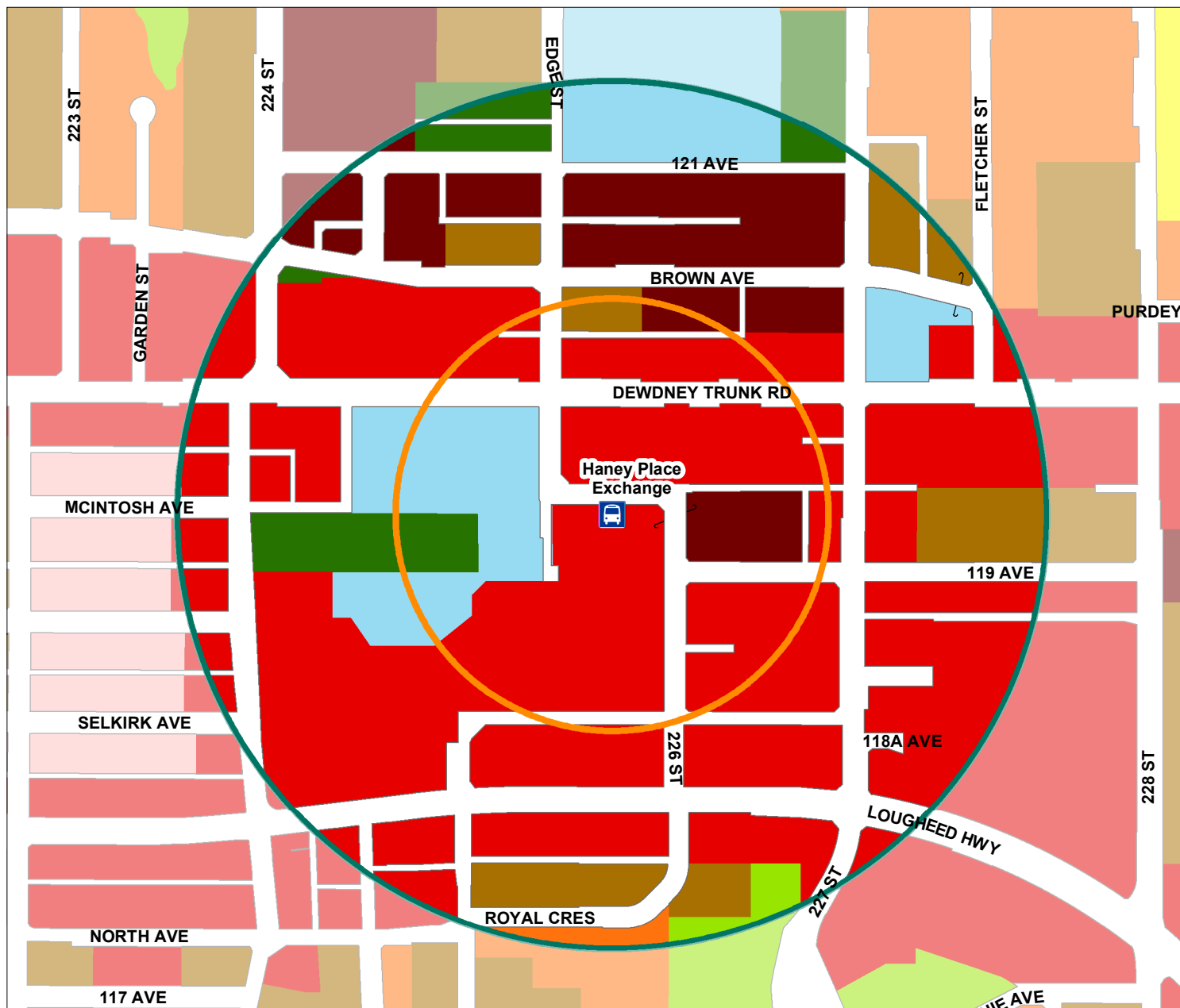


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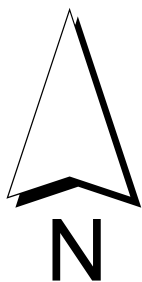
BY: DT



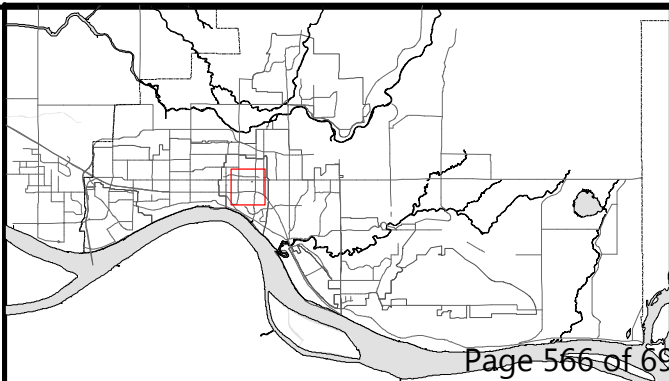


## Legend

TOA 200m Tier	Single-Family Residential	Medium and High-Rise Apartment	Institutional
TOA 400m Tier	Port Haney Multi-Family, Commercial and Mixed-Use	Low-Rise Apartment	Conservation
Ground-Oriented Multi-Family	Flexible Mixed-Use	Town Centre Commercial	Park



Scale: 1:5,500



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## TOA Haney Place Exchange Existing Land Use

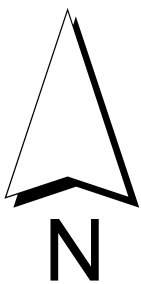
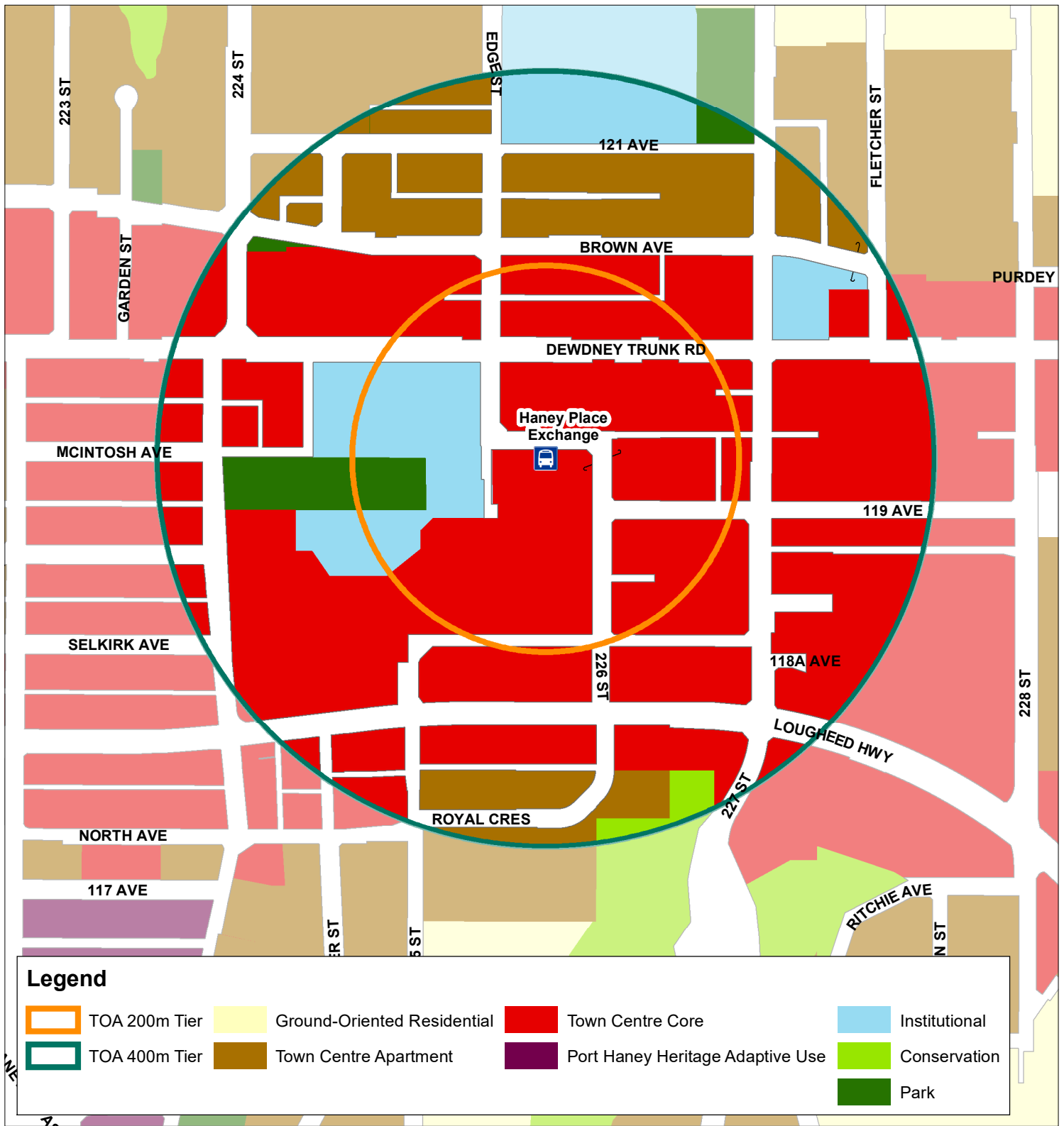
PLANNING DEPARTMENT



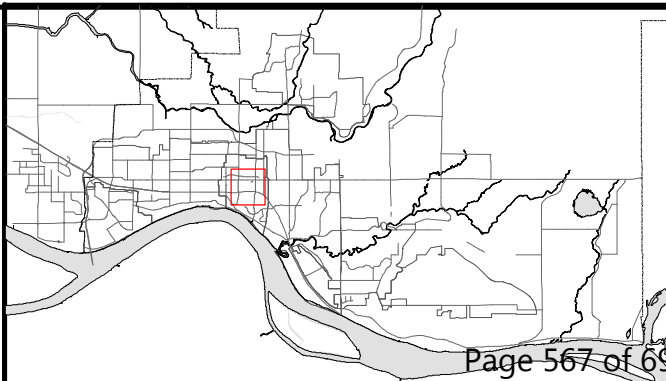
# Maple Ridge

FILE: HaneyPlace\_ExistingLandUse.mxd  
DATE: May 8, 2025

BY: DT



Scale: 1:5,500



Page 567 of 690

## TOA Haney Place Exchange Proposed Land Use

PLANNING DEPARTMENT

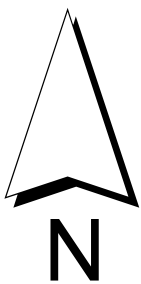
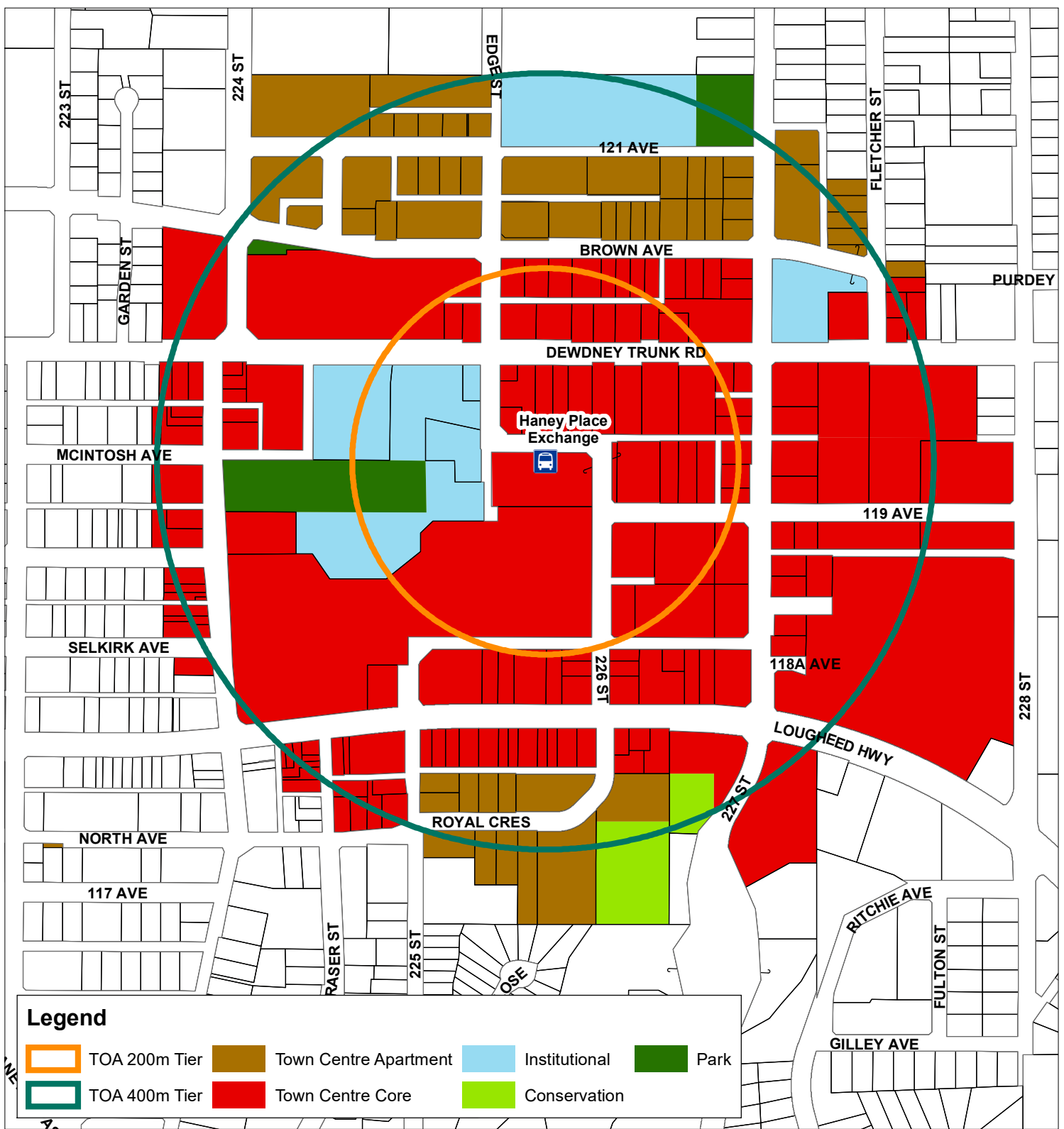


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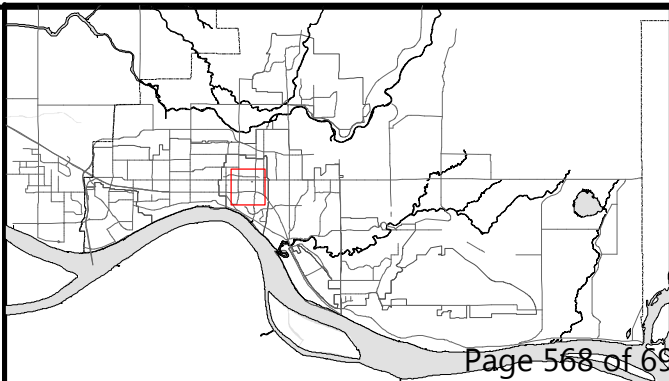
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DATE: May 8, 2025

BY: DT





Scale: 1:5,500



Page 568 of 690

## TOA Haney Place Exchange Proposed Land Use

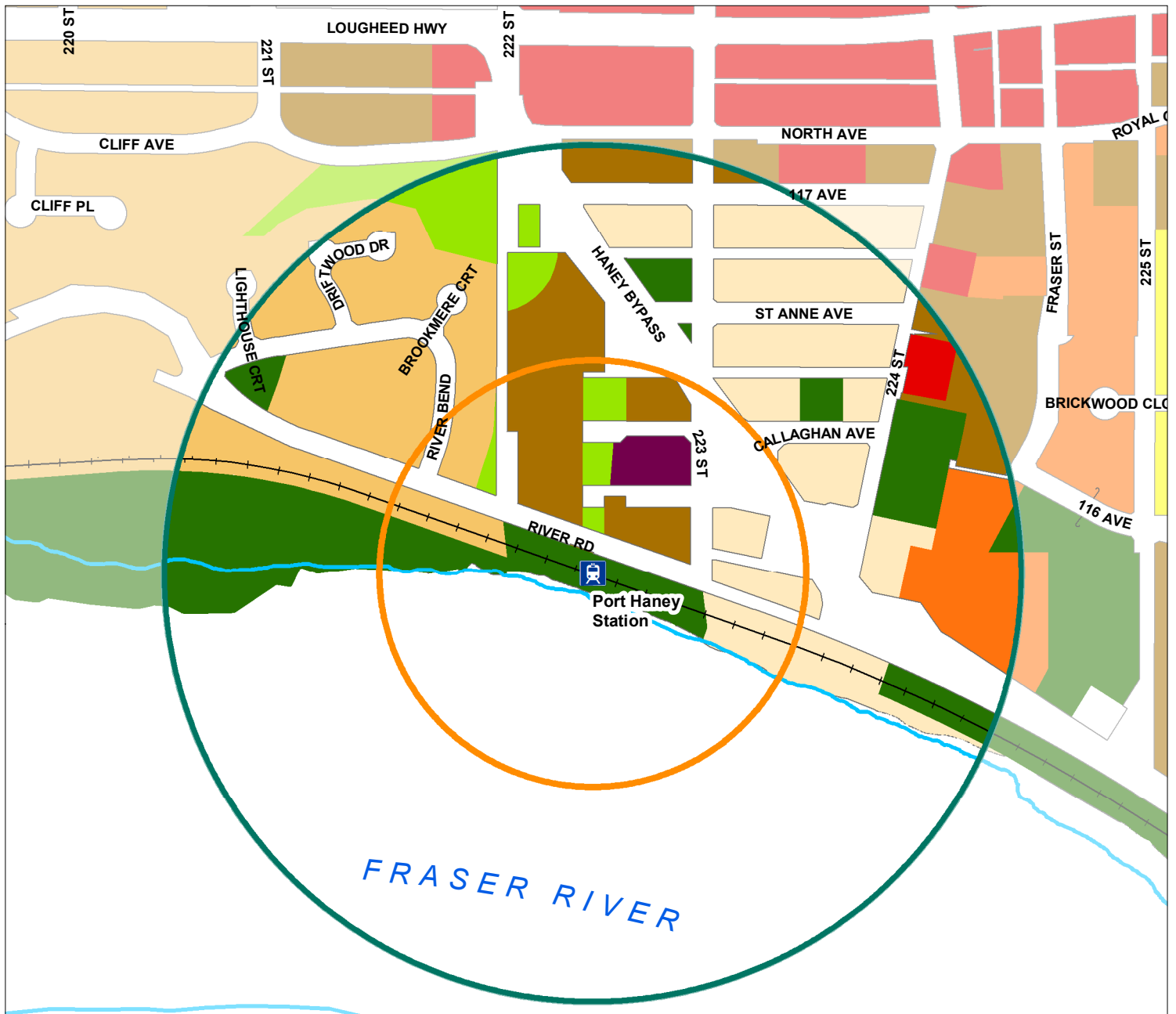
PLANNING DEPARTMENT



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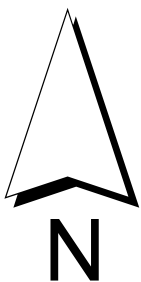
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BY: DT

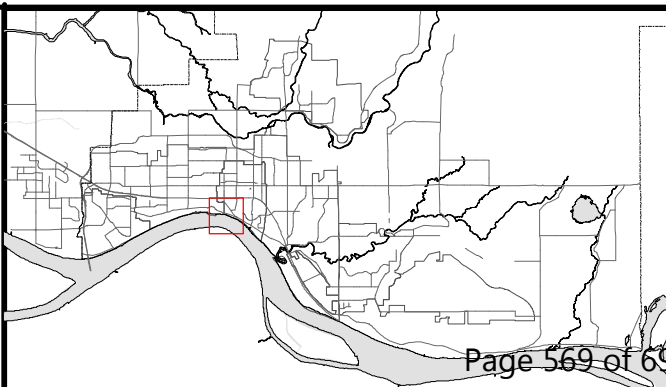


### Legend

TOA 200m Tier	Urban Residential	Port Haney Multi-Family, Commercial and Mixed-Use	Town Centre Commercial
TOA 400m Tier	Single-Family Residential	Ground-Oriented Multi-Family	Conservation
	Port Haney Heritage Adaptive Use	Low-Rise Apartment	Park



Scale: 1:5,500



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## TOA Port Haney Station Existing Land Use

PLANNING DEPARTMENT



# Maple Ridge











FILE: PortHaney\_ExistingLandUse\_wMask.mxd  
DATE: May 8, 2025

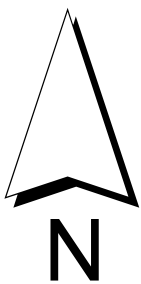
BY: DT



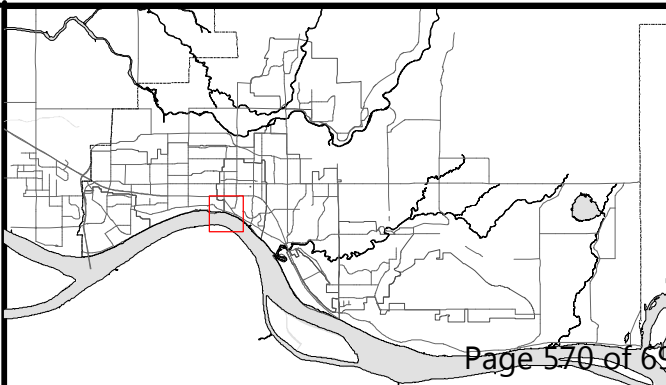


### Legend

	TOA 200m Tier		Ground-Oriented Residential		Town Centre Mixed-Use		Conservation
	TOA 400m Tier		Town Centre Apartment		Town Centre Core		Park
			Institutional		Urban Residential		



Scale: 1:5,500



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## TOA Port Haney Station Proposed Land Use

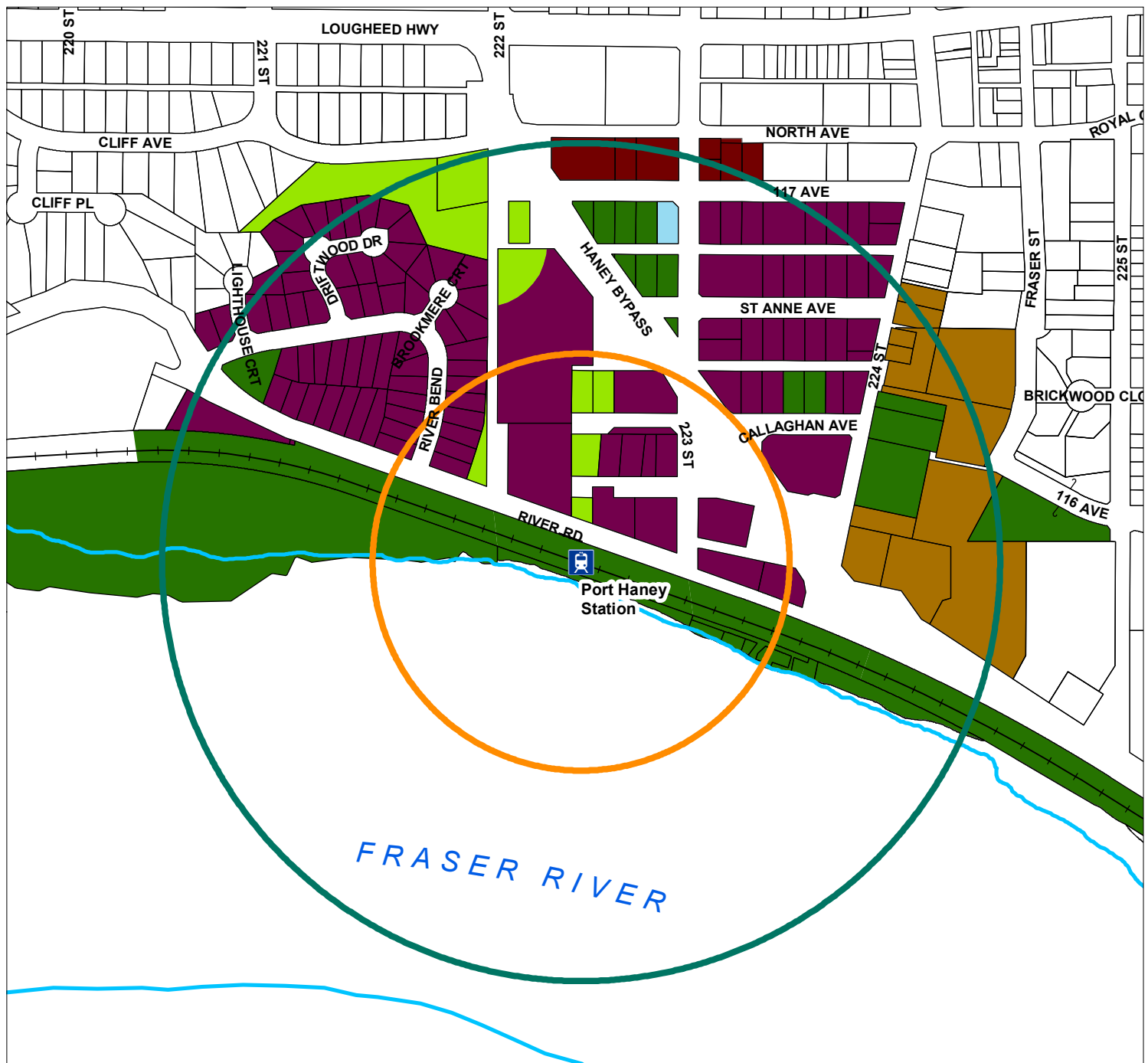
PLANNING DEPARTMENT



# Maple Ridge

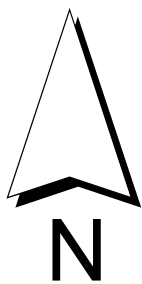
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BY: DT

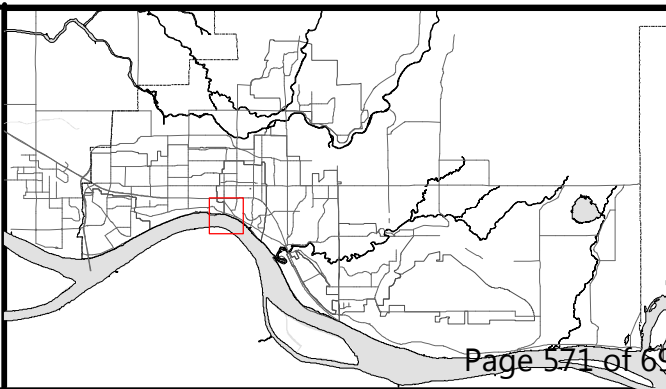


### Legend

	TOA 200m Tier		Town Centre Apartment		Port Haney Heritage Adaptive Use		Conservation
	TOA 400m Tier		Town Centre Mixed-Use		Institutional		Park



Scale: 1:5,500



## TOA Port Haney Station Proposed Land Use

PLANNING DEPARTMENT



# Maple Ridge

FILE: PortHaney\_ProposedLandUse.mxd  
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BY: DT

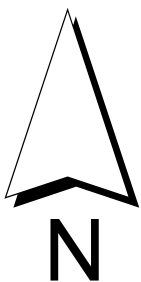


City of  
Pitt  
Meadows

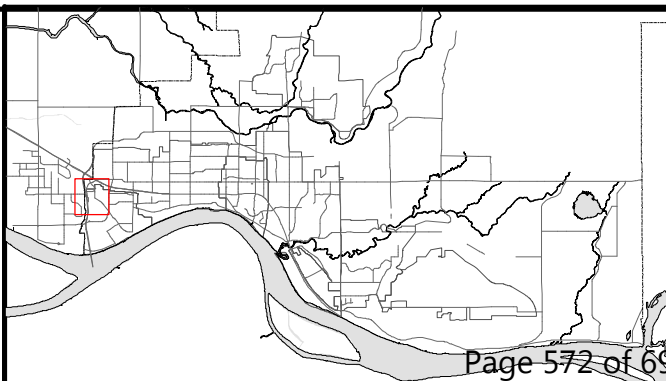


### Legend

- |               |                                     |                             |              |
|---------------|-------------------------------------|-----------------------------|--------------|
| TOA 200m Tier | Single-Family & Compact Residential | Maple Meadows Business Park | Conservation |
| TOA 400m Tier | Low Density Multi-Family            | Institutional               | Commercial   |
|               | Medium Density Multi-Family         | Parks & Open Space          |              |



Scale: 1:5,500



## TOA Maple Meadows Station Existing Land Use

PLANNING DEPARTMENT













**Maple Ridge**

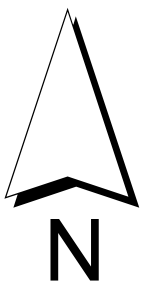
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DATE: May 8, 2025  
BY: DT

City of  
Pitt  
Meadows

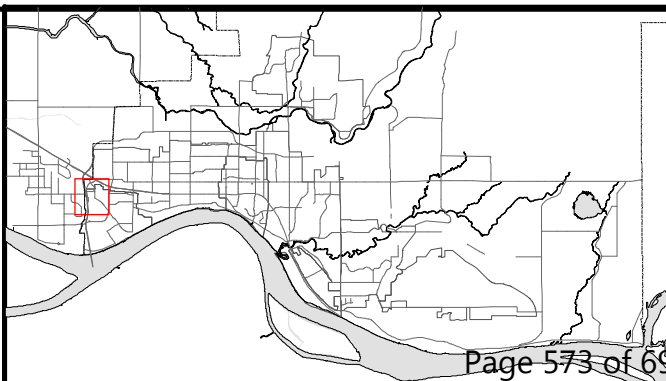


### Legend

- |   |   |   |   |
|---|---|---|---|
|  TOA 200m Tier |  Medium Density Multi-Family |  Maple Meadows Business Park |  Conservation  |
|  TOA 400m Tier |  Maple Meadows Mixed-Use     |  Parks & Open Space          |  Institutional |
|   |  Maple Meadows Residential   |   |  Commercial    |



Scale: 1:5,500



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## TOA Maple Meadows Station Proposed Land Use

PLANNING DEPARTMENT



# Maple Ridge


FILE: MapleMeadowsStation\_ProposedLandUse\_wMask.mxd  
DATE: May 8, 2025  
BY: DT

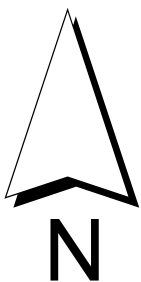


City of  
Pitt  
Meadows

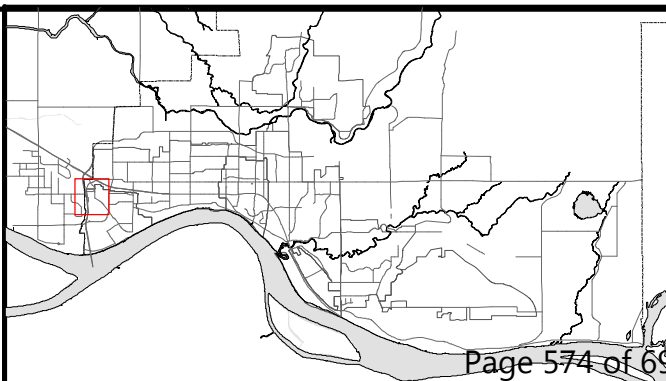


#### Legend

- |   |               |   |                           |   |                             |   |              |
|---|---------------|---|---------------------------|---|-----------------------------|---|--------------|
|  | TOA 200m Tier |  | Maple Meadows Mixed-Use   |  | Maple Meadows Business Park |  | Conservation |
|  | TOA 400m Tier |  | Maple Meadows Residential |  | Parks & Open Space          |   |              |



Scale: 1:5,500



## TOA Maple Meadows Station Proposed Land Use

PLANNING DEPARTMENT



# Maple Ridge

FILE: MapleMeadowsStation\_ProposedLandUse\_MRStyle.mxd  
DATE: May 8, 2025  
BY: DT

## **Albion Area Plan Update**

### **Official Community Plan Amending Bylaw No. 8032-2025**

#### **Recommendations:**

**THAT in accordance with Section 475 of the *Local Government Act* Council direct staff to proceed with the consultation strategy as outlined in the staff report dated May 20, 2025, for *Official Community Plan Amending Bylaw No. 8032-2025*.**

**THAT an opportunity for early and ongoing consultation be provided by way of posting *Official Community Plan Amending Bylaw No. 8032-2025* on the City's website, and Council considers holding a Public Hearing on the Bylaw to be sufficient consultation.**

**THAT *Official Community Plan Amending Bylaw No. 8032-2025*, to update the Albion Area Plan, be given first reading.**

#### **Report Purpose and Summary Statement:**

To provide a summary of the Official Community Plan amendments necessary to update the Albion Area Plan to reflect changes brought into effect with the new provincial housing legislation, as well as to redesignate portions of the North East Albion Area to support townhome style development.

#### **Previous Council Action:**

At the March 5, 2024 Council Workshop meeting, Council received information regarding the Provincial legislative changes related to housing.

At the February 4, 2025, Committee of the Whole meeting, Council directed staff to prepare an Official Community Plan Amendment Bylaw for North East Albion Area Plan.

#### **Strategic Alignment:**

Liveable Community



**To:** Mayor and Council

**File number:** 2025-036-CP

## **Albion Area Plan Update**

### ***Official Community Plan Amending Bylaw No. 8032-2025***

#### **BACKGROUND:**

Originally adopted in 2006, the Albion Area Plan is one of Maple Ridge's earliest area plans. Other than site-specific land use designation changes through development application approvals, it has not been reviewed as a whole since the early 2010s. Since that time, fundamental housing market and legislative shifts have occurred and a number of amendments are now proposed in alignment with:

- Changes to the Provincial Legislative Framework (2023 - 2024); and
- The North East Albion Land Use Servicing Review (2023-2025)

#### **Changes to the Provincial Legislative Framework:**

In November 2023, the provincial government passed new legislation intended to increase housing supply in all B.C. municipalities. This legislation made changes to allow more homes and offer different, more intensified housing types across the province in zoned single-detached neighbourhoods, as well as near transit hubs and other frequent transit corridors.

Given the characteristics of the Albion Area, the Albion Area Plan has largely been influenced by the *Housing Statutes (Residential Development) Amendment Act* (Bill 44), which enables up to three, four or six units as-of-right on single-detached and duplex-zoned lots. The size of the respective parcel and proximity to frequent transit services informs the possible number of units allowed and is set out by the Province.

Per provincial regulation, local governments, including the City of Maple Ridge, have until December 31, 2025 to update their Official Community Plans to align with Bill 44.

#### **North East Albion:**

On September 27, 2022, Council adopted the North East Albion Land Use Plan as an amendment to the Albion Area Plan of the City's Official Community Plan (OCP).

At the October 24, 2023, Council Workshop, largely influenced by changing interest rates and evolving preference demands for housing, Council directed staff to review the land uses and servicing for the southern part of the North East Albion Land Use Plan (south of 110 Avenue).

Over the following two years, a public engagement process took place (summarized in the [June 11, 2024 Council Workshop Report](#)) as well as more detailed servicing analysis (summarized in the [February 4, 2025 Committee of the Whole Council Report](#)). At the [February 25, 2025 Council Meeting](#), Council directed staff to prepare the necessary OCP amending bylaws to update the North East Albion Land Uses south of 110 Avenue. The updates outlined in this report reflect the changes heard from the community and the public, and primarily propose increasing the portion of townhouse style development within the southern portion of North East Albion.

The purpose of this report is to provide a summary of the OCP amendments necessary to update the Albion Area Plan to be consistent with the new provincial housing legislation, and to redesignate portions of the North East Albion Area to support more townhouse style development.

## **DISCUSSION:**

*Official Community Plan Amending Bylaw No. 8032-2025* (Attachments A1 and A2) has been prepared to respond to provincial requirements and Council direction, as well as taking the opportunity to modernize the Albion Area Plan to align with other updated City strategies and associated references. These amendments include:

- Consolidating and updating land uses designations to align with provincial housing legislation and Council direction for North East Albion; this includes complementary updates to the Zoning Matrix and associated Land Use Maps, including Schedule 1;
- Updating Albion Area Plan policies to remove redundant references, update terminology and reflect needed changes to the Albion Area Plan; and
- Updating graphics and maps, and where possible, creating a more user-friendly document.

Given the scope of changes, a marked-up copy of the Albion Area Plan has been provided in Attachment B for ease of reference.

### **Provincial Housing Legislation and Consolidated Residential Land Use Designations:**

As per the changes set out in Bill 44, most single-detached zoned properties within the Albion Area Plan can now allow up to three units for lots under 280 m<sup>2</sup>, and up to four units for lots between 280 m<sup>2</sup> and 4,050 m<sup>2</sup>. Lots larger than 4,050 m<sup>2</sup> would require subdivision to enable the densities permitted under Bill 44.

As these number of units no longer align with the intent and naming convention of most residential land use designations used historically within the Albion Area Plan, the following consolidation of land use designation is proposed:



<b>Current</b>		<b>Proposed</b>
Low Density Residential	→	Low Density Residential
Single Family	→	Ground Oriented Residential
Low / Medium Density Residential	→	Ground Oriented Residential
Medium Density Residential	→	Ground Oriented Residential
Ground-Oriented Multi-Family	→	Removed
Cluster Residential	→	Cluster Residential
Townhouse	→	Townhouse

This results in seven residential land use designations collapsing into a revised set of four land use designations:

- 1) Low Density Residential: Corresponds to existing areas which are topographically challenging in North Albion. This land use designation will allow up to four residential units per lot, subject to environmental features.

This land use designation correlates with the larger suburban lot sizes and will permit the future rezoning and subdivision to the RS-1d (Single Detached (Half Acre) Residential) zoned lots. This allows, through the Albion Area Density Bonus Amenity Contribution, the ability to subdivide at the RS-1b (Single Detached (Medium Density) Residential lot size.

- 2) Ground-Oriented Residential: Replaces three existing single-detached land use designations into one land use which will allow up to four units per property, depending on lot size. This may take the form of accessory dwelling units (i.e., secondary suites or detached garden suites) or forms of multiplexes (i.e., duplex, triplex, or fourplex).

The associated zones will include legacy zones, such as the R-1, R-2 and R-3 zones, as well as introducing new zones to the Albion Area, including the R-4 and RUR Urban Infill Residential Zone, recently introduced into the City's Zoning Bylaw in response to the provincial housing legislative changes.

- 3) Cluster Residential: Remains as a land use designation intended for multiple units that are clustered together to minimize development footprint and maximize environmental protection of the surrounding areas within sensitive areas of North East Albion. It is intended to capture ground-oriented housing forms including townhouses and multiplexes. The associated zones will include the RM-1 (Low Density Townhouse Residential) as well as zones that enable multiplexes and street townhouses.

- 4) Townhouse: Remains as a land use designation intended to accommodate ground-oriented housing forms including townhouses and multiplexes.

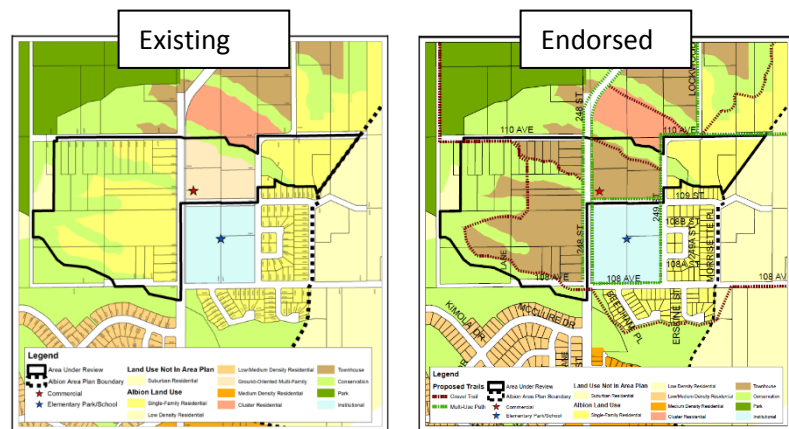
There are no changes being proposed to non residential land uses such as Village Commercial, Park, Conservation and Institutional. The Albion Area Plan Zoning Matrix is proposed to be updated to align with the land uses and zones described above.

### North East Albion Land Use Designations:

Based on the infrastructure servicing analysis, and what was heard from the community early in 2024, Council directed the following amendments to the land use designations within the southern portion of North East Albion:

- 1) Changing the land use designation west of 248 Street from Single Family to Townhouse;
- 2) Changing the land use designation between 248 Street and 249 Street from Ground-Oriented Multi-Family to Townhouse:
  - This change will remove the Ground-Oriented Multi-Family land use from the Area Plan altogether; and
- 3) Renaming the land use east of 249 Street from Single Family to Ground-Oriented Residential.

By increasing the proportion of land designated Townhouse, while retaining the ability for a commercial node between 248 and 249 Street, the area may be able to experience an increase in housing diversity while keeping the cost and extent of additional infrastructure upgrades to a minimum. These amendments have been incorporated into the proposed OCP Amending Bylaw.



### Commercial Nodes:

Recognizing the lack of neighbourhood commercial opportunities within the Albion Area Plan, the commercial node notation has been expanded from the North East Albion area into the broader Albion Area Plan. The following intersections will include the notation for 'Albion Village Commercial Node' in addition to the properties already designated as Village Commercial:

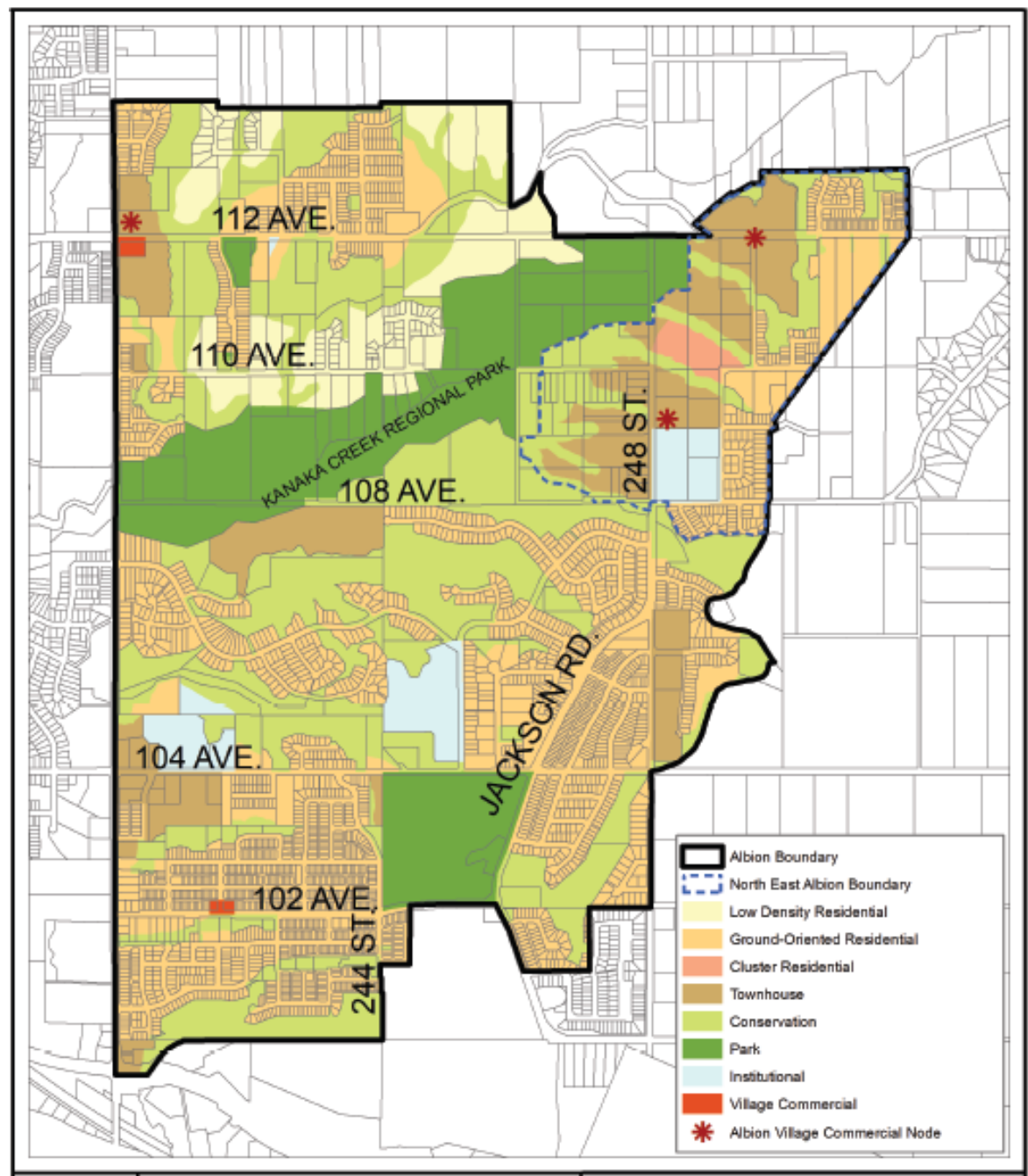
- Intersection of 248 Street and 109 Avenue;
- Intersection of 112 Avenue and Lockwood Street; and
- Intersection of 240 Street and 112 Avenue.



**Updated Land Use Map:**

With the proposed consolidation of the land use designations as outlined above, as well as the expanded Commercial Node notation, the Albion Area Land Use Map has been updated (Figure 1). Please see Schedule 1 of the Albion Area Plan (Attachment A2) for a full-sized version.

**Figure 1. Updated Albion Area Land Use Map**



## **Albion Area Plan Policy Updates:**

*Official Community Plan Amending Bylaw No. 8032-2025* also proposes to update existing policies within the Albion Area Plan in the following areas to:

1) Implement the recent legislative changes by the Province:

Largely these policy edits reflect changes in terminology and the as-of-right housing densities in single-detached and duplex-zoned neighbourhoods enabled through Bill 44.

2) Update the general development related policies:

Terminology as well as some amendments to reflect updates to other City-wide documents, including the Strategic Transportation Plan.

3) Consolidate community amenity policies:

The introduction of Council Policy 6. 31 – Community Amenity Contribution Program in 2017 now supersedes the requirements included in the Albion Area Plan (introduced in 2006). As such, the policies have been removed from the Albion Area Plan. The existing Albion Area Density Bonus Program and Community Amenity Contribution Program remains in effect for the area.

4) Update park amenities anticipated for the North East Albion Areas:

The original version of the North East Albion Area Plan stipulated that residents would have access to a neighbourhood park within a 5-minute walk. With the proposed relocation of the northern neighbourhood park as part of Development Application 2022-229-RZ and 2024-094-RZ, the policy has been updated to reflect a 10-minute walk.

The proposed co-located park with the Elementary School remains within the southern portion of the North East Albion Area.

The proposed changes are illustrated visually within the Albion Area Plan (Attachment A2) and Figure 1.

The complete set of policies is included in Attachment A2, with the marked-up copy in Attachment B.

## **NEXT STEPS:**

In accordance with Section 475 of the *Local Government Act*, should *Official Community Plan Amending Bylaw No. 8032-2025* receive first reading, Council must consider whether consultation is required with:

- a) The Board of the Regional District in which the area covered by the plan is located;
- b) The Board of any Regional District that is adjacent to the area covered by the plan;
- c) The Council of any municipality that is adjacent to the area covered by the plan;
- d) First Nations;
- e) Boards of Education, Greater Boards and Improvement District Boards; and
- f) The Provincial and Federal Governments and their agencies.

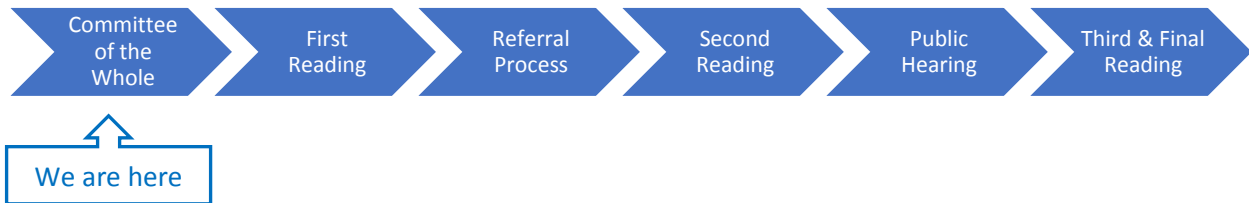


In consideration of this, staff recommend that no additional consultation be required in respect of this matter beyond the City’s typical referral process and early posting of the proposed Official Community Plan amendments on the City’s website.

Should *Official Community Plan Amending Bylaw No. 8032-2025* receive first reading, staff will submit referral notices to the following agencies regarding the proposed updates to the Albion Area Plan: Metro Vancouver, School District No. 42, local First Nations, TransLink, and the Ministry of Transportation and Transit.

Following the referral process, the bylaw can be considered by Council for second reading and move through the standard land use bylaw adoption process, including a Public Hearing, as shown in Figure 2.

**Figure 2. Bylaw Adoption Process**



New and in-stream development applications within the impacted area will continue to be evaluated against the proposed Schedule 1, in Attachment A2, while the OCP Amending Bylaw is under the review process.

The City has until December 31, 2025, to update the Official Community Plan’s to align with Bill 44 and the province’s requirements set out in the associated regulation. *Official Community Plan Amending Bylaw No. 8032-2025* forms a part of the work underway to meet the requirements of the Province.

**STRATEGIC ALIGNMENT:**

The update of the Albion Area Plan aligns with the Liveable Community pillar within the 2023-2026 Council Strategic Plan.

**FINANCIAL IMPACT:**

This work is accommodated within the Planning and Building Department’s 2025 Work Plan.

## **CONCLUSION:**

This report presents *Official Community Plan Amending Bylaw No. 8032-2025* to update the Albion Area Plan for Council consideration of first reading.

"Elona Saro"

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Prepared by: Elona Saro, Community Planner

1

## **Attachments:**

(A1) Albion Area Plan Update - *Official Community Plan Amending Bylaw No. 8032-2025*

(A2) DRAFT – OCP 2025 Concept Chapter 10.2 Albion Area Plan

(B) Albion Area Plan – Marked-Up Copy



## Report Approval Details

Document Title:	Albion Area Plan Update - Official Community Plan Amending Bylaw No. 8032-2025.docx
Attachments:	<ul style="list-style-type: none"><li>- Attachment A1 - Official Community Plan Amending Bylaw No. 8032-2025 .docx</li><li>- Attachment A2 - DRAFT - OCP 2025 Concept Chapter 10.pdf</li><li>- Attachment B - Albion Area Plan Marked-Up Copy.docx</li></ul>
Final Approval Date:	May 13, 2025

This report and all of its attachments were approved and signed as outlined below:

Amanda Grochowich, Manager of Community Planning

James Stiver, Director of Planning and Building

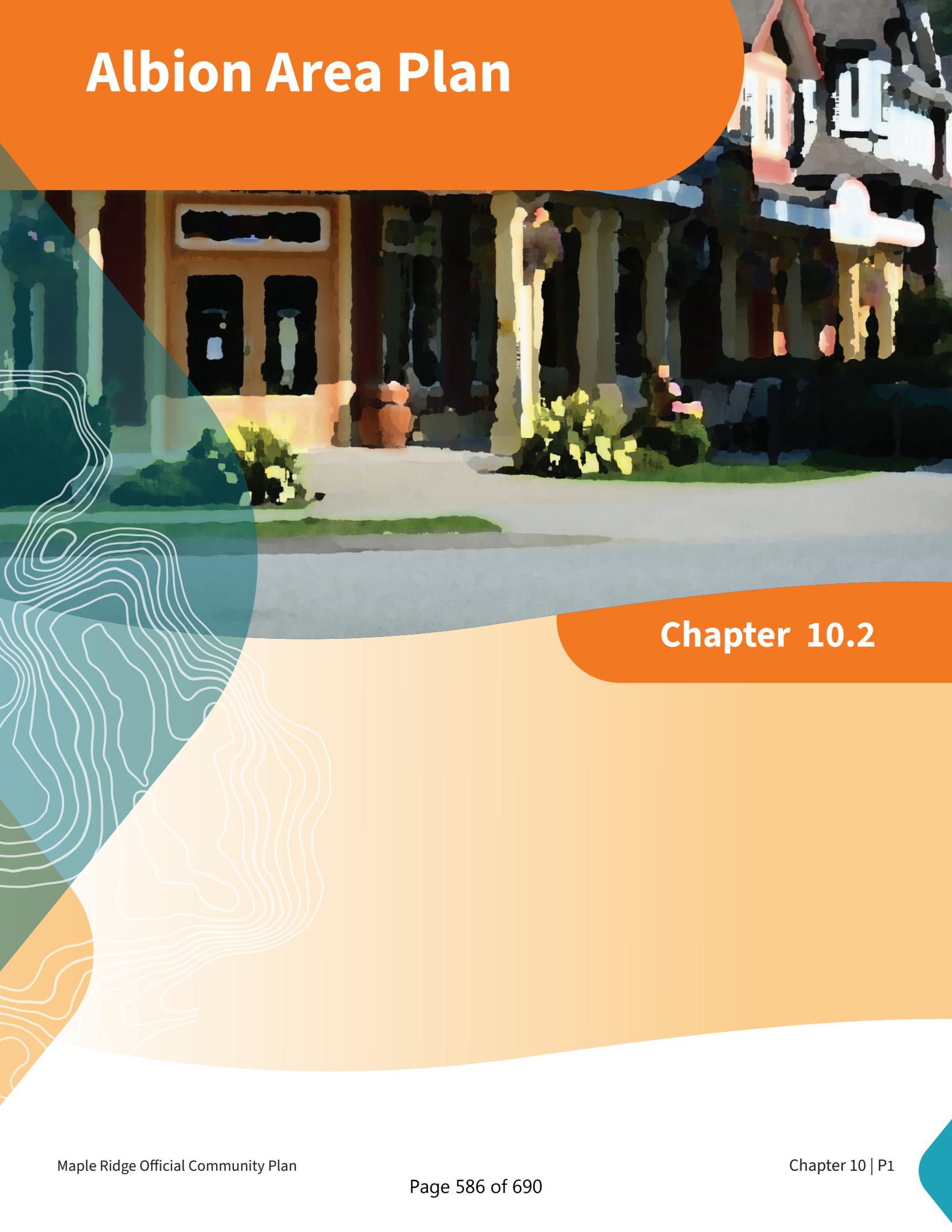
Carolyn Mushata, Director of Legislative Services and Corporate Officer

Scott Hartman, Chief Administrative Officer





# Albion Area Plan



## Chapter 10.2

### Background

The following policies are adopted to guide development within the Albion urban area.

A land use designation describes an area of land within which a specific set of policies are intended to achieve orderly growth and development within the community.





## 10.2.1 | Land Use Designations

### A. Residential Designations

The following residential land use designations are identified for the Albion Area Plan (Schedule 1):

1. **Low Density Residential** corresponds with single detached residential development at a low density urban standard with lot sizes at 2000 m<sup>2</sup> (half acre).
2. **Ground-Oriented Residential** is intended to provide housing options that range from a low density detached to medium-high density attached form of ground-oriented housing that will generally be with ground level access to each unit. The development forms include a single detached with secondary suite and/or detached garden suite, duplex, triplex, and fourplex. These variations provide for a range of housing styles and densities, including smaller lot housing.
3. **Cluster Residential** land use is located within areas that are environmentally sensitive and where some development may be accommodated through integrated site design that considers the natural features and topography of the area. The designation permits clustered forms of ground-oriented multi-family units, including townhouse, courtyard and fourplex units that minimizes impacts to the land and environment.
4. **Townhouse** permits multi-family housing forms. This designation is in areas with access to major corridors, commercial areas, as well as public amenities. While intended for Townhouse form, there may be circumstances where smaller single family residential forms may be incorporated onto a townhouse site, with the focus on minimizing the development footprints on the land.

Notwithstanding the above, higher densities may be supportable in compliance with the Community Amenity Program regulations prescribed in the Zoning Bylaw.

## 10.2.1 | Land Use Designations cont.

### **B. Albion Village Commercial Designation**

This designation is intended to provide for convenience shopping and personal services to residents of surrounding neighbourhoods in a compact village form.

### **C. Park Designation**

This designation identifies the location of existing or future municipal or Regional park sites.

### **D. Conservation Designation**

This designation identifies ecologically sensitive lands that require protection in order to ensure that their health, diversity and integrity are maintained. They are considered to be of high environmental or geologic sensitivity.

### **E. Institutional Designation**

This designation identifies lands for present or future use as civic or public use and includes school and/or park sites.

The corresponding zones for these designations are outlined in Section 10.2.6 Zoning Matrix.



## 10.2.2 | Albion Area Residential Development

### Background

The Albion Area Plan strives to balance new residential development with the protection of environmentally sensitive areas, and the provision of a range of housing styles to meet diverse housing needs.

### General Development Policies

- 10 – 4 Development applications seeking a land use designation change through an Official Community Plan amendment, to allow an increase in density, will be submitted with an analysis of the existing downstream system to a limit determined by the City. Capacity constraints identified within the existing system are to be resolved prior to additional density being advanced.
- 10 – 5 The sequence of development is to proceed in the most efficient manner, with capital costs and ongoing servicing costs to be minimized. To achieve this, lot consolidation may be required.
- 10 – 6 Land assembly or lot consolidation proposed in conjunction with development or development should meet the following conditions:
  - A) That any residual lots or remaining land parcels are left in a configuration and lot area which are suitable for a future development proposal, or can be consolidated with other abutting residual lots or land parcel and would be able to comply with the applicable Land Use Designation(s) and Policies of this Plan;
  - B) The use of any residual abutting lots or land parcels can continue to function in accordance with the applicable Land Use Designation and Policies of this Plan;
  - C) Residual abutting lots or land parcels are not to become isolated or left in a condition which is unsuitable for redevelopment or unsuitable for the maintenance of the existing land use; and
  - D) The land assembly proposal will incorporate adequate impact mitigation measures such as the provision of buffers, landscaping, site design, building arrangements and building design to ensure compatibility with abutting existing land uses.

### General Multi-Family Housing Development Policies

- 10 – 7 The siting, form, character and massing of multi-family residential development and areas for parking, storage and landscaping should be designed to facilitate orderly development of the area and to be compatible with single detached residential developments.
- 10 – 8 Multi-family housing should be provided with appropriate amenities to enhance the character of development.
- 10 – 9 The siting of development and construction techniques employed should avoid impacts to watercourses, reduce the risk of erosion in the area, minimize visual disruption to the neighbourhood and protect the visual character of the landscape.
- 10 – 10 Natural vegetation should be retained where possible to enhance the character of the area. Significant treed slopes or natural vegetation will need to be retained in order to minimize disruption to watercourses and to minimize erosion. Geotechnical assessments may be required.
- 10 – 11 Development in the Albion Area is subject to the Natural Features policies of the Official Community Plan.
- 10 – 12 Development must also adhere to the Design Guidelines detailed in Section 8 Development Permit Area Guidelines.
- 10 – 13 The intersection on 240 Street and 112 Avenue will provide commercial services for the area while also allowing for residential units above in a compact village form.



## 10.2.3 | Community Amenity Program and Density Bonus Framework

### Background

The Community Amenity Program is intended to meet the needs of the community and respond to changes in housing form and demand over time by enabling an additional means of providing neighbourhood amenities.

The Albion Area Plan Community Amenity Program provides the opportunity for a Density Bonus within a number of zones identified within the Albion Zoning Matrix, see Section 10.2.6. Within these zones, 'bonus' density may be achieved through an Amenity Contribution toward community amenities that will be located within the boundaries of the Albion Area Plan

### Objective

To support increases in density as a means of providing additional amenities in the Albion Area Plan.

### Policies

- 10 – 14 The city-wide Community Amenity Program, established through Section 2.1.2 Compact and Unique Community, will apply to the entire Albion Area Plan.
- 10 – 15 A Density bonus will only be permitted on those lands that are located entirely within the boundaries of the Albion Area Plan and Urban Area Boundary.

**“Promoting diverse, safe  
secure neighbourhoods.”**



## 10.2.4 | North East Albion Area Development

### Background

The North East Albion Area is bound by the Kanaka Creek Regional Park to the north, existing Albion Area residential developments and conservation areas to the southwest, including the Kanaka Creek Regional Park, and rural residential developments to the east.

The North East Albion Area Plan emphasizes the development of a complete community by integrating environmental stewardship, the protection of natural resources and the preservation of ecosystems, while balancing the social and economic objectives of the community.

The North East Albion Area Plan sets out residential pockets nestled between existing watercourses and linked by a network of pathways and trails. A multi-modal network links neighbourhood amenities, such as, an elementary school, and shopping nodes within the community and to the larger Albion Area and Kanaka Creek Regional Park.

The North East Albion Area Plan focuses multi-family development around commercial and amenity nodes; offering residents gathering spaces and commercial opportunities, and bringing to life one of the City's growth neighbourhoods. It is intended that the Area Plan policies facilitate the creation of an inclusive and sustainable community with equal opportunities for people of all ages, cultures, lifestyles, and abilities.

Development in the North East Albion Area is to be reflective of the consultation with area residents and other stakeholders and should align with the principles and intent laid out in this Plan.

### Policies

10 – 16 Development in the North East Albion Area (Schedule 1) will be subject to the North East Albion Servicing Strategy Policy as well as other relevant City documents policies and regulations.

#### Environment

10 – 17 Maple Ridge will continue to apply regulations for the protection of watercourses and steep slopes in North East Albion (Figure 1) in order to enhance and maintain environmental health integrity.

10 – 18 Development in the North East Albion Area is to be conducted in such a manner as to mitigate impact on the environment by increasing connections for wildlife, preserving existing hydrological processes, protecting fish and fish habitat, minimizing the potential for stream erosion, preventing surface flooding, and ensuring major storm conveyance systems are capable of conveying significant flows and minimize damage to life and properties under extreme storm conditions.

10 – 19 Maple Ridge will encourage the retention of significant tree stands that are not already protected through existing legislation, bylaws, or policies, through thoughtful site design.



## 10.2.4 | North East Albion Area Development cont.

- 10 – 20 For sites identified on North East Albion Figure 1, additional floorspace or a reduction in permissible lot size may be supported if environmentally sensitive areas are protected and permanently preserved beyond what is required through the land dedication process.
- 10 – 21 Maple Ridge recognizes that Kanaka Creek Regional Park provides valuable habitat for wildlife and movement corridors, and as such, supports preserving existing movement corridors along Kanaka Creek and Grant Hill. Enhancement of landscaping and stormwater management features is also encouraged with the aim of supporting wildlife conservation and resident safety.
- 10 – 22 Maple Ridge will connect the North of 112 Avenue precinct, including the trailhead to Kanaka Creek Regional Park, to the co-located school and park site in the south via a ‘Spine Trail’, that will serve as an important stormwater feature and protect and celebrate the water resources of the area (Figure 2).
- 10 – 23 Envisioned as a key feature of the North East Area Plan, a series of connected ponds will be woven into the Spine Trail system (Figure 3).
- 10 – 24 Maple Ridge will integrate a Northern Greenway trail, which will include similar features as the Spine Trail (Figure 3), along Kanaka Creek Regional Park at the north end of the North East Albion Plan Area.
- 10 – 25 Maple Ridge will establish a buffer (Figure 2) between the Kanaka Creek Regional Park and future areas of residential development to protect and improve wildlife passage and resident safety in and around the North East Albion Area.
- 10 – 26 Maple Ridge encourages strata developments adjacent to regional park land to ensure better stewardship of the environmental buffer.
- 10 – 27 The North East Albion Area Plan outlines a vision and strategy for sustainable water stewardship practices. As such, development is to:
- A) Maximize groundwater protection and water conservation efforts;
  - B) Incorporate stormwater management best practices, including the integration of visible stormwater management features, into the site design; and
  - C) Require a geotechnical investigation, including a ground-water impact assessment and subsequent impact mitigation measures, in order to protect Kanaka Creek and drinking water resources.

## 10.2.4 | North East Albion Area Development cont.

### Servicing

- 10 – 28 Maple Ridge will encourage the integration of aesthetically pleasing designs with appropriate siting, building form, landscaping and screening into the civic infrastructure necessary for the build-out of the North East Albion Area, such as the proposed pump station on 112th Street.
- 10 – 29 Development in the North East Albion Area will be subject to the outcomes of the water analysis, conducted for the development of the North East Albion Servicing Strategy, as well as other relevant City documents.
- 10 – 30 Development in the North East Albion Area will be subject to the proposed transportation network identified in Figure 4 & 5 as well as the City's Strategic Transportation Plan, as amended from time to time.

### Mobility

- 10 – 31 Maple Ridge will ensure that the design of new development aligns with the City's requirements for creating a safe, integrated, multi-modal transportation system.
- 10 – 32 The North East Albion Area Plan sets out an integrated active transportation network (Figure 6) which includes proposed bike lanes, trails and multi-use pathways.
- 10 – 33 North East Albion is identified in the City's Strategic Transportation Plan as an area intended for improved connectivity for active travel modes. It is anticipated that future streetscape improvements will be provided through the development process.
- 10 – 34 The North East Albion Area Plan sets out an equestrian trail (Figure 6) that connects riders from the existing Kanaka Creek Crossing to Grant Hill. This trail will help to create a buffer between environmentally sensitive areas and future development.
- 10 – 35 North East Albion is identified as an area that is not well served by transit. It is anticipated that long-term transit services will continue to be provided via Community Shuttle transit services, and that Maple Ridge will continue to support TransLink for the development of an efficient transit system through the North East Albion Area.



## 10.2.4 | North East Albion Area Development cont.

### Parks and Community Amenities

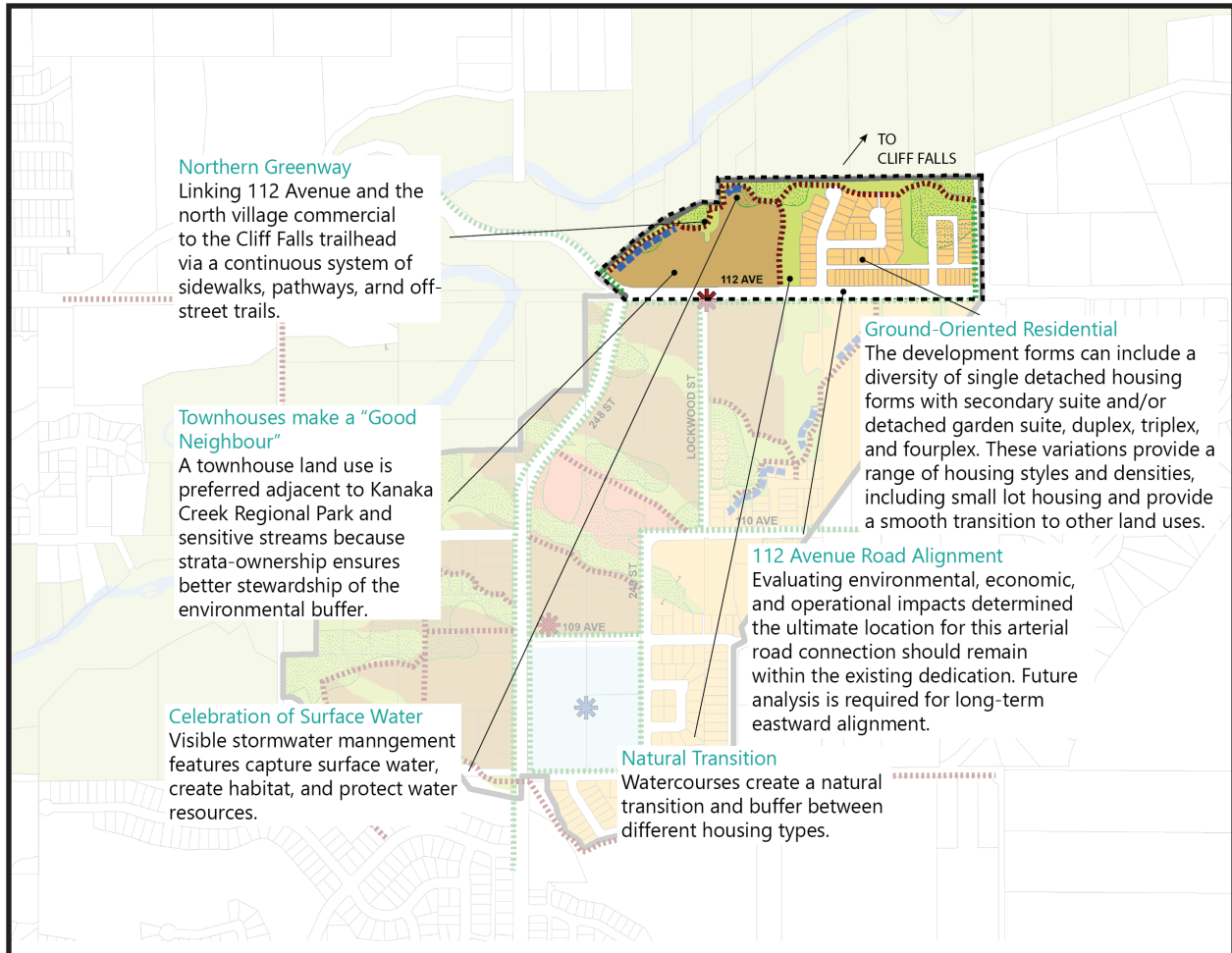
- 10 – 36 The North East Albion Area Plan identifies the co-location of a park and elementary school with a sports field, playground, and pathways to meet the needs of the growing community (Figure 2).
- 10 – 37 The North East Albion Area Plan proposes an additional neighbourhood park space (Figure 2) to the co-located park and elementary school. Neighbourhood parks provide important amenities such as as neighbourhood parks are important components of complete, liveable communities, and will include amenities such as playgrounds, sports courts, dog off leash areas, small splash pads, open areas, seating areas and looping pathways. The specific park amenities will be determined through a park design and engagement process with future residents.
- 10 – 38 The North East Albion Area Plan stipulates that all residents will have access to a park within a 10- minute walk.
- 10 – 39 North East Albion Area park amenities will include spaces for dogs, such as a dog-off leash park nearby.
- 10 – 40 The North East Albion Area Plan includes a network of trails and multi-use pathways to provide neighbourhood connections to community amenities. These trails and pathways will act as a greenway and buffer to the sensitive environmental areas while creating an attractive trail network and public realm for residents.
- 10 – 41 Maple Ridge will encourage the inclusion of Public Art into residential and community amenities.

### Residential Development

- 10 – 42 The North East Albion Area is broken up into the following neighbourhood precincts:
- A) North of 112 Avenue
  - B) East of Lockwood Street
  - C) Between 248 Street and Lockwood Street
  - D) West of 248 Street

## 10.2.4 | North East Albion Area Development cont.

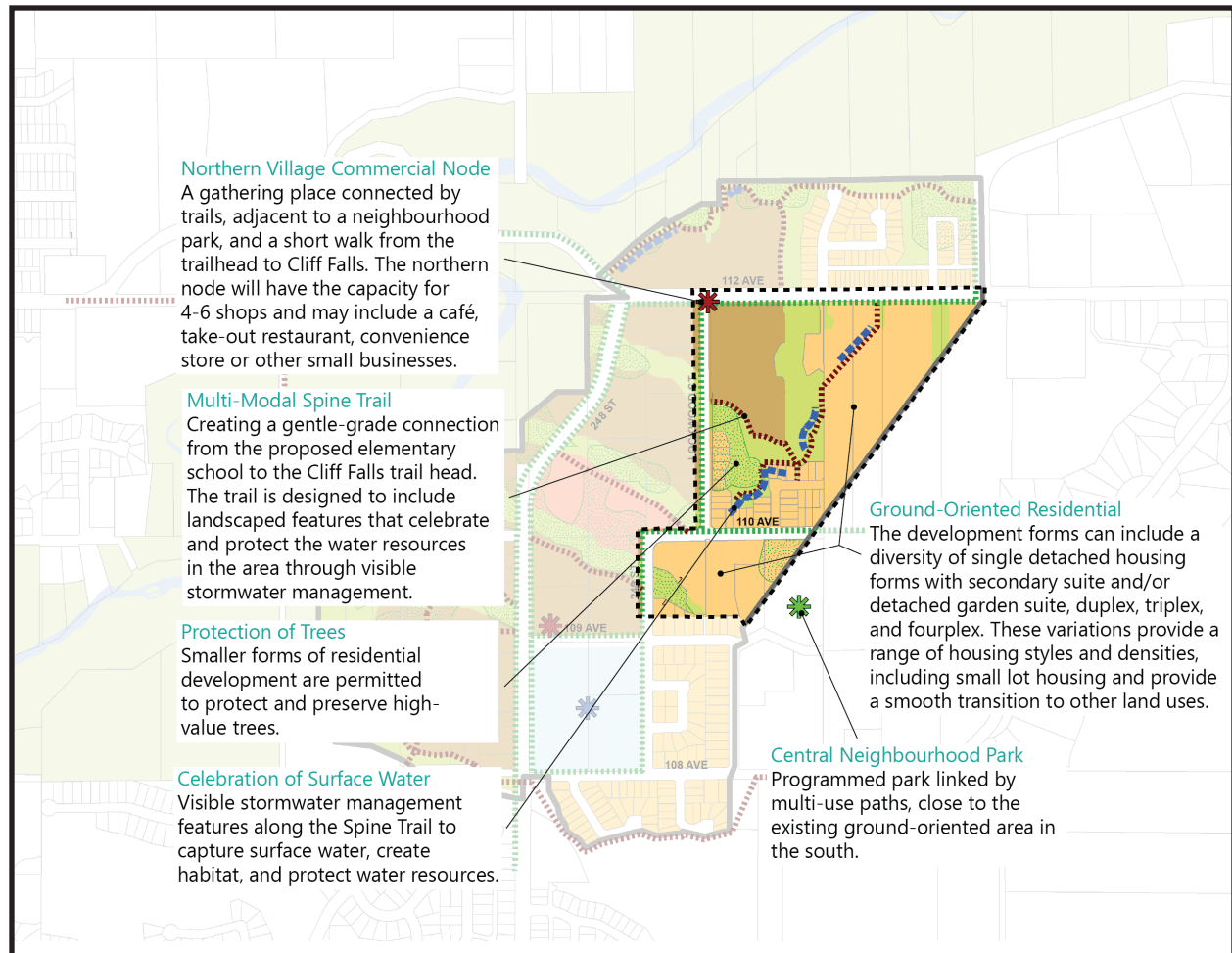
### A) North of 112 Avenue





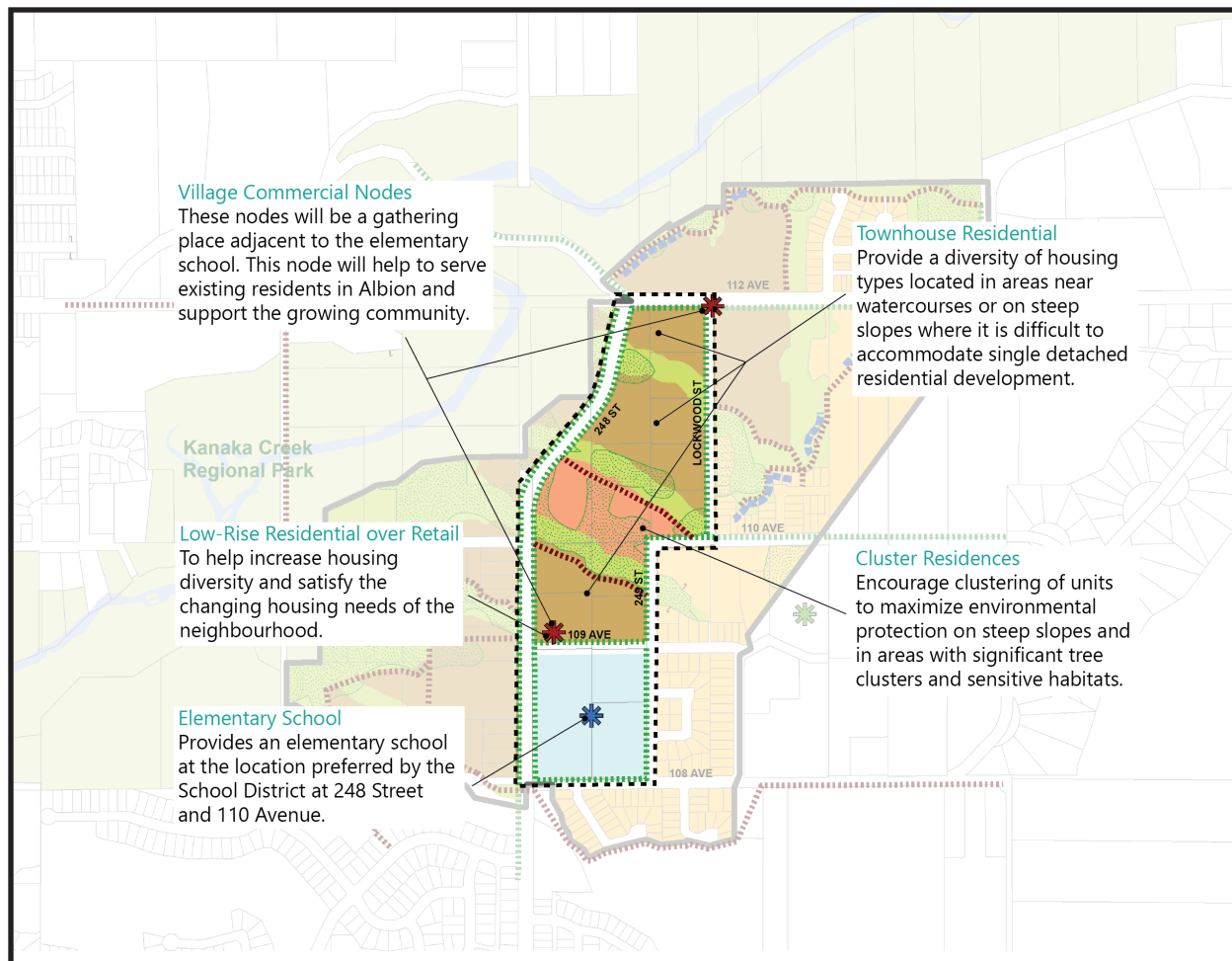
## 10.2.4 | North East Albion Area Development cont.

### B) East of Lockwood Street



## 10.2.4 | North East Albion Area Development cont.

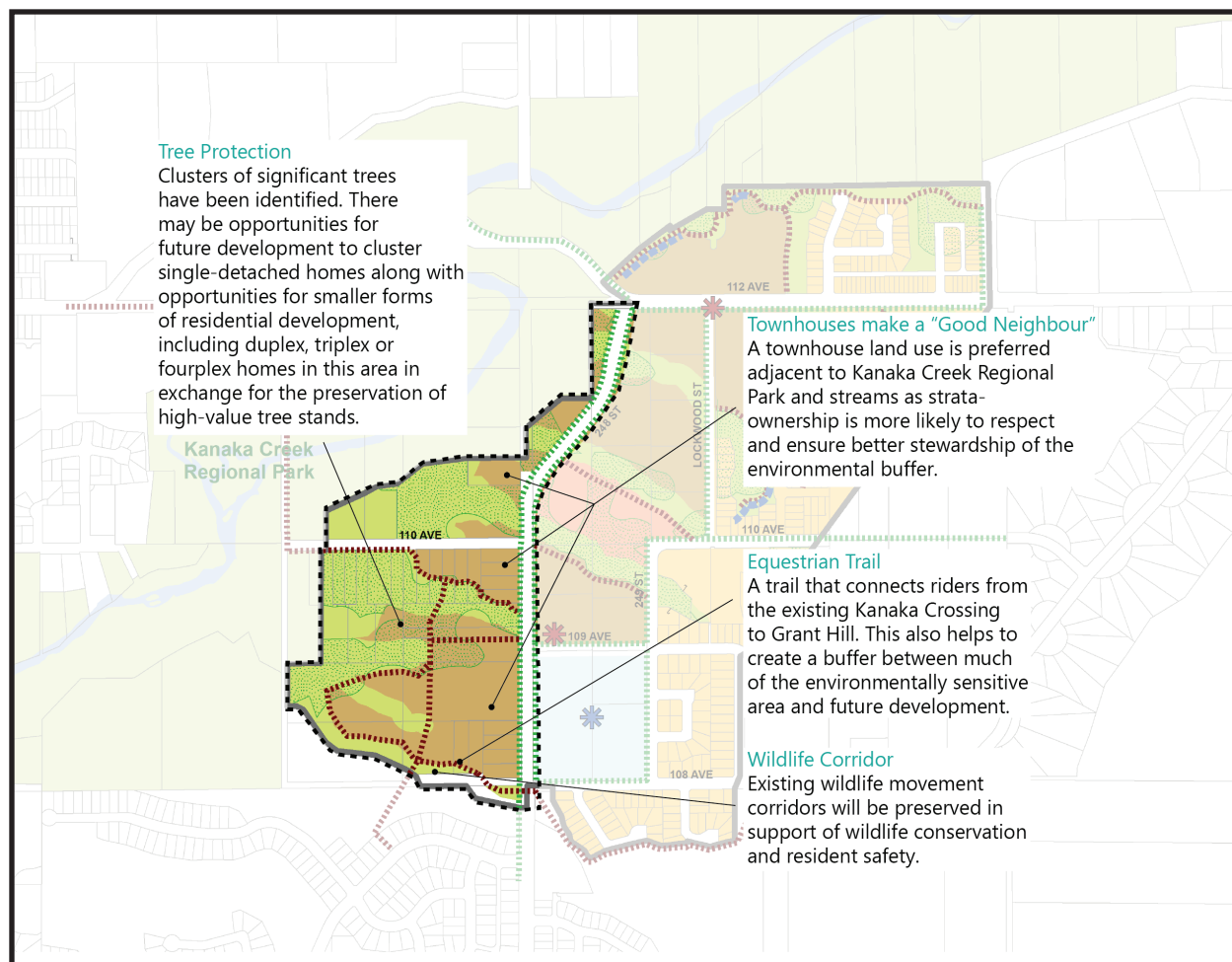
### C) Between 248 Street and Lockwood Street





## 10.2.4 | North East Albion Area Development cont.

### D) West of 248 Street



## 10.2.4 | North East Albion Area Development cont.

- 10 – 43 North East Albion residential developments are to reflect and respect character elements of the existing residential inventory, North East Albion Plan and North East Albion Servicing Strategy Council Policy and Albion Area Plan.

### **Ground-Oriented Residential**

Ground-Oriented Residential corresponds with the single detached residential housing form as well as different combinations of multiplexes and accessory dwelling units.

- 10 – 44 The location of larger residential lots will be encouraged along the eastern Plan boundary, adjacent to the rural properties outside of the Urban Area Boundary, to respect and protect the Urban Area Boundary.
- 10 – 45 Rear-loaded forms of housing will be encouraged along collector and arterial roads.
- 10 – 46 A smooth transition between single detached homes and more intensive forms of housing development, in particular for housing developments along 112th Avenue, 110th Avenue and 248th Street, should be provided through a range of lot sizes.

### **Cluster Residential**

The North East Albion Cluster Residential Land Use Designation is intended for multi-family units that are clustered together to minimize the development footprint and maximize environmental protection in sensitive areas. This designation is intended for ground-oriented multi-unit housing forms.

- 10 – 47 Maple Ridge encourages thoughtful site design for Cluster Residential sites that is considerate to the natural features and topography of the area. Site designs should protect environmentally sensitive areas and permanently preserve important natural features and open space.
- 10 – 48 Cluster residential developments are to locate ground-oriented, rear-loaded housing forms along 248th Street and 110th Avenue and along internal development roads, where possible.
- 10 – 49 Cluster residential developments are encouraged to provide more areas for open space, recreation and social interaction on site.

### **Townhouse**

The North East Albion Townhouse Land Use Designation is intended to accommodate urban townhouses in areas with access to transportation, commercial services and public amenities.

- 10 – 50 Townhouse or strata developments are encouraged to locate adjacent to Kanaka Creek Regional Park as well as sensitive streams to ensure better stewardship of the environmental buffer.
- 10 – 51 North East Albion Area townhouses will consist primarily of two (2) to three (3) storey building forms that house multiple dwelling units. Stacked townhouses may also be considered.



### Background

The Albion Area Plan contains a village commercial centre which will be implemented by a Village Centre Commercial (C-5) Zone. The commercial area will be encouraged to incorporate a building form that is street- front oriented, pedestrian friendly, and accommodate apartments above commercial space. The traditional shopping centre development form with large expanses of parking will not serve the village centre function.

### Policies

#### General Commercial

10 - 52 The siting and form of commercial buildings and areas for parking and storage should facilitate orderly development and ensure the development presents a coherent image, identifying a pedestrian oriented village centre that appropriately integrates adjacent residential uses.

#### North East Albion Area Commercial

10 – 53 The North East Albion Area identifies two commercial nodes (Schedule 1), which are envisioned as anchors for community activity: connected by trails, adjacent to new neighbourhood parks, and a short walk from trailheads into Kanaka Creek Regional Park.

10 – 54 The North East Albion Area intends to blend commercial, residential and institutional uses to create an activated and animated street presence at the commercial nodes.

10 – 55 The North East Albion Area anticipates the northern commercial node developing during early- stages of the Plan build-out. The second commercial node, adjacent to the elementary school site, allows for additional commercial space when sufficient demand exists.

10 – 56 Commercial nodes are to be sized appropriately to meet the needs of the growing North East Albion community with additional room to expand, as demand evolves.

10 – 57 Commercial nodes are anticipated to have capacity for approximately 4 – 6 shops with possible retail uses including café's, restaurants, or other small businesses. At least one commercial node, if not both, should contain childcare or related services, subject to local demand.

10 – 58 Commercial nodes are anticipated to be two (2) – three (3) storeys, with multi-family apartments or townhouses stacked above ground-level commercial, allowing for both affordable home ownership and the possibility of rental housing. Commercial building heights should taper to adjacent lower density land uses to provide a sensitive transition.

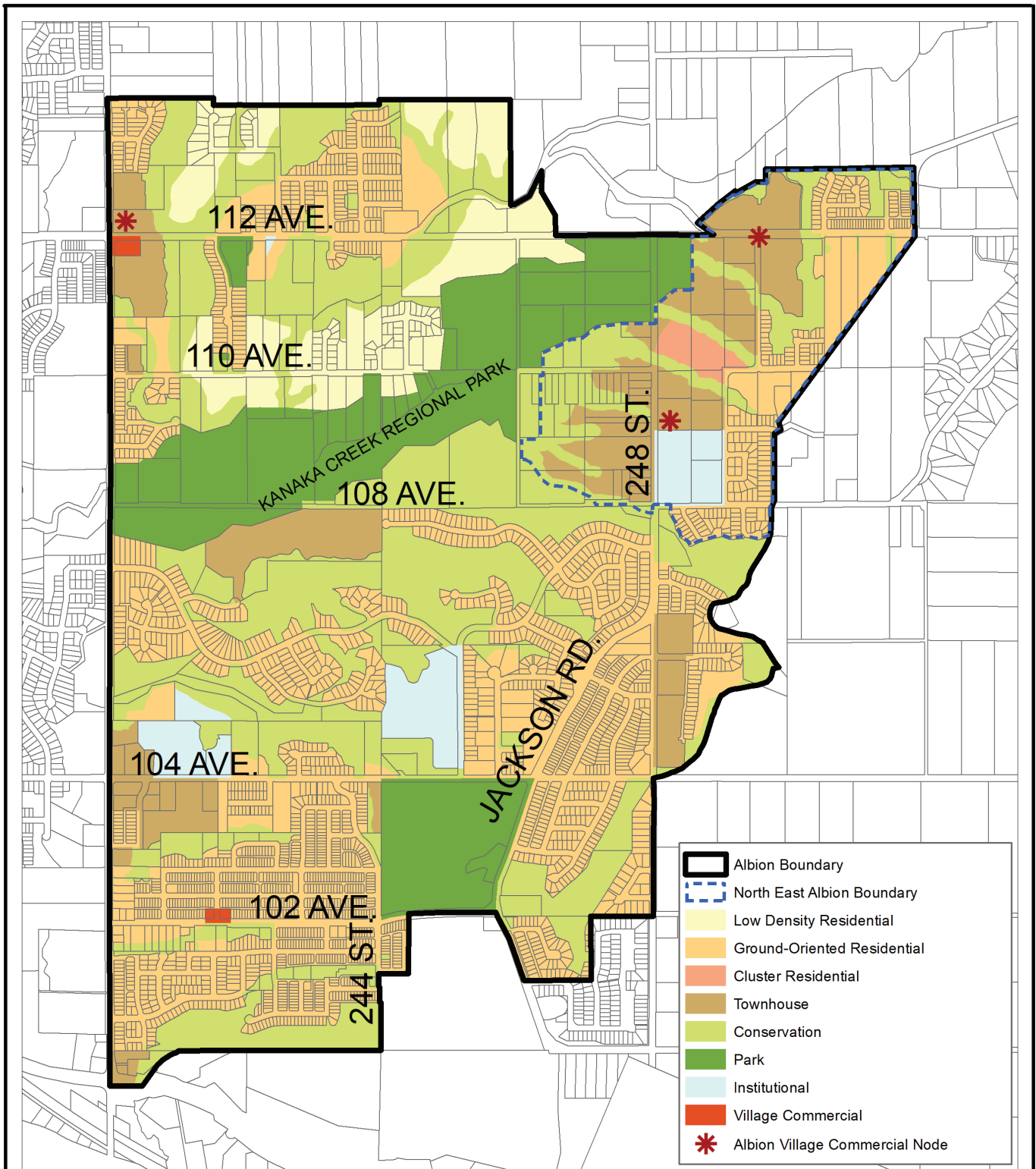
10 – 59 Co-locating the commercial nodes adjacent to multi-family units is encouraged to help support commercial units and create areas of activity and gathering. To animate the streetscape, ground- oriented and street facing units are encouraged along 112th Ave, Lockwood Street, and 248th Street.

10 – 60 Village Commercial Development must also adhere to the Design Guidelines detailed in Section 8 Development Permit Guidelines.

## 10.2.6 | Albion Zoning Matrix

Land Use Designation	Zone(s)
<b>Low Density Residential</b>	RS-1d Single Detached (half acre) Residential*
<b>Ground-Oriented Residential</b>	RS-1 Single Detached Residential RS-1b Single Detached (Medium Density) Urban Residential R-1 Single Detached (Low Density) Urban Residential ** R-2 Single Detached (Medium Density) Urban Residential ** R-3 Single Detached (Intensive) Urban Residential ** R-4 Single Detached (Infill) Urban Residential RUR Urban Infill Residential RT-1 Two-Unit (Duplex) Urban Residential
<b>Cluster Residential</b>	RM-1 Townhouse Residential RST-NEA Street Townhouse Residential – North East Albion
<b>Townhouse</b>	RM-1 Low Density Townhouse Residential RST-NEA Street Townhouse Residential – North East Albion RT-3 Ground-Oriented Residential North East Albion
<b>Albion Village Commercial</b>	C-5 Village Commercial
<b>Park</b>	P-1 Park and School
<b>Conservation</b>	Current or proposed zoning would apply
<b>Institutional</b>	P-1 Park and School P-2 Special Institutional P-3 Children's Institutional P-4 Place of Worship Institutional P-4a Place of Worship Institutional & Educational P-6 Civic Institutional
* Density Bonus provisions are provided in each of these zones ** Legacy Use zones “zone no longer in use”	
<b>Conditions:</b>  a) This Matrix is to be read in conjunction with the policies and criteria in the Albion Area Plan and Official Community Plan and is not the only source for determining applicable zones;  b) All zones corresponding to each land use designation or category may not be appropriate for a specific site, and are not applicable in all areas of the municipality;  c) Lot consolidation may be required to meet the minimum lot area, lot width and/or lot depth requirements of the Zoning Bylaw and/or Subdivision Control Bylaw;  The P-4 Place of Worship Institutional zones would be considered in all Land Use Designations subject to satisfying all requirements of the zone.	





N.T.S.

# ALBION AREA PLAN "SCHEDULE 1"

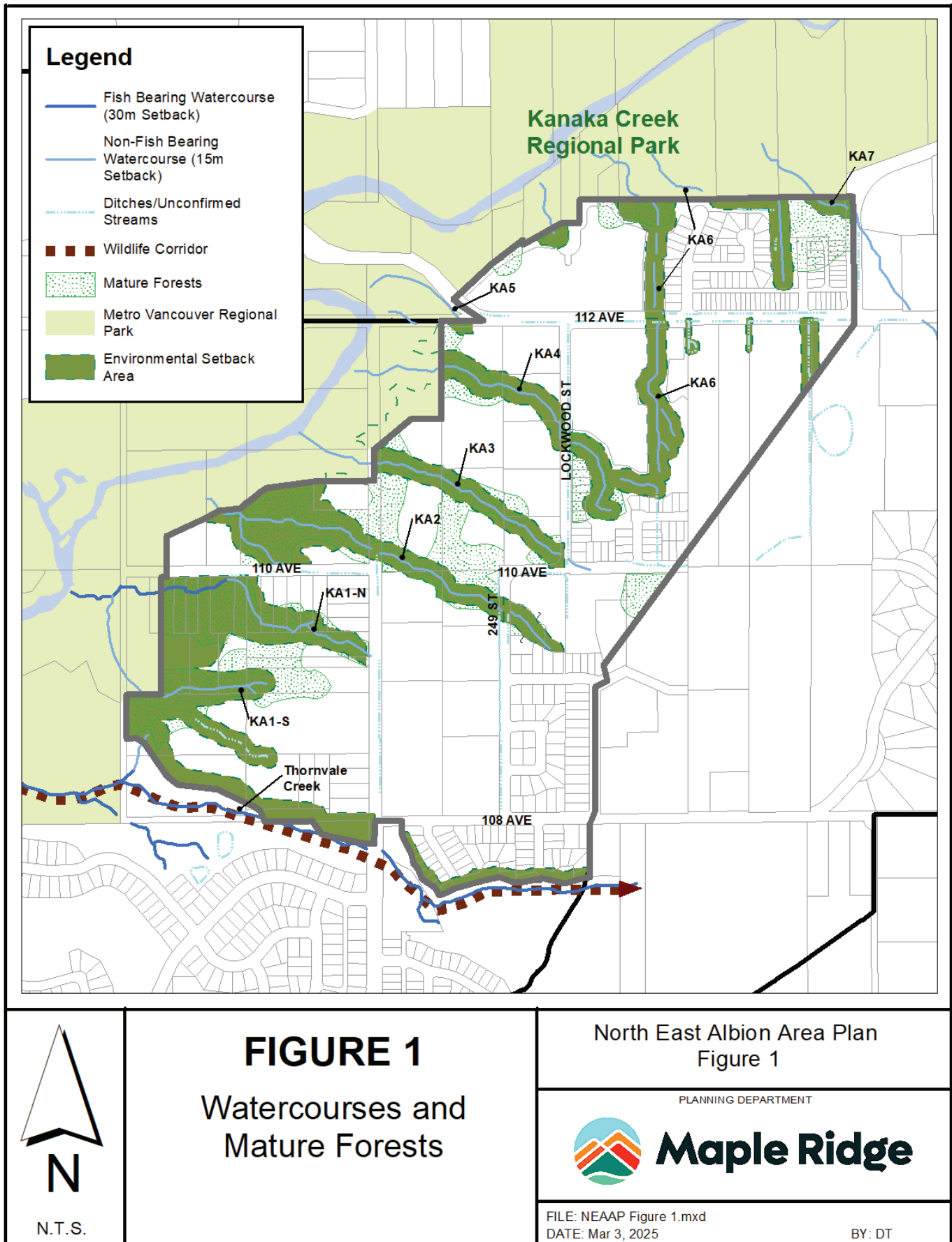


**Maple Ridge**

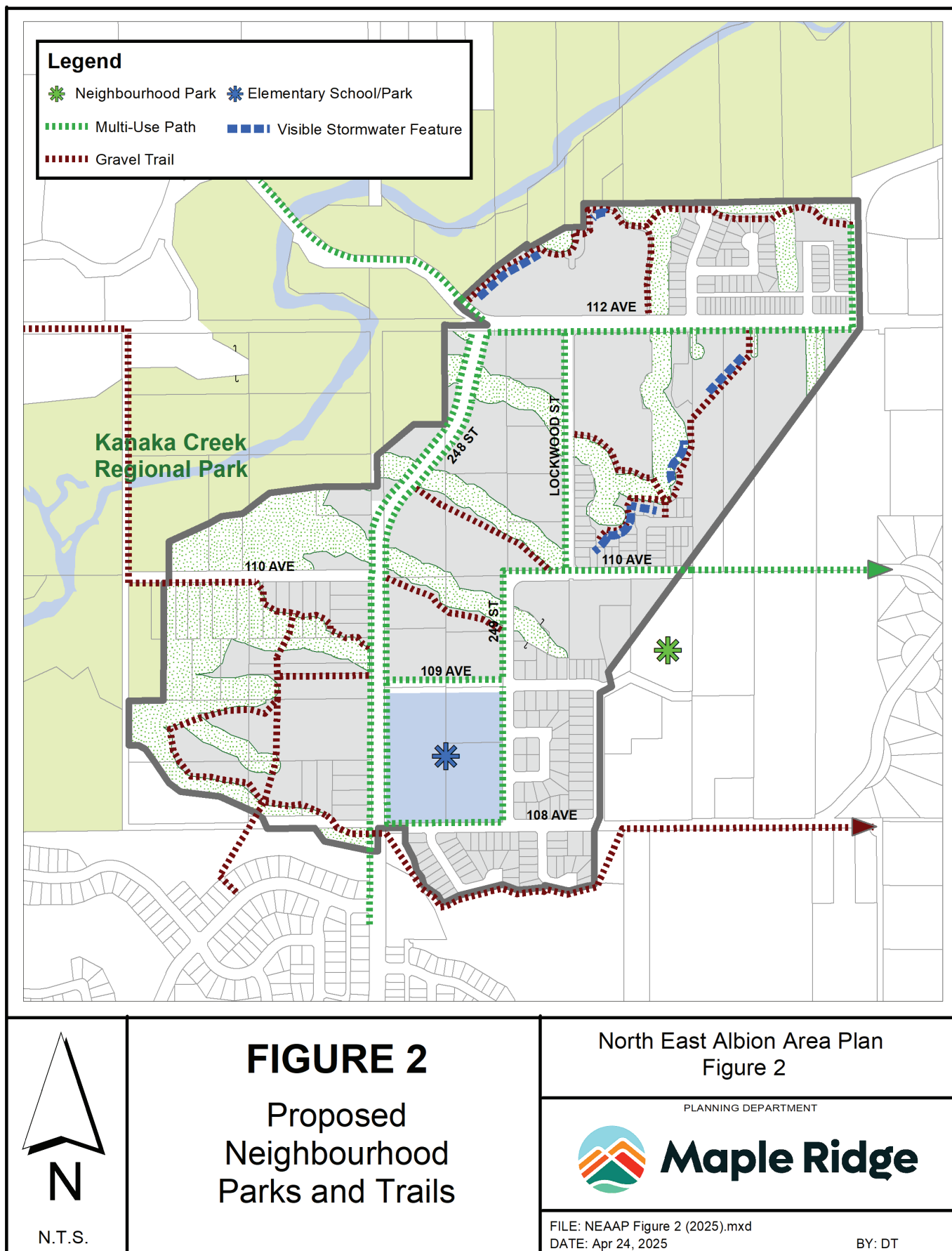
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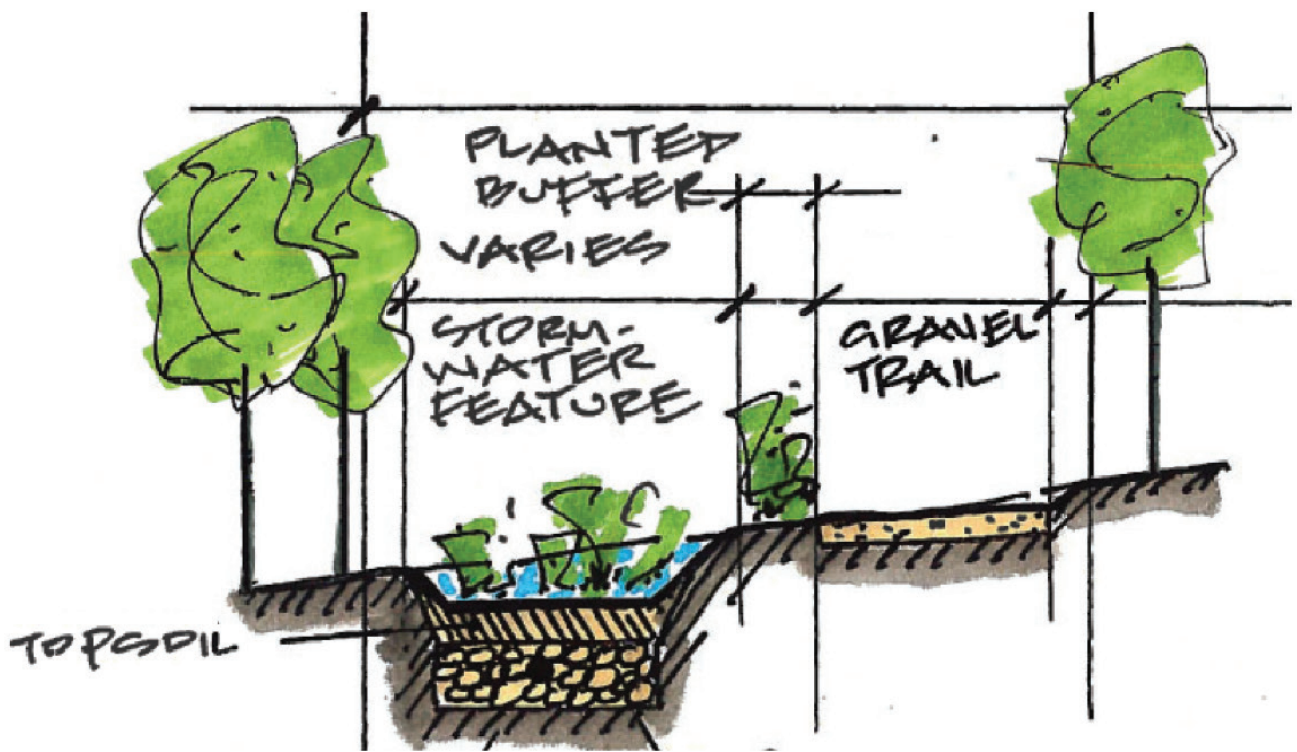
DATE OF LAST REVISION: MMM DD, YYYY

BY: DT









N.T.S.

**FIGURE 3**  
Trail and Stormwater  
Feature Section

North East Albion Area Plan  
Figure 3

PLANNING DEPARTMENT

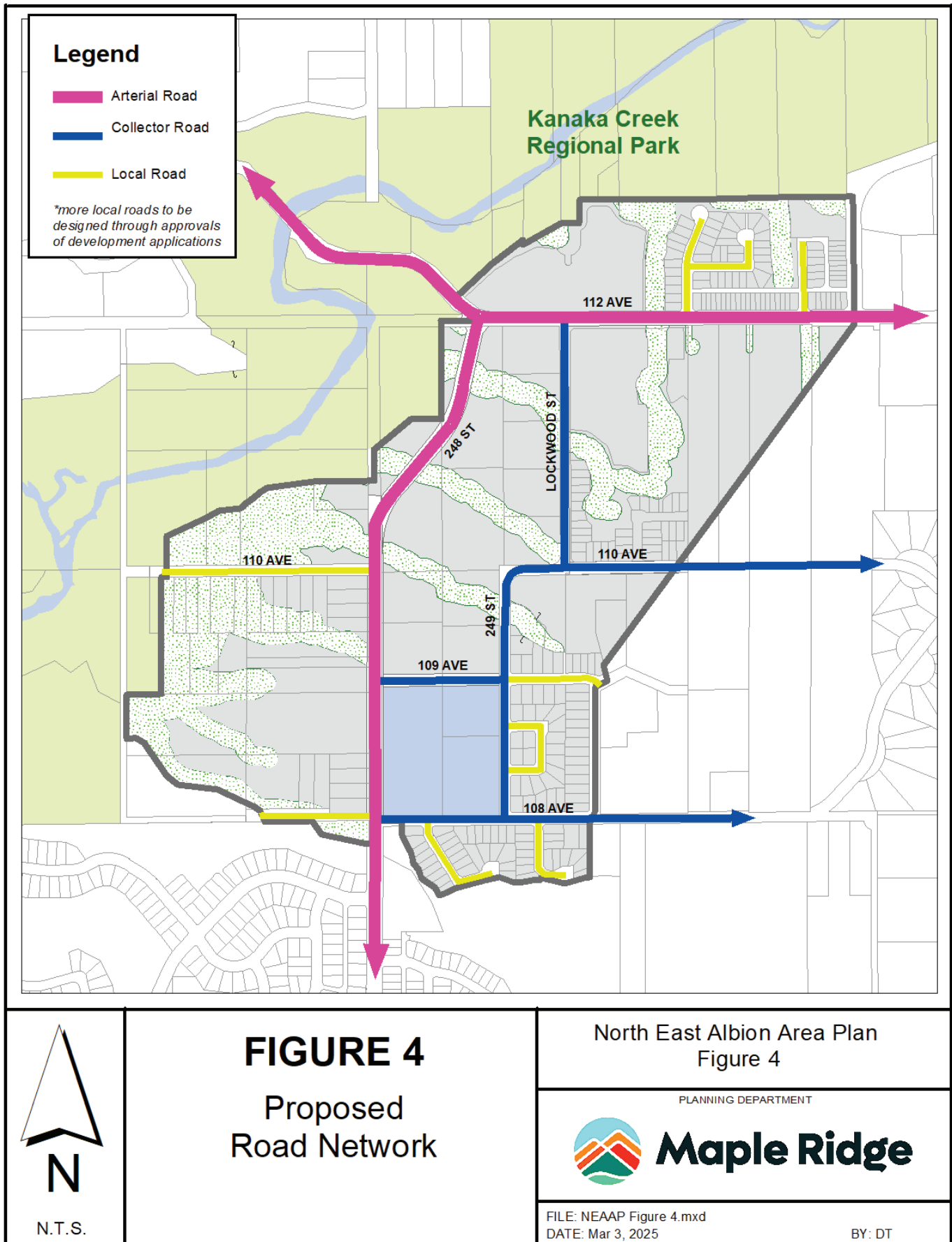


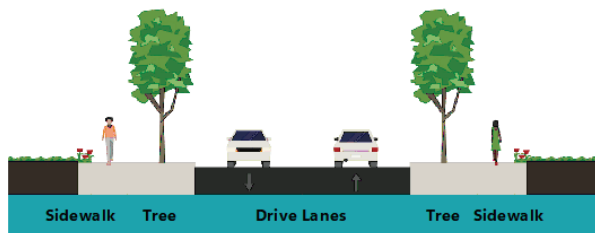
**Maple Ridge**

FILE: NEAAP Figure 3.mxd  
DATE: Mar 12, 2025

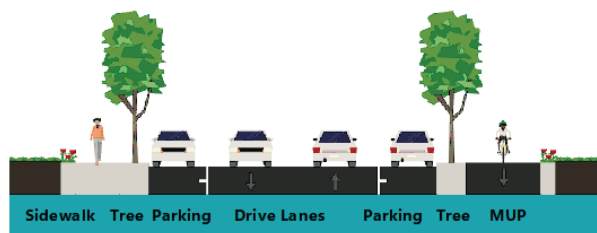
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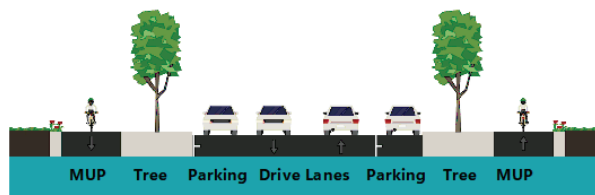




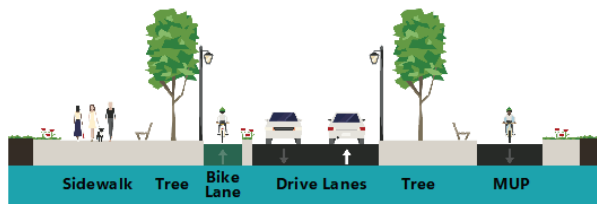
*Typical Section, Local Road  
18m ROW*



*Typical Section, Collector Road  
20m ROW*



*Typical Section, 248 Street  
26m ROW*



*Typical Section, 112 Avenue  
26m ROW*



N.T.S.

## FIGURE 5

### Road Sections

North East Albion Area Plan  
Figure 5

PLANNING DEPARTMENT

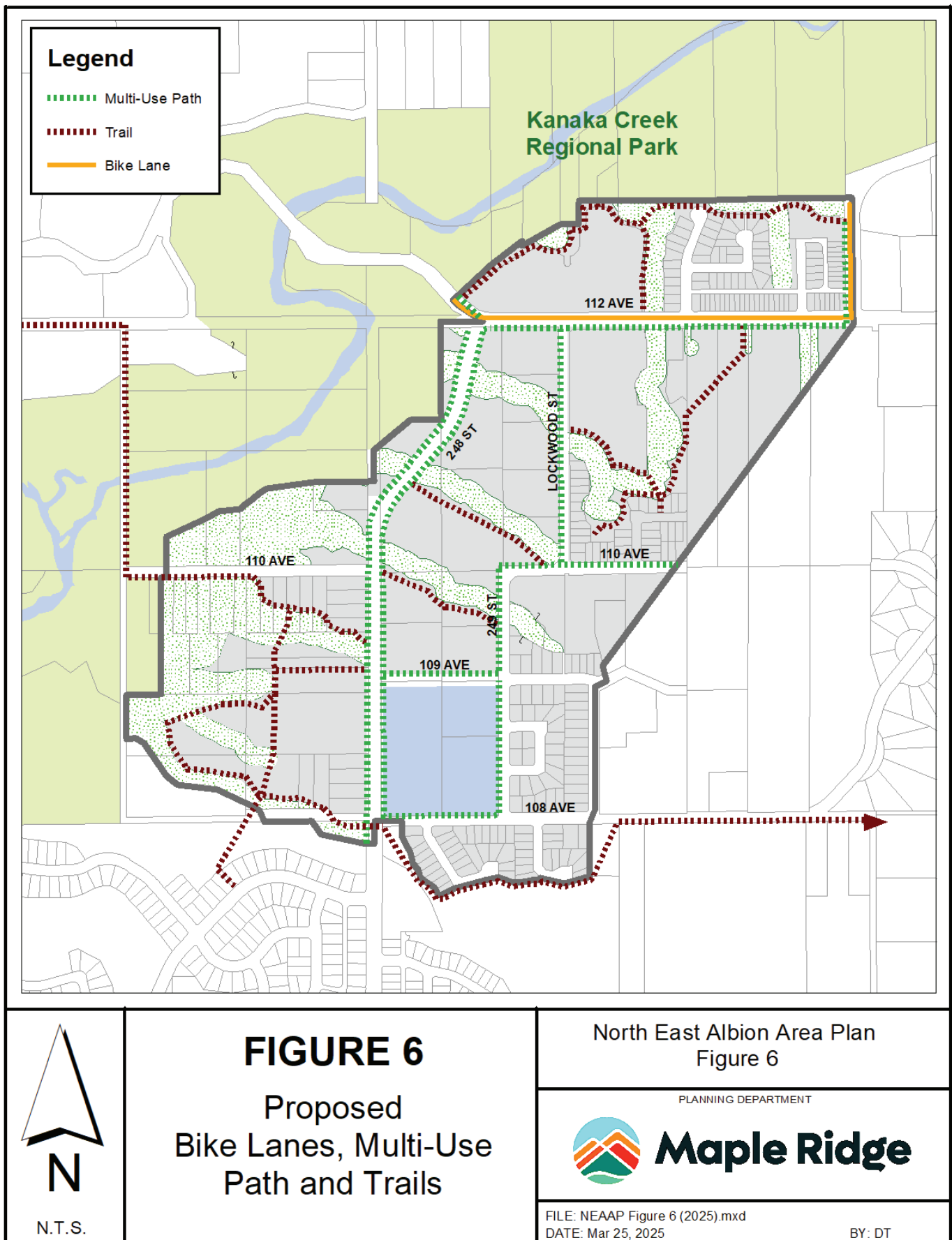


**Maple Ridge**

FILE: NEAAP Figure 5 (2025).mxd  
DATE: Mar 14, 2025

BY: DT







## 10.2 ALBION AREA PLAN





# 10.2 ALBION AREA PLAN

7698-2021

The following policies are adopted to guide development within the Albion urban area.

A land use designation describes an area of land within which a specific set of policies are intended to achieve orderly growth and development within the community.

## 10.2.1 LAND USE DESIGNATIONS

### A. RESIDENTIAL DESIGNATIONS

The following residential land use designations are identified for the **Albion Area Plan** (Schedule 1), ~~outside of the North East Albion area:~~

1. **Low Density Residential** corresponds with single ~~family~~ detached residential development at a low density urban standard with lot sizes at 2000 m<sup>2</sup> (half acre).
2. **Ground-Oriented Residential** is intended to provide housing options that range from a low density detached to medium-high density attached form of ground-oriented housing that will generally be with ground level access to each unit.

The development forms include a single detached with secondary suite and/or detached garden suite, duplex, triplex, and fourplex. These variations provide for a range of housing styles and densities, including smaller lot housing. Duplex, Triplex, Fourplex is a residential use where dwelling units are joined by a common party wall and may take the form of a duplex, triplex, or fourplex.

- ~~3. **Low-Medium Density Residential** corresponds with single detached or duplex housing with lot sizes ranging from 891 m<sup>2</sup> (9590 ft<sup>2</sup>) to 557 m<sup>2</sup> (5996 ft<sup>2</sup>).~~
  - ~~a) notwithstanding the above, a minimum parcel area of "371m<sup>2</sup> (3993 ft<sup>2</sup>)" is only permitted for the properties located at Lot A Section 15 Township 12 New Westminster District Plan 22387 and Lot 1 Section 15 Township 12 New Westminster District Plan 7709.~~
- ~~4. **Medium Density Residential** allows for a range of housing styles and densities, including smaller lot single detached housing, townhouse and duplex dwelling units.~~

~~The following residential land use designations are identified for the **North East Albion Area** (Schedule 2):~~

- ~~1. **Single Family** allows detached dwellings on a range of lot sizes. The intent of the designation is to help protect the rural edge by sensitively locating larger lot sizes adjacent to the eastern Plan boundary while permitting smaller lot sizes closer to the community nodes. This designation also allows secondary suites and/or detached garden suites, where zoning permits.~~
- ~~2. **X-Plex (Duplex, Triplex, Fourplex)** is a residential use where dwelling units are joined by a common party wall and may take the form of a duplex, triplex, or fourplex.~~
3. **Cluster Residential** land use is located within areas that are environmentally sensitive and where some development may be accommodated through integrated site design that considers the natural features

and topography of the area. The designation permits clustered forms of ground-oriented multi-family units, including townhouse, courtyard and fourplex units that minimizes impacts to the land and environment.

4. **Townhouse** permits multi-family housing forms. This designation is in areas with access to major corridors, commercial areas, as well as public amenities. While intended for Townhouse form, there may be circumstances where smaller single family residential forms may be incorporated onto a townhouse site, with the focus on minimizing the development footprints on the land.

Notwithstanding the above, higher densities may be supportable in compliance with the Community Amenity Program regulations prescribed in the Zoning Bylaw.

## **B. ALBION VILLAGE COMMERCIAL DESIGNATION**

This designation is intended to provide for convenience shopping and personal services to residents of surrounding neighbourhoods in a compact village form.

## **C. PARK DESIGNATION**

This designation identifies the location of existing or future municipal or Regional park sites.

## **D. CONSERVATION DESIGNATION**

This designation identifies ecologically sensitive lands that require protection in order to ensure that their health, diversity and integrity are maintained. They are considered to be of high environmental or geologic sensitivity.

## **E. INSTITUTIONAL DESIGNATION**

This designation identifies lands for present or future use as civic or public use and includes school and/or park sites.

The corresponding zones for these designations are outlined in Section 10.2.6 Zoning Matrix.



## 10.2.2 ALBION AREA RESIDENTIAL DEVELOPMENT

### BACKGROUND

The Albion Area Plan strives to balance new residential development **with** the protection of environmentally sensitive areas, and the provision of a range of housing styles to meet diverse housing needs.

### General Development Policies

- 10 – 4** Development applications seeking a land use designation change through an Official Community Plan amendment, to allow an increase in density, will be submitted with an analysis of the existing downstream system to a limit determined by the City. Capacity constraints identified within the existing system are to be resolved prior to additional density being advanced.
- 10 – 5** The sequence of development is to proceed in the most efficient manner, with capital costs and ongoing servicing costs to be minimized. To achieve this, lot consolidation may be required.
- 10 – 6** Land assembly or lot consolidation proposed in conjunction with development or redevelopment should meet the following conditions:
- A) That any residual lots or remaining land parcels are left in a configuration and lot area which are suitable for a future development proposal, or can be consolidated with other abutting residual lots or land parcel and would be able to comply with the applicable Land Use Designation(s) and Policies of this Plan;
  - B) The use of any residual abutting lots or land parcels can continue to function in accordance with the applicable Land Use Designation and Policies of this Plan;
  - C) Residual abutting lots or land parcels are not to become isolated or left in a condition which is unsuitable for redevelopment or unsuitable for the maintenance of the existing land use; and
  - D) The land assembly proposal will incorporate adequate impact mitigation measures such as the provision of buffers, landscaping, site design, building arrangements and building design to ensure compatibility with abutting existing land uses.

### General Multi-Family Housing Development Policies

- 10 – 7** The siting, form, character and massing of multi-family residential development and areas for parking, storage and landscaping should be designed to facilitate orderly development of the area and to be compatible with single **family detached** residential developments.

- 10 – 8** Multi-family housing should be provided with appropriate amenities to enhance the character of development.
- 10 – 9** The siting of development and construction techniques employed should avoid impacts to watercourses, reduce the risk of erosion in the area, minimize visual disruption to the neighbourhood and protect the visual character of the landscape.
- 10 – 10** Natural vegetation should be retained where possible to enhance the character of the area. Significant treed slopes or natural vegetation will need to be retained in order to minimize disruption to watercourses and to minimize erosion. Geotechnical assessments may be required.
- 10 – 11** Development in the Albion Area is subject to the Natural Features policies of the Official Community Plan.
- 10 – 12** Residential Development must also adhere to the Design Guidelines detailed in Section 8 Development Permit Area Guidelines.
- 10 – 13** The intersection on 240 Street and 112 Avenue will provide commercial services for the area while also allowing for residential units above in a compact village form.





## 10.2.3 COMMUNITY AMENITY PROGRAM AND DENSITY BONUS FRAMEWORK

### BACKGROUND

The Community Amenity Program is intended to meet the needs of the community and respond to changes in housing form and demand over time by enabling an additional means of providing neighbourhood amenities.

The Albion Area Plan Community Amenity Program provides the opportunity for a Density Bonus within a number of zones identified within the Albion Zoning Matrix, see Section 10.2.6. Within these zones, 'bonus' density may be achieved through an Amenity Contribution toward community amenities that will be located within the boundaries of the Albion Area Plan.

### OBJECTIVE

To support increases in density as a means of providing additional amenities in the Albion Area Plan.

### POLICIES

**10 – 142** The city-wide Community Amenity Program, established ~~in~~through Section 2.1.2 Compact and Unique Community, will apply to the entire Albion Area Plan.

~~**10 – 153** A Density Bonus option will be permitted on lands designated Low Density Residential, Ground Oriented Residential, Cluster Residential, Townhouse and Village Commercial. Low Medium Density Residential and Medium Density Residential in the Albion Area Plan, as well as on the lands designated Single Family, Cluster Residential, Townhouse and Village Commercial in the North East Albion Area Plan.~~

~~**10 – 164** Where the density bonus option is utilized in a single-family detached subdivision, the density bonus framework provisions established in the Maple Ridge Zoning Bylaw will apply to all lots that exceed the base density permitted in the zone, in addition to the city-wide Community Amenity Program established in Official Community Plan Section 2.1.2 Compact and Unique Community.~~

~~**10 – 175** Where the density bonus option is utilized in a multi-family development, the density bonus framework provisions established in the Maple Ridge Zoning Bylaw will apply to all dwelling units that exceed the base density permitted in the zone, in addition to the city-wide Community Amenity Program established in Official Community Plan Section 2.1.2 Compact and Unique Community.~~

~~**10 – 186** Where the density bonus option is utilized in a mixed-use commercial development, the density bonus framework provisions established in the Maple Ridge Zoning Bylaw will apply to all dwelling units that exceed the base density permitted in the zone, in addition to the city-wide Community Amenity Program established in the Official Community Plan Section 2.1.2 Compact and Unique Community.~~

~~**10 – 197** Maple Ridge Council may consider Density Bonuses as part of the development review process for Albion Area Plan amendment applications seeking a land use designation change that would permit a higher density than currently permitted.~~

**10 – 15 18** A Density bonus will only be permitted on those lands that are located entirely within the boundaries of the Albion Area Plan and Urban Area Boundary.

## 10.2.4 NORTH EAST ALBION AREA DEVELOPMENT

### North East Albion Area

The North East Albion Area is bound by the Kanaka Creek Regional Park to the north, existing Albion Area residential developments and conservation areas to the southwest, including the Kanaka Creek Regional Park, and rural residential developments to the east. ~~The North East Albion Area boundary and Land Use Designations are shown on Schedule 2 of this Plan.~~

The North East Albion Area Plan emphasizes the development of a complete community by integrating environmental stewardship, the protection of natural resources and the preservation of ecosystems, while balancing the social and economic objectives of the community.

The North East Albion Area Plan sets out residential pockets nestled between existing watercourses and linked by a network of pathways and trails. A multi-modal network links neighbourhood amenities, such as ~~parks~~, an elementary school, and shopping nodes within the community and to the larger Albion Area and Kanaka Creek Regional Park.

The North East Albion Area Plan focuses multi-family development around commercial and amenity nodes; offering residents gathering spaces and commercial opportunities, and bringing to life one of the City's growth neighbourhoods. It is intended that the Area Plan policies facilitate the creation of an inclusive and sustainable community with equal opportunities for people of all ages, cultures, lifestyles, and abilities.

Development in the North East Albion Area is to be reflective of the consultation with area residents and other stakeholders and should align with the principles and intent laid out in this Plan.

The following residential land use designations are identified for the North East Albion Area Plan (Schedule 1)

**1. Ground-Oriented Residential** corresponds with:

- a. Single Detached Residential allows detached dwellings on a range of lot sizes. The intent of the designation is to help protect the rural edge by sensitively locating larger lot sizes adjacent to the eastern Plan boundary while permitting smaller lot sizes closer to the community nodes. This designation also allows secondary suites and/or detached garden suites, as well as duplex where zoning permits.
- b. Duplex, Triplexes, Fourplexes and different combinations of a Single Detached unit with a Detached Garden Suite and/or a Secondary Suite where zoning permits. These variations provide for a range of housing styles and densities, including smaller lot housing. Duplex, Triplex, Fourplex is a residential use where dwelling units are joined by a common party wall and may take the form of a duplex, triplex, or fourplex.

**2. Cluster Residential** land use is located within areas that are environmentally sensitive and where some development may be accommodated through integrated site design that considers the natural features and topography of the area. The designation permits clustered forms of ground-oriented multi-family units, including townhouse, courtyard and fourplex



units that minimizes impacts to the land and environment.

3. **Townhouse** permits multi-family housing forms. This designation is in areas with access to major corridors, commercial areas, as well as public amenities. While intended for Townhouse form, there may be circumstances where smaller single family residential forms may be incorporated onto a townhouse site, with the focus on minimizing the development footprints on the land.

## **POICIES**

**10 – 169** Development in the North East Albion Area will be subject to the North East Albion Servicing Strategy Policy as well as other relevant City documents policies and regulations.

### **Environment**

**10 – 1720** Maple Ridge will continue to apply regulations for the protection of watercourses and steep slopes in North East Albion (Figure 1) in order to enhance and maintain environmental health integrity.

**10 – 1821** Development in the North East Albion Area is to be conducted in such a manner as to mitigate impact on the environment by increasing connections for wildlife, preserving existing hydrological processes, protecting fish and fish habitat, minimizing the potential for stream erosion, preventing surface flooding, and ensuring major storm conveyance systems are capable of conveying significant flows and minimize damage to life and properties under extreme storm conditions.

**10 – 1922** Maple Ridge will encourage the retention of significant tree stands that are not already protected through existing legislation, bylaws, or policies, through thoughtful site design.

**10 – 20** For sites identified on North East Albion Schedule 2, additional floorspace or a reduction in permissible lot size may be supported if environmentally sensitive areas are protected and permanently preserved beyond what is required through the land dedication process.

**10 – 214** Maple Ridge recognizes that Kanaka Creek Regional Park provides valuable habitat for wildlife and movement corridors, and as such, supports preserving existing movement corridors along Kanaka Creek and Grant Hill. Enhancement of landscaping and stormwater management features is also encouraged with the aim of supporting wildlife conservation and resident safety.

**10 – 225** Maple Ridge will connect the North of 112 Avenue precinct, including the trailhead to Kanaka Creek Regional Park, to the co-located school and park site in the south via a 'Spine Trail', that will serve as an important stormwater feature and protect and celebrate the water resources of the area (Figure 2).

**10 – 236** Envisioned as a key feature of the North East Area Plan, a series of connected ponds will be woven into the Spine Trail system (Figure 3).

**10 – 247** Maple Ridge will integrate a Northern Greenway trail, which will include similar features as the Spine Trail (Figure 3), along Kanaka Creek Regional Park at the north end of the North East Albion Plan Area.

**10 – 258** Maple Ridge will establish a buffer (Figure 2) between the Kanaka Creek Regional Park and future areas of residential development to protect and improve wildlife passage and resident safety in and around the North East Albion Area.

- 10 – 269 Maple Ridge encourages strata developments adjacent to regional park land to ensure better stewardship of the environmental buffer.
- 10 – 2730 The North East Albion Area Plan outlines a vision and strategy for sustainable water stewardship practices. As such, development is to:
- A) Maximize groundwater protection and water conservation efforts;
  - B) Incorporate stormwater management best practices, including the integration of visible stormwater management features, into the site design; and
  - C) Require a geotechnical investigation, including a ground-water impact assessment and subsequent impact mitigation measures, in order to protect Kanaka Creek and drinking water resources.

## Servicing

- 10 – 2831 Maple Ridge will encourage the integration of aesthetically pleasing designs with appropriate siting, building form, landscaping and screening into the civic infrastructure necessary for the build-out of the North East Albion Area, such as the proposed pump station on 112th Street.
- 10 – 2932 Development in the North East Albion Area will be subject to the outcomes of the water analysis, conducted for the development of the North East Albion Servicing Strategy, as well as other relevant City documents.
- 10 – 3033 Development in the North East Albion Area will be subject to the proposed transportation network identified in Figure 4 & 5 as well as the City's Strategic Transportation Plan, as amended from time to time.

## Mobility

- 10 – 314 Maple Ridge will ensure that the design of new development aligns with the City's requirements for creating a safe, integrated, multi-modal transportation system.
- 10 – 325 The North East Albion Area Plan sets out an integrated active transportation network (Figure 6) which includes proposed bike lanes, trails and a multi-use pathways.
- 10 – 336 North East Albion is identified in the City's Strategic Transportation Plan as an area intended for improved ~~sidewalk~~ connectivity for active travel modes. It is anticipated that future ~~sidewalk construction and~~ streetscape improvements, including street lighting, will be provided through the development process.
- 10 – 347 The North East Albion Area Plan sets out an equestrian trail (Figure 6) that connects riders from the existing Kanaka Creek Crossing to Grant Hill. This trail will help to create a buffer between environmentally sensitive areas and future development.
- 10 – 358 North East Albion is identified as an area that is not well served by transit. It is anticipated that long-term transit services will continue to be provided via Community Shuttle transit services, and that Maple Ridge will continue to support TransLink for the development of an efficient transit system through the North East Albion Area.



## Parks and Community Amenities

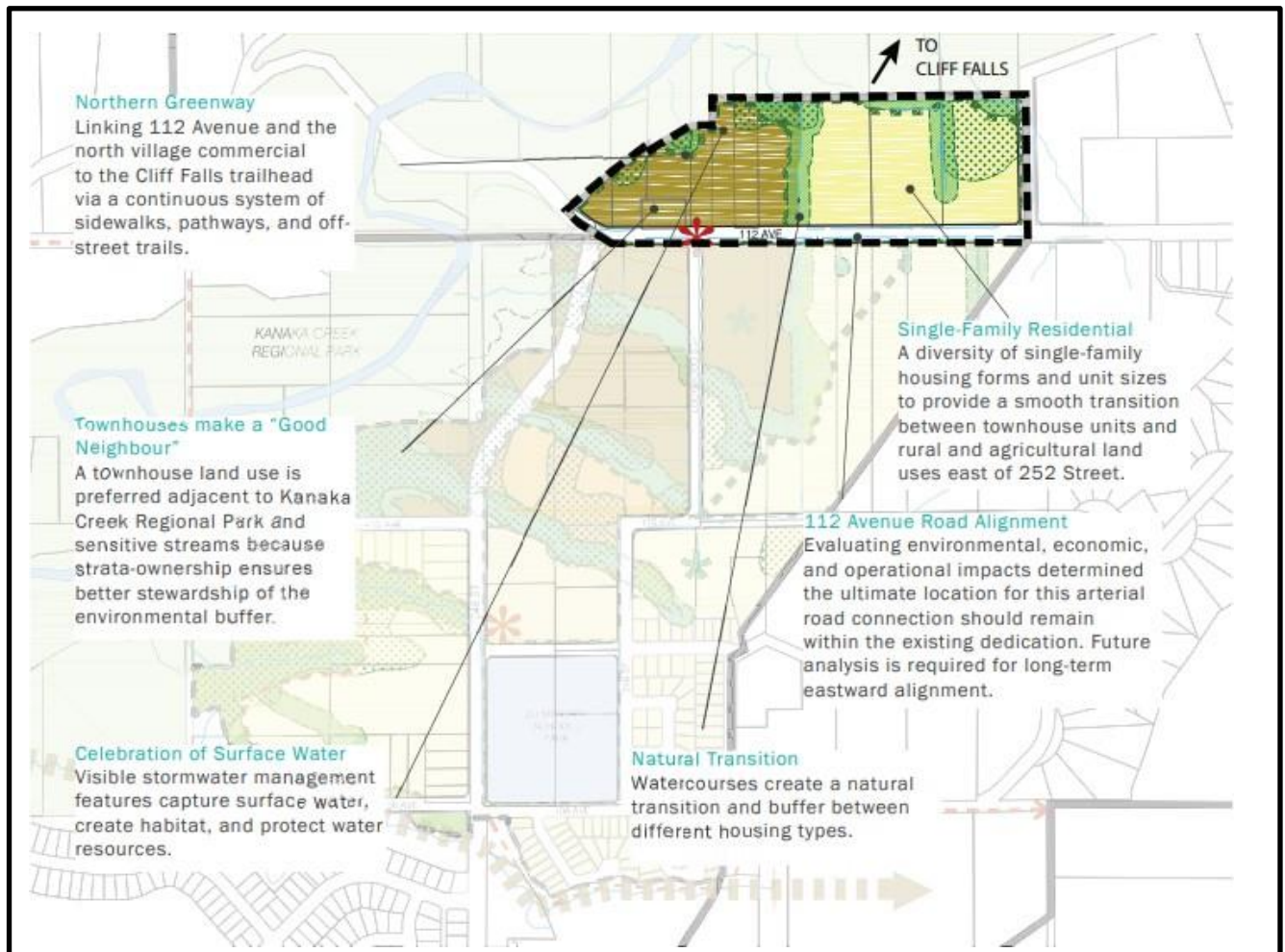
- 10 – 369 The North East Albion Area Plan identifies the co-location of a park and elementary school with a sports field, playground, and pathways to meet the needs of the growing community (Figure 2).
- 10 – 3740 The North East Albion Area Plan proposes an additional neighbourhood park space (Figure 2) to the co-located park and elementary school. Neighbourhood parks provide important amenities such as neighbourhood parks are important components of complete, liveable communities, and will include amenities such as playgrounds, sports courts, dog off leash areas, small splash pads, open areas, seating areas and looping pathways. The specific park amenities will be determined through a park design and engagement process with future residents.
- 10 – 3841 The North East Albion Area Plan stipulates that all residents will have access to a park within a 510-minute walk.
- 10 – 3942 North East Albion Area park amenities will include spaces for dogs, such as a dog-off leash park-nearby.
- 10 – 4043 The North East Albion Area Plan includes sets-out a network of trails and multi-use pathways to provide neighbourhood connections to link the residential areas with the community amenities. These trails and pathways will act as a greenway and buffer to the sensitive environmental areas while creating an attractive trail network and public realm for residents (Figure 2).
- 10 – 414 Maple Ridge will encourage the inclusion of Public Art into residential and community amenities.

## Residential Development

- 10 – 425 The North East Albion Area is broken up into the following neighbourhood precincts:

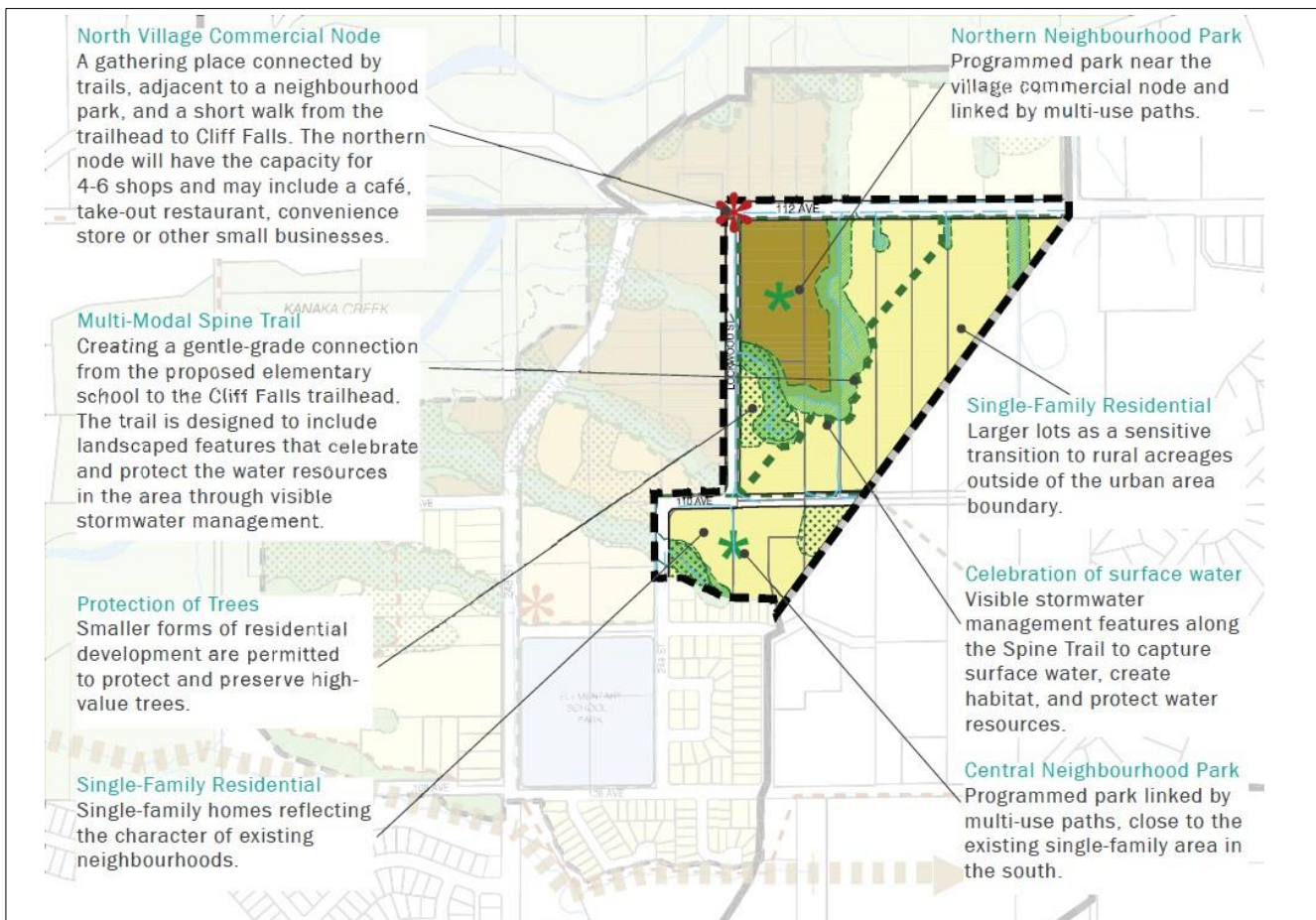
- A) North of 112 Avenue
- B) East of Lockwood Street
- C) Between 248 Street and Lockwood Street
- D) West of 248 Street

## A) North of 112 Avenue

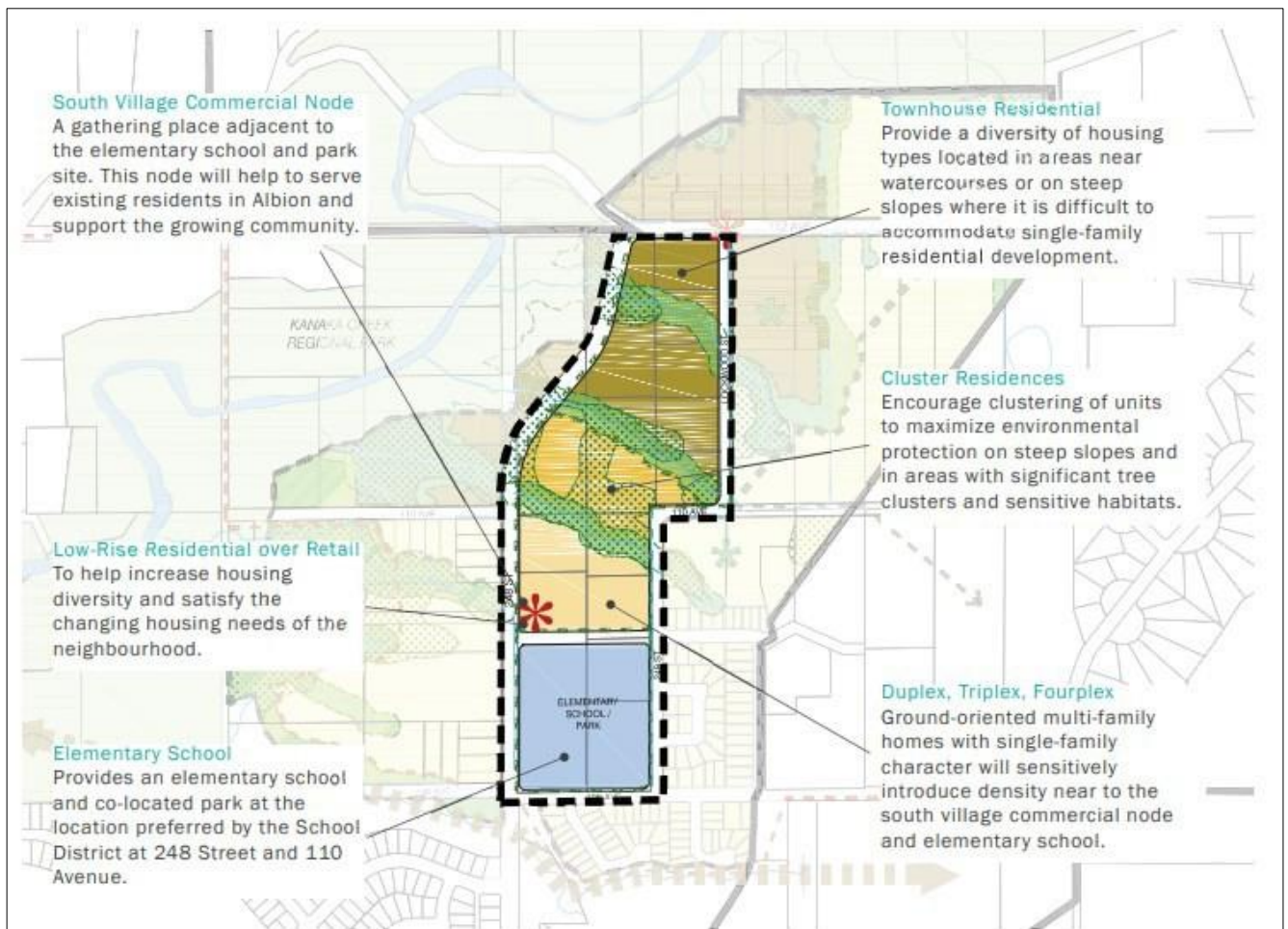




## B) East of Lockwood Street

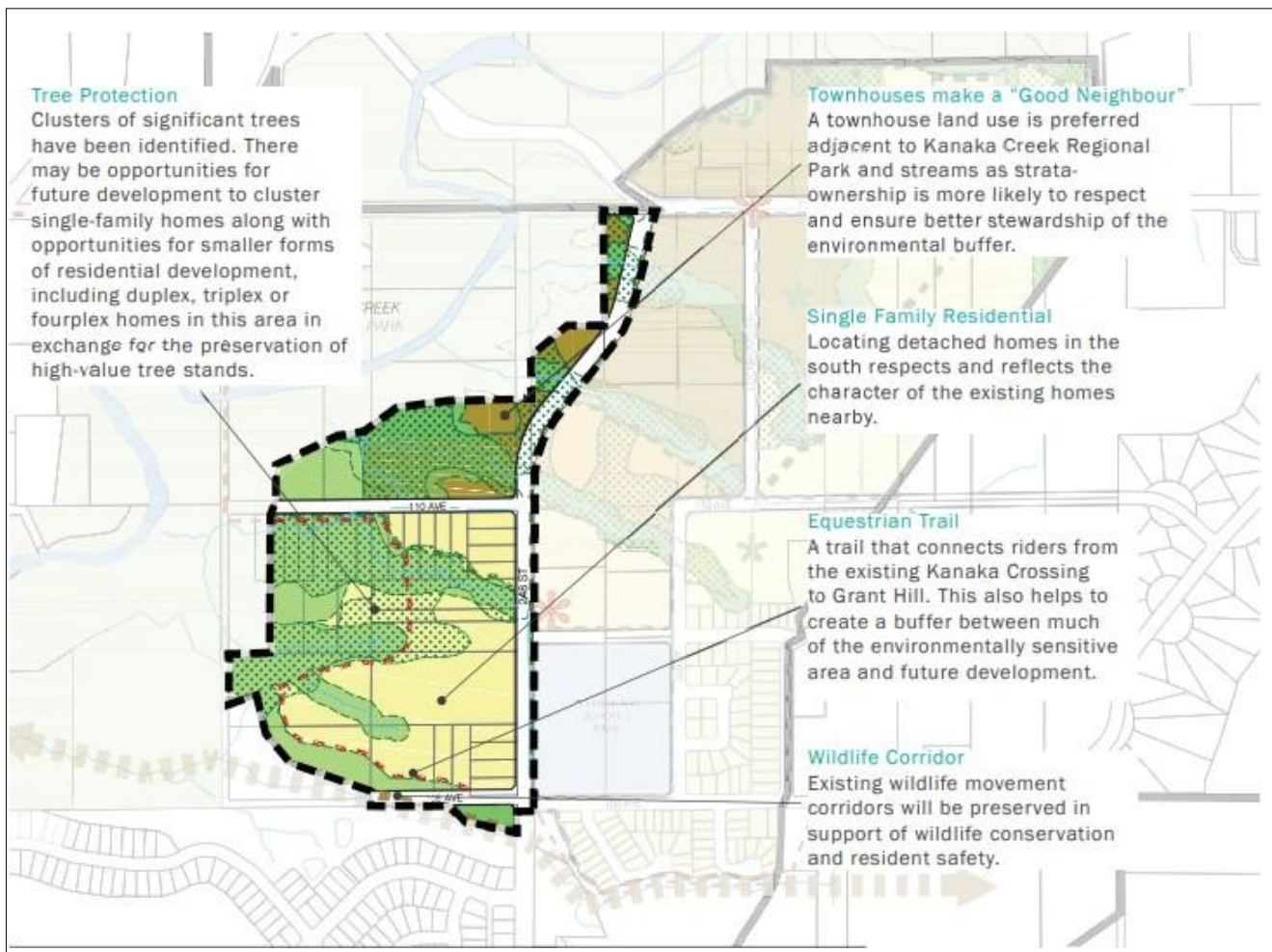


### C) Between 248 Street and Lockwood Street





## D) West of 248 Street



**10 – 436** North East Albion residential developments are to reflect and respect character elements of the existing residential inventory, North East Albion Plan and North East Albion Servicing Strategy Council Policy and Albion Area Plan.

## ~~Single Family~~

~~The North East Albion Area Single Family designation is intended for detached dwellings on rural to smaller urban lots, allowing for a range of homeownership opportunities.~~

## Ground-Oriented Residential

Ground-Oriented Residential corresponds with the single detached residential housing form as well as different combinations of multiplexes and accessory dwelling units.

~~10 – 47 Maple Ridge will encourage the inclusion of secondary suites and detached garden suites, where regulations permit, to enhance the range of housing affordability options.~~

10 – ~~44~~8 The location of larger residential lots will be encouraged along the eastern Plan boundary, adjacent to the rural properties outside of the Urban Area Boundary, to respect and protect the Urban Area Boundary.

10 – ~~45~~9 Rear-loaded forms of housing will be encouraged along collector and arterial roads.

10 – ~~46~~9 A smooth transition between single detached family homes and more intensive forms of housing development, in particular for housing developments along 112th Avenue, 110th Avenue and 248th Street, should be provided through a range of lot sizes.

## Cluster Residential

The North East Albion Cluster Residential Land Use Designation is intended for multi-family units that are clustered together to minimize the development footprint and maximize environmental protection in sensitive areas. This designation is intended for ground-oriented multi-unit housing forms.

10 – ~~47~~52 Maple Ridge encourages thoughtful site design for Cluster Residential sites that is considerate to the natural features and topography of the area. Site designs should protect environmentally sensitive areas and permanently preserve important natural features and open space.

10 – ~~48~~53 Cluster residential developments are to locate ground-oriented, rear-loaded housing forms along 248th Street and 110th Avenue and along internal development roads, where possible.

10 – ~~49~~54 Cluster residential developments are encouraged to provide more areas for open space, recreation and social interaction on site.

## ~~X-Plex (Duplex, Triplex, Fourplex)~~

~~The North East Albion Area X-Plex Land Use Designation provides for housing options with the outward appearance of a single family home yet feature multiple units and distinct entries.~~

~~10 – 51 X-plex developments should be ground-oriented, rear-loaded housing forms along 248th Street and 110th Avenue and encourage rear-loaded housing forms along internal development roads, where possible.~~

## Townhouse

The North East Albion Townhouse Land Use Designation is intended to accommodate urban townhouses in areas with access to transportation, commercial services and public amenities.

10 – ~~50~~55 Townhouse or strata developments are encouraged to locate adjacent to Kanaka Creek Regional Park as well as sensitive streams to ensure better stewardship of the environmental buffer.

10 – ~~51~~56 North East Albion Area townhouses will consist primarily of two (2) to three (3) storey building forms that



house multiple dwelling units. Stacked townhouses may also be considered.

## 10.2.5 ALBION VILLAGE COMMERCIAL

The Albion Area Plan contains a village commercial centre which will be implemented by a Village Centre Commercial (C-5) Zone. The commercial area will be encouraged to incorporate a building form that is street-front oriented, pedestrian friendly, and accommodate apartments above commercial space. The traditional shopping centre development form with large expanses of parking will not serve the village centre function.

### POLICIES

#### General Commercial

**10 - 5257** The siting and form of commercial buildings and areas for parking and storage should facilitate orderly development and ensure the development presents a coherent image, identifying a pedestrian oriented village centre that appropriately integrates adjacent residential uses.

#### North East Albion Area Commercial

**10 – 5358** The North East Albion Area Plan identifies two commercial nodes (Schedule 12), which are envisioned as anchors for community activity: connected by trails, adjacent to new neighbourhood parks, and a short walk from trailheads into Kanaka Creek Regional Park.

**10 – 5459** The North East Albion Area Plan intends to blend commercial, residential and institutional uses to create an activated and animated street presence at the commercial nodes.

**10 – 5560** The North East Albion Area Plan anticipates the northern commercial node developing during early-stages of the Plan build-out. The second commercial node, adjacent to the elementary school site, allows for additional commercial space when sufficient demand exists.

**10 – 5661** Commercial nodes are to be sized appropriately to meet the needs of the growing North East Albion community with additional room to expand, as demand evolves.

**10 – 5762** Commercial nodes are anticipated to have capacity for approximately 4 – 6 shops with possible retail uses including café's, restaurants, or other small businesses. At least one commercial node, if not both, should contain childcare or related services, subject to local demand.

**10 – 5863** Commercial nodes are anticipated to be two (2) – three (3) storeys, with multi-family apartments or townhouses stacked above ground-level commercial, allowing for both affordable home ownership and the possibility of rental housing. Commercial building heights should taper to adjacent lower density land uses to provide a sensitive transition.

**10 – 5964** Co-locating the commercial nodes adjacent to multi-family units is encouraged to help support commercial units and create areas of activity and gathering. To animate the streetscape, ground-oriented and street facing units are encouraged along 112th Ave, Lockwood Street, and 248th Street.

**10 – 6065** Village Commercial Development must also adhere to the Design Guidelines detailed in Section 8 Development Permit Guidelines.



## 10.2.6 ALBION ZONING MATRIX

LAND USE DESIGNATION	ZONE(s)
Low Density Residential	RS-1d <del>Single Detached One-Family Urban</del> (half acre) Residential*
<b>Ground-Oriented Residential</b>  <del>Low / Medium Density Residential</del>  <b>Medium Density Residential</b>	<del>R-1 Residential District</del> <del>R-2 Urban Residential District</del> <del>R-3 Special Amenity Residential District</del> <del>RM-1 Townhouse Residential* RMH Mobile Home District</del>  RS-1 Single Detached Residential  RS-1b Single Detached (Medium Density) Residential  R-1 Single Detached (Low Density) Urban Residential **  R-2 Single Detached (Medium Density) Urban Residential **  R-3 Single Detached (Intensive) Urban Residential **  R-4 Single Detached (Infill) Urban Residential  RUR Urban Infill Residential  RT-1 Two-Unit (Duplex) Urban Residential
Cluster Residential	<del>RM-1 Low Density Townhouse Residential *</del> RT-3 Ground-Oriented Residential North East Albion RST-NEA Street Townhouse Residential – North East Albion
Townhouse	RM-1 Low Density Townhouse Residential * RT-3 Ground-Oriented Residential North East Albion RST-NEA Street Townhouse Residential – North East Albion
Albion Village Commercial	C-5 Village Commercial
Park	P-1 Park and School
Conservation	Current or proposed zoning would apply

<b>Institutional</b>	P-1 Park and School P-2 Special Institutional P-3 Children’s Institutional P-4 Place of Worship Institutional P-4a Place of Worship Institutional & Educational P-6 Civic Institutional
* Density Bonus provisions <del>and details</del> are provided in each of these zones. ** Legacy Use zones “zone no longer in use”	
<b>Conditions:</b> <ol style="list-style-type: none"> <li>This Matrix is to be read in conjunction with the policies and criteria in the Albion Area Plan and Official Community Plan and is not the only source for determining applicable zones;</li> <li>All zones corresponding to each land use designation or category may not be appropriate for a specific site, and are not applicable in all areas of the municipality;</li> <li>Lot consolidation may be required to meet the minimum lot area, lot width and/or lot depth requirements of the Zoning Bylaw and/or Subdivision Control Bylaw;</li> <li>The P-4 Place of Worship Institutional zones would be considered in all Land Use Designations subject to satisfying all requirements of the zone.</li> </ol>	



NORTH EAST ALBION AREA SPECIFIC	
LAND USE DESIGNATION	ZONE(S)
<del>X-Plex (Duplex, Triplex, Fourplex)</del>	<del>RT-1 Two-Unit Urban Residential RT-3 Ground-Oriented Residential North-East Albion RST-NEA Street Townhouse Residential—North-East Albion</del>
<del>Single Family</del>	<del>RS-1b Single-Detached (Medium-Density) Urban Residential* RS-1 Single-Detached Residential Along eastern Plan- boundary RS-1c Single-Detached (Low-Density) Residential</del>
<del>Albion Village Commercial</del>	<del>C-5 Village Commercial</del>
*Density Bonus provisions and details are provided in each of these zones.	
<b>Conditions:</b> <ul style="list-style-type: none"> <li><del>a) This Matrix is to be read in conjunction with the policies and criteria in the Albion Area Plan and Official Community Plan and is not the only source for determining applicable zones;</del></li> <li><del>b) All zones corresponding to each land use designation or category may not be appropriate for a specific site, and are not applicable in all areas of the municipality;</del></li> <li><del>c) Lot consolidation may be required to meet the minimum lot area, lot width and/or lot depth requirements of the Zoning Bylaw and/or Subdivision Control Bylaw;</del></li> <li><del>d) The P-4 Place of Worship Institutional zones would be considered in all Land Use Designations subject to satisfying all requirements of the zone.</del></li> </ul>	

## **Amendment to Maple Ridge Fees and Charges Bylaw No. 7575-2019**

### **Recommendation:**

**THAT Maple Ridge Fees and Charges Amending Bylaw No. 7993-2024 be given first, second and third reading.**

### **Report Purpose and Summary Statement:**

The purpose of this report is to implement amendments to the *Maple Ridge Fees and Charges Bylaw No. 7575-2019*, via *amending Bylaw No. 7993-2024*, to update fees and charges and consolidate fees from multiple bylaws into one consolidated fees and charges bylaw. This report fulfills key results in Council's Strategic Plan 2023-2026.

### **Previous Council Action:**

Council adopted the *Maple Ridge (Consolidated) Fees and Charges Bylaw No. 7575-2019* in 2020 to bring many departmental fees under one bylaw. Council received a staff report on fees for Police Services and General Administration services on March 4, 2025, a report on Recreation Services Fees on March 25, 2025, a report on Filming Fees on April 1, 2025, and a report on Planning, Building, Board of Variance and Engineering Fees on April 15, 2025, all of which were received for information. Council made some recommendations for adjustments which are reflected in this amendment.

### **Financial Impact:**

These fee updates will improve cost recovery given rising costs to the City to maintain and enhance service levels. The City will capture increased revenue from updated fees and charges.

### **Strategic Alignment:**

Governance & Corporate Excellence

### **Applicable Legislation/ Bylaw/Policy:**

*Maple Ridge Fees and Charges Bylaw No. 7575-2019, Maple Ridge Amending Bylaw No. 7993-2024, Maple Ridge Building Bylaw No. 6925-2012, Maple Ridge Highway and Traffic Bylaw No. 6704-2009, Maple Ridge Drinking Water Conservation Plan Bylaw No. 7425-2018, Maple Ridge Soil Removal Bylaw No. 6398-2006, Maple Ridge Fees Imposition Bylaw No. 6748-2010, Maple Ridge Board of Variance Bylaw No. 6290-2005 Maple Ridge Police Services Fees Bylaw No. 6937-2012. The Maple Ridge Filming Policy No. 30.40 was updated in 2023.*



**To:** Mayor and Council

**File number:**

## **Amendment to Maple Ridge Fees and Charges Bylaw No. 7575-2019**

### **BACKGROUND:**

Council consolidated fees and charges for several areas of city operations under *Maple Ridge Fees and Charges Bylaw No. 7575-2019*. In July 2021, schedules were added to the consolidated bylaw for Planning fees. *Maple Ridge Fees and Charges Bylaw No. 7575-2019* was last updated in January 2024 to add a schedule for Filming Fees.

In March 2025, the City brought together the building and planning functions under one department, the Planning and Building Department, and in line with consolidation of other fees into the *Maple Ridge Fees and Charges Bylaw No. 7575-2019*, this report proposes several updates to fees and charges for the existing schedules in *Maple Ridge Fees and Charges Bylaw No. 7575-2019*, while also including Building, Engineering and Board of Variance fees in the schedules for the first time.

### **ANALYSIS:**

#### **Discussion:**

Staff undertook a review of fees and charges across City departments in late 2024-early 2025. Considerations have informed adjustments to fees and charges in several areas including inflationary adjustments and adjustments related to improved cost recovery and revenue generation for discretionary services.

Staff have sought to maintain a balance between equity and accessibility of City services while also ensuring that cost recovery occurs at a reasonable level appropriate for each service area. For example, some services are more heavily subsidized because of the public benefit provided, such as recreation services where provision of these services ensures an engaged and healthy community in line with Council's priorities for its Strategic Plan. In other areas, individuals receiving services benefit financially and directly from the services e.g., development permit fees that enable profit from the sale or rental of developments, and as such services may have a higher ratio of cost recovery.

Consolidation of fees and charges across City services within the *Maple Ridge Fees & Charges Bylaw No. 7575-2019* improves transparency and coordination of fee updates over time. Holding fee schedules in multiple bylaws can create confusion and make it difficult for customers to understand which services carry fees and which may not.

It is important to note that staff are also undertaking two related areas of work in 2025 that will improve and streamline setting and updating of fees in the future. First, is the development of a

One City approach and methodology for fees and charges, which will create foundational principles consistent across all departments. Secondly, staff in both the Planning and Building and the Finance Departments are working on a review of Development Cost Charges (DCCs) and Amenity Cost Charges (ACCs) that will adjust these charges in the future. Changes to the City's DCC and ACCs are not included in this amendment to *Maple Ridge Fees and Charges Bylaw No. 7575-2019*.

The Development Services Optimization project, which is streamlining processes for development applications and digitizing services, is also a key part of updates to fees and charges. The metrics tracked through process improvements currently being implemented will inform future fee updates to bring services in line with cost savings to the city and conversely recognizing services that may have benefitted in the past from too significant a subsidization.

Staff will review *Maple Ridge Fees and Charges Bylaw No. 7575-2019* annually to ensure that updates to fees and charges are brought forward regularly to keep pace with changing costs to the City, identify opportunities to generate additional revenue that offsets the burden on taxpayers, and take into account external factors impacting affordability for the community.

In addition, future updates to *Maple Ridge Fees and Charges Bylaw No. 7575-2019* will include further consolidation of other bylaws which continue to set fees and charges for specific service areas e.g., Fire Protection, Cemetery, and Water, Sewer and Drainage fees that are not included in this amendment.

### **Strategic Alignment:**

This amendment supports improved governance and corporate excellence in line with Council's Strategic Plan. There are four initiatives related to the Key Result "*Review and consolidate City fees and charges into a single bylaw with annual review dates*" in the Strategic Plan. The amendments to *Maple Ridge Fees and Charges Bylaw No. 7575-2019* support achievement of this Key Result.

### **Financial Impact:**

Amendments proposed in this report will lead to increased revenue for the City from users of services that will offset the burden on taxpayers who may or may not directly benefit from specific services. They will help the City to manage inflationary impacts on services delivery even as we strive to continuously improve efficiency and streamline overheads and other direct costs related to the services.

### **Applicable Legislation/Bylaw/Policy:**

This Amendment to the *Maple Ridge Fees and Charges Bylaw No. 7575-2019*, includes updates to fees and charges from the following bylaws. *Maple Ridge Building Bylaw No. 6925-2012*, *Maple Ridge Highway and Traffic Bylaw No. 6704-2009*, *Maple Ridge Drinking Water Conservation Plan Bylaw No. 7425-2018*, *Maple Ridge Soil Removal Bylaw No. 6398-2006*, *Maple Ridge Fees Imposition Bylaw No. 6748-2010*, *Maple Ridge Board of Variance Bylaw No. 6290-2005*, *Maple Ridge Police Services Fees Bylaw No. 6937-2012*. Amendment to the *Maple Ridge Fees and Charges Bylaw No. 7575-2019* is achieving via Amending Bylaw No. 7993-2024.



When the amendments to Bylaw No 7575-2019 come into effect, this will trigger a series of consequential bylaw amendments to update the related bylaws from which fees will be removed. These amendments may be brought forward as an omnibus package or individually as several of the bylaws are under review for other content updates and revisions beyond fees and charges.

## **CONCLUSION:**

Council has demonstrated its commitment to finding efficiencies and reducing confusion for service users by directing staff to update and consolidate fees and charges to improve transparency, accountability and usability of City services. Staff have taken an equity lens to ensure that updates to fees and charges proposed in the amendments to the *Maple Ridge Fees and Charges Bylaw No. 7575-2019* reflect reasonable and moderate increases that will offset cost increases incurred by the City and reduce the burden on taxpayers, especially for discretionary services that may not benefit all community members equally. This amendment will fulfil Council's commitment to achieving the "*Review and consolidate City fees and charges into a single bylaw with annual review dates*" Key Result in its Strategic Plan.

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Prepared by: Phil Sanderson, Manager of  
Corporate Strategy & Business  
Transformation

## **Attachments:**

Attachment 1 - Amending Bylaw No. 7993-2024 to amend  
Maple Ridge Fees and Charges Bylaw No. 7575-2019 - With  
Schedules.

Attachment 2 – Current *Maple Ridge Fees and Charges  
Bylaw No. 7575-2019*.

## Report Approval Details

Document Title:	Amendment to Maple Ridge Fees and Charges Bylaw No. 7575-2019.docx
Attachments:	<ul style="list-style-type: none"><li>- Attachment 1 - Amending Bylaw No. 7993-2024 to amend Maple Ridge Fees and Charges Bylaw No. 7575-2019 - With Schedules.pdf</li><li>- Attachment 2 - Current Maple Ridge Fees and Charges Bylaw No. 7575-2019.pdf</li></ul>
Final Approval Date:	May 14, 2025

This report and all of its attachments were approved and signed as outlined below:

Zvi Lifshiz, Director Strategic Development, Communications & Public Engagement

Carolyn Mushata, Director of Legislative Services and Corporate Officer

Scott Hartman, Chief Administrative Officer



**CITY OF MAPLE RIDGE  
BYLAW NO. 7993-2024**

A Bylaw to amend the Maple Ridge Fees and Charges Bylaw No. 7575-2019, which  
establishes fees and charges for City services and information.

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**WHEREAS**, the Council of the City of Maple Ridge deems it expedient to provide for fees and charges for services and information;

**AND WHEREAS**, Section 194 of the *Community Charter* authorizes municipalities by bylaw to impose fees in respect of a service of the municipality or provision of information;

**AND WHEREAS**, Council deems it necessary and desirable to exercise the authority provided by the *Community Charter* to cover costs of providing services and information where appropriate;

**NOW THEREFORE**, the Council of the City of Maple Ridge enact as follows:

1. This Bylaw shall be cited for all purposes as “Maple Ridge Fees and Charges Amending Bylaw No. 7993-2024”.

2. Maple Ridge Fees and Charges Bylaw No. 7575-2019 is hereby amended by:

a. Replacing the following existing schedules in their entirety:

- Schedule “A” – Engineering Fees
- Schedule “C” – Planning Fees

with a revised Schedule “A” – Building, Planning, Board of Variance and Engineering Fees.

b. Replacing the following existing schedules in their entirety:

- Schedule “B” – Recreation Services Fees;
- Schedule “D” – General Administration Fees – Copying Costs, Record Requests;
- Schedule “E” – Electric Vehicle Parking Fees;
- Schedule “F” – Filming Fees;

with the following revised schedules:

- Schedule “B” – Recreation Services Fees
- Schedule “C” – General Administration Fees

- Schedule “D” – Filming Fees
  - Schedule “E” – Electric Vehicle Parking Fees
- c. Inserting a new Schedule “F” – Police Services Fees, as attached hereto, into the Bylaw;
- d. Replacing the text in Part 4 (Fees and Charges), Section 4.1 with the following text:
- “The City hereby imposes the fees for the provision of services and information as specified in the schedules which are attached hereto and form part of this Bylaw”.
- e. Replacing the text in Part 5 (Schedules) with the following text:
- “Schedule “A” – Building, Planning, Board of Variance and Engineering Fees  
Schedule “B” – Recreation Services Fees  
Schedule “C” – General Administration Fees  
Schedule “D” – Filming Fees  
Schedule “E” – Electric Vehicle Parking Fees  
Schedule “F” – Police Services Fees”.
3. Maple Ridge Fees and Charges Bylaw No. 7575-2019 is hereby amended accordingly.

**READ** a first time the                      day of                      , 20

**READ** a second time the                      day of                      , 20

**READ** a third time the                      day of                      , 20

**ADOPTED** the                      day of                      , 20

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**PRESIDING MEMBER**

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**CORPORATE OFFICER**



## SCHEDULE “A “

### Building, Planning, Board of Variance, and Engineering Fees

Note:

1. All fees are exclusive of tax, which will be added, if applicable.

<b>PLANNING FEES</b>	
Subdivision Fees	
Subdivision Application Base Fee, plus:	\$2,732.59
- Fee Per Lot	\$109.18
Subdivision Review (lot line adjustment and consolidation)	\$1,388.44
Each Phase Review	\$546.93
Phased Strata Plan Approval	\$1,092.83
Phased Strata Plan Amendments	\$546.93
Form P Approval	\$546.93
Strata Title Conversions	\$2,623.41
Subdivision Plan Stale Dated Approval	\$272.95
Subdivision PRL Extension Fee ( <i>charged at a rate of 50% of Subdivision Application Fee</i> )	<i>Variable</i>
Rezoning Fees	
Single Detached Dwelling Rezoning Application Base Fee, plus:	\$3,279.52
- Fee per Lot	\$109.18
Multi-Unit Dwelling Rezoning Application Base Fee plus:	\$4,153.99
- Fee per Unit	\$87.55
Commercial Rezoning Application Base Fee, plus:	\$3,825.42
- Fee per 100 sq m	\$109.18
Institutional Rezoning Application Base Fee, plus:	\$3,825.42
- Fee per 100 sq m	\$109.18
Industrial Rezoning Application Base Fee, plus:	\$3,825.42
- Fee per 100 sq m	\$54.59
Comprehensive Development Base Fee, plus:	\$5,465.18
- Residential per unit	\$109.18
- Commercial per 100 sq m	\$109.18
- Industrial per 100 sq m	\$109.18

Rezoning Extension at Third Reading <i>(charged at 50% of Rezoning Application Fee)</i>	<i>Variable</i>
Zoning Bylaw Text Amendment	\$3,825.42
Official Community Plan Amendment	\$4,372.35
Official Community Plan Amendment Extension at Third Reading <i>(charged at 50% of Rezoning Application Fee)</i>	<i>Variable</i>
Official Community Plan Text Amendment	\$2,732.59
Public Hearing	\$1,311.19
Pre - Application Meeting	\$1,000.00
Development Permit Fees	
Multi-Unit Dwelling Residential/Intensive Infill Base Fee, plus:	\$3,279.52
- Fee per unit	\$109.18
Commercial Base Fee, plus:	\$3,279.52
- Fee per 100 square metre	\$109.18
Industrial Base Fee, plus:	\$3,279.52
- Fee per 100 square metre	\$109.18
Environmental Development Permit	\$3,279.52
Single Detached Dwelling Environmental Development Permit	\$819.88
Development Variance Permit	\$3,279.52
Single Detached Dwelling Development Variance Permit	\$1,639.76
Development Permit Minor Amendment	\$546.93
Heritage Revitalization Agreement	\$546.93
Heritage Conservation Restrictive Covenant	\$546.93
Heritage Alteration Permit	\$546.93
Landscape Design Review and Inspection Fees	
<i>Design Review and Inspection fee (calculated at 2% of landscape value)</i>	
- Minimum fee	\$327.54
- Maximum Fee	\$3,279.52
Landscape Re-Inspection Request	\$163.77
Other Planning Fees	
Temporary Use Permit	\$2,732.59
Temporary Use Permit Extension	\$2,732.59
Development Information Meeting	\$546.93
Board of Variance	\$546.93



Illegal Work Penalty	\$272.95
Legal Document Registration, Discharge or Modification	\$218.36
Legal Document Registration, Discharge or Modification with Council Approval	\$436.72
Legal Document Review ( <i>calculated based on City hourly cost to complete review</i> )	<i>Variable</i>
Advisory Design Panel Submission	\$163.77
LIDAR Investigation Map	\$54.59
Letter of Inquiry	\$272.95
Telecommunication Tower/Antennae Report	\$3,279.52
<b>Tree Cutting Permit Fees</b>	
Trees in urban areas and urban reserve and rural parcels less than 0.5ha.	
- For First Tree	\$77.25
- For Each Additional Tree	\$25.75
<i>Trees in rural areas on parcels greater than 0.5ha.</i>	
- For 11th Tree	\$77.25
- For Each Additional Tree	\$25.75
<i>Development &amp; Large Scale Clearing Base Fee</i>	\$309.00
- Plus, per tree	\$25.75
Tree Replacement	\$618.00
Parcels with Active Farm Use	<i>no fee</i>
<b>BUILDING FEES</b>	
Permit Application Fee per permit for Single Detached and Two-Unit (duplex) Dwellings	\$180.25
Permit Application Fee per permit for other than Single Detached and Two-Unit (duplex) Dwellings	\$602.55
Additional Fee for Building Permit for other than Single Detached Dwellings (per unit)	\$103.00
Permit Assignment or Transfer Fee (any permit for building, plumbing, electrical, gas)	\$154.50
Permit Renewal Fee (any permit for building, plumbing, electrical, gas)	\$77.25
Fee for market value of the building, pool or construction up to a value of \$5,000	\$154.50
Fee for market value of the building, pool or construction over the value of \$5,000	

- For each additional \$1,000 or part thereof up to \$25,000	\$14.16
- For each additional \$1,000 or part thereof up to \$50,000	\$13.49
- For each additional \$1,000 or part thereof up to \$75,000	\$10.82
- For each additional \$1,000 or part thereof up to \$100,000	\$10.30
- For each additional \$1,000 or part thereof over \$100,000 up to infinite	\$10.04
<i>Note: Building Values shall be based upon current estimated Construction costs. The current edition of the Marshall Valuation Service, the Marshall and Swift Residential Cost Handbook or other valuation tables may be used by the Chief Building Official to determine the market value for the purpose of assessing Permit fees.</i>	
Temporary Building Permit (Bylaw No. 6925-2012 details costs of additional securities beyond the Temporary Building Permit)	\$180.25
Permit to erect a retaining wall	
- First 20 metres or portion thereof	\$120.51
- For each additional 10 metres or portion thereof	\$54.59
Permit to install a fireplace, stove or chimney (for each fireplace, stove or flue) or to install a Dry Chemical Fire Extinguishing System	\$120.51
Building Demolition Permit (per building)	\$180.25
Permit Fee Refund	
- Where an Owner or his agent applies in writing for the cancellation of a Building Permit, Plumbing Permit, Electrical Permit and or Gas Permit issued under the Building Bylaw, 75% of any Permit fee paid in excess of \$150.00 may be refunded to the holder of the Permit, provided that the Building Permit, Plumbing Permit, Electrical Permit and or Gas Permit had not expired at the time the application for cancellation was received and no Construction had commenced under that Building Permit, Plumbing Permit, Electrical Permit and or Gas Permit.	Variable
Provisional Occupancy Permit Fees	
Single Detached Dwelling (90-day maximum period)	\$257.50
- Single Detached Dwelling Renewal (up to 90-day maximum period)	\$128.75
Multi-Unit Dwelling - (per unit for 120 Day maximum period)	\$123.60
- Multi-Unit Dwelling Renewal Fee (per unit up to 60-day maximum)	\$61.80
Other than Residential - (per unit 60-day maximum period)	\$602.55
- Other than Residential Renewal Fee (per unit 60-day maximum)	\$257.50



Change of Occupancy of Use Fee (where a Building Permit is not required)	\$180.25
Address Changes and Additional Unit Numbers:	
- If Permit has been issued but no Occupancy Permit issued (per unit)	\$180.25
- Permit application in process but Permit not issued (per unit)	\$90.13
Where Construction is started prior to obtaining any building, electrical, gas or plumbing Permit (per building), the applicable Permit fee shall be doubled but in no case shall the penalty amount doubled exceed:	\$2,575.00
If the applicant makes an erroneous declaration on any Permit application to obtain a lesser Permit fee, the Permit shall be revoked and a new Permit issued using the corrected value. The new Permit shall be calculated according to the corrected Permit value and a 50% administrative fee shall be added to the calculated fee.	Variable
<i>Notwithstanding the permit fees set out, a building permit fee may be reduced or waived should the application for the works be made under a City initiated revitalization program. The amount or percentage of reduction applicable will be that value as expressed within the most current revitalization report endorsed by the Council of the City. Should a dispute as to the amount of reduction arise, the form as retained by the City's Legislative Services Department will be the enforceable copy.</i>	
Inspection, Plan Checking and Record Searching Fees	
Business Licence Inspection Fee	
- Minimum per business licence inspection	\$90.13
- Maximum per business licence inspection	\$180.25
Building, Plumbing, Gas, Electrical Permit Inspection Fees	
- During normal working hours (per hour)	\$108.15
- Outside normal working hours (per hour)	\$144.20
Re-inspection Fee where more than 1 re-inspection is required due to the fault of the holder of any building, plumbing, electrical or gas permit or in the event of faulty workmanship or materials requiring rework for each extra re-inspection required.	\$175.00
Additional fee for any inspection performed outside the boundaries of the City per total km travelled, measured from the Municipal Hall to the site of the inspection along the shortest available highway route and return travel back to municipal hall will be charged at a rate of \$0.58 per km.	Variable

A fee for environmental inspection shall be paid for each residential unit with a value in excess of \$20,000.00.	\$90.13
A fee for environmental inspection shall be paid for each non-residential unit (per unit) where the value exceeds \$40,000.00.	\$90.13
Inspection fee for Building Code and bylaw compliance for Temporary Mobile Homes to be occupied for Temporary Residential Use.	\$150.00
<b>Plumbing Permit Fees</b>	
Minimum fee for any Plumbing Permit or Inspection	\$103.00
<i>Plumbing Permit for plumbing construction which involves the installation of fixtures (for the purpose of this Bylaw, "fixtures" shall include Pools, interceptors, hot water storage tanks, automatic washers, roof drains, rainwater leaders, floor drains, radiant heating loops, solar panels, geothermal and built-in dishwashers.)</i>	
- for the first fixture	\$23.69
- for each additional fixture	\$22.66
- for each roughed in fixture location	\$12.36
Where an application is made for a Plumbing Permit for other than Single Detached Dwellings, there will be an additional fee per dwelling unit.	\$48.41
Permit fees for finishing Plumbing only (installation of fixtures where rough-in Plumbing exists) are assessed at 50% of the usual fee based on the type above.	Variable
Permit fees for plumbing construction which involves the connection of hydraulic equipment or the installation of vacuum breakers, backflow prevention devices or similar equipment (per item connected or installed).	\$48.41
Permit fees for plumbing construction which involves the installation of lawn irrigation systems.	\$77.25
Permit Fees for plumbing construction which involves the installation of standpipes, Fire Department connections, fire hose connections and fire hydrants (for each hydrant or hose connection).	\$42.23
Permit fees for plumbing construction which involves the installation of fire sprinkler systems:	
- for up to fifteen (15) sprinkler heads	\$154.50
- for each additional sprinkler head	\$1.24
Permit fees for plumbing construction which involves the replacement of a building's water piping system or portion thereof:	



- (Per Unit) in Multi-Unit residential buildings, hotels and motels	\$28.84
- In all other buildings: water piping first 30 metres or part thereof (each metre)	\$72.10
- In all other buildings: water piping each additional 30 metres or part thereof (each metre)	\$36.05
Permit Fee for plumbing construction which involves the installation of storm sewers, perimeter foundation drains, sanitary sewers or water service lines:	
For Single Detached and Two-Unit (duplex) Dwellings (each dwelling unit)	\$72.10
For other than Single Detached and Two-Unit (duplex) Dwellings	
- for the first 30 metres or part thereof (each metre)	\$72.10
- for each additional 30 metres or part thereof (each metre)	\$72.10
- for each sump, catch basin, rock pit, dry well or manhole	\$36.05
Gas Construction Permit Fees	
For gas Construction which involves the replacement of an appliance or the installation of a new gas appliance.	
- Minimum fee for Single Detached or Multi-Unit Dwellings	\$103.00
- Additional fee for Single Detached or Multi-Unit Dwellings: per appliance	\$48.41
- Minimum fee for other than Single Detached or Multi-Unit Dwellings: up to 102,000 BTU/hr.	\$154.50
- Additional fee for other than Single Detached or Multi-Unit Dwellings: up to 102,000 BTU/hr. (per appliance)	\$72.10
- Minimum fee for other than Single Detached or Multi-Unit Dwellings: up to 102,001 - 409,000 BTU/hr.	\$154.50
- Additional fee for other than Single Detached or Multi-Unit Dwellings: 102,001 - 409,000 BTU/hr (per appliance)	\$84.46
<i>NOTE: Fees for additional appliances are calculated on BTU rating</i>	
Where an application is made for a Permit for other than Single Detached Dwellings, there will be an additional fee per dwelling unit.	\$48.41
Gas Heated Buildings - Building heat loss calculation review	
- Heat/loss calculation fee per Single Detached Dwelling.	\$90.13
- Heat/loss calculation fee per unit for Multi-Unit Use Dwellings.	\$18.54
- Heat/loss calculation fee (Multi-Unit Dwelling use) not less than per Building/cluster	\$90.13

- Heat/loss calculation fee per Building for other than Residential.	\$180.25
For gas Construction which involves the installation of vents or furnace plenums only (each).	\$90.13
For gas Construction which involves the installation of house piping:	
For Single Detached and Two-Unit (duplex) Dwellings (per unit)	\$90.13
For other than Single Detached and Two-Unit (duplex) Dwellings:	
- first 30 metres or part thereof - (per unit plus)	\$72.10
- each additional 30 metres or part thereof (per unit)	\$36.05
<b>Oil and Propane Construction Permit Fees</b>	
For oil and propane Construction which involves the replacement of an appliance or the installation of a new oil and propane appliance.	
- Minimum fee for Single Detached or Multi-Unit Dwellings	\$103.00
- Additional fee for Single Detached or Multi-Unit Dwellings: per appliance	\$48.41
-Minimum fee for other than Single Detached or Multi-Unit Dwellings: up to 102,000 BTU/hr.	\$154.50
- Additional fee for other than Single Detached or Multi-Unit Dwellings: up to 102,000 BTU/hr. (per appliance)	\$72.10
- Minimum fee for other than Single Detached or Multi-Unit Dwellings: up to 102,001 - 409,000 BTU/hr.	\$154.50
- Additional fee for other than Single Detached or Multi-Unit Dwellings: 102,001 - 409,000 BTU/hr (per appliance)	\$84.46
<i>NOTE: Fees for additional appliances are calculated on BTU rating</i>	
Where an application is made for a Permit for other than Single Detached Dwellings, there will be an additional fee per dwelling unit.	\$48.41
<b>Oil and Propane Heated Buildings - Building heat loss calculation review</b>	
- Heat/loss calculation fee per Single Detached Dwelling.	\$90.13
- Heat/loss calculation fee per unit for Multi-Unit Use Dwellings.	\$18.54
- Heat/loss calculation fee (Multi-Unit Dwelling use) not less than per Building/cluster	\$90.13
- Heat/loss calculation fee per Building for other than Residential.	\$180.25
For oil and propane Construction which involves the installation of vents or furnace plenums only (each).	\$90.13
For oil and propane Construction which involves the installation of house piping:	
For Single Detached and Two-Unit (duplex) Dwellings (per unit)	\$90.13



For other than Single Detached and Two-Unit (duplex) Dwellings:	
- first 30 metres or part thereof - (per unit plus)	\$72.10
- each additional 30 metres or part thereof (per unit)	\$36.05
Electrical Equipment Permit Fees	
For Single Detached and Two-Unit (duplex) Dwellings including additions, the Permit fee shall be: 15% of the Building Permit fees or the minimum electrical Permit fee, whichever is greater.	Variable
The following additional charges are applicable to Single Detached and Two-Unit (duplex) Dwellings when the electrical Permit is taken out in conjunction with a building permit:	
- Each hot tub or spa	\$18.54
- Each hydro massage tub bath	\$18.54
- Electrical Heating or based on the value of electrical heating contract, whichever is greater (minimum)	\$90.13
-Air Conditioning/Heat Pumps (per unit)	\$18.54
- Each sub panel	\$18.54
For market value of Electrical Equipment, including costs of installation of \$0.00 - \$1000.00, plus:	\$154.50
- for each additional \$1,000.00 or part thereof up to \$10,000.00 plus	\$27.81
- for each additional \$1,000.00 or part thereof up to \$100,000.00 plus	\$12.36
- for each additional \$1,000.00 or part thereof up to \$250,000.00 plus	\$8.24
- for each additional \$1,000.00 or part thereof up to \$300,000.00 plus	\$6.18
- for each additional \$1,000.00 or part thereof over \$300,000.00 to infinite.	\$5.15
<i>NOTE: Market values shall be based upon current estimated electrical installation costs</i>	
Where an application is made for an Electrical Permit for other than Single Detached dwellings, there will be an additional fee per dwelling unit.	\$30.90
<i>Note: Low Voltage Electrical Permits are exempted from this unit charge.</i>	
Underground Service Duct permit	\$90.13
Temporary to permanent connection conversion permit	\$90.13
Temporary Service connection permit	\$90.13
Temporary current Permit for uses other than carnivals:	\$0.00
- Initial six (6) month period	\$206.00

- Each additional six (6) month renewal period	\$154.50
Special Event Permit Including Carnivals: (Minimum) plus:	\$180.25
- Additional Fee for special events including carnivals (per location)	\$90.13
Electrical Permit for Movie Locations (by number of location)	
- One location – valid for 180 days from issuance	\$180.25
- Two locations – valid for 180 days	\$252.35
- Three locations – valid for 180 days	\$324.45
- Four locations – valid for 180 days	\$360.50
- Five locations – valid for 180 days	\$432.60
- Six locations – valid for 180 days	\$504.70
- Seven locations – valid for 180 days	\$576.80
- Eight locations – valid for 180 days	\$648.90
- Nine locations – valid for 180 days	\$721.00
- Annual Permit – unlimited locations	\$793.10
Annual Electrical Permit:	
For residential, commercial or industrial facilities:	
- minimum fee plus	\$180.25
- per KVA of service capacity (up to a maximum of \$2,884.00)	\$0.26
For educational or institutional facilities (for each classroom, shop, laboratory, office, etc.)	\$9.27
Permit for the installation of rough-in low voltage or finish of a previously installed and permitted rough-in installation only for Single Detached and Two-Unit (duplex) dwellings (for the first \$1,000.00 or part thereof in contract value)	\$90.13
Pool Grounding Permit	\$120.51
Examination of Plans, Permits and Alternative Solutions Fees	
Plan Check Fee	
- Per hour (minimum includes up to two hours)	\$180.25
- Per hour after the first two hours	\$90.13
- Single Detached and Two-Unit (duplex) Dwellings Minimum per unit	\$154.50
- Other than Single Detached and Two-Unit (duplex) Dwellings minimum per building	\$154.50



Each submission for revisions being requested to plans issued in support of a building permit shall be assessed as an additional plan review fee.	\$154.50
Initial review of each alternate solution	\$482.04
- Additional reviews of each alternate solution (per revision)	\$242.05
For each written Building record search, for legal purposes a fee per parcel or file is applicable plus:	\$206.00
- Additional Fee per page for supporting documentation.	\$12.36
Microfilm or Digitization fee for plans (per page) additional to the Building Permit Fee (a 50-page minimum fee for Single Detached and Two-Unit (duplex) Dwelling plans and a 300-page minimum for Multi-Unit Dwellings, Commercial, Industrial and Institutional)	\$6.03
<b>ENGINEERING FEES</b>	
Highway and Traffic Fees	
Highway Use Permit	\$175.00
Oversize Permit	\$25.75
Overload Permit	\$103.00
Residential Parking Permit	\$25.00
Replacement Residential Parking Permit	\$15.00
Removal and Detention of light, medium, and heavy-duty vehicles will be charged per the provincial ICBC towing and storage rates.	<i>Variable</i>
Removal and Detention of Chattels and Other Obstructions Fees	
Construction materials including bricks, rock, gravel, sand, lumber etc. removal per person hour	\$51.50
Construction materials including bricks, rock, gravel, sand, lumber etc. removal per equipment where lifting/excavating equipment needed, per hour	\$154.50
Construction materials including bricks, rock, gravel, sand, lumber etc. detention per cubic metre	\$10.30
Portable building including construction trailer or office removal	\$1,030.00
Portable building including construction trailer or office detention per day	\$20.60
Removal of other chattels and obstructions (e.g., furniture, shopping carts, skateboard ramps, basketball hoops etc.) (per item)	\$150.00
Detention and storage of removed chattels and obstructions (per day)	\$5.00
Commercial Litter bin removal per bin	\$1,030.00

Commercial litter bin detention per day	\$20.60
Industrial waste container removal (per container)	\$1,030.00
Industrial waste container detention (per day)	\$20.60
Other removal under 45kg in weight	\$51.50
Detention of other items under 45kg in weight (per day, per kg)	\$1.03
Other removal of items 45kg in weight and over (per person-hour), plus	\$51.50
Other removal of items 45kg in weight and over (per equipment hour if excavating/lifting equipment required)	\$154.50
Detention of other items 45kg in weight and over (per day, per kg)	\$1.03
<b>Watering and Sprinkling Fees</b>	
Newly Seeded Lawns or Landscaping sprinkling permit for single detached dwelling unit or non-residential property, plus	\$30.90
- Additional for each dwelling unit in a multiple residential development	\$30.90
- Additional to a maximum for 5 or more dwelling units in a multiple residential development	\$154.50
Sprinkling fee for Government School or Park	<i>no fee</i>
<b>Soil Removal Fees</b>	
Soil Removal Permit	\$103.00
Soil Removal fee per cubic metre removed	\$0.52
<b>Pavement Restoration Fees</b>	
Pavement Restoration Fee based on age of years since last paved as determined by the City of Maple Ridge and calculated per square metre of excavation area of pavement:	
0-5 years (fee per square metre of pavement requiring restoration)	\$43.04
6-10 years (fee per square metre of pavement requiring restoration)	\$32.29
11-15 years (fee per square metre of pavement requiring restoration)	\$21.53
16-20 years (fee per square metre of pavement requiring restoration)	\$10.76
21 years and greater	<i>no fee</i>
<b>Maps and Other Engineering Fees</b>	
Construction Specifications Book (per book)	\$123.60
Small Street Maps (per map)	\$2.32
Large Street Maps (per map)	\$6.95
Section Maps (1:2500 scale, per map)	\$2.60



Topographical and Cadastral Maps (per map)	\$3.61
Set and Key Maps (per map)	\$142.14
Topographical Set and Key Maps (per map)	\$206.00
Digital Cadastral or Topographical Maps (per map)	\$103.00
Digital Street Maps (per map)	\$121.54
Digital Photo Maps (per map)	\$51.50
Subject Maps (small, colour, 8x11, per map)	\$5.15
Subject Maps (large, colour, 11x17, per map)	\$10.30
Road Classification Maps (per map)	\$25.75
Large Key Servicing Maps (sanitary, storm, water, and streetlight, per map)	\$11.33
Banners	\$386.25
Fire Hydrant Use Permits (per day)	\$51.50
Fire Hydrant Use Permits (per week)	\$206.00
Tenders (minimum)	\$51.50
Tenders (maximum)	\$103.00

## SCHEDULE “B”

### Recreation Services Fees

Note:

1. Rate increases come into effect on January 1 of each year.
2. If no new rate is identified, the most recently stated rate will continue to be applied.
3. All fees are exclusive of tax, which will be added, if applicable.
4. Specified facility fees are charged by the hour. However, once the rental period exceeds 8 hours in a single day, a daily maximum rate will be applied.
5. Non-prime time rates will be a 50% reduction of the regular rates and applied to all hours outside of the prime-time windows listed below:

#### **Arenas**

Monday – Friday 6:00 pm – 11:00 pm  
Saturday, Sunday 8:00 am – 11:00 pm

#### **All Other Facilities**

Monday – Friday 5:00 pm – 9:00 pm  
Saturday, Sunday 8:00 am – 9:00 pm

6. Super Senior Rates are only available to Maple Ridge Residents.
7. Student Rates are only available to those who provide proof of full-time enrollment (3+ Courses).
8. Non-prime time rates are not applicable to Commercial and Non-Resident Fees.
9. The following Facility Fee Category Definitions apply:
  - **Youth/Senior Non-Profit:** Facility fees for a non-profit community or sport association where the majority of participants are youth or seniors.
  - **Adult Non-Profit:** Facility fees for a non-profit community or sport association where the majority of participants are adults.
  - **Private:** Facilities fees for an activity or booking that is not open to the general public where the Renter resides in Maple Ridge.
  - **Commercial:** Facility fees for a business or where the activity object is to gain profit.
  - **Non-Resident:** Facility fees for an activity or booking where the Renter does not reside in Maple Ridge.



<b>Recreation Facility Access</b>	<b>Fee</b>
<b>Preschool (0-4 Years)</b>	
Single	Free
<b>Child (5-12 Years)</b>	
Single	\$3.17
10 Pass	\$27.15
20 Pass	\$50.69
1 Month	\$28.52
3 Months	\$76.03
1 Year	\$247.95
<b>Youth (13-19 Years)</b>	
Single	\$4.27
10 Pass	\$36.52
20 Pass	\$68.17
1 Month	\$38.35
3 Months	\$102.25
1 Year	\$333.45
<b>Student (19+ years – with ID)</b>	
Single	\$4.27
10 Pass	\$36.52
20 Pass	\$68.17
1 Month	\$38.35
3 Months	\$102.25
<b>Adult (19-59 Years)</b>	
Single	\$6.29
10 Pass	\$53.37
20 Pass	\$99.63
1 Month	\$56.05
3 Months	\$149.44
1 Year	\$487.35
<b>Senior 1 (60-64 Years)</b>	
Single	\$4.27
10 Pass	\$36.52
20 Pass	\$68.17
1 Month	\$38.35
3 Months	\$102.25
1 Year	\$333.45

<b>Senior 2 (65-79 Years)</b>	
Single	\$2.13
10 Pass	\$18.26
20 Pass	\$34.08
1 Month	\$19.17
3 Months	\$51.13
1 Year	\$166.73
<b>Super Senior (80+ Years)</b>	
Single	Free
<b>Family</b>	
Single	\$11.74
10 Pass	\$105.45
20 Pass	\$187.84
1 Month	\$110.72
3 Months	\$295.26
1 Year	\$935.37
<b>Drop in Skating – Single</b>	
Preschool (0-4 Years)	Free
Child (5-12 Years)	\$3.17
Youth (13-19 Years)	\$4.27
Student (19+ Years)	\$4.27
Adult (19-59 Years)	\$6.29
Senior 1 (60-64 Years)	\$4.27
Senior 2 (65-75 Years)	\$2.13
Super Senior (80+ Years)	Free
Family	\$11.74
<b>Outdoor Pool Admissions – Single</b>	
Preschool (0-4 Years)	Free
Child (5-12 Years)	\$1.90
Youth (13-19 Years)	\$1.90
Student (19+ Years)	\$1.90
Adult (19-59 Years)	\$2.86
Senior 1 (60-64 Years)	\$1.90
Senior 2 (65-75 Years)	\$1.90
Super Senior (80+ Years)	Free



<b>Arena Use</b>	<b>Hourly Fee</b>
<b>Ice – Prime Time</b>	
Youth/Senior Non-Profit	\$122.90
Adult Non-Profit	\$184.35
Private	\$301.33
Commercial	\$333.18
Non-Resident	\$322.31
<b>Ice – Non-Prime Time</b>	
Youth/Senior Non-Profit	\$90.38
Adult Non-Profit	\$138.26
Private	\$226.00
Commercial	\$264.80
Non-Resident	\$241.73
<b>Ice – Junior B</b>	
Prime Time/Games	\$126.27
Non-Prime/Practices	\$94.72
<b>Dry Floor – Prime Time</b>	
Youth/Senior Non-Profit	\$45.81
Adult Non-Profit	\$70.74
Private	\$114.53
Commercial	\$152.51
Non-Resident	\$137.18

<b>Fairground Fees</b>	<b>Hourly Fee</b>
<i>(Stage included at no-charge when booking grounds)</i>	
<b>Barns</b>	
Youth/Senior Non-Profit	\$8.91
Adult Non-Profit	\$13.37
Private	\$21.65
Commercial	\$23.81
Non-Resident	\$25.98
<b>Grounds &amp; Showrings</b>	
Youth/Senior Non-Profit	\$7.80
Adult Non-Profit	\$11.70
Private	\$18.95
Commercial	\$21.45
Non-Resident	\$23.40

<b>Field Fees</b>	<b>Hourly Fee</b>
<b>Youth/Senior Non-Profit</b>	
<b>Artificial Turf</b>	
Commercial	\$25.55
Special Event	\$6.39
<b>Class A</b>	
Commercial	\$24.11
Special Event	\$5.72
<b>Class B</b>	
Commercial	\$17.96
Special Event	\$4.49
<b>Class C</b>	
Commercial	\$5.99
Special Event	\$1.50
<b>Adult Non-Profit</b>	
<b>Artificial Turf</b>	
Commercial	\$25.55
Special Event	\$19.76
Regular Use	\$14.71
<b>Class A</b>	
Commercial	\$24.11



Special Event	\$17.76
Regular Use	\$13.60
<b>Class B</b>	
Commercial	\$17.96
Special Event	\$12.50
Regular Use	\$7.48
<b>Class C</b>	
Commercial	\$5.99
Special Event	\$3.01
<b>Local, Private or Political (Hourly)</b>	
Artificial Turf	\$25.54
Class A	\$24.11
Class B	\$17.96
Class C	\$5.99
<b>Commercial (Hourly)</b>	
Artificial Turf	\$28.10
Class A	\$28.10
Class B	\$20.93
Class C	\$6.99
<b>Non-Resident</b>	
Artificial Turf	\$32.12
Class A	\$30.31
Class B	\$25.66
Class C	\$8.92
<b>Sports Field User Annual Contribution</b>	
Fall/Winter Clubs - per registration	\$30.00
Spring/Summer Clubs - per registration	\$15.00

<b>Hall Rental</b> <i>(non-prime time rates will be a 50% reduction of the regular rates. Facility rate is hourly, kitchen rates are daily)</i>	<b>Fee</b>
<b>Great Hall - Albion Community Centre Hourly</b>	
Youth/Senior Non-Profit	\$51.34
Adult Non-Profit	\$77.01
Private	\$128.35
Commercial	\$261.66
Non-Resident	\$246.92
<b>Hammond Hall - Hourly</b>	
Youth/Senior Non-Profit	\$27.81
Adult Non-Profit	\$41.73
Private	\$67.55
Commercial	\$96.28
Non-Resident	\$82.90
<b>Whonnock Hall - Hourly</b>	
Youth/Senior Non-Profit	\$88.27
Adult Non-Profit	\$132.41
Private	\$220.68
Commercial	\$269.35
Non-Resident	\$246.92
<b>Kitchen Daily Rate</b> Fairgrounds, Hammond Hall, Whonnock Lake, Albion Community Centre	
Youth/Senior Non-Profit	\$35.54
Adult Non-Profit	\$53.33
Private	\$90.64
Commercial	\$127.67
Non-Resident	\$127.67



<b>Pool Use</b> <i>(competition and teaching pool rates are per lane, Hammond Pool rate is for entire pool)</i>	<b>Hourly Fee</b>
<b>Competition/Teach Pool (Per Lane)</b>	
Swim Club - Competition Pool	\$6.38
Swim Club - Teach Pool	\$5.43
Youth/Senior Non-Profit	\$11.11
Adult Non-Profit	\$16.66
Private	\$26.99
Commercial	\$35.29
Non-Resident	\$31.98
<b>Hammond Pool (Whole Pool)</b>	
Swim Club	\$21.72
Youth/Senior Non-Profit	\$29.23
Adult Non-Profit	\$43.84
Private	\$70.98
Commercial	\$81.66
Non-Resident	\$72.31

<b>Room Rental Rates</b> (non-prime time will be at 50% of regular rates)	<b>Hourly Fee</b>
<b>Multi-Purpose Room (capacity 1-15)</b>	
Youth/Senior Non-Profit	\$4.98
Adult Non-Profit	\$7.46
Private	\$12.42
Commercial	\$15.26
Non-Resident	\$15.26
<b>Multi-Purpose Room (capacity 15-30)</b>	
Youth/Senior Non-Profit	\$9.93
Adult Non-Profit	\$14.91
Private	\$24.84
Commercial	\$30.50
Non-Resident	\$30.50
<b>Multi-Purpose Room (capacity 40-60)</b>	
Youth/Senior Non-Profit	\$13.04
Adult Non-Profit	\$19.57
Private	\$32.61
Commercial	\$39.22
Non-Resident	\$35.96
<b>Multi-Purpose Room (capacity 60-80)</b>	
Youth/Senior Non-Profit	\$15.37
Adult Non-Profit	\$23.06
Private	\$38.42
Commercial	\$53.32
Non-Resident	\$50.01
<b>Gymnasium</b>	
Youth/Senior Non-Profit	\$51.82
Adult Non-Profit	\$77.72
Private	\$129.54
Commercial	\$162.03
Non-Resident	\$139.27



<b>Miscellaneous</b>	
Park Shelter (day rate)	\$77.18
Event Trailer (day rate)	\$147.31
Tennis/Sport Court (Commercial)	\$16.54
Tennis/Sport Court (Special Event) - YNP	\$5.81
Tennis/Sport Court (Special Event) - ANP	\$8.72

## SCHEDULE “C”

### General Administration Fees

Note:

1. All fees are exclusive of tax, which will be added if applicable.
2. Property owners will continue to receive property tax certificates at no charge.

General Administration Services	Fee
Credit Balance Refund Fee (initiated by customer)	\$25.00
Payment Transfer Fee (initiated by customer)	\$25.00
Photocopying Fee (per page, standard format and size 8"x11")	\$1.00
Property Apportionment Fee (per child parcel)	\$35.00
Property Tax Certificate requests through BC Online (note: BC online charges customers additional administration fee that will be added to total)	\$45.00
Properties included on mortgage listings - charged to mortgage companies – per property (folio/roll number)	\$15.00
Returned Cheque Fee	\$35.00
Fire Department Record Requests	
- Fire Incident/Investigation Reports (including colour photographs)	\$200.00
- Motor Vehicle Accident/ Medical Incident Report	\$104.04

## SCHEDULE “D”

### Filming Fees

Notes:

1. All fees are exclusive of tax, which will be added if applicable.
2. **Production Levels are specified as follows:**
  - a. Level 1 Independent Level 1, Still Photography, Student and Non-Profit Filming
  - b. Level 2 Movie of the Week, Feature Film Level 1, TV Series Level 1, Commercial, Independents Level 2
  - c. Level 3 Feature Film Level 2, TV Series Level 2
3. Fees to rent other Municipal Facilities are outlined in Schedule “B” Recreation Services Fees. However, the Film Coordinator may assess a negotiable rate in addition to the base rate in Schedule “B” Recreation Services Fees to be determined by the level of production and site-specific requirements.
4. Seasonal rates apply per the following timeframes. Summer: June 1 – September 22, Fall/Winter/Spring: September 23 – May 31
5. Student and Non-profit filming require a Film Permit but are exempted from the Film Permit application fee.
6. At the City’s discretion, certain filming fees may be waived for projects where the City is partnering with the organization for community outcomes.

Base Fees for all Production Types	Fee
Film Permit Application - Per Application, for applications received more than 10 days in advance of the proposed filming date, plus:	\$250.00
- Additional Fee for each Additional Location	\$125.00
Filming Business License Fee <i>(per Maple Ridge Business Licensing and Regulation Bylaw No. 6815-2011)</i>	\$110.00
Special Effects Permit	\$200.00
Change to Parking Requirements (within 48 hours of estimated start of filming)	\$50.00
Noise Exemption Fee (per day of filming, to be received 5 business days prior to filming commencing)	\$50.00
Penalty Fee for failure to obtain a permit prior to filming in Maple Ridge (plus applicable filming fees)	\$500.00



<b>Additional Fees by Production Type - Level 1 (small productions as defined in the notes)</b>	<b>Fee</b>
Use of Neighbourhood Park Fee (per day)	\$100.00
Use of Destination Park Fee (per day)	\$200.00
Highway Use Permit <i>(per Maple Ridge Highway and Traffic Bylaw No. 6704-2009, if applicable)</i>	\$154.00
<b>Additional Fees by Production Type - Level 2 (medium productions as defined in the notes)</b>	<b>Fee</b>
Expedited Film Permit Application Fee <i>(per application received 5 business days or fewer, prior to anticipated start date of filming)</i>	\$250.00
Film Permit Change Fee <i>(to be assessed for major changes to schedule and/or impacts of production to the approved Film Permit that are requested 5 or fewer business days prior to the anticipated start date of filming - not applicable if expedited film permit is approved)</i>	\$100.00
Material Breach of Film Permit Fee <i>(assessed to productions who breach the terms of the film permit without authorization from the film office e.g., extension of filming beyond curfew, or parking beyond agreed areas/time).</i>	\$500.00
Waterfront Lot Usage Fee (per day for half the lot with partial electrical access – 2 camlocks)	\$1,050.00
Waterfront Lot Usage Fee (per day for half the lot with full electrical access – 4 camlocks)	\$1,250.00
Waterfront Lot Usage Fee (per day for the full lot with full electrical access – 4 camlocks)	\$1,500.00
City Parking Lot Rental <i>(full lot – excludes waterfront railway lot, per day)</i>	\$850.00
Work Truck parking fee in neighbourhood <i>(per block within a neighbourhood, per day)</i>	\$150.00
Work Truck parking fee in Town Centre <i>(downtown core, per block, per day)</i>	\$225.00
Neighbourhood Access Closure (per day)	\$500.00
Town Centre Access Closure (downtown core, per day)	\$700.00
Arterial Road/Large Impact Access Closure Fee (per day)	\$1,000.00
Neighbourhood Intermittent Traffic Control Fee (per day)	\$250.00
Town Centre Intermittent Traffic Control Fee <i>(downtown core, per day)</i>	\$375.00
Arterial Road/Large Impact Intermittent Traffic Control Fee	\$500.00

(per day)	
Use of Neighbourhood Park Fee (per day)	\$500.00
Use of Destination Park Fee – Winter/Fall/Spring (per day)	\$1,200.00
Use of Destination Park Fee - Summer (per day)	\$1,500.00
Use of Whonnock Lake Park Fee - Winter/Fall/Spring (per day)	\$1,800.00
Use of Whonnock Lake Park Fee - Summer (per day)	\$2,500.00
<b>Additional Fees by Production Type - Level 3 (larger productions as defined in the notes)</b>	<b>Fee</b>
Expedited Film Permit Application Fee <i>(per application received 5 business days or fewer, prior to anticipated start date of filming)</i>	\$1,000.00
Film Permit Change Fee <i>(to be assessed for major changes to schedule and/or impacts of production to the approved Film Permit that are requested 5 or fewer business days prior to the anticipated start date of filming - not applicable if expedited film permit is approved)</i>	\$250.00
Material Breach of Film Permit Fee <i>(assessed to productions that breach the terms of the film permit without prior authorization from the film office e.g., extension of filming beyond curfew, or parking beyond agreed areas/time).</i>	\$1,000.00
Waterfront Lot Usage Fee (per day for half the lot with partial electrical access – 2 camlocks)	\$1,250.00
Waterfront Lot Usage Fee (per day for half the lot with full electrical access – 4 camlocks)	\$1,500.00
Waterfront Lot Usage Fee (per day for the full lot with full electrical access – 4 camlocks)	\$2,000.00
City Parking Lot Rental (full lot – excludes waterfront railway lot per day)	\$1,000.00
Work Truck parking fee in neighbourhood (per block within a neighbourhood, per day)	\$175.00
Work Truck parking fee in Town Centre (downtown core, per block, per day)	\$250.00
Neighbourhood Access Closure (per day)	\$750.00
Town Centre Access Closure (downtown core, per day)	\$1,400.00
Arterial Road/Large Impact Closure Fee (per day)	\$2,000.00
Neighbourhood Intermittent Traffic Control Fee (per day)	\$375.00
Town Centre Intermittent Traffic Control Fee (downtown core, per day)	\$750.00

Arterial Road/Large Impact Intermittent Traffic Control Fee (per day)	\$1,000.00
Use of Neighbourhood Park Fee (per day)	\$750.00
Use of Destination Park Fee - Winter/Fall/Spring (per day)	\$1,800.00
Use of Destination Park Fee - Summer (per day)	\$3,000.00
Use of Whonnock Lake Park Fee - Winter/Fall/Spring (per day)	\$2,500.00
Use of Whonnock Lake Park Fee - Summer (per day)	\$4,000.00

**Additional Fees to be assessed at variable rates based on specific factors.**

1. Administration Fee will be calculated at 10% of the applicable permit fees
2. Preparation and Production Wrap Charges are assessed at a rate of 50% of the applicable location fees.
3. Level 1 Productions will be assessed a fee for use of the Waterfront Lot, either in part or whole, based on the nature of the production and level of electrical access required.
4. Production Deposit will be received prior to filming and will be assessed relative to the production level.
5. Damage drawdowns from Production Deposits will be made as they are incurred based on assessment of the cost of repairs.
6. The Cancellation of Production Booking Fee is assessed at a rate of 50% of the applicable location fees.
7. Fire Department fees for staff, equipment and vehicles to ensure safety on set will be determined by the Fire Chief based on requirements for the production and hourly rate for equipment and staff.
8. RCMP fees for staff, equipment, and vehicles to ensure safety on set will be determined by the Officer in Charge of the RCMP Detachment based on requirements for the production and hourly rate for equipment and staff.
9. Municipal staff time to support the production (e.g., Film Coordinator) will be charged at a rate representative of two times (x2) the hourly rate as per the collective agreement.
10. Fines for infringements of City bylaws by film productions will be assessed at the rates stated in the applicable bylaws.



## **SCHEDULE “E”**

### **Electric Vehicle Parking Fees**

Note:

1. Electric vehicle parking fees are exclusive of taxes, which will be added if applicable.

<b>Albion Community Centre Parking</b>	<b>Fee</b>
Level 2 EV Charging Session Parking Rate	\$0.029/min (\$1.75/hr.)

## **SCHEDULE “F”**

### **Police Services Fees**

Note:

1. All fees are exclusive of tax, which will be added, if applicable.

<b>Police Services</b>	<b>Proposed Fee</b>
Audio and Video CD & DVD per set	\$55.00
Collision Analyst Report (Tech Reconstruction of MVA)	\$650.00
Court Ordered File - Processing Fee (Processing & Disclosure) per USB	\$60.00
File Synopsis Letter (Confirmation of Loss or Theft of Property)	\$65.00
Fingerprints - Indigenous Name Change	\$0.00
Fingerprints	\$65.00
Insurance Claim Report	\$65.00
Mechanical Report (Certified Mechanic's Vehicle Inspection Report)	\$225.00
MV6020/Synopsis Accident Report	\$60.00
Pardon Application (Local Police Records Check)	\$65.00
Passport Letter (Confirmation of Lost Passport or Identity)	\$60.00
Photocopies per page	\$1.00
Photographs CD & DVD per set	\$55.00
Photographs per photo	\$6.00
Physical Evidence Report	\$680.00
Police Certificate (Visas, Foreign Work Permit, Travel, etc.)	\$65.00
Police Information Check - Volunteers & Students	\$0.00
Police Information Check	\$65.00
Scale Plan Diagram (Motor Vehicle Accident Scene to Scale)	\$150.00
Scene Measurement Log per page (Measurements take at Motor Vehicle Accident Scene)	\$60.00
Transcription Fee per hour (Transcribing statements)	\$55.00



## **City of Maple Ridge**

### **Maple Ridge Fees & Charges Bylaw No. 7575-2019**

Consolidated for Reference: January 16, 2024



THE FOLLOWING DOCUMENT HAS BEEN PRODUCED FOR CONVENIENCE ONLY and is a consolidation of the below bylaws. Copies of the original bylaw and individual amending bylaws can be obtained by contacting the Legislative Services Department.

<b>Bylaw Title</b>	<b>Adoption Date</b>	<b>Summary</b>
Maple Ridge Fees & Charges Bylaw No. 7575-2019	July 14, 2020	Original Bylaw, added Schedule A (Parks and Recreation Fees)
Maple Ridge Fees & Charges Amending Bylaw No. 7755-2021	July 27, 2021	Amended Bylaw language, updated Schedule A and added Schedules B and C
Maple Ridge Fees & Charges Amending Bylaw No. 7850-2022	May 24, 2022	Amended Bylaw language and format, added Schedule D and E
Maple Ridge Fees & Charges Amending Bylaw No. 7966-2023	November 14, 2023	Updated Schedule B
Maple Ridge Fees & Charges Amending Bylaw No. 7978-2023	January 16, 2024	Added Schedule F

## Table of Contents

<b>Fees</b>	<b>Description</b>
<b>Schedule A</b>	Engineering Fees
<b>Schedule B</b>	Recreation Services Fees
<b>Schedule C</b>	Planning Fees
<b>Schedule D</b>	General Administration Fees – Copying Costs, Record Requests
<b>Schedule E</b>	Electric Vehicle Parking Fees
<b>Schedule F</b>	Filming Fees

## **City of Maple Ridge**

### **Fees & Charges Bylaw No. 7575-2019**

A Bylaw to establish fees and charges for City services and information

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**WHEREAS** the Council of the City of Maple Ridge deems it expedient to provide for fees and charges for services and information;

**AND WHEREAS** Section 194 of the *Community Charter* authorizes municipalities by bylaw to impose fees in respect of a service of the municipality or provision of information;

**AND WHEREAS** Council deems it necessary and desirable to exercise the authority provided by the *Community Charter* to cover costs of providing services and information where appropriate;

**NOW THEREFORE**, the Council of the City of Maple Ridge enacts as follows:

#### **Part 1 Citation**

1.1 This bylaw may be cited as Maple Ridge Fees & Charges Bylaw No. 7575-2019.

#### **Part 2 Severability**

2.1 If a portion of this bylaw is held invalid by a Court of competent jurisdiction, then the invalid portion must be severed and the remainder of this bylaw is deemed to have been adopted without the severed section, subsequent paragraph, subparagraph, clause or phrase.

#### **Part 3 Previous Bylaw Repeal**

3.1 Bylaw No. 4117-1988 as amended is hereby repealed in its entirety.

3.2 Bylaw No. 7137-2017 as amended is hereby repealed in its entirety.

#### **Part 4 Fees and Charges**

4.1 The City hereby imposes the fees for the provision of services and information as specified in Schedule “A” which is attached hereto and forms part of this bylaw.



- 4.2 Fees or charges imposed under this Bylaw for the provision of services or information shall apply instead of any fees or charges imposed under other bylaws for the same services or information in the event of any conflict between this Bylaw and any other bylaw. A reference to a more specific matter in this Bylaw supersedes a reference to a more general matter.

## **Part 5 Schedules**

Schedule “A” – Engineering Fees

Schedule “B” – Recreation Services Fees

Schedule “C” – Planning Fees

Schedule “D” – General Administration Fees

Schedule “E” – Electric Vehicle Parking Fees

Schedule “F” – Filming Fees

**READ** for the first time the 23<sup>rd</sup> day of June, 2020.

**READ** for the second time the 23<sup>rd</sup> day of June, 2020.

**READ** for the third time the day of 23<sup>rd</sup> day of June, 2020.

**ADOPTED**, the 14<sup>th</sup> day of July, 2020.

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PRESIDING MEMBER

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CORPORATE OFFICER

### Schedule “A” – Engineering Fees

Note:

1. All fees are exclusive of tax, which will be added, if applicable.
2. Rate increases come into effect April 1 of each year.

<b>Subdivision &amp; Development Servicing</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>
Building Permit Engineering Service Review	\$ 50	\$ 51	\$ 52	\$ 53
Yards Inspection - Single Family	\$ 50	\$ 51	\$ 52	\$ 53
Yards Inspection - MF/Commercial/Industrial	\$100	\$ 102	\$ 104	\$ 106
Yards Re-inspection, If Work Incomplete	\$250	\$ 255	\$ 260	\$ 265
Engineering Inspection & Review	4% of Works and Services Costs			

## Schedule “B” – Recreation Services Fees

Note:

1. New rates take effect January of the noted year.
2. If no new rate is identified, the most recently stated rate will continue to be applied.
3. All fees are exclusive of tax, which will be added, if applicable.
4. Non-prime time rates will be a 50% reduction of the regular rates and applied to all hours outside of the prime-time windows listed below:

Days	Hours
Monday – Friday	5:00 pm – 9:00 pm
Saturday/Sunday (Year-Round)	8:00 am – 9:00 pm

Admissions	
Child	2024
Single	\$3.11
10 Pass	\$26.62
20 Pass	\$49.69
1 Month	\$27.96
3 Months	\$74.54
1 Year	\$247.95
Youth/Student/Senior	2024
Single	\$4.18
10 Pass	\$35.80
20 Pass	\$66.83
1 Month	\$37.60
3 Months	\$100.25
1 Year	\$333.45
Adult	2024
Single	\$6.11
10 Pass	\$52.33
20 Pass	\$97.68
1 Month	\$54.95
3 Months	\$146.51
1 Year	\$487.35



**Schedule “B” – Recreation Services Fees (continued)**

<b>Admissions (continued)</b>	
<b>Family</b>	<b>2024</b>
Single	\$11.18
10 Pass	\$100.43
20 Pass	\$178.90
1 Month	\$105.45
3 Months	\$281.20
1 Year	\$935.37
<b>Drop-In Skating - Single</b>	<b>2024</b>
Child	\$3.05
Youth/Student/Senior	\$4.11
Adult/Parent & Tot	\$5.55
Family	\$9.77
<b>Outdoor Pool Admission - Single</b>	<b>2024</b>
Child	\$1.90
Youth/Student/Senior	\$1.90
Adult/Parent & Tot	\$2.86

**Schedule “B” – Recreation Services Fees (continued)**

<b>Arenas</b>			
All rates are listed hourly			
<b>Ice – Prime Time</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$118.18		
Adult Non-Profit	\$177.26		
Private	\$295.42	\$301.33	\$307.36
Commercial	\$326.64	\$333.18	\$339.84
Non-Resident	\$315.99	\$322.31	\$328.75
<b>Ice – Non-Prime Time</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$86.90		
Adult Non-Profit	\$132.95		
Private	\$221.56	\$226.00	\$230.52
Commercial	\$252.19	\$264.80	\$278.04
Non-Resident	\$236.99	\$241.73	\$246.56
<b>Dry Floor – Prime Time</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$44.91		
Adult Non-Profit	\$67.37		
Private	\$112.28	\$114.53	\$116.82
Commercial	\$149.52	\$152.51	\$155.56
Non-Resident	\$134.49	\$137.18	\$139.92
<b>Ice – Junior B</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Prime Time/Games	\$123.80	\$126.27	\$128.80
Non-Prime/Practices	\$92.86	\$94.72	\$96.61

**Schedule “B” – Recreation Services Fees (continued)**

<b>Fairgrounds</b> Stage included at no-charge when booking grounds All rates are hourly			
<b>Barns – Beef, Exhibition, Commercial</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$8.49		
Adult Non-Profit	\$12.73		
Private	\$21.23	\$21.65	\$22.08
Commercial	\$23.35	\$23.81	\$24.29
Non-Resident	\$25.47	\$25.98	\$26.50
<b>Grounds &amp; Showrings</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$7.43		
Adult Non-Profit	\$11.14		
Private	\$18.57	\$18.95	\$19.32
Commercial	\$20.43	\$20.84	\$21.26
Non-Resident	\$22.29	\$22.73	\$23.19



**Schedule “B” – Recreation Services Fees (continued)**

<b>Fields</b>			
<i><b>Youth/Senior Non-Profit</b></i>			
<b>Artificial Turf</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Commercial	\$24.33	\$25.54	\$26.82
Special Event	\$6.09	\$6.39	\$6.71
Regular Use			
<b>Class A</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Commercial	\$23.63	\$24.11	\$24.59
Special Event	\$5.61	\$5.72	\$5.84
Regular Use			
<b>Class B</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Commercial	\$17.61	\$17.96	\$18.32
Special Event	\$4.41	\$4.49	\$4.58
Regular Use			
<b>Class C</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Commercial	\$5.88	\$5.99	\$6.11
Special Event	\$1.47	\$1.50	\$1.53
Regular Use			

**Schedule “B” – Recreation Services Fees (continued)**

<b>Fields</b>			
<b><i>Adult Non-Profit</i></b>			
<b>Artificial Turf</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Commercial	\$24.33	\$25.55	\$26.82
Special Event	\$18.82	\$19.76	\$20.75
Regular Use	\$14.01		
<b>Class A</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Commercial	\$23.63	\$24.11	\$24.59
Special Event	\$17.41	\$17.76	\$18.11
Regular Use	\$12.95		
<b>Class B</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Commercial	\$17.61	\$17.96	\$18.32
Special Event	\$12.25	\$12.50	\$12.75
Regular Use	\$7.12		
<b>Class C</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Commercial	\$5.88	\$5.99	\$6.11
Special Event	\$2.95	\$3.01	\$3.07
<b><i>Local, Private or Political</i></b>			
	<b>2024</b>	<b>2025</b>	<b>2026</b>
<b>Artificial Turf</b>	\$24.33	\$25.54	\$26.82
<b>Class A</b>	\$23.63	\$24.11	\$24.59
<b>Class B</b>	\$17.61	\$17.96	\$18.32
<b>Class C</b>	\$5.88	\$5.99	\$6.11
<b><i>Commercial</i></b>			
	<b>2024</b>	<b>2025</b>	<b>2026</b>
<b>Artificial Turf</b>	\$26.76	\$28.10	\$29.51
<b>Class A</b>	\$26.76	\$28.10	\$29.51
<b>Class B</b>	\$19.93	\$20.93	\$21.97
<b>Class C</b>	\$6.66	\$6.99	\$7.34
<b><i>Non-Resident</i></b>			
	<b>2024</b>	<b>2025</b>	<b>2026</b>
<b>Artificial Turf</b>	\$30.59	\$32.12	\$33.72
<b>Class A</b>	\$29.71	\$30.31	\$30.91
<b>Class B</b>	\$25.15	\$25.66	\$26.17
<b>Class C</b>	\$8.74	\$8.92	\$9.09

**Schedule “B” – Recreation Services Fees (continued)**

<b>Halls</b> Non-Prime time rates will be a 50% reduction of the regular rates. Facility rates are hourly Kitchen rates are daily			
<b>Great Hall – Albion Community Centre</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$50.34		
Adult Non-Profit	\$75.50		
Private	\$125.84	\$128.35	\$130.92
Commercial	\$256.53	\$269.35	\$282.82
Non-Resident	\$242.08	\$246.92	\$251.86
<b>Hammond Hall</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$26.49		
Adult Non-Profit	\$39.74		
Private	\$66.23	\$67.55	\$68.90
Commercial	\$91.70	\$96.28	\$101.10
Non-Resident	\$81.27	\$82.90	\$84.56
<b>Whonnock Hall</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$86.54		
Adult Non-Profit	\$129.82		
Private	\$216.35	\$220.68	\$225.09
Commercial	\$256.53	\$269.35	\$282.82
Non-Resident	\$242.08	\$246.92	\$251.86
<b>Kitchen</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Fairgrounds Hammond Hall Whonnock Lake Centre Albion Community Centre			
Youth/Senior Non-Profit	\$35.54		
Adult Non-Profit	\$53.33		
Private	\$88.86	\$90.64	\$92.45
Commercial	\$125.16	\$127.67	\$130.22
Non-Resident	\$125.16	\$127.67	\$130.22



**Schedule “B” – Recreation Services Fees (continued)**

<b>Pools</b> All rates are hourly Competition & Teach Pool rates are per lane Hammond Pool rates are for the whole pool			
<b>Competition Pool/Teach Pool</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Swim Club – Competition Pool	\$6.08		
Swim Club – Teach Pool	\$5.17		
Youth/Senior Non-Profit	\$10.58		
Adult Non-Profit	\$15.87		
Private	\$26.46	\$26.99	\$27.53
Commercial	\$34.60	\$36.33	\$38.14
Non-Resident	\$31.35	\$31.98	\$32.62
<b>Hammond Pool (Whole Pool)</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Swim Club	\$20.69		
Youth/Senior Non-Profit	\$27.84		
Adult Non-Profit	\$41.75		
Private	\$69.58	\$70.98	\$72.40
Commercial	\$77.77	\$81.66	\$85.75
Non-Resident	\$70.89	\$72.31	\$73.75

**Schedule “B” – Recreation Services Fees (continued)**

<b>Rooms</b> Non-Prime time rates will be a 50% reduction of the regular rates. Rooms are listed hourly			
<b>Multi-Purpose Room (capacity 1-15)</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$4.74		
Adult Non-Profit	\$7.10		
Private	\$11.83	\$12.42	\$13.04
Commercial	\$14.53	\$15.26	\$16.02
Non-Resident	\$14.53	\$15.26	\$16.02
<b>Multi-Purpose Room (capacity 15-30)</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$9.46		
Adult Non-Profit	\$14.20		
Private	\$23.66	\$24.84	\$26.09
Commercial	\$29.05	\$30.50	\$32.03
Non-Resident	\$29.05	\$30.50	\$32.03
<b>Multi-Purpose Room (capacity 40-60)</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$12.42		
Adult Non-Profit	\$18.64		
Private	\$31.06	\$32.61	\$34.24
Commercial	\$37.35	\$39.22	\$41.18
Non-Resident	\$34.25	\$35.96	\$37.76
<b>Multi-Purpose Room (capacity 60-80)</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$14.64		
Adult Non-Profit	\$21.96		
Private	\$36.59	\$38.42	\$40.34
Commercial	\$50.78	\$53.32	\$55.98
Non-Resident	\$47.63	\$50.01	\$52.51

**Schedule “B” – Recreation Services Fees (continued)**

<b>Rooms (continued)</b> Non-Prime time rates will be a 50% reduction of the regular rates. Rooms are listed hourly			
<b>Gymnasium</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Youth/Senior Non-Profit	\$49.35		
Adult Non-Profit	\$74.02		
Private	\$123.37	\$123.37	\$123.37
Commercial	\$154.31	\$154.31	\$154.31
Non-Resident	\$132.64	\$132.64	\$132.64
<b>Sports Field User Annual Contribution</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Fall/Winter Clubs – per registration	\$30.00	\$30.00	\$30.00
Spring/Summer Clubs – per registration	\$15.00	\$15.00	\$15.00
<b>Miscellaneous</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>
Park Shelter (day rate)	\$73.50	\$77.18	\$81.03
Event Trailer (day rate)	\$147.31	\$147.31	\$147.31
Tennis/Sport Court (commercial)	\$15.75	\$16.54	\$17.36



## **Schedule “C” – Planning Fees**

**Note:**

1. All fees are exclusive of tax, which will be added, if applicable.
2. Rate increases come into effect April 1 of each year.
3. Notwithstanding anything in the bylaw contained, where an applicant has paid to the Corporation of the City of Maple Ridge a Subdivision or Rezoning fee contained within this schedule and thereafter:

- a. the applicant withdraws the application; or
- b. the Council of the City determines not to proceed with the application or with any proposed bylaw resulting therefrom;

then in either case above (hereinafter referred to as a "refund event") the applicant shall be entitled to a refund of the fee actually paid determined as follows:

- c. where a refund even occurs prior to fourteen (14) days after submission 90% of fees paid shall be refunded;
  - d. where a refund event occurs after fourteen (14) days has lapsed since the submission date and prior to statutory notice pursuant to the Local Government Act having been given the amount refunded shall be no greater than the Public Hearing fee;
  - e. where a refund event occurs after an application has been given statutory notice pursuant to the Local Government Act, advertised to proceed to Public Hearing, there shall be no refund of fees paid;
4. There shall be no refund event permitted for a development application that does not require public hearing (subdivision, development variance permit, development permit, temporary commercial or industrial permits, soil permits and tree cutting permits).

**Schedule “C” – Planning Fees (continued)**

<b>Subdivision</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>
Subdivision Base:	\$2,500	\$2,550	\$2,601	\$2,653
Per Lot	\$100	\$102	\$104	\$106
Subdivision Review (Lot-line adjustment & consolidation)	\$1,270	\$1,295	\$1,321	\$1,348
Phased Strata Plan Approval	\$1,000	\$1,020	\$1,040	\$1,061
Each Phase Review	\$500	\$510	\$520	\$531
Phased Strata Plan Amendments	\$500	\$510	\$520	\$531
Form P Approval	\$500	\$510	\$520	\$531
Strata Title Conversions	\$2,400	\$2,448	\$2,497	\$2,547
Phased Strata Plan Amendments	\$500	\$510	\$520	\$531
Subdivision PRL Extension Fee	50% of Subdivision Application Fee			
Subdivision Plan Stale Dated Approval	\$250	\$255	\$260	\$265
<b>Rezoning</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>
Single Family - plus:	\$3,000	\$3,060	\$3,121	\$3,184
Per Lot	\$100	\$102	\$104	\$106
Multi-Family - plus: Per Unit	\$3,800	\$3,876	\$3,954	\$4,033
	\$80	\$82	\$83	\$85
Commercial - plus:	\$3,500	\$3,570	\$3,641	\$3,714
Per 100 sq. m.	\$100	\$102	\$104	\$106
Industrial - plus:	\$3,500	\$3,570	\$3,641	\$3,714
Per 100 sq. m.	\$50	\$51	\$52	\$53
Comprehensive Development - plus:	\$5,000	\$5,100	\$5,202	\$5,306
Residential - Per Unit:	\$100	\$102	\$104	\$106
Commercial - Per 100 sq. m.:	\$100	\$102	\$104	\$106
Industrial - Per 100 sq. m.:	\$100	\$102	\$104	\$106
Rezoning Extension at 3rd Reading	50% of Rezoning Application Fee			
Zoning Bylaw Text Amendment	\$3,500	\$3,570	\$3,641	\$3,714
Official Community Plan Amendment	\$4,000	\$4,080	\$4,162	\$4,245
Official Community Plan Amendment Extension at 3rd Reading	50% of Rezoning Application Fee			
Official Community Plan Text Amendment	\$2,500	\$2,550	\$2,601	\$2,653

**Schedule “C” – Planning Fees (continued)**

<b>Development Permits</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>
Multi-family Residential/Intensive Infill - plus:	\$3,000	\$3,060	\$3,121	\$3,184
Per unit	\$100	\$102	\$104	\$ 106
Commercial - plus:	\$3,000	\$3,060	\$3,121	\$3,184
Per 100 sq. m.	\$100	\$102	\$104	\$106
Industrial - plus:	\$3,000	\$3,060	\$3,121	\$3,184
Per 100 sq. m.	\$100	\$102	\$104	\$106
Environmental Development Permit	\$3,000	\$3,060	\$3,121	\$3,184
Single-family Environmental Development Permit	\$750	\$765	\$780	\$796
Development Variance Permit	\$3,000	\$3,060	\$3,121	\$3,184
Single-Family Development Variance Permit	\$1,500	\$1,530	\$1,561	\$1,592
Development Permit Minor Amendment	\$500	\$510	\$520	\$531
Heritage Revitalization Agreement	\$500	\$510	\$520	\$531
Heritage Conservation Restrictive Covenant	\$500	\$510	\$520	\$531
Heritage Alteration Permit	\$500	\$510	\$520	\$531
<b>Other Fees</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>
Temporary Use Permit	\$2,500	\$2,550	\$2,601	\$2,653
Temporary Use Permit Extension	\$2,500	\$2,550	\$2,601	\$2,653
Development Information Meeting	\$500	\$510	\$520	\$531
Board of Variance	\$500	\$510	\$520	\$531
Illegal Work Penalty	\$250	\$255	\$260	\$265
Legal Document Registration, Discharge or Modification	\$200	\$204	\$208	\$212
With Council Approval	\$400	\$408	\$416	\$424
Legal Document Review	Reimbursement of City Costs			
Advisory Design Panel Submission	\$150	\$153	\$156	\$159
Lidar Investigation Map	\$50	\$51	\$52	\$53
Landscape Re-Inspection Request	\$150	\$153	\$156	\$159



**Schedule “C” – Planning Fees (continued)**

<b>Landscape Design Review and Inspection</b>	<b>2% of Landscape Value</b>			
Minimum	\$300	\$306	\$312	\$318
Maximum	\$3,000	\$3,060	\$ 3,121	\$3,184
Letter of Inquiry	\$250	\$255	\$260	\$265
Public Hearing	\$1,200	\$1,224	\$1,248	\$1,273
Pre-Application Meeting	\$350	\$357	\$364	\$371
With Engineering Participation	\$500	\$510	\$520	\$531
Telecommunication Tower/Antennae Report	\$3,000	\$3,060	\$3,121	\$3,184
<b>Tree Cutting Permit (see Bylaw 7133-2015)</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>
Trees in Urban Area and Urban Reserve and Rural parcels less than 0.5 ha	\$75 for first tree + \$25 each additional tree			
Trees in the Rural Area on parcels greater than 0.5 ha	\$75 for 11th tree + \$25 each additional tree			
Development & Large Scale Clearing	\$300 base fee + \$25 per tree			
Tree Replacement	\$600			
Parcels With Active Farm Use	\$0			

### Schedule “D” – General Administration Fees

<b>Copying Costs</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>
Standard copies – 8.5 x 11	\$0.50	\$0.51	\$0.52	\$0.53
<b>Property Information Request/Tax Certificate</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>
From property owner	No charge	No charge	No charge	No charge
From non-owner	\$40.00	\$40.80	\$41.62	\$42.45
Requests through BC Online (BC Online customers will have BC Online administration fee added to this fee)	\$40.00	\$40.80	\$41.62	\$42.45
<b>Fire Department Record Requests</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>
Fire Incident/Investigation Reports (including color photographs)	\$200.00	\$204.00	\$208.08	\$212.24
MVA/Medical Incident Report	\$100.00	\$102.00	\$104.04	\$106.12

**Schedule “E” – Electric Vehicle Parking Fees**

<b>Albion Community Centre</b>	<b>2022</b>
Level 2 EV Charging Session Parking Rate	\$0.025/min (\$1.50/hr)



## Schedule “F” – Filming Fees

Note:

1. New rates take effect January of the noted year.
2. If no new rate is identified, the most recently stated rate will continue to be applied.
3. Fees are exclusive of tax, which will be added, if applicable.

Filming Fees		2024
Film Permit Application - Per Application, plus:		\$250
Each Additional Location		\$75
Student/Non-Profit Film Application		\$50
Administration Fee		10%
Parking Lot - Per Day		\$500
Waterfront Parking Lot With Electrical Access - Per Day, Per Section		\$750
Waterfront Parking Lot - Per Day, Per Section		\$600
Special Effects Permit		\$200
Neighbourhood Park - Level 1 Production		\$250
Neighbourhood Park - Level 2 Production		\$500
Neighbourhood Park - Level 3 Production		\$600
Destination Park - Level 1 Production		\$500
Destination Park - Level 2 Production		\$1,000
Destination Park - Level 3 Production		\$1,500
Municipal Facility		See Schedule B - PRC Fees
Preparation and Production Wrap		50% of Location Fee
Noise Exemption Per Day - Received 5 Days Prior to Filming		\$50
Noise Exemption Per Day - Received Less Than 5 Days Prior to Filming		\$100
Security Deposit/Damage Deposit/Fees and Charges Deposit		\$1,000-\$30,000
Damage Deductions		As Incurred
Cancellation Fee		50% of Application & Location Fee
Fire Department equipment, staff and vehicles		Determined by Fire Chief
RCMP equipment, staff and vehicles		Determined by RCMP
Municipal staff		Two times (2x) hourly position rate in the Collective Agreement
Production Levels		
Level 1	Independent Level 1, Still Photography, Student Filming	
Level 2	Movie of the Week, Independents Level 2, TV Series, Commercial	
Level 3	Feature Film, TV Series Level 2	